

The Maryland-National Capital Park and Planning Commission
 Prince George's County Planning Department
 Development Review Division
 301-952-3530



Note: Staff reports can be accessed at www.mncppc.org/pgco/planning/plan.htm.

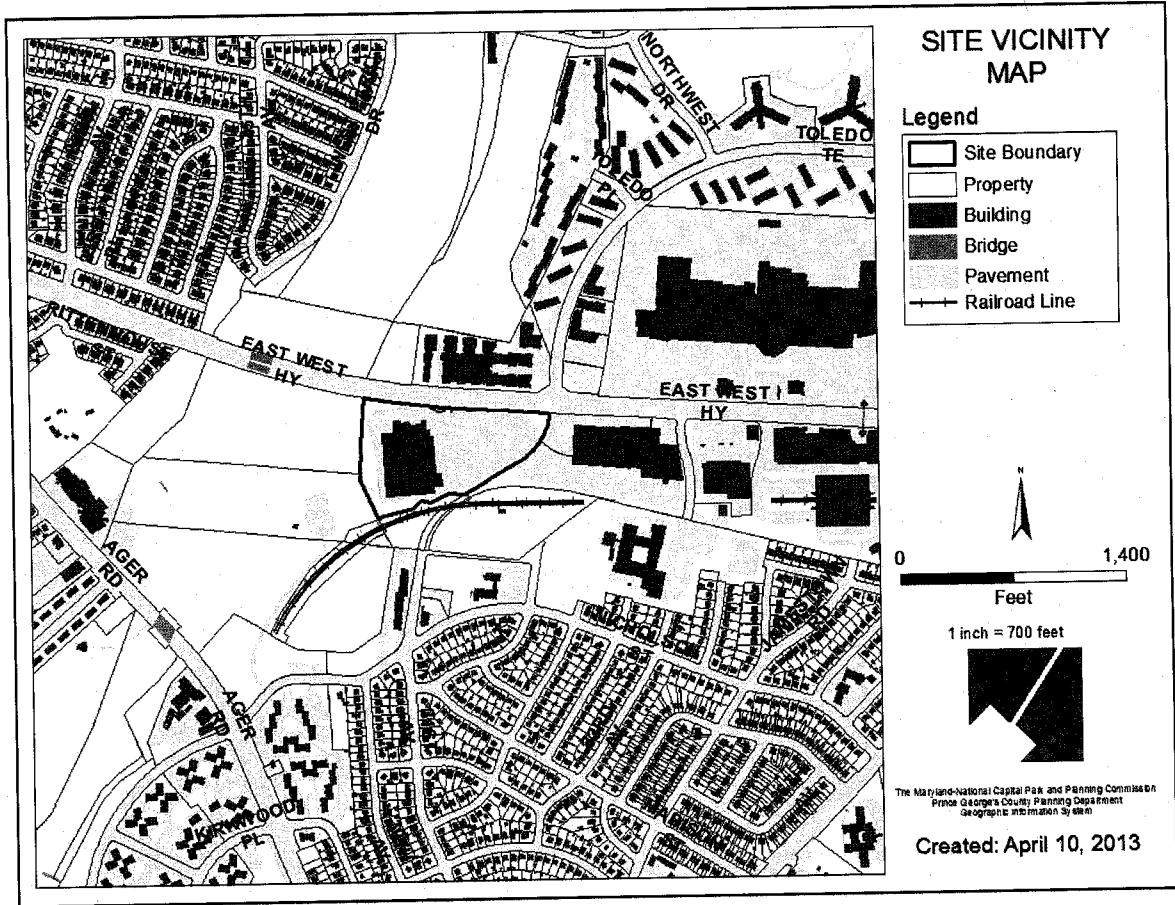
Detailed Site Plan

DSP-99006-01

| Application | General Data | |
|--|------------------------------|---------------|
| Project Name: McDonald's-Hyattsville Location: South side of East-West Highway (MD 410) at its intersection with Toledo Terrace. Applicant/Address: McDonald's Corp. 6903 Rockledge Drive Bethesda, MD 20817 | Planning Board Hearing Date: | 04/25/13 |
| | Staff Report Date: | 04/10/13 |
| | Date Accepted: | 02/14/13 |
| | Planning Board Action Limit: | 04/25/13 |
| | Plan Acreage: | 13.21 |
| | Zone: | C-S-C/T-D-O-Z |
| | Dwelling Units: | N/A |
| | Gross Floor Area: | 4,197 sq. ft. |
| | Planning Area: | 68 |
| | Tier: | Developed |
| | Council District: | 02 |
| | Election District | 17 |
| | Municipality: | Hyattsville |
| 200-Scale Base Map: | 208NE03 | |

| Purpose of Application | Notice Dates | |
|--|------------------------|----------|
| Amending the Table of Uses of the 1998 <i>Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone</i> , to permit a proposed Freestanding Eating and Drinking Establishment with Drive-Through. | Informational Mailing: | 03/30/11 |
| | Acceptance Mailing: | 02/12/13 |
| | Sign Posting Deadline: | 03/26/13 |

| | | | |
|----------------------|--------------------------|---|------------|
| Staff Recommendation | | Staff Reviewer: Jill Kosack Phone Number: 301-952-4689 Email: Jill.Kosack@ppd.mncppc.org | |
| APPROVAL | APPROVAL WITH CONDITIONS | DISAPPROVAL | DISCUSSION |
| | X | | |



THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION

PRINCE GEORGE'S COUNTY PLANNING BOARD

STAFF REPORT

SUBJECT: Detailed Site Plan DSP-99006-01
Type II Tree Conservation Plan TCPII-046-99-01
McDonald's-Hyattsville

The Urban Design staff has reviewed the detailed site plan for the subject property and presents the following evaluation and findings leading to a recommendation of APPROVAL with conditions as described in the Recommendation Section of this report.

EVALUATION

The detailed site plan (DSP) was reviewed and evaluated for conformance with the following criteria:

- a. The requirements of the Zoning Ordinance in the Commercial Shopping Center (C-S-C) and Transit District Overlay (T-D-O) Zones.
- b. The requirements of the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone*.
- c. The requirements of Preliminary Plan of Subdivision 4-99004.
- d. The requirements of Detailed Site Plan SP-99006.
- e. The requirements of the 2010 *Prince George's County Landscape Manual*.
- f. The requirements of the 1993 Prince George's County Woodland Conservation and Tree Preservation Ordinance.
- g. The requirements of the Prince George's County Tree Canopy Coverage Ordinance.
- h. Referral comments.

FINDINGS

Based upon the analysis of the subject application, the Urban Design staff recommends the following findings:

1. **Request:** The subject application requests amendment of the Table of Uses of the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP), to permit a 4,197-square-foot freestanding eating and drinking establishment with drive-through, specifically a McDonald's, in the Commercial Shopping Center (C-S-C) and Transit District Overlay (T-D-O) Zones.

2. **Development Data Summary:**

| Zone Use(s) | EXISTING | PROPOSED |
|-----------------------------|--|---|
| | C-S-C/T-D-O-Z Building Supply Store | C-S-C/T-D-O-Z Building Supply Store/Fast-food restaurant |
| Acreage | 13.21 | 13.21 |
| Building Square Footage/GFA | 134,284 | 138,481 (4,197 new) |

Parking

| | PERMITTED | PROPOSED |
|---|-----------|----------|
| Home Depot – 134,284 square feet (Preferred Ratio of <4.35 spaces/100 s.f.)* | 584 | 424 |
| McDonald's – 4,197 square feet (Preferred Ratio of <4.35 spaces/100 s.f.) | 18 | 64** |
| Total Parking | 602 | 488 |

*Note: Per the TDDP, Mandatory Development Requirement P7.

**Note: The applicant is providing 64 parking spaces within the lease area, for an excess of 46 parking spaces. However, given that the total number of on-site parking spaces will be reduced with the subject application, the site is not subject to payment of an impact fee under either the preferred or the premium parking caps, as defined in the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP).

3. **Location:** The subject site is located on the south side of East-West Highway (MD 410) at its intersection with Toledo Terrace, in Planning Area 68, Council District 2, in the Developed Tier.
4. **Surrounding Uses:** The site is bounded to the north by East-West Highway (MD 410) with the Post Park multifamily and retail development beyond; to the east and south by the Kiplinger Property, currently in the C-S-C and T-D-O Zone, but the subject of Conceptual Site Plan CSP-11002, which was recently approved by the Planning Board on February 28, 2013 (PGCPB Resolution No. 13-20), to rezone the property to the M-X-T Zone and propose a mixed-use, residential and commercial development; to the south by property owned by the Washington Metropolitan Area Transit Authority (WMATA) and containing above ground Metro rail tracks; and to the west by The Maryland-National Capital Park and Planning Commission (M-NCPPC) Heurich Community Park in the R-O-S Zone.

5. **Previous Approvals:** The 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone (T-D-O-Z)* rezoned the subject property from the M-X-T Zone to the C-S-C Zone. Subsequently, Preliminary Plan of Subdivision, 4-99004, for one parcel, was approved by the Planning Board on May 6, 1999 (PGCPB Resolution No. 99-78). Detailed Site Plan DSP-99006 was approved by the Planning Board for the existing building supply store, Home Depot, on-site on September 9, 1999 (PGCPB Resolution No. 99-146).
6. **Design Features:** Parcel A is developed with an existing 117,975-square-foot, one-story, 25-foot-high prototypical Home Depot building supply store, with an attached 16,309-square-foot, one-story, 24-foot-high garden center developed pursuant to the original DSP-99006 approval. The existing building sits at the far western end of the site, facing east, and the associated 507-space parking lot spreads out in front of it. Access to the site is from one private drive that intersects East-West Highway (MD 410) at the south side of its signaled intersection with Toledo Terrace at the far northeastern corner of the site. The drive then curves to the southwest, following the southern property line, with multiple openings to the parking aisles. The streetscape along East-West Highway (MD 410) is fully improved per the TDDP and the subject application does not propose to remove or change these improvements at all, except for a minor revision to the design of the existing brick screen wall.

The proposed 4,197-square-foot, one-story, approximately 23-foot-high, freestanding McDonald's fast food restaurant will be located in the far northeastern corner of the existing on-site parking lot, closest to the site's access drive intersection with East-West Highway (MD 410). The building itself will sit along the northern edge of the property, within 23 feet of the right-of-way of East-West Highway, with a proposed drive-through located immediately adjacent to the south side of the building. The existing parking lot will then be reconfigured between the drive-through and the access drive to the south, including a new entrance onto the access drive. A trash corral and a loading space will be located at the western end of the building. During plan review, staff noted that vehicles queued for the drive-through could conflict with vehicular circulation into and out of the parking area adjacent to the drive-through. The applicant agreed with staff's analysis and informally submitted a revised plan showing an improved circulation plan. If the use with the drive-through is approved, the DSP will have to be revised prior to certification to address this queuing issue. A sidewalk connects the sidewalk within the East-West Highway (MD 410) right-of-way to the front of the building. There is an existing meandering brick screen wall along the northern edge of the property which will be reconfigured into a straight, three-foot-high portion between the proposed building and the right-of-way.

The proposed building itself is specified as a "2011 Series 38101 R5 plus F5" and has a contemporary appearance with some architectural detailing. The main entrance is located along the eastern façade, facing the access drive, with another public entrance along the northern façade, facing East-West Highway (MD 410). Additionally, multiple painted, metal service doors are located along the northern and western facades. The one-story, flat-roof building, with a maximum building height of 23 feet four inches, is finished with a combination of red and dark brown face brick, cultured stone, and aluminum doors and coping. The aluminum coping and cultured stone have been used as accent elements to break up the dominance of brick on all of the elevations. A roof cap element at the front of the building, on the east façade, is provided in the yellow characteristic of McDonald's restaurants. A tower element has been used at the front entrance, in the southeastern corner of the building. The flat plane of the roof of the tower element is broken with the addition of a stone veneer parapet and a golden, metal, sloping curve as a roof-cap design element. The dark brown brick is proposed on the southern side of the building in the area of the drive-through pick-up windows, and in the form of wide horizontal

bands between the pick-up windows. Yellow and grey metal awnings are proposed along most sides of the building above doors, dining room windows and the drive-through pick-up windows.

A total of approximately 120 square feet of building-mounted signage has been proposed with this DSP. No new freestanding signage, except directional and menu boards, is proposed due to the building's close proximity to the right-of-way. The building-mounted signage includes the prototypical, internally-illuminated McDonald's golden arch corporate sign on all four facades and white name letters on all facades, except for the western side.

COMPLIANCE WITH EVALUATION CRITERIA

7. **Prince George's County Zoning Ordinance:** The subject application has been reviewed for compliance with the requirements of the C-S-C and T-D-O Zones of the Zoning Ordinance and found to be in conformance as discussed below.

- a. The subject application is for a proposed freestanding fast food restaurant and a change in the Table of Uses for the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP). This document supersedes the Table of Uses for permitted uses in the Zoning Ordinance for the C-S-C Zone.

The Zoning Ordinance in Section 27-548.09.01(b)(1), Amendment of the Approved Transit District Overlay Zone, states the following:

(b) Property Owner.

- (1) A property owner may ask the District Council, but not the Planning Board, to change the boundaries of the T-D-O Zone, a property's underlying zone, the list of the allowed uses, building height restrictions or parking standards in the Transit District Development Plan. The Planning Board may amend the parking provisions concerning the dimensions, layout, or the design of parking spaces or parking lots.**

Comment: The section above allows the owner of a property to request an amendment of the list of allowed uses. The owner's representative has filed a request to amend the table of uses to allow a freestanding eating and drinking establishment with drive-through service on the subject site only.

Additionally, Section 27-548.09(b)(5) states the following:

- (5) The District Council may approve, approve with conditions, or disapprove any amendment requested by a property owner under this Section. In approving an application and site plan, the District Council shall find that the proposed development conforms to the purposes and recommendations for the Transit Development District, as stated in the Transit Development District Plan, and meets applicable site plan requirements.**

Comment: The District Council has mandatory review of this project because the applicant is asking for an amendment to the use table that is only allowed if

granted by the District Council. In regard to the change to the use table, the Planning Board provides a recommendation to the District Council.

The purposes of the T-D-O-Z and the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP) contained in Section 27-548.03 of the Zoning Ordinance and on page 9 of the TDDP. The purposes are stated below and following each is the applicant's justification of how the subject proposal conforms to those purposes:

(1) To enhance the development opportunities in the vicinity of transit stations;

"McDonald's is a nationally recognized restaurant chain that will provide the residents of the area with an additional dining choice. The applicant believes that this location will provide the residents of the area as well as transit riders with an additional dining option within close proximity to the Metro."

(2) To promote the use of transit facilities;

"The placement of the McDonald's building along the East-West Highway frontage of the property will help further define the urban edge and create a pedestrian friendly environment that will in turn help create a stronger sense of identity for the Metro Station."

(3) To increase the return on investment in a transit system and improve local tax revenues;

"As noted above, this area of the site is currently being used for parking. The addition of another national chain restaurant to this site will increase the value of the property and in turn increase the tax revenues generated by the property."

(4) To create a process which coordinates public policy decisions, supports regional and local growth and development strategies, and creates conditions which make joint development possible;

"The applicant will be coordinating the development of the property with state and local agencies."

(5) To create a process which overcomes deficiencies in ordinary planning processes and removes obstacles not addressed in those processes;

"The TDOZ allows for flexibility in the development process through the use of amendments to the TDDP. In this case, the amendment to the Table of Uses for the property as part of the DSP process will allow further redevelopment of Subarea 9 in a manner that is more conducive to a changing market and to the proposed development of other subareas in the transit district."

(6) To minimize the costs of extending or expanding public services and facilities, by encouraging appropriate development in the vicinity of transit stations;

“Additional retail commercial development within close proximity to the Prince George’s Plaza Metro Station will encourage Metro ridership and in turn decrease the use of the surrounding road network. In addition, the proposed McDonald’s will be an additional dining choice in close proximity to Metro as well as the surrounding residential communities. The Applicant will provide connections to the existing pedestrian network and further enhance an environment that encourages pedestrian traffic through the transit district.”

- (7) To provide mechanisms to assist in financing public and private costs associated with development;**

“There will be no public costs associated with this development. All costs will be borne by the applicant.”

- (8) To provide for convenient and efficient pedestrian and vehicular access to Metro stations;**

“The site is located approximately 2,000 feet from the Prince George’s Plaza Metro Station. As noted earlier, the Applicant will provide connections to the existing pedestrian network and further enhance an environment that encourages pedestrian traffic through the transit district.”

- (9) To attract an appropriate mix of land uses;**

“The Applicant’s proposal will add one more dining choice to the area. As indicated above, this portion of Subarea 9 is occupied by parking spaces.”

- (10) To encourage uses which complement and enhance the character of the area;**

“The DSP and associated elevations of the building demonstrate that the proposed building will, through the use of architecture, compliment other properties in the area that have been developed in conformance with the guidelines set forth in the Transit District Development Plan.”

- (11) To insure that developments within the Transit District possess a desirable urban design relationship with one another, the Metro station, and adjoining areas; and**

“The DSP demonstrates that the proposed building complies with or exceeds the site design guidelines. Although the building located along East-West Highway does not meet the build-to-line requirements, it does conform to the amendment previously approved for the plaza. In addition, the applicant has oriented the building to create a greater street presence and help continue to enhance the urban edge along East-West Highway.”

- (12) To provide flexibility in the design and layout of buildings and structures, and to promote a coordinated and integrated development scheme.**

“Amendments to the TDDP were previously approved for the subject property as part of the DSP for the Home Depot. The applicant is not proposing any additional amendments. In fact, by placing the building along the East-West Highway frontage, the applicant’s proposal brings the site into further conformance with the goals and requirements of the TDDP.”

In addition to the general purposes of the T-D-O-Z contained in the Zoning Ordinance, the purpose of Subarea 9 is stated on page 114 of the TDDP, which states:

To provide for retail, service and office uses. A mixed-use development should be considered in the future for this property given its close proximity to the Metro Station.

“The applicant’s proposal will comply with the TDDP’s purpose of providing additional retail space in Subarea 9. With regard for the recommendation regarding a mixed use development, since the approval of the TDDP, Post Park was approved and constructed. This development reduced the amount of retail commercial development in the Transit District and increased the amount of residential, further creating a balanced mix of land uses in the Transit District.”

Staff Comment: Staff agrees with the applicant’s proposal to include a freestanding eating and drinking establishment within the existing Home Depot parking lot. This addition will help increase the mix of uses and density on the site, while minimizing the costs to public services. However, the proposed drive-through service cannot be found to promote the use of transit facilities or create a desirable urban relationship to enhance the character of the area. Therefore, staff recommends that the drive-through service be removed from the use and a condition requiring this has been included in the Recommendation Section of this report.

b. Section 27-548.08(c) lists the required findings for approving a DSP in the T-D-O Zone as follows:

(A) The Transit District Site Plan is in strict conformance with any mandatory requirements of the Transit District Development Plan;

Comment: The DSP is in strict conformance with the mandatory requirements of the 1998 *Approved Transit District Development Plan for the Prince George’s Plaza Transit District Overlay Zone* (TDDP) as discussed further in Finding 8 below.

(B) The Transit District Site Plan is consistent with, and reflects the guidelines and criteria for development contained in, the Transit District Development Plan;

Comment: The DSP is consistent with the guidelines and criteria for development contained in the TDDP as discussed further in Finding 8 below.

(C) The Transit District Site Plan meets all of the requirements of the Transit District Overlay Zone, and applicable regulations of the underlying zones;

Comment: The detailed site plan meets all the requirements of the T-D-O-Z and the applicable regulations of the underlying zone as elaborated further in this finding and Finding 8.

(D) The location, size, and design of buildings, signs, other structures, open spaces, landscaping, pedestrian and vehicular circulation systems, and parking and loading areas maximize safety and efficiency, and are adequate to meet the purposes of the Transit District Overlay Zone;

Comment: The proposed restaurant, with the elimination of the drive-through service, fits into the existing site layout in order to maximize safety and efficiency and is adequate to meet the purposes of the T-D-O-Z as described in Finding 7a above.

(E) Each structure and use, in the manner proposed, is compatible with other structures and uses in the Transit District, and with existing and proposed adjacent development.

Comment: The proposed structure and use, if the proposed drive-through service is removed, will be compatible with the surrounding and existing development by placing more building frontage along the East-West Highway (MD 410) right-of-way.

8. **The 1998 Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone (TDDP):** The subject property is defined as Subarea 9 in the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP). This makes it subject to all of the District-wide Mandatory Development Requirements and Site Design Guidelines, along with Subarea-specific Mandatory Development Requirements and Site Design Guidelines as listed on page 114 of the TDDP.

Section 27-548.08(c)(1) of the Zoning Ordinance requires that the Planning Board find the site plan is in strict conformance with any Mandatory Development Requirements and is consistent with the guidelines and criteria for development in the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP). The applicant submits that the DSP meets all of the applicable standards and, therefore, has not requested any modifications to these requirements.

Staff reviewed the submitted DSP and found it to be in conformance with the applicable Mandatory Development Requirements and consistent with the guidelines. The following requirements warrant discussion at this time:

S5 All primary and secondary pedestrian routes shall be constructed using special paving materials. (See Figure 7 for crosswalks.)

Comment: A condition has been included in the Recommendation Section of this report requiring the sidewalk connecting the restaurant to the streetscape along East-West Highway (MD 410) be revised to provide special paving.

G11 Primary entrances should be designed as one of the major architectural features so they are clearly identifiable and offer a sense of arrival.

Comment: Staff believes that the sense of arrival at the primary entrance, on the eastern façade, would be enhanced if the DSP was revised to include an expanded paved area, including specialty paving, tables and chairs for customer use and decorative plantings. A condition requiring such has been included in the Recommendation Section of this report.

S30 All new retail development shall provide four bike racks per 10,000 gross square feet of floor space with each rack holding a minimum of two bicycles.

Comment: The applicant submitted that sufficient bike parking was provided with the previous Home Depot DSP approval. However, bike parking is required for this proposed retail space and it should be located in a spot convenient to the McDonald's. These revisions have been included as a condition in the Recommendation Section of this report.

S31 At the time of Detailed Site Plan, the number of trash cans and locations shall be shown on the plan. Trash receptacles should be placed in strategic locations to prevent litter from accumulating in and around the proposed development.

Comment: This requirement has partially been met with this submission. The detailed site plan identifies an area on the west side of the proposed structure as a trash corral. However, there is no information to say how many dumpsters are located in this corral area. Additionally, individual trash receptacles should be strategically located to prevent litter accumulation. The DSP should be revised prior to certification to clarify these issues and a condition requiring such has been included in the Recommendation Section of this report.

9. **Conformance to Preliminary Plan of Subdivision 4-99004:** Preliminary Plan of Subdivision 4-99004 (PGCPB Resolution No. 99-78) for Home Depot was approved by the Planning Board on May 6, 1999. The resolution for the approved preliminary plan contains eleven conditions which warrant the following discussion:

2. **A Type II Tree Conservation Plan shall be approved in conjunction with the Detailed Site Plan.**

Comment: The Environmental Planning Section indicated that the site has a previously approved Type II Tree Conservation Plan, TCPII-046-99, with which the proposed development is in conformance because no change is proposed to the previously approved limit of disturbance (LOD).

3. **Development of this site shall be in conformance with the approved stormwater concept plan, Concept 998003250.**

Comment: The Department of Public Works and Transportation (DPW&T) indicated that the subject application is not consistent with the approved Stormwater Management Concept Plan, 2259-2011. Therefore, a revised concept approval is required prior to certification of the DSP and the approved and valid date should be noted on the plan.

5. **In addition to the typical review, the Detailed Site Plan shall examine:**
 - a. **The possible location of a site for a Medical Evacuation Unit.**
 - b. **The alignment of the trail connection.**
 - c. **Road improvements required by the State Highway Administration.**
 - d. **Stormwater management outfalls onto park property.**

Comment: The previous DSP approval found conformance with this condition and the subject application does not propose any improvements that affect any of the mentioned improvements.

6. **At the time of Detailed Site Plan, the applicant shall demonstrate conformance to the requirements and guidelines for the subject subarea as defined in the Transit District Development Plan (TDDP) for the Prince George's Plaza Transit District Overlay Zone, as well as general conformance to all other regulations in the same document. If the proposed parking in addition to already approved total parking exceeds that which is allowed by the TDDP, the applicant shall submit new studies as required by staff in order to determine any additional facilities needed to support the planned development.**

Comment: Conformance with the TDDP and TDO Zone are discussed in Findings 7 and 8 above. The subject application actually proposes to reduce the number of total parking spaces on the site.

10. **Conformance to Detailed Site Plan SP-99006:** The Planning Board approved Detailed Site Plan DSP-99006 on September 9, 1999 (PGCPB Resolution No. 99-146) subject to seven conditions, which warrant the following discussion:

1. **Prior to certification, the Detailed Site Plan shall be revised as follows:**
 - a. **The sidewalk connecting the store to the streetscape along East West Highway shall be revised to provide special paving.**

Comment: The subject application does not affect the sidewalk connecting the Home Depot to the streetscape; however, staff recommends that a similar condition be enforced for the sidewalk connecting the proposed McDonald's to the streetscape along East-West Highway (MD 410).

- i. **A note shall be provided stating that any stormdrain inlets associated with the development and all inlets on the subject subarea shall be stenciled with "Do Not Dump. Chesapeake Bay Drainage".**

Comment: The subject application proposes new stormdrain inlets; therefore, staff recommends that a similar condition be enforced for the subject application.

3. **Prior to issuance of any building permit, the following improvements shall be in place, fully bonded or permitted for construction:**

- a. **Reconstruct Kiplinger Access Drive as a 4-lane divided access roadway from its intersection with MD 410 to the proposed first site access driveway and as a 4-lane undivided roadway per Prince George's County DPW&T standards to the site's second access point.**
 - b. **Provision of 30 feet turning radii for the right-turning traffic to and from the Kiplinger Access Drive.**
4. **The location of the proposed hiker/biker trail on Park Property shall be staked in the field, and approved by Park Planning and Development staff prior to construction.**
5. **Prior to the issuance of a building permit, a Recreational Facilities Agreement shall be executed for the trail, and a performance bond posted for the construction of the trail, and to warrant the restoration, repair or improvements for the stormdrain system on park property.**

Comment: A building permit has already been issued for the existing Home Depot on the subject property; therefore, the three above conditions have been fulfilled as required.

11. **The 2010 Prince George's County Landscape Manual:** Per Section 27-450 of the Zoning Ordinance, landscaping, screening, and buffering of all development in commercial zones shall be provided pursuant to the provisions of the 2010 *Prince George's County Landscape Manual* (Landscape Manual). Additionally, the TDDP (page 30) requires that all properties within the transit district satisfy the requirements of the Landscape Manual. The requirements apply as follows:

- a. **Section 4.2, Requirements for Landscaped Strips along Streets**—Section 4.2 specifies that, for all nonresidential uses in any zone and for all parking lots, a landscape strip shall be provided on the property abutting all public and private streets. The previous approved DSP met this requirement and the proposed improvements do not require the removal of any of the plantings in this area. Additionally, the submitted DSP provides the appropriate schedule detailing how the requirements of this section will be met along the frontage of East-West Highway (MD 410) immediately adjacent to the McDonald's through the use of shade trees and a three to four-foot-high masonry wall.
- b. **Section 4.3, Parking Lot Requirements**—Section 4.3 specifies that proposed parking lots larger than 7,000 square feet provide planting islands throughout the parking lot to reduce the impervious area. The DSP proposes revisions to the existing parking compound on-site. It provides schedules for Parking Lot Perimeter Landscape Strips, Section 4.3(c)(1), which does not apply to the subject application. These schedules should be removed from the plan.

Regarding Section 4.3(c)(2), Parking Lot Interior Planting Requirements, the landscape plan provides the correct schedule for the two different compounds on-site. However, there are several inaccuracies in the schedules that should be corrected, including the schedule not matching the one in the Landscape Manual (see 7. and 10. b) and the number of trees required should be calculated based on the provided interior planting area. This means that Parking Compound 1 is short of the required number of shade trees

and additional trees should be added. Additionally, the schedule for Parking Compound 2 shows the requirements being met; however, no plan was provided showing this. These issues have been included in conditions of approval in the Recommendation Section of this report.

- c. **Section 4.4, Screening Requirements**—Section 4.4 requires that all dumpsters, loading spaces, and mechanical areas be screened from adjoining existing residential uses, land in any residential zone, and constructed public streets. The subject DSP provides a six-foot-high brick screen wall between the loading space and East-West Highway (MD 410) and encloses the proposed trash area with an eight-foot-high brick-veneered corral to meet the requirements of this section.
- d. **Section 4.7, Buffering Incompatible Uses**—The subject application is exempt from the requirements of this section because it does not involve an total increase in gross floor area (GFA) on-site of ten percent, or 5,000 square feet, it does not extend any new structure or paved area closer to an adjacent property in a less-intense use category, and it does not involve a change of use from a lower- to higher-intensity use category. This should be noted as such on the plan.
- e. **Section 4.9, Sustainable Landscaping Requirements**—This DSP application is subject to Section 4.9, which requires that a percentage of the proposed plant materials be native plants. However, the number of plants in the schedule does not match the plant schedule and the plant schedule does not designate which plants are native species. These issues should be revised on the DSP prior to certification.

12. **Prince George's County Woodland Conservation Ordinance:** The property is subject to the 1993 Prince George's County Woodland Conservation and Tree Preservation Ordinance (WCO) because the site has a previously approved tree conservation plan. A Tree Conservation Plan (TCPII-046-99) was approved for the site on August 12, 1999, and the site has been developed in conformance with that approval. The previous tree conservation plan had an overall requirement of 3.94 acres that was met with 0.28 acres of reforestation/afforestation, 3.57 acres of off-site woodland credits and 0.09 acres of on-site woodland preservation.

The proposed development is in conformance with the existing Type II tree conservation plan (TCPII) because no change is proposed to the previously approved limit of disturbance. Because this is a major addition to the subject site, the TCPII should be revised to show the proposed McDonald's restaurant facility and parking lot revisions. Conditions regarding this revision have been included in the Recommendation Section of this report.

13. **Tree Canopy Coverage Ordinance:** The project is subject to the requirements of Subtitle 25, Division 3, The Tree Canopy Coverage Ordinance (TCC), because it will require a permit for more than 1,500 square feet of disturbance. The requirement for the subject property is ten percent of the gross tract area, or 1.32 acres (57,543 square feet), based on the C-S-C zoning. The required worksheet was provided for the tree canopy coverage (TCC) requirement showing it being met by a combination of 0.37 acres of on-site woodland conservation, 1.02 acres of other existing trees on-site and 3,525 square feet of proposed landscape trees. However, no information was provided about how the area of existing trees was calculated. Therefore, a condition has been included in the Recommendation Section of this report requiring this information be supplied prior to certification.

14. **Referral Comments:** The subject application was referred to the concerned agencies and divisions. The referral comments are summarized as follows:

- a. **Community Planning Division**—In a memorandum dated March 22, 2013, the Community Planning Division stated that the subject application is consistent with the 2002 General Plan Development Pattern policies for centers in the Developed Tier. This application conforms to the land use recommendations of the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone*. The proposed addition of drive-through service is contrary to the goals of the Prince George's Plaza Transit District.

Although the TDDP does not specifically identify the intended future land uses for parcels within the plan area, it links the allowed/desired uses to the zone and provides a purpose statement that calls for retail, service and office uses with consideration of mixed-used development in the future. In this case, the zoning of the property is C-S-C, and the permitted uses are generally in conformance with that zone, emphasizing commercial retail and office uses.

The transit district development plan does not permit eating and drinking establishments with drive-through service in the C-S-C Zone. The applicant must request an amendment to the development plan to allow the requested use which will require the approval of the District Council.

While the applicant should be commended for proposing an innovative drive-through configuration that strives to preserve the primary pedestrian network along East West Highway (MD 410), the drive-through component by its very nature is contrary to the goals for the Prince George's Plaza Transit District to create a pedestrian-friendly environment along East-West Highway (MD 410) (page 14). The TDDP emphasizes the importance of pedestrian access and circulation (page 28) and identifies the public right-of-way as the location of the primary pedestrian system and focus of pedestrian connectivity. The parking area and drive-through for the proposed McDonald's have been designed to be less suburban in character and more pedestrian-friendly and walkable. The proposed restaurant is sited with the longer side of the building close to the East-West Highway right-of-way. The parking lot, the drive-through, and the associated circulation for the two functions are located behind the building. The main entrance will connect with an existing six-foot sidewalk adjacent to East-West Highway (MD 410). An eight-foot-high screen wall and landscaped plantings will conceal the loading area from public view along the street. The façades of the building will be articulated with brick, cultured stone, and metal roof panels which will provide visual interest along the public right-of-way. Should the amendment be approved, the relationship between the drive-through queuing area and parking drive aisles should be clarified by the applicant to ensure that there are no issues with vehicular stacking and egress during peak operating hours. Additionally, the portion of the East-West Highway (MD 410) elevation near the service doors should have greater façade articulation to help break up the blank wall areas.

Although the planned configuration of the McDonald's is less suburban and promotes a more pedestrian-friendly environment, staff finds any potential amendment to the development plan to permit drive-through service would be detrimental to the transit district goals and desired development pattern. However, should this application be amended or conditioned to preclude the drive-through component the proposed restaurant

building would help reinforce the street-wall and pedestrian environment and would contribute positively to the transit district.

Comment: A condition has been included in the Recommendation Section of this report requiring the removal of the drive-through service from the proposed use.

- b. **Transportation Planning Section**—In a memorandum dated April 4, 2013, the Transportation Planning Section offered the following:

In September 1999 PGCPB No. 99-146 approved a detailed site plan (DSP-99006) for a retail store of 117,975 square feet in Parcel 9 of the T-D-O-Z. The proposed restaurant would be located adjacent to and east of the existing retail building.

The enactment of County Council CB-56-2011 by the District Council, and the resulting amendment of Section 27-107.01 of the Zoning Ordinance, essentially combined fast-food and drive-in restaurants with the more general category termed eating or drinking establishments. The use table in the TDDP, however, distinguishes between a fast-food restaurant and other types of eating and drinking establishments, and greatly limits the circumstances in which a fast-food restaurant can be located. In general, the fast-food restaurant, following the guidance in the TDDP use table, can only be located within another building or located as an accessory to a recreation facility.

In reviewing the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP), that document attempted to limit fast-food restaurants by placing them in locations where the provision of drive-through service would be difficult if not impossible. Therefore, while the transportation staff is inclined to support the placement of a fast-food restaurant on the subject site, it is believed that the inclusion of drive-through service is inconsistent with the TDDP. This contention is based on the goals of the transit district, including the creation of a pedestrian-friendly environment. It is also based on the general goals of the Transit District Overlay (T-D-O) Zone given in Section 27-548.03. Furthermore, the "Transportation Review Guidelines, Part 1" document includes a checklist to assist in the determination of excellent or successful transit-oriented development (TOD). The checklist, which is based on review of similar checklists and sets of regulations around the United States, indicates that lack (or non-provision) of drive-through facilities is a positive characteristic.

The existing retail store consisting of 117,975 square feet with a 16,309-square-foot garden center on Parcel A was approved with 507 parking spaces on 13.21 acres of land. The T-D-O-Z required a maximum parking of 4.35 spaces per 1,000 square feet or 18 parking spaces for the proposed McDonald's use. The applicant is providing 64 parking spaces, an excess of 46 parking spaces. Combined, there will be net loss of 19 parking spaces proposed by this site plan, leaving 488 parking spaces remaining within Parcel A. Given that parking spaces will be eliminated under this site plan, the site is not subject to payment of an impact fee under either preferred or the premium parking caps, as defined in the TDDP.

One of the required findings for a detailed site plan within the T-D-O-Z is that the buildings, pedestrian and vehicular circulation systems and parking/loading areas maximize safety and are adequate to meet the purposes of the T-D-O-Z. The site plan shows the fast food restaurant adjacent to East-West Highway (MD 410). The proposed

drive-through service is located directly south and adjacent to the building. This creates a conflict between vehicles and pedestrians seeking to enter the building. Virtually all of the restaurant's patrons must cross the drive-through service lane in order to enter the proposed fast-food restaurant, including the handicapped patrons using the designated parking spaces.

The configuration of the drive-through service lane and the potential queuing creates issues with persons seeking to enter or exit the parking on the site. Even the queuing of two or three cars at the entrance to the drive-through service lane, where the menu board is located and where orders are taken, would block much of the site's parking. This area requires additional signage even if drive-through service is maintained in order to lessen the likelihood of queued vehicles blocking parking.

These operational issues provide another justification for the elimination of the drive-through service. Given the site constraints, the plan would be much more functional for all users, including delivery vehicles, if drive-through service were eliminated on the plan. Beyond these issues, the site plan is acceptable. Access is acceptable.

In summary, the Transportation Planning Section finds that the subject application does generally conform to the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP) and other prior approved plans. It is determined that while the use is acceptable at this location, the provision of the drive-through service (a) poses issues with the TDDP and the general goals of the T-D-O Zone, (b) is not consistent with excellent TOD, (c) creates conflicts with patrons entering the building, and (d) results in queuing issues affecting vehicles entering and leaving, and should be eliminated from the plan.

Comment: A condition has been included in the Recommendation Section of this report requiring the removal of the drive-through service from the proposed use.

- c. **Subdivision Review Section**—In a memorandum dated March 29, 2013, the Subdivision Review Section stated that the subject property is known as Parcel A, located on Tax Map 41 in Grid E-2, within the C-S-C Zone and is 13.21 acres. They provided a review of applicable conditions attached to approval of the relevant Preliminary Plan of Subdivision 4-99004 which have been incorporated into Finding 10 above. They also offered the following discussion:

Parcel A was recorded in Plat Book VJ 188-33 on November 19, 1999. The record plat contains five notes and the following notes in **bold** relate to the review of this application:

4. **This plat is subject to a recreation facilities agreement recorded in Liber 13466 at Folio 335.**

The recorded recreation facilities agreement indicates that the developer is to construct an eight-foot-wide asphalt trail and stone veneer headwall. The construction of the recreation facilities are to be completed prior to issuance of the certificate of occupancy. The Home Depot store was built in 2000 and is currently occupied.

5. This plat is subject to an access easement, in accordance with Section 24-128(b)(9) of the Subdivision Regulations, Prince George's code, recorded in Liber 13408 at Folio 634.

The DSP is not proposing any access to East-West Highway, (MD 410). The proposed McDonald's restaurant will be using the existing access location of the Home Depot onto the access easement. No new access locations are being proposed with this DSP.

The bearings, distances, and public utility easement (PUE) on the site plan are consistent with the record plat. However, the DSP has some inconsistencies that need to be address. The DSP should be revised with the following technical corrections:

- (1) Show and label the entire acreage for Parcel A
- (2) Clearly show and label the access easement Liber 13408 Folio 634 as reflected on the record plat VJ 188-33.
- (3) Clearly show and label the PEPCO easement Liber 6909 Folio 325 as reflected on the record plat VJ 188-33.

The Detailed Site Plan DSP-99006-01 is in substantial conformance with the approved Preliminary Plan of Subdivision 4-99004 and record plat if the above comments have been addressed. Failure of the site plan and record plat to match will result in the grading and building permits being placed on hold until the plans are corrected. There are no other subdivision issues at this time.

Comment: The suggested conditions have been included in the Recommendation Section of this report.

- d. **Trails**—In a memorandum dated March 7, 2013, the trails coordinator of the Transportation Planning Section stated that the applicant's proposal is in strict conformance with the mandatory development requirements of the TDDP.

The Transportation Planning Section then offered the following summarized review comments:

The applicant has submitted the required information, specifically a general description of the proposed pedestrian system that appears to be adequate for the intended use. The applicant's proposal conforms with the required information to be submitted with a Detailed Site Plan, as described in Section 27-546(b)(1) of the Zoning Ordinance. A statement regarding the "description of the relationship between vehicular and pedestrian circulation systems" is required site plan content.

In this regard, and in their Statement of Justification, the applicant states that the landscape plan "incorporates pedestrian safety into its design. Clear lines of site for both automobiles and pedestrians are provided so as to allow for safe travel throughout Subarea 9."

Staff agrees with this assertion. No new curb cuts are proposed along East-West Highway (MD 410). The proposed building is located along East-West Highway (MD 410). The proposal includes a vehicular access via the existing East-West Highway (MD 410) access drive at the Home Depot. A direct sidewalk connection to the existing sidewalks and streetscape along East-West Highway (MD 410) is shown on the plans. The sidewalks and the pedestrian zone appear to be adequate to meet the purposes of the Transit District Overlay Zone (T-D-O-Z) as required by 27-548.08(c) of the Zoning Ordinance. The streetscape contains pedestrian scaled lighting and other amenities and is in strict conformance with the TDDP's District Wide Mandatory Development Requirements.

Further, the applicant's proposal shall be in conformance with the requirements of the T-D-O (Transit District Overlay) Zone. The applicant's proposal is consistent with the "Regulations" contained within Section 27-548.06(e) of the Zoning Ordinance, which requires that the "pedestrian system within a Transit District shall be oriented toward serving the Metro station, as well as other development within the District."

The design of the building and drive through layout orients sidewalks towards East-West Highway (MD 410) in order to provide access to the nearby Prince George's Plaza Metro Station. The sidewalks are wide enough for a bicycle to use, and bicycles can ride within the limits of East-West Highway (MD 410). The site is close to the access to the Northwest Branch Trail and the proposal conforms to the TDDP's District Wide and Subarea 9 Mandatory Development Requirements that are described in the TDDP.

A more detailed analysis of trails-related Mandatory Development Requirements was then provided. Based on that analysis, the trails staff concluded that the proposed pedestrian walkways, lighting, and bicycle parking do not conflict with the TDDP. The site will be conveniently located along East-West Highway (MD 410) and will be accessible to the Prince George's Plaza Metro Station and the Northwest Branch Trail. Staff recommends that the following conditions be considered:

- a. *Install two u-shaped bicycle parking spaces close to the main entrance to the building anchored into a concrete base.*
- b. *Prior to certification, the detailed site plan shall show two u-shaped bicycle parking spaces near the main entrance to the building.*
- c. *Details of the bicycle parking and signage shall be provided on the detailed site plan.*
- d. *A bicycle parking area sign (MUTCD D4-3) shall be erected at the parking location (see MUTCD Part 9, Traffic Control for Bicycle Facilities, Section 9B.23.).*

Comment: The suggested conditions have been included in the Recommendation Section of this report.

- e. **The Department of Parks and Recreation (DPR)**—In a memorandum dated March 6, 2013, DPR indicated they had no comment on the subject application.

- f. **Permit Review Section**—In a memorandum dated March 20, 2013, the Permit Review Section indicated that the fast-food restaurant is not a permitted use in the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP) and no further comments would be provided at this time.
- g. **Environmental Planning Section**—In a memorandum dated March 11, 2013, the Environmental Planning Section offered a summary of the environmental site description and provided an analysis of the site plan's conformance with environmental requirements in the applicable 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* (TDDP). This analysis is discussed in Finding 8 above as necessary.

A Natural Resource Inventory Equivalency letter, NRI-090-11, in conformance with the environmental regulations, was issued on April 25, 2011 and submitted with the current application. The site has less than 10,000 square feet of woodlands and no regulated environmental features will be impacted as part of the proposed application.

- h. **Fire/EMS Department**—At the time of writing of this technical staff report, no response has been received from the Prince George's Fire/EMS Department.
- i. **Department of Public Works and Transportation (DPW&T)**—In a memorandum dated March 12, 2013, DPW&T offered comment regarding needed coordination with the State Highway Administration (SHA) regarding the roads adjacent to the subject project and standard comments regarding storm drainage systems and soils investigation. Additionally, they indicated that the proposed DSP was not consistent with the approved Stormwater Management Concept 2259-2011.

Comment: A condition has been included in the Recommendation Section of this report that would require proof of an approved stormwater management concept plan and letter consistent with the proposed layout prior to certification of the DSP.

- j. **Prince George's County Police Department**—In a memorandum dated February 25, 2013, the Prince George's County Police Department stated that there are no crime prevention through environmental design (CPTED) concerns at this time with the subject application.
- k. **Prince George's County Health Department**—In a memorandum dated March 1, 2013, the Environmental Engineering Program of the Prince George's County Health Department offered the following comments and recommendations:

- (1) The photometric plan reflects what appear to be areas of excessive light levels on the site, but does not extend far enough to include light levels at the adjacent property lines. There is an increasing body of scientific research suggesting that artificial light pollution can have lasting adverse impacts on human health. The photometric plan should be revised to indicate that all proposed exterior light fixtures will be shielded, fixed in the full cut-off position and located so as to minimize light trespass caused by spill light onto adjacent properties.

Comment: A condition has been included in the Recommendation Section requiring this revision to the DSP.

- (2) There are seven existing carry-out/convenience store food facilities and two markets/grocery stores within a one-half mile radius of this location. Research has found that people who live near an abundance of fast-food restaurants and convenience stores compared to grocery stores and fresh produce vendors, have a significantly higher prevalence of obesity and diabetes.

Comment: The subject application does propose a fast-food restaurant; however, given the existing development on-site, it is not likely that there would be room for a market or grocery store instead of the proposed restaurant which has such a small footprint and impact.

- (3) During the demolition/construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.

Comment: This requirement will be enforced at the time of permit; however, a note should be provided on the DSP indicating conformance with these requirements.

- (4) During the demolition/construction phases of this project, no noise should be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.

Comment: This requirement will be enforced at the time of permit; however, a note should be provided on the DSP indicating conformance with these requirements.

1. **Maryland State Highway Administration (SHA)**—In a memorandum dated March 25, 2013, SHA requested a Traffic Impact Study (TIS) for review to determine if any off-site improvements will be required. SHA stated that if it is concluded that off-site improvements are required to East-West Highway (MD 410), an access permit will be required.

Comment: The Transportation Planning Section provided the following response to the requested TIS:

Aside from the fact that issues of off-site transportation adequacy are generally not reviewed at the time of detailed site plan, the specific requirements of the applicable TDDP greatly reduce the need for adequacy review at any stage of development provided that the development proposal conforms to the TDDP. For these reasons, no traffic study has been requested or reviewed. However, the applicant shall be made aware that SHA has requested a study, and that agency may compel the provision of a study by the applicant in the event that any permits from SHA are needed to implement this use.

- m. **Washington Metropolitan Area Transit Authority (WMATA)**—At the time of writing of this technical staff report, no response has been received from WMATA.
- n. **Washington Suburban Sanitary Commission (WSSC)**—In a memorandum dated February 27, 2013, WSSC offered comments on needed coordination with buried utilities and WSSC easements and the requirements for connection to the existing water and sewer lines.
- o. **Potomac Electric Power Company (PEPCO)**—At the time of writing of this technical staff report, no response has been received from PEPCO.
- p. **The City of Hyattsville**—In an e-mail dated April 4, 2013, a City representative indicated that the City of Hyattsville Council was in opposition to the request to amend the table of uses and that the official motion to oppose would be on the consent agenda for their April 15th meeting. They offered the following statements for inclusion in the technical staff report:

The City of Hyattsville does not support the applicant's request for an amendment to the table of uses to permit eating and drinking establishments with drive-through service, as this request is inconsistent with the pedestrian and transit oriented intent of the Prince George's Plaza Transit District Development Plan. If the applicant requires a drive-through as part of the development, then the City is opposed to this proposed detailed site plan.

The City of Hyattsville is supportive of the applicant's proposed compliance with landscape manual and tree canopy.

RECOMMENDATION

Based on the foregoing evaluation and analysis, the Urban Design staff recommends that the Planning Board adopt the findings of this report and recommend to the District Council that it APPROVE Detailed Site Plan DSP-99006-01, McDonald's—Hyattsville, and Type II Tree Conservation Plan TCPII-046-99-01, subject to the following conditions:

1. Prior to signature approval of the plans, the applicant shall revise the detailed site plan or provide additional information as follows:
 - a. Revise the DSP to remove the drive-through portion of the use and convert the area into green space or amenity space.
 - b. Revise the DSP to show an expanded paved area in the front of the restaurant, to the east, to include specialty paving, tables and chairs for customer use and decorative plantings.
 - c. Submit a copy of an approved Stormwater Management Concept plan and letter consistent with the proposed layout.
 - d. The sidewalk connecting the McDonald's to the streetscape along East-West Highway (MD 410) shall be revised to provide special paving to be approved by the Urban Design Section as designee of the Planning Board.

- e. Revise the DSP to include notes and a detail regarding the stenciling of storm drain inlets with "Do Not Dump – Chesapeake Bay Drainage." A copy of the sediment and erosion control plan containing notes and details regarding the same stenciling shall be submitted.
- f. Remove the Section 4.3(c)(1) Parking Lot Perimeter Landscape Strip schedules from the DSP.
- g. Revise the Section 4.3(c)(2) schedules to match the one in the 2010 *Prince George's County Landscape Manual* (Landscape Manual) and to accurately reflect the required number of shade trees, and that requirement being met. Additionally, provide a landscape plan for the entirety of Parking Compound 2.
- h. Revise the DSP to note the exemption from Section 4.7 of the 2010 *Prince George's County Landscape Manual*.
- i. Revise the Section 4.9 schedule on the DSP to match the number of plants in the plant schedule and revise the plant schedule to designate which plants are native species to be in conformance with the 2010 *Prince George's County Landscape Manual*.
- j. Revise the Type II Tree Conservation Plan TCPII-046-99 as follows:
 - (1) Show the proposed building and parking lot layout for the proposed application.
 - (2) Show the previous TCP approval and add "01" to the approval block.
 - (3) Have the revised plan signed and dated by the qualified professional preparing the plan.
- k. Revise the DSP to indicate how many trash receptacles are within the corral area and where proposed individual trash receptacles, at least two, will be located throughout the proposed seating and parking areas adjacent to the McDonald's.
- l. Revise the DSP to show how the area of existing trees credited in the tree canopy coverage schedule was calculated.
- m. Revise the DSP to show and label the entire acreage for Parcel A
- n. Revise the DSP to clearly show and label the access easement, Liber 13408 Folio 634, and the Potomac Electric Power Company (PEPCO) easement, Liber 6909 Folio 325, as reflected on the record plat.
- o. Revise the DSP to show a minimum of two u-shaped bicycle parking spaces, anchored into a concrete base, close to the main entrance of the building and provide details of the bicycle parking and signage (MUTCD D4-3).
- p. Revise the photometric plan to indicate that all proposed exterior light fixtures will be shielded and fixed in the full cut-off position and footcandle levels throughout the proposed area of improvement will be minimized to the maximum extent practicable.

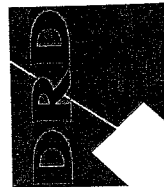
- q. Provide a plan note that indicates conformance to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.
- r. Provide a plan note that indicates the applicant's intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.



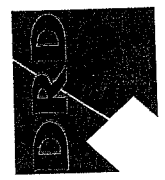
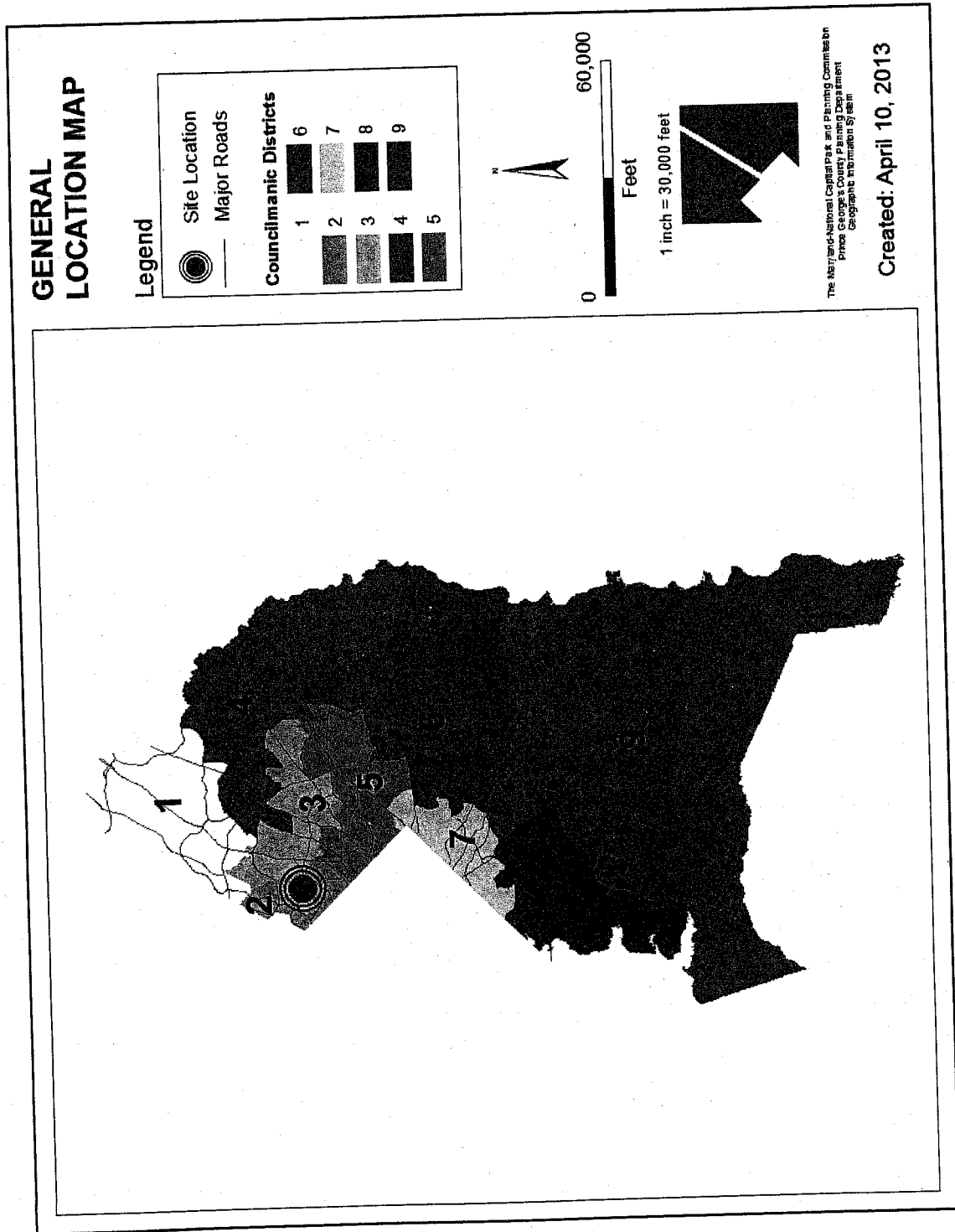
ITEM:

CASE: DSP-99006-01

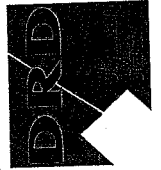
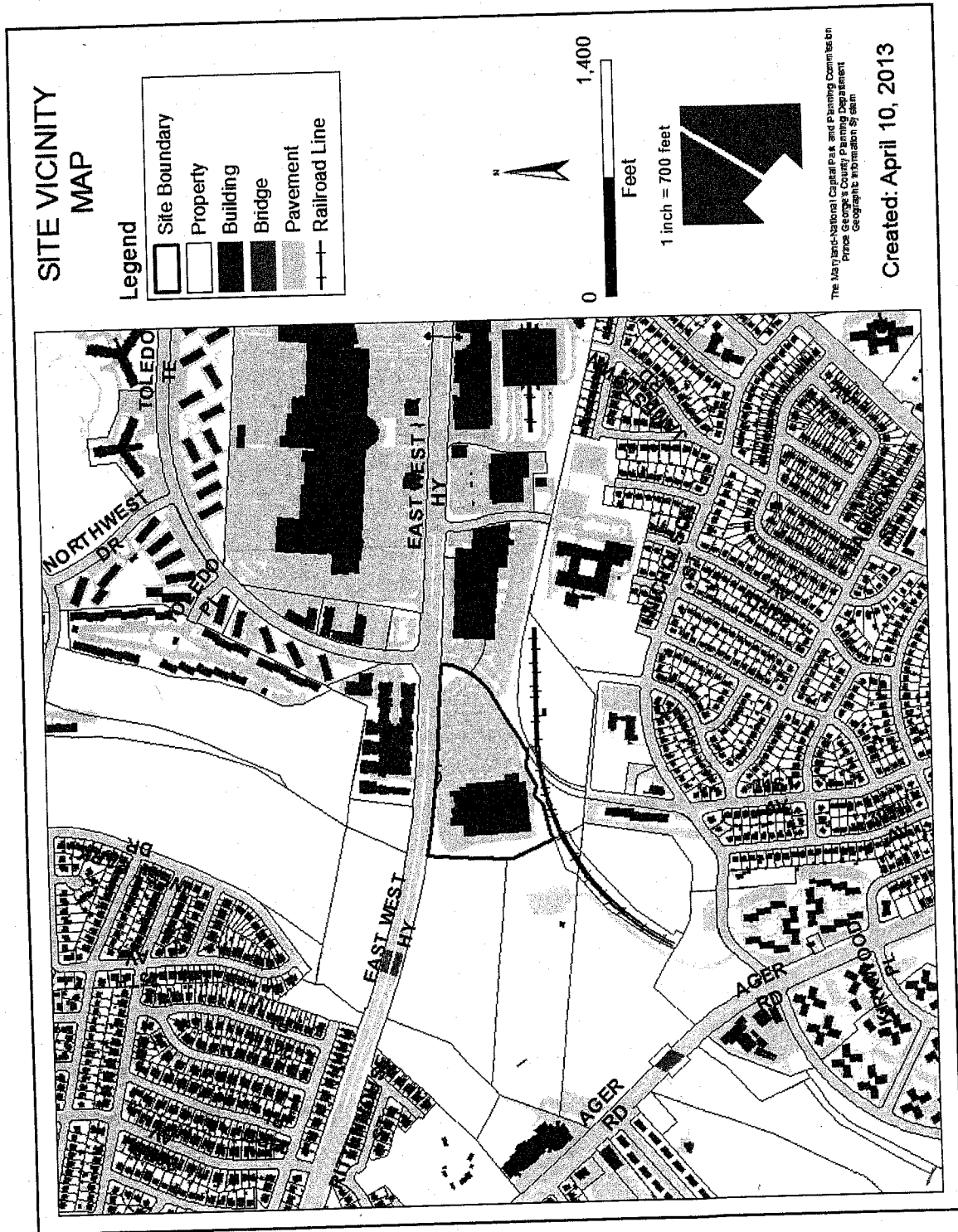
**MCDONALD'S
HYATTSVILLE**



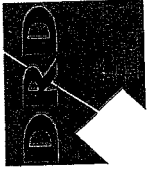
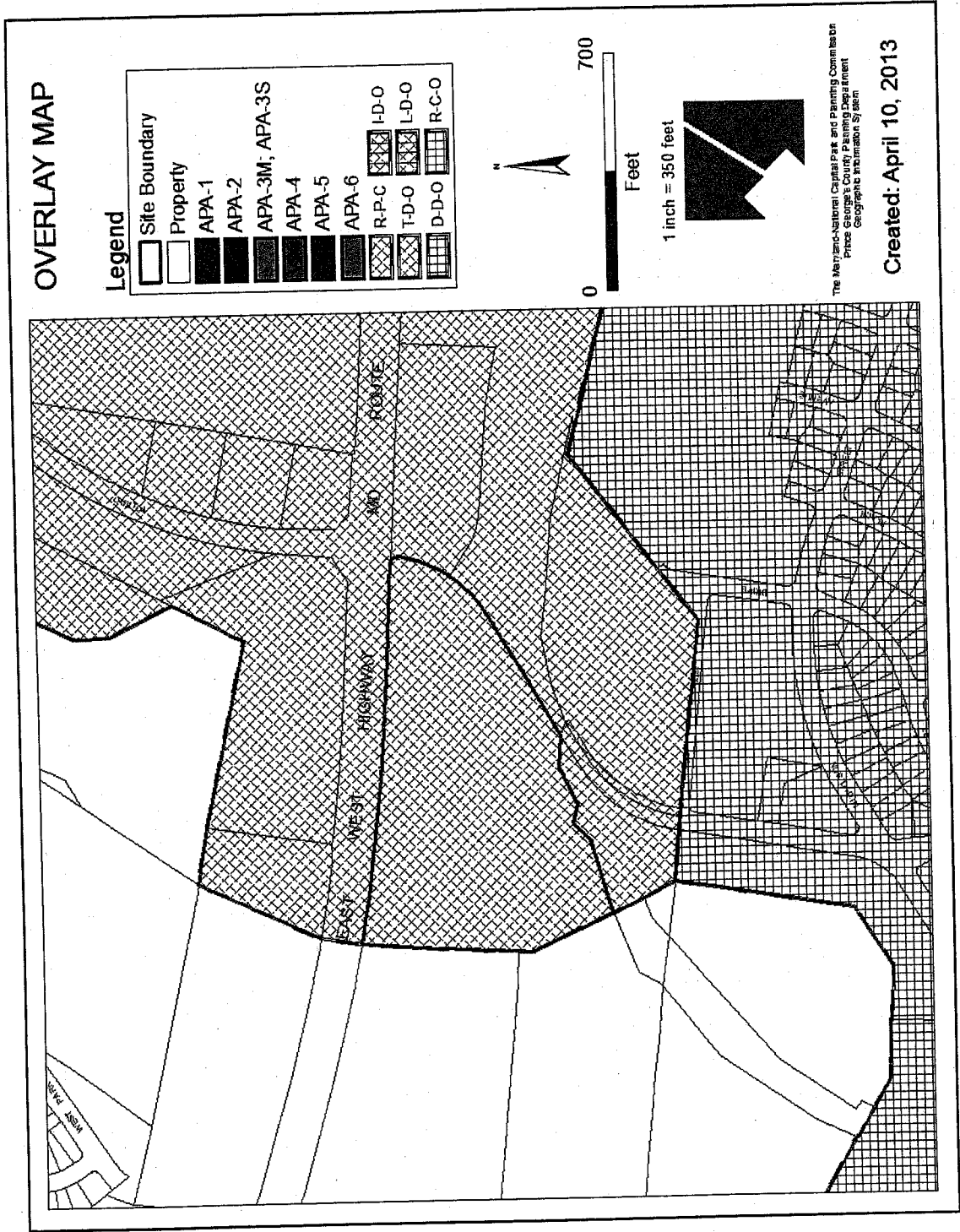
GENERAL LOCATION MAP



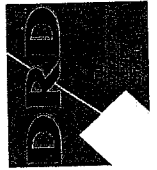
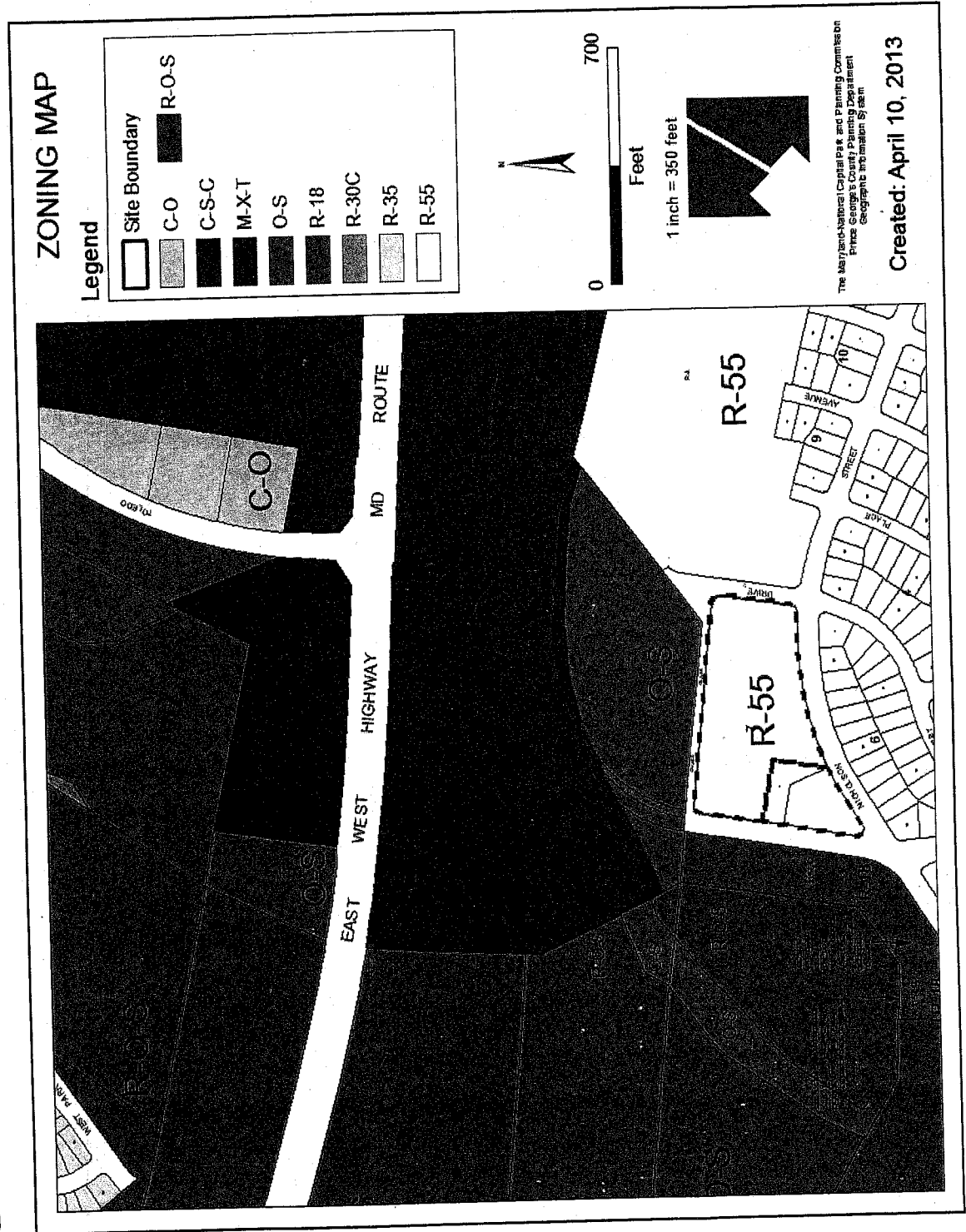
SITE VICINITY



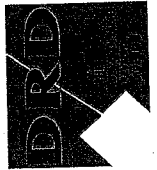
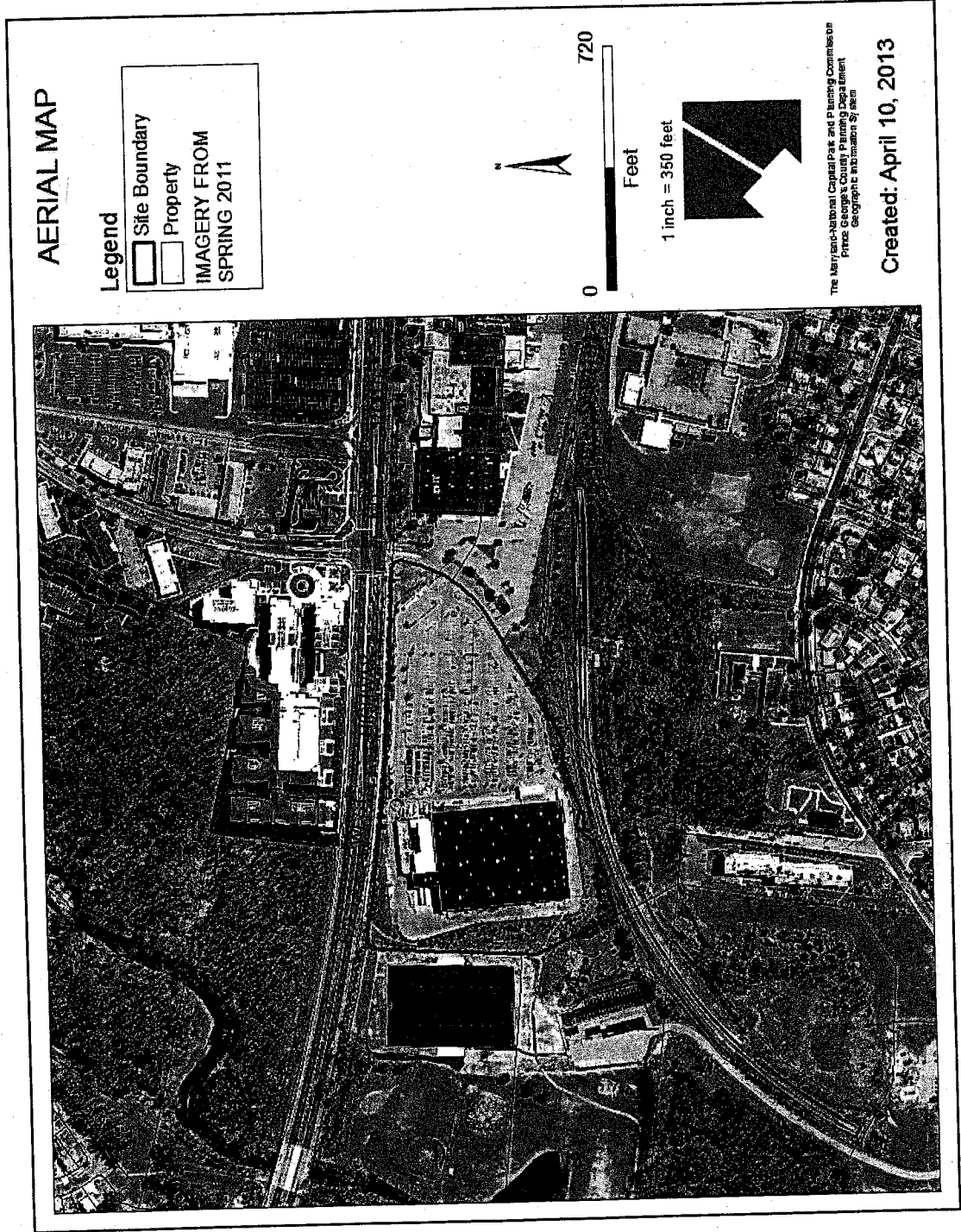
OVERLAY MAP



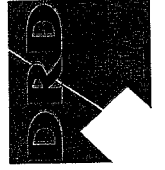
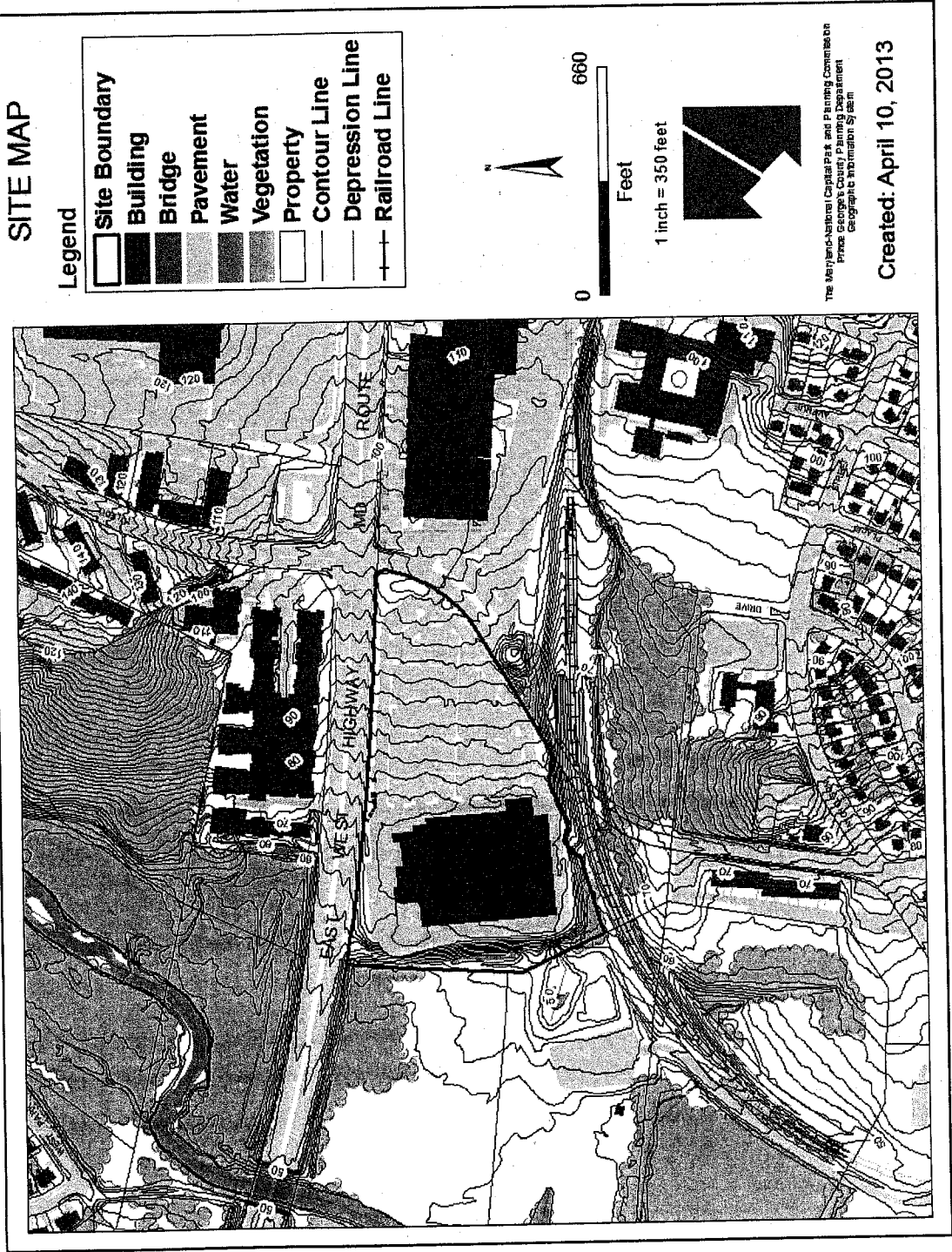
ZONING MAP



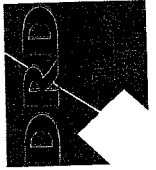
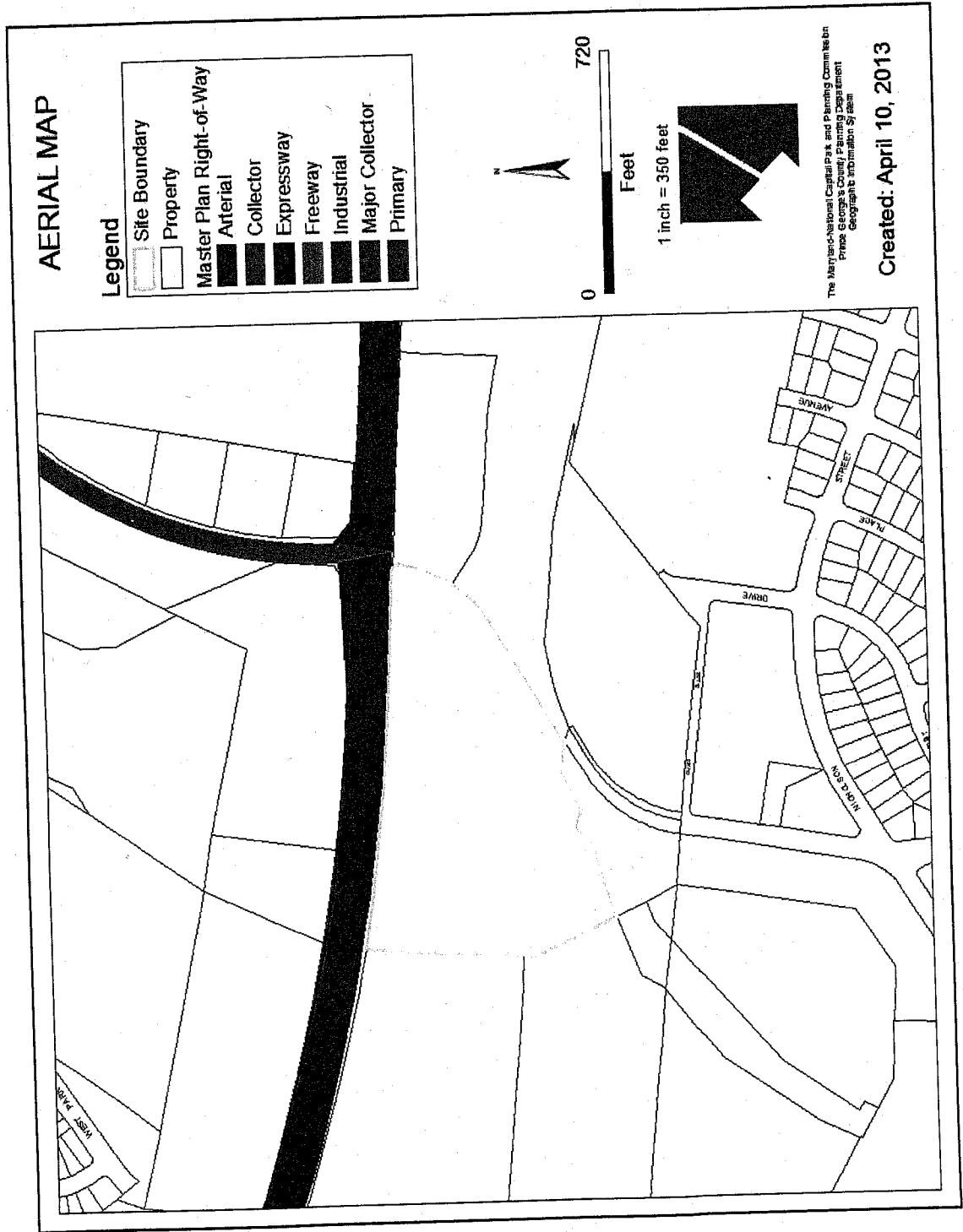
AERIAL MAP



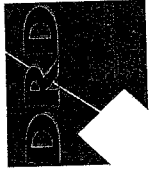
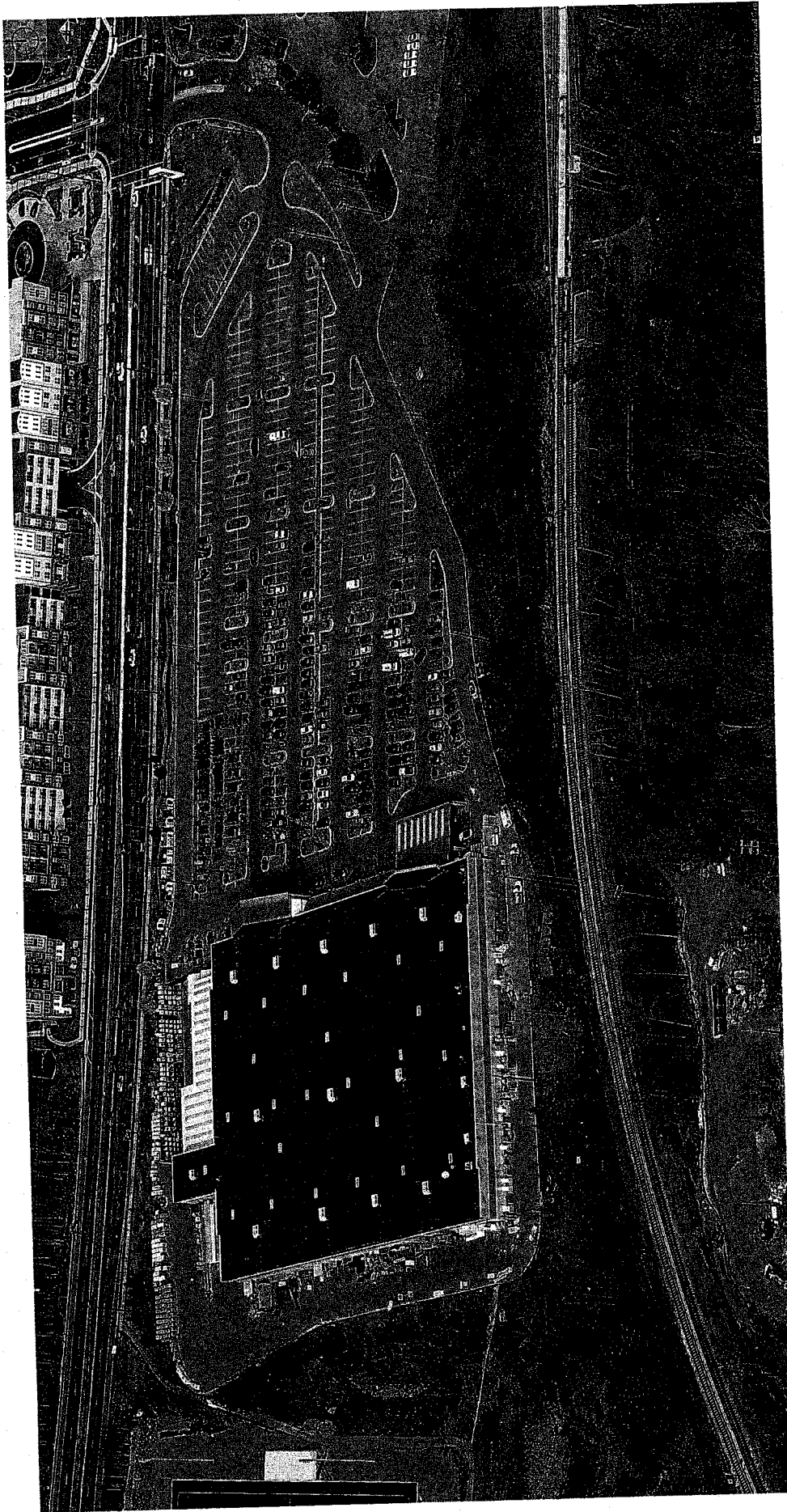
SITE MAP



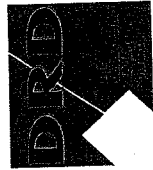
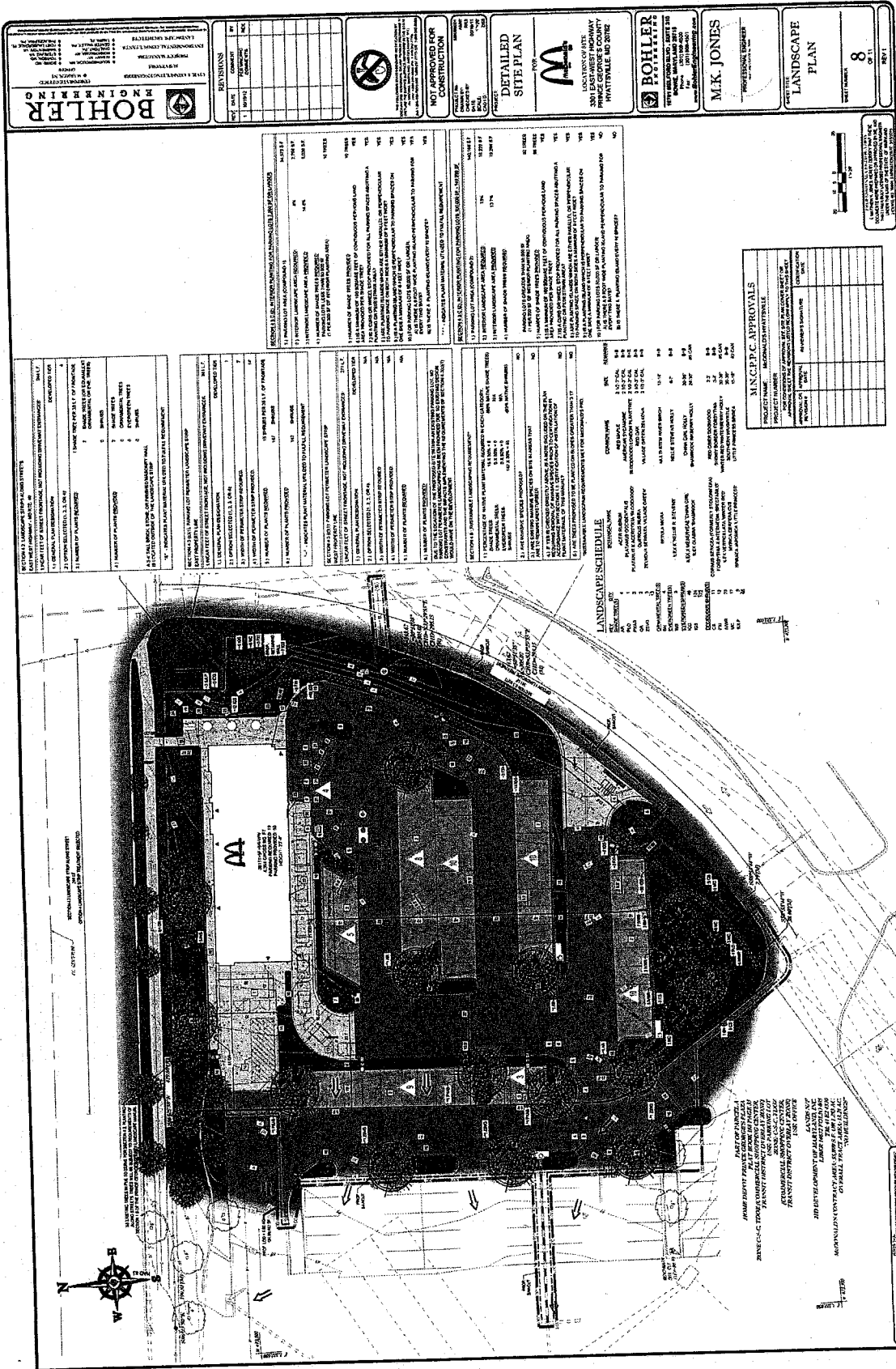
MATER PLAN RIGHT-OF-WAY MAP



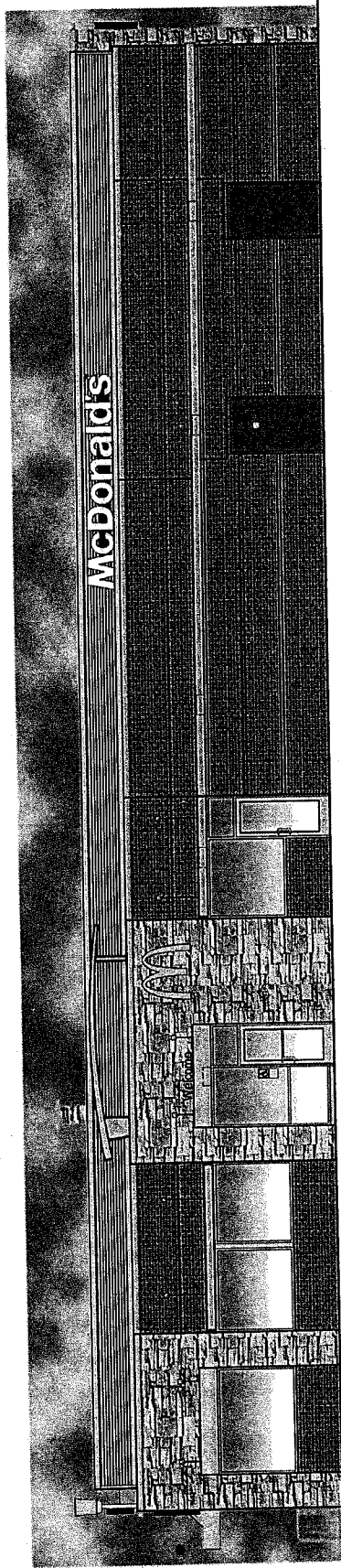
BIRD'S-EYE VIEW



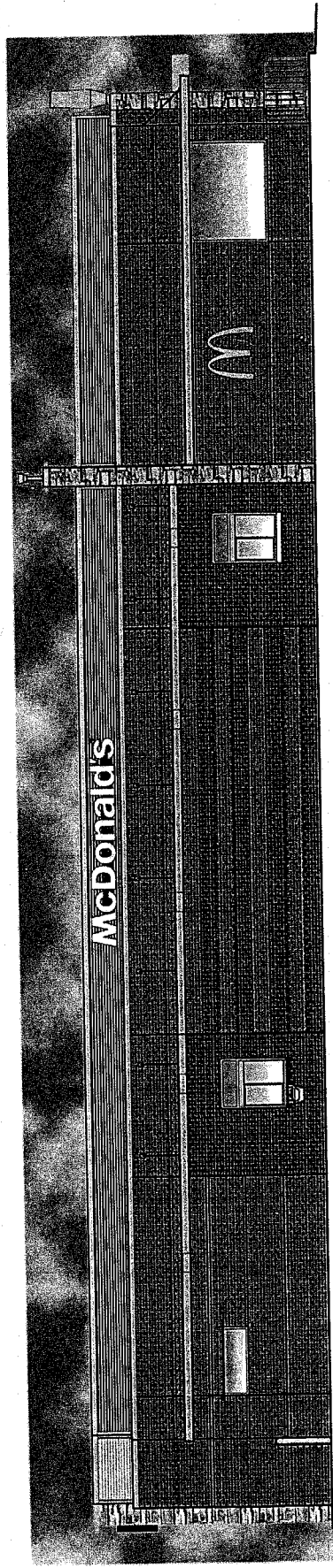
RENDERED LANDSCAPE PLAN



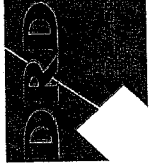
ARCHITECTURAL ELEVATIONS 2 SIDES



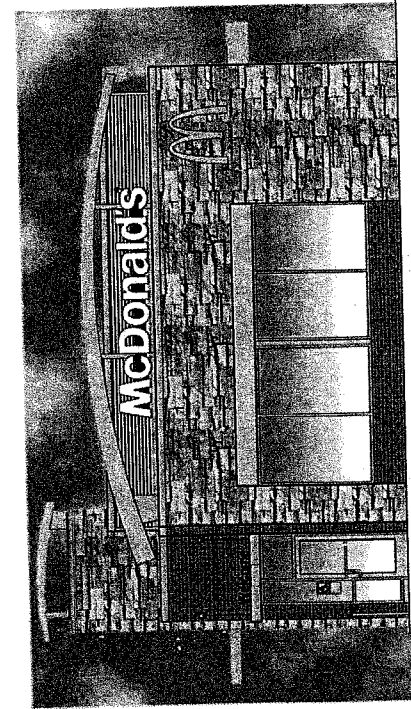
Non-Drive-Thru Side Elevation



Drive-Thru Side Elevation

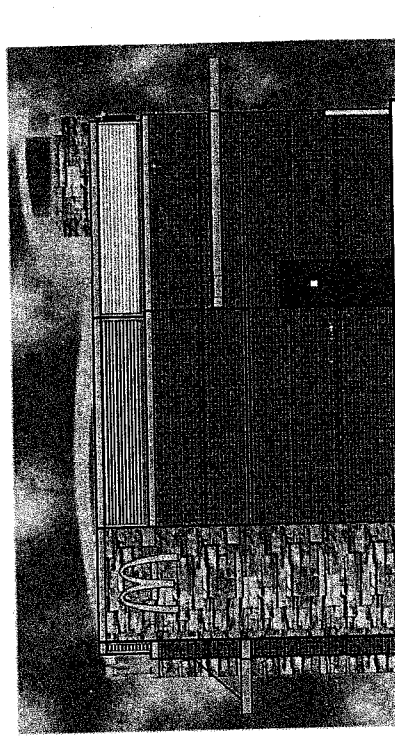


ARCHITECTURAL ELEVATIONS FRONT AND REAR

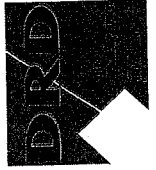


Front Elevation

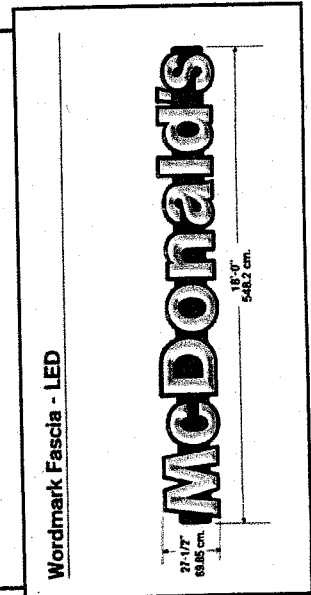
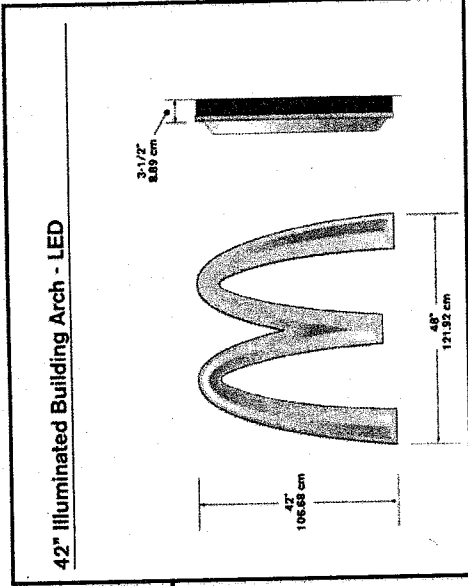
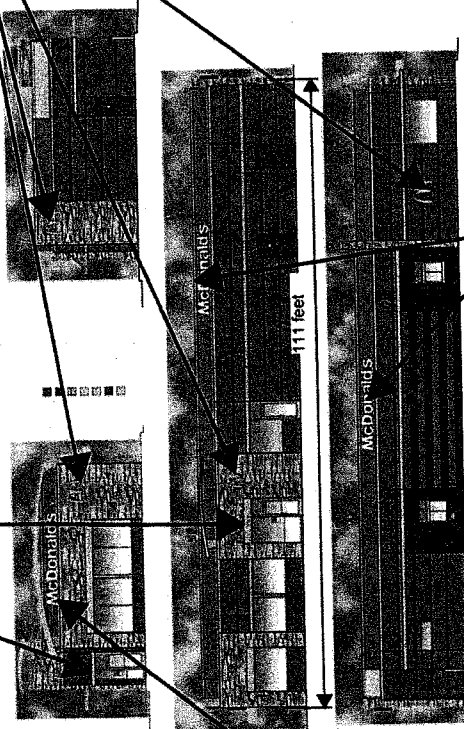
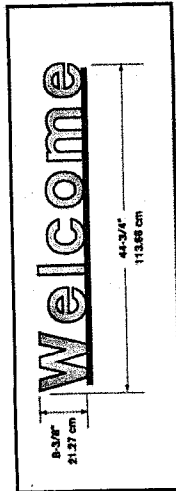
| MATERIALS LEGEND | |
|------------------|------------------------------------|
| | FACEBRICK |
| | RUSSET RED YELLOW BY BELDEN |
| | FACEBRICK |
| | MIDLAND BLEND BY BELDEN |
| | CULTURED STONE |
| | ROGAL 1800 HWEST Phc-FT-LEDGESTONE |
| | ROOF CAP ELEMENT |
| | McDONALD'S STANDARD YELLOW |
| | METAL TRELLIS |
| | COLOR - ALUMINUM |
| | HOLLOW METAL DOORS |
| | MATCH RUSSET RED BRICK |
| | STOREFRONT |
| | COLOR - ALUMINUM |



Rear Elevation



BUILDING SIGNAGE



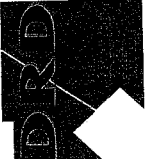
SIGN AREA
McDonald's Home Depot, Hyattsville, Maryland, LC 018-1333

| Item Type | Item Description | Quantity | Unit Price | Total Price |
|-----------|-------------------------------------|----------|------------|-------------|
| 1 | 42" Illuminated Building Arch - LED | 1 | 12,192.00 | 12,192.00 |
| 2 | Wordmark Fascia - LED | 1 | 54,820.00 | 54,820.00 |
| 3 | Welcome Sign | 1 | 2,137.00 | 2,137.00 |
| 4 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 5 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 6 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 7 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 8 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 9 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 10 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 11 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 12 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 13 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 14 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 15 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 16 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 17 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 18 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 19 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 20 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 21 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 22 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 23 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 24 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 25 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 26 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 27 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 28 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 29 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 30 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 31 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 32 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 33 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 34 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 35 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 36 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 37 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 38 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 39 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 40 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 41 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 42 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 43 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 44 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 45 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 46 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 47 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 48 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 49 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |
| 50 | McDonald's Sign | 1 | 11,386.00 | 11,386.00 |

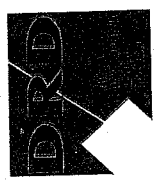
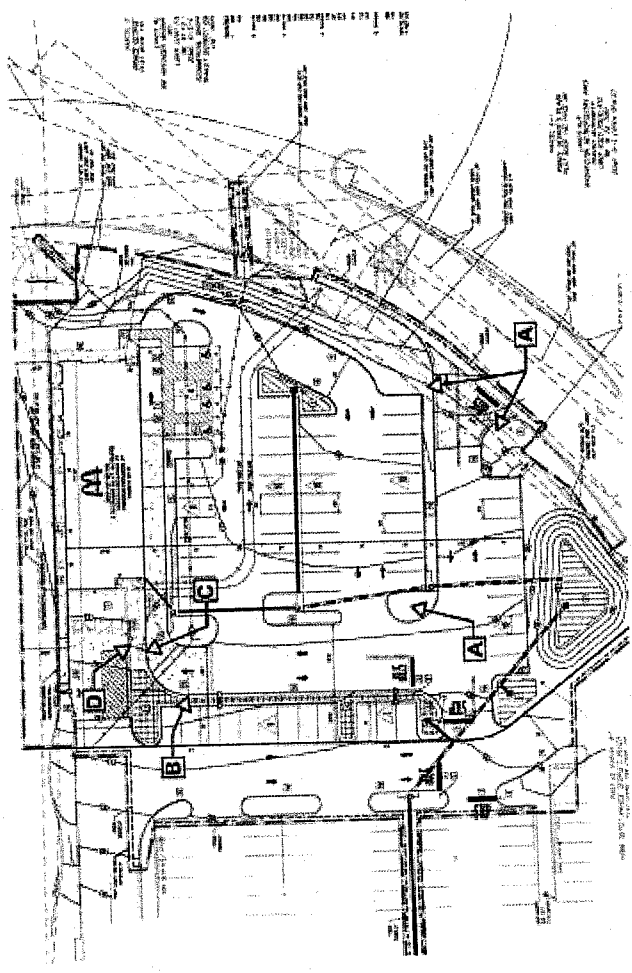
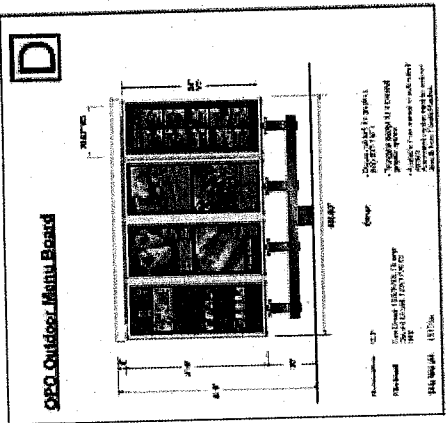
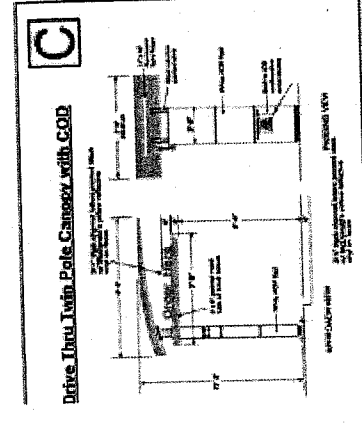
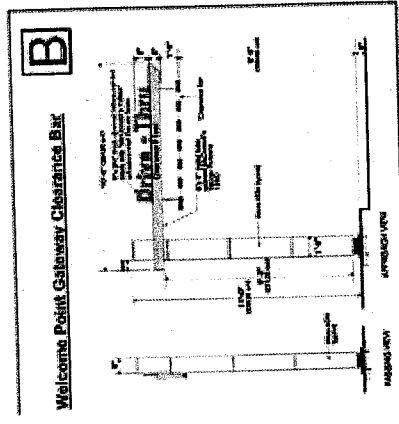
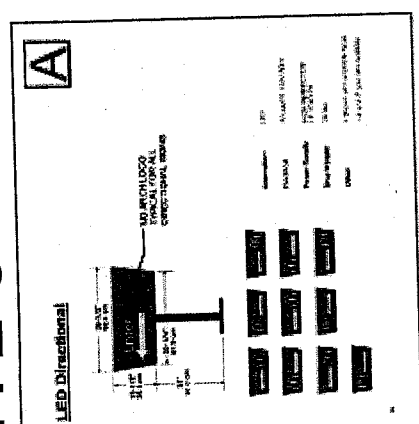
MUNICIPAL APPROVALS

| Project Name | Project Number | City/County | Approval Date | Approval Authority |
|----------------------------|----------------|-------------|---------------|--------------------|
| HOME DEPOT/ATM/LED SIGNAGE | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

BUILDING SIGN AMENITIES
7 DEC. 2012
MCDONALD'S AT HOME DEPOT
L/C 019-1333
HYATTSVILLE, MARYLAND



SITE SIGNAGE



CIRCULATION PLAN

EAST WEST HIGHWAY-MD RTE. 410
VARIABLE WIDTH ROADWAY

The main body of the drawing is a detailed circulation plan for a site. It shows building footprints, parking areas, and various circulation paths including sidewalks, paths, and ramps. A north arrow is located in the upper left corner. The plan is annotated with numerous callouts, dimensions, and site keynotes. A large 'M' logo is prominently displayed in the center of the site. The drawing is framed by a title block and technical specifications.

BOHLER ENGINEERING

BOHLER ENGINEERING INC.
3800 W. ROUTE 202
MATERIALS, MD 20701

| NO. | DATE | BY | REVISIONS |
|-----|------|----|-----------|
| 1 | | | |
| 2 | | | |
| 3 | | | |
| 4 | | | |
| 5 | | | |
| 6 | | | |
| 7 | | | |
| 8 | | | |
| 9 | | | |
| 10 | | | |

NOT APPROVED FOR CONSTRUCTION

THIS PLAN IS NOT TO BE USED FOR CONSTRUCTION UNLESS IT IS APPROVED BY THE APPROPRIATE AGENCIES.

MA

1612 W. WEST
3800 W. ROUTE 202
MATERIALS, MD 20701

BOHLER ENGINEERING

BOHLER ENGINEERING INC.
3800 W. ROUTE 202
MATERIALS, MD 20701

M.K. JONES

PROFESSIONAL ENGINEER

DETAILED SITE AND GRADING PLAN

6
DATE

NOTES

1. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

2. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

3. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

4. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

NOTES

1. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

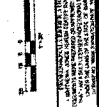
2. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

3. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

4. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

MNC/PEC APPROVALS

| PROJECT NAME | APPROVAL DATE | APPROVER | DATE |
|--------------|---------------|----------|------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

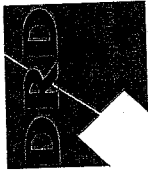


THIS DRAWING IS THE PROPERTY OF BOHLER ENGINEERING INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS WITHOUT THE WRITTEN PERMISSION OF BOHLER ENGINEERING INC.

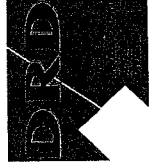
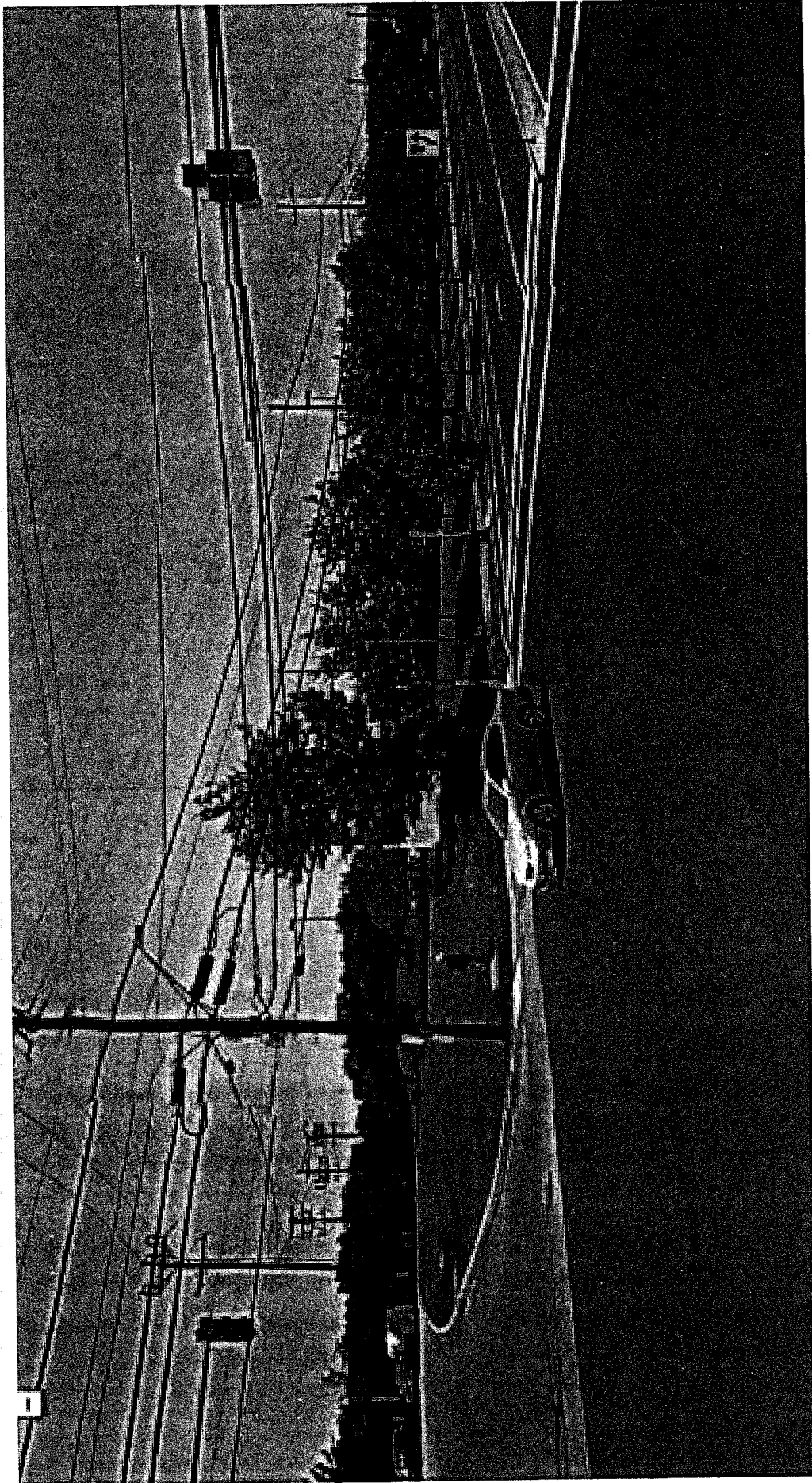
DATE OF ISSUE: 4/25/2013

PROJECT NO: DSP-99006-01

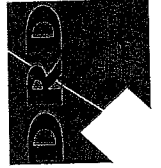
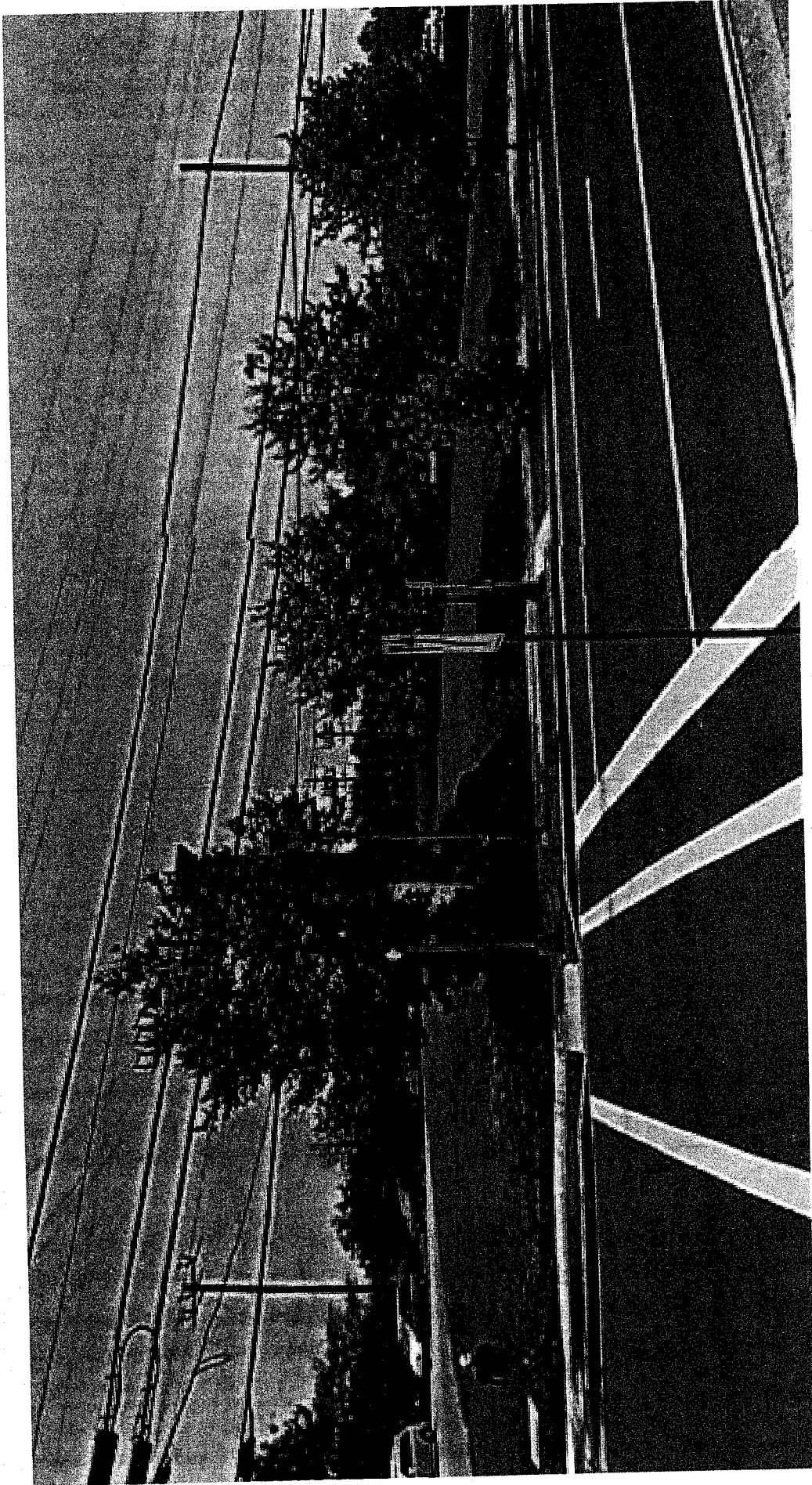
SHEET NO: 1 OF 1



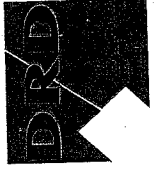
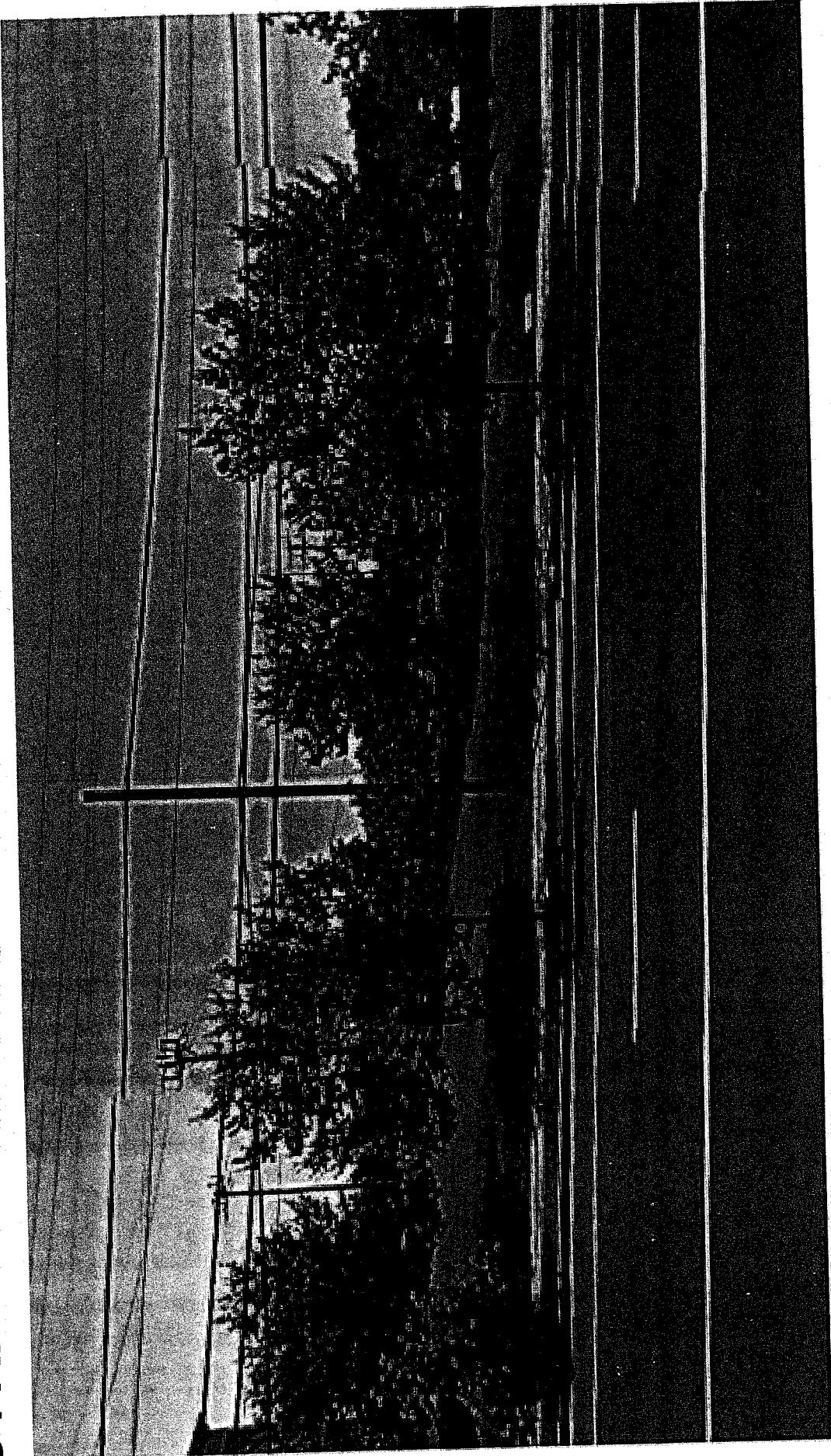
STREET VIEW PHOTOS



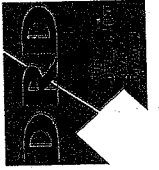
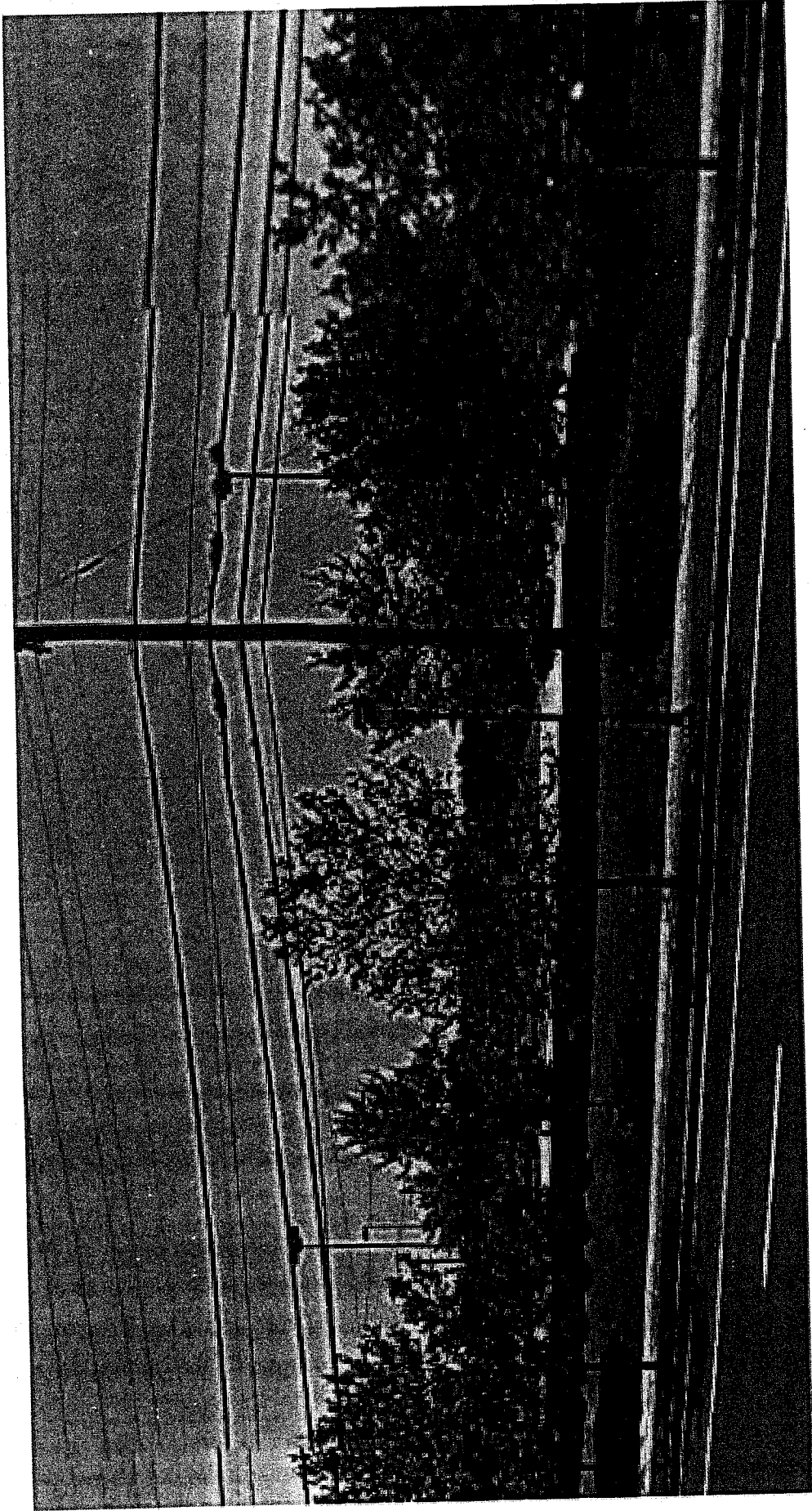
STREET VIEW PHOTOS



STREET VIEW PHOTOS



STREET VIEW PHOTOS



STATEMENT OF JUSTIFICATION

DSP-99006/01

McDonald's; Prince George's Plaza Home Depot

APPLICANT: McDonalds USA LLC
6903 Rockledge Drive
Suite 110
Bethesda, Maryland 20817

CORRESPONDENT: Daniel F. Lynch, Esq.
McNamee Hosea
6411 Ivy Lane, Suite 200
Greenbelt, Maryland 20770
(301) 441-2420 Voice
(301) 982-9450 Fax
dlynch@mhlawyers.com E-mail

REQUEST: Detailed Site Plan, Sections 27-281, 27-274 and 27-461(b)(1)(E) of the Zoning Ordinance – Eating and Drinking Establishment with Drive-Through Service

I. DESCRIPTION OF PROPERTY

1. Address – 3301 East West Highway, Hyattsville, Maryland, Maryland 20782
2. Proposed Use – Eating and Drinking Establishment with Drive-Through Service
3. Election District – 17
4. Parcel – A
5. Total Area – 13.21 acres
6. Tax Map – 41/E2
7. Location – Located on the south side of East West Highway at its intersection with Toledo Terrace.
8. Zoned – C-S-C
9. Owner – HD Development of Maryland, Inc.
10. Zoning Map – 207NE03

II. INTRODUCTION

McDonalds USA, LLC (hereinafter the "Applicant"), in accordance with Section 27-548.08 of the Zoning Ordinance, is pleased to submit a revision to Detailed Site Plan DSP-99006 for the purpose of constructing a pad site for an eating and drinking establishment with drive-through service of 4,197 square feet in the Prince George's Plaza Home Depot in Subarea 9 of the Prince George's Plaza Approved Transit Development Plan (hereinafter the "TDDP").

III. PROCEDURAL HISTORY

A Transit District Overlay Zone was first established for Prince George's Plaza in 1992. In 1998, Council Resolution re-established the Transit District Overlay Zone and adopted the TDDP. The Prince George's Plaza Home Depot was originally developed in 2000. The original Detailed Site Plan, DSP-99006 was reviewed and approved by the Planning Board in September of 1999.

IV. PROJECT SUMMARY

The purpose of this revision to DSP-99044 is to construct a 4,197 square foot eating and drinking establishment with drive-through service. The pad site will be located in the northeast corner of the property adjacent to MD 410 (East West Highway). This area of the center is currently used for parking. The TDDP for Prince George's Plaza does not permit eating and drinking establishments with drive-through service. Thus, the applicant is also requesting an amendment to the table of uses in accordance with Section 27-548.09.01 of the Zoning Ordinance.

V. CRITERIA FOR APPROVAL OF AMENDMENT OF APPROVED TRANSIT DISTRICT OVERLY ZONE

Section 27-548.09.01 of the Zoning Ordinance requires an applicant requesting an amendment in a TDOZ demonstrate that the proposed development conforms to the purposes and recommendations for the Transit District as stated in the Transit District Development Plan ("TDDP"). The purposes of the TDOZ and the Prince George's Plaza Transit District are contained in Section 27-548.03 of the Zoning Ordinance and page 9 of the TDDP. The Applicant believes that the proposed amendment to the Table of Uses for Subarea 9 complies with these purposes as follows:

- (1) **To enhance the development opportunities in the vicinity of transit stations;**

COMMENT: McDonalds is a nationally recognized restaurant chain that will provide the residents of the area with an additional dining choice. The applicant believes that this location will provide the residents of the area as well transit riders with an additional dining option within close proximity to the Metro.

- (2) **To promote the use of transit facilities;**

COMMENT: The placement of the McDonald's building along the East-West Highway frontage of the property will help further define the urban edge and create a pedestrian friendly environment that will in turn help create a stronger sense of identity for the Metro station.

(3) To increase the return on investment in a transit system and improve local tax revenues;

COMMENT: As noted above, this area of the site is currently being used for parking. The addition of another national chain restaurant to this site will increase the value of the property and in turn increase the tax revenues generated by the property.

(4) To create a process which coordinates public policy decisions, supports regional and local growth and development strategies, and creates conditions which make joint development possible;

COMMENT: The Applicant will be coordinating the development of the property with State and local agencies.

(5) To create a process which overcomes deficiencies in ordinary planning processes and removes obstacles not addressed in those processes;

COMMENT: The TDOZ allows for flexibility in the development process through the use of amendments to the TDDP. In this case, the amendment to the Table of Uses for the property as part of the Detailed Site Plan process will allow further redevelopment of Subarea 11 in a manner that is more conducive to a changing market and to the proposed development of other subareas in the transit district.

(6) To minimize the costs of extending or expanding public services and facilities, by encouraging appropriate development in the vicinity of transit stations;

COMMENT: Additional retail commercial development within close proximity to the Prince George's Plaza Metro station will encourage Metro ridership and in turn decrease the use of the surrounding road network. In addition, the proposed McDonalds will be an additional dining choice in close proximity to the Metro as well as the surrounding residential communities. The Applicant will provide connections to the existing pedestrian network and further enhance an environment that encourages pedestrian traffic through the transit district.

(7) To provide mechanisms to assist in financing public and private costs associated with development;

COMMENT: There will no public costs associated with this development. All costs will be borne by the applicant.

(8) To provide for convenient and efficient pedestrian and vehicular access to Metro stations;

COMMENT: The site is located approximately 2,000 feet from the Prince George's Plaza Metro station. As noted earlier, the Applicant will provide connections to the existing pedestrian network and further enhance an environment that encourages pedestrian traffic through the transit district.

(9) To attract an appropriate mix of land uses;

COMMENT: The Applicant's proposal will add one more dining choice to the area. As indicated above, this portion of Subarea 9 is occupied by parking spaces.

(10) To encourage uses which complement and enhance the character of the area;

COMMENT: The Detailed Site Plan and associated elevations of the building demonstrate that the proposed building will, through the use of architecture, compliment other properties in the area that have been developed in conformance with the guidelines set forth in the Transit District Development Plan.

(11) To insure that developments within the Transit District possess a desirable urban design relationship with one another, the Metro station, and adjoining areas; and

COMMENT: The Detailed Site Plan demonstrates that the proposed building complies with or exceeds the site design guidelines. Although the building located along East-West Highway does not meet the build-to line requirements it does conform to the amendment previously approved for the plaza. In addition, the applicant has oriented the building to create a greater street presence and help continue to enhance the urban edge along East West Highway.

(12) To provide flexibility in the design and layout of buildings and structures, and to promote a coordinated and integrated development scheme.

COMMENT: Amendments to the TDDP were previously approved for the subject property as part of the Detailed Site Plan for the Home Depot. The applicant is not proposing any additional amendments. In fact, by placing the building along the East-West Highway frontage, the applicant's proposal brings the site into further conformance with the goals and requirements of the TDDP.

In addition to the general purposes of the TDOZ contained in the Zoning Ordinance, the purpose of Subarea 9 is contained on page 114 of the TDDP, which states:

To provide for retail, service and office uses. A mixed-use development should be considered in the future for the property given its close proximity to the Metro station.

The Applicant's proposal will comply with the TDDP's purpose of providing additional retail space in Subarea 9. With regard to the recommendation regarding "mix use development," since the approval of the TDDP, Post Park was approved and constructed. This development reduced the amount of retail commercial development in the Transit District and increased the amount of residential and further creating a balanced mix of land uses in the Transit District.

VI CRITERIA FOR APPROVAL

The required findings for a Detailed Site Plan in the TDOZ are stated in Section 27-548.08(c) of the Zoning Ordinance. As will be demonstrated below, the Applicant complies with the criteria as follows:

1. The Transit District Site Plan is in strict conformance with any mandatory development requirements of the TDDP.

COMMENT: The Detailed Site Plan is in strict conformance with the Mandatory Development Requirements except as specifically noted below:

Pedestrian Access and Circulation

The proposed development complies with Mandatory Development Requirements S-1 through S-6. With specific regard to S3, the photometric plan submitted in conjunction with the Detailed Site Plan demonstrates that pedestrian walkways will be lighted to a minimum standard of 1.25 footcandles.

Landscaping Planting Design

The proposed development will comply with S7 and G3. The Applicant has developed a landscape plan that incorporates pedestrian safety into its design. Clear lines of site for both automobiles and pedestrians are provided so as to allow for safe travel throughout Subarea 9. As recommended by G3, this surface parking will be screened from East West Highway, Toledo Terrace, and the corresponding pedestrian network via a low wall that was previously constructed.

Streetscape

The proposed development will comply with P1 (the streetscape improvements are in place) and S8 (a Secondary Amendment was previously approved for S-8) through S13

Architecture

The proposed development will comply with S14 and G4 through G15. Specifically, and as demonstrated on the architectural elevations submitted in conjunction with this Detailed Site Plan, the building fronts East-West Highway and has been oriented so that the longest side faces East West Highway and creates a great street presence. In addition, the applicant will use high quality materials including, but not limited to, a brick masonry exterior finish. Finally, as demonstrated on the elevations, each side of the building has been given equal treatment.

Plazas

The applicant is not proposing any plazas as part of the application.

Parking and Loading

The proposed development complies with S17 through S23. Most of the parking associated with this project will be located behind the building, but there will be some spaces located to the east and west of the building, but as required by S17, no parking space will be located beyond the front build-to line.

Lighting

The proposed development will comply with S24 through S26. As indicated above, the applicant has provided a photometric plan demonstrating that the minimum level of 1.25 footcandles on pedestrian walkways has been met. The details contained in the Detailed Site Plan demonstrate compliance with S24 and S27.

Signs

The proposed development complies with P2, P3, P4 and P5. The sign plan for the property has been revised to include the applicant's signage. Thus the application complies with S28.

Bicycle Facilities

The proposed development complies with S29 and S30. Bicycle racks were installed as part of the Home Depot in accordance with S30.

Parking

The preferred parking ratio for this site permits 584 parking spaces. The approved Detailed Site Plan provided for 507 parking spaces. The addition of the McDonalds to the subject property will result in a loss of 19 parking spaces and therefore this Detailed Site Plan provides for 488 spaces, well within the preferred parking ratio.

Pedestrian Facilities

The proposed development complies with P20, in that a sidewalk was previously constructed along East-West Highway frontage.

Environment

The proposed development complies with P25 through P32 and S31 through 34. The applicant has submitted a copy of the approved Type II Tree Conservation Plan with this revision to the Detailed Site Plan.

Parks and Recreation Facilities

The applicant is not proposing residential development and therefore compliance with P34 is not required.

Subarea Requirements

As indicated, McDonalds will be located in Subarea 9 of the Prince George's Plaza TDOZ. The proposed development will comply with the Mandatory Development Requirements P92, P93, P94, P95 and P98.

2. The Transit District Site Plan is consistent with, and reflects the Site Design Guidelines and criteria contained in, the TDDP.

COMMENT: The proposed McDonalds, as shown on the Detailed Site Plan is consistent with the applicable Site Design Guidelines contained in the TDDP.

3. The Transit District Site Plan meets all the requirements of the TDOZ and applicable regulations of the underlying zone.

COMMENT: The regulations for the TDOZ are contained in Section 27-548.06 of the Zoning Ordinance. The proposed McDonalds as shown on the Detailed Site Plan, complies with these regulations.

4. The location, size and design of buildings, signs, other structures, open spaces, landscaping, pedestrian and vehicular circulation systems, and parking and loading areas maximize safety and efficiency and are adequate to meet the purposes of the TDOZ.

COMMENT: As indicated in greater detail above, the application meets the purposes of the TDOZ. The applicant is utilizing an area of the subject property which is underutilized and introducing a building along East-West Highway that will help define space and create a pedestrian friendly environment and further reduce the views of the parking associated with the Home Depot.

5. Each structure and use, in the manner proposed, is compatible with other structures and uses in the transit district and with existing and proposed development.

COMMENT: Again, the applicant is introducing another restaurant to the Transit District and thus increasing the dining options for the surrounding community as well as those working in the area. In addition, the building location is compatible with other buildings along East-West Highway and helps reduce the views of the parking associated with the Home Depot.

6. In addition to the findings above, the following is required for Detailed Site Plans:

- a. **The Planning Board shall find that the Detailed Site Plan is in general conformance with the approved Conceptual Site Plan (if one is required).**

COMMENT: The property does not have an approved Conceptual Site Plan.

VI. CONCLUSION

McDonalds has submitted a request to amendment to the Table of Uses for the TDDP for Prince George's Plaza in accordance with 27-548.09.01 of the Zoning Ordinance. In addition, McDonalds is requesting the approval of a revision to the approved Detailed Site Plan for Subarea 9 (Prince George's Plaza Home Depot). The applicant believes that these requests meet with the requirements set forth in Sections 27-548.08 and 27-548.09.01 and respectfully request their approval.

Respectfully submitted,

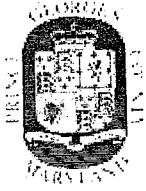
MCNAMEE & HOSEA

By: 

Daniel F. Lynch

THE PRINCE GEORGE'S COUNTY GOVERNMENT

DEPARTMENT OF PUBLIC WORKS & TRANSPORTATION
OFFICE OF ENGINEERING
9400 PEPPERCORN PLACE, SUITE 420
LARGO, MARYLAND 20774
(301) 883-5730



STORMWATER MANAGEMENT CONCEPT APPROVAL

CASE NAME: MCDONALD'S HYATTSVILLE - EAST WEST HWY
APPLICANT'S NAME: Mcdonald's Usa Llc
ENGINEER : Bohler Engineering

CASE #: 2259-2011-00

REQUIREMENTS:

Technical Review is required for PUBLIC/PRIVATE Storm Drain/SWM Construction.

Type of Storm Drainage/SWM Construction is PRIVATE.

These additional approvals are required: None.

These fees apply: REVIEW, FEE-IN-LIEU.

These bonds apply: None.

Required water quality controls: BIORETENTION.

Required water quantity controls: None.

A maintenance agreement is required.

No special conditions apply.

Required easements: None.

Storm Water Management fee payment of \$4,587.00 in lieu of providing on-site attenuation/quality control measures.
(Fee-In-Lieu subject to change during technical review.)

CONDITIONS OF APPROVAL:

1. THIS PROJECT INVOLVES REDEVELOPMENT OF AN EXISTING DEVELOPED SITE.
2. A PER MDE W/Q TREATMENT OF 100% OF NEW AND EXISTING IMPERVIOUS AREA IS REQUIRED.
3. LANDSCAPE PLANS ARE REQUIRED AT TECHNICAL REVIEW REVIEWED BY EM.

APPROVED BY:

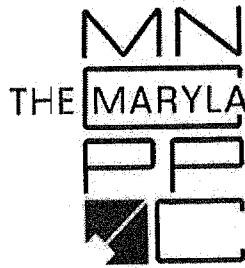
Rey De Guzman

APPROVAL DATE: April 19, 2011
EXPIRATION DATE: April 19, 2014

FOR OFFICE USE ONLY

ADC MAP: 5409-G6 200' SHEET: 207NE03
STREET NAME: EAST WEST HWY
WATERSHED: 13-Northwest Branch
NUMBER OF DU'S: 0 COST PER DWELLING: 0

CC: APPLICANT, SCD, PERMITS
P.G.C. FORM #3693 (REV 04/93)



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-3796

LETTER OF CONFORMANCE WITH
APPROVED TYPE II TREE CONSERVATION PLAN

| | |
|---|------------------------------------|
| Property Identification: 207NE03 3301 East West Highway Hyattsville, MD 20782 Home Depot Prince Georges Plaza Subdivision | Receipt #: N/A |
| | Date Issued: March 24, 2011 |
| | TCP2: 46/99 |

TO: HD Development of Maryland Inc
P.O. Box 10582
Atlanta, GA 30348-5842

VIA: Katina Shoulars, Planner Coordinator, Environmental Planning Section *KS*

FROM: Ernest J. Fields Jr., Senior Planning Technician, Environmental Planning Section *ef*

The Environmental Planning Section has reviewed the above property for conformance with the approved Type II Tree Conservation Plan (TCP2-046-99) for the Home Depot Prince Georges Plaza Subdivision.

Review of the approved Type II Tree Conservation Plan (TCP2-046-99) has determined that there are no required woodland conservation areas that are associated with the portion of the site where a proposed McDonald's restaurant will be constructed. The property owner proposes to obtain a county building permit for construction of the restaurant where no tree preservation areas from the plan are located. This letter of conformance is based on review of a Stormwater Concept Plan, prepared by Bohler Engineering, dated December 22, 2010 for Home Depot Prince Georges Plaza Subdivision. The information was obtained from a 2009 Color Imagery aerial photography and the pgtlas.com environmental layer and the M-NCPPC Log Book

A copy of this letter must be submitted with all permit application

If you have any further questions concerning this matter, please contact me at 301-952-4312 during the business day.

EF: ef

I:\Environ\Woodland Conservation\Letters and Correspondence\Letter of Conformance with TCP2\3301 East West Highway.ef.doc

RESOLUTION

WHEREAS, Kiplinger Washington Editors is the owner of a 13.21-acre parcel of land known as Home Depot (Parcel A), said property being in the 17th Election District of Prince George's County, Maryland, and being zoned C-S-C and TDOZ; and

WHEREAS, on February 12, 1999, Home Depot filed an application for approval of a Preliminary Subdivision Plat (Staff Exhibit #1) for one parcel; and

WHEREAS, the application for approval of the aforesaid Preliminary Subdivision Plat, also known as Preliminary Plat 4-99004, was presented to the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission by the staff of the Commission on May 6, 1999, for its review and action in accordance with Article 28, Section 7-116, Annotated Code of Maryland and the Regulations for the Subdivision of Land, Subtitle 24, Prince George's County Code; and

WHEREAS, the staff of The Maryland-National Capital Park and Planning Commission recommended APPROVAL of the application with conditions; and

WHEREAS, on May 6, 1999, the Prince George's County Planning Board heard testimony and received evidence submitted for the record on the aforesaid application.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to the provisions of Subtitle 24, Prince George's County Code, the Prince George's County Planning Board APPROVED the Type I Tree Conservation Plan (TCPI/5/99), approved a Variation Request from Section 24-131 of the Subdivision Regulations and further APPROVED Preliminary Plat of Subdivision 4-99004 with the following conditions:

1. Development of this subdivision shall be in compliance with the approved Type I Tree Conservation Plan (TCP I/5/99). The following note shall be placed on the Final Plat of Subdivision:

"Development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCP I/5/99), or as modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy."

2. A Type II Tree Conservation Plan shall be approved in conjunction with the Detailed Site Plan.
3. Development of this site shall be in conformance with the approved stormwater concept plan, Concept 998003250.

4. Prior to signature approval of the preliminary plat:
 - a. The Type I TCP worksheet shall be revised to reflect no clearing of woodlands in the 100-year floodplain.
 - b. A note shall be added to the preliminary plat identifying the approved stormwater concept plan number and approval date.
 - c. A note shall be added indicating that access is provided pursuant to Section 24-128(b)(9).
5. In addition to the typical review, the Detailed Site Plan shall examine:
 - a. The possible location of a site for a Medical Evacuation Unit.
 - b. The alignment of the trail connection.
 - c. Road improvements required by the State Highway Administration.
 - d. Stormwater management outfalls onto park property.
6. At the time of Detailed Site Plan, the applicant shall demonstrate conformance to the requirements and guidelines for the subject subarea as defined in the Transit District Development Plan (TDDP) for the Prince George=s Plaza Transit District Overlay Zone, as well as general conformance to all other regulations in the same document. If the proposed parking in addition to already approved total parking exceeds that which is allowed by the TDDP, the applicant shall submit new studies as required by staff in order to determine any additional facilities needed to support the planned development.

BE IT FURTHER RESOLVED, that the findings and reasons for the decision of the Prince George's County Planning Board are as follows:

1. The subdivision, as modified, meets the legal requirements of Subtitles 24 and 27 of the Prince George's County Code and of Article 28, Annotated Code of Maryland.
2. The property is located on the south side of East West Highway (MD 410), just west of its intersection with Toledo Terrace.
3. An area of 100-year floodplain has been found to occur at the northwest corner of this property. Mandatory Development Requirement P3 in the *Prince George=s Plaza TDOZ* states

If the development is part of the subdivision process, then an approval of a variation request to the subdivision shall be obtained for proposed impacts to the floodplain.®

Although the TCP as revised indicates that 0.04 acres of floodplain impacts will occur, the Limit of Disturbance shown on the plan is located at the edge of the floodplain. Therefore a variation to address floodplain impacts will not be required.

Although no streams or nontidal wetlands have been shown on the Preliminary Plan a copy of the Jurisdictional Determination as made by the US Army Corps of Engineers has identified an area at the southwest corner of the property as Waters of the US. The applicant proposes to pipe this water course. The applicant submitted a variation request, dated April 5, 1999, that addresses the proposed impacts to this stream and makes the findings as required by Section 24-113 of the Subdivision Ordinance. The variation request to allow disturbances to the stream and stream buffer located at the southwest corner of this property is approved based on the following findings set forth in Section 24-113 of the Subdivision Regulations:

- A. **That the granting of the variation will not be detrimental to the public safety, health or welfare, or injurious to other property.** The impacted water course is a man-made channel; it is not within the 100-year floodplain. A stormwater management concept plan has been approved and conveyance of stormwater will be accomplished in conformance with that approval. Therefore, filling this man-made water course will not be detrimental to public health, safety and welfare.
- B. **The conditions of which the variation is based are unique to the property for which the variation is sought and are not applicable generally to other properties.** The property was used for the deposition of fill material during construction of the metro line. This fill created a semi-circular hill, in some places 20 feet high. This hill must be stabilized during development of the site. This hill, in part, created the man made channel. The use of the property as a fill site for metro construction is a unique situation not generally applicable to other properties.
- C. **The variation does not constitute a violation of any other applicable law, ordinance, or regulation.** Approval of this variation request will not result in a violation of other applicable laws, ordinances or regulations. The piping of the water course and stormwater control must be accomplished in accordance with the approved stormwater concept plan.
- D. **Because of the particular physical surroundings, shape, or topographical conditions of the specific property involved, a particular hardship to the owner would result, as distinguished from a mere inconvenience, if the strict**

letter of these regulations is carried out. The applicant would suffer a hardship if the strict letter of the regulations is followed since the existence of the fill and water course would substantially limit the viable development of the property. According to the applicant, A[the substantial amount of fill located on the property raised the finished elevation of the property substantially, but was not deposited uniformly along the subject property.@ Because of this, a variation is necessary to allow development of the site.

The limits of the 65 dBA noise contour have been shown on the Preliminary Plan of Subdivision and found to accurately reflect the limits of the noise impact zone. Mandatory Development Requirement P1 requires all applications to include the 65 dBA noise contour. The submittal satisfies this Mandatory Requirement. A Noise Study will not be required as part of this development plan since the use as a retail facility will not be adversely impacted by the 65 dBA noise contour as identified on the Preliminary Plat of Subdivision.

The property is in Sewer and Water service categories S-3 and W-3. There are no Scenic or Historic Roads located on or adjacent to this property.

Soils on this property include Hatboro silt loam and Sunnyside-Urban land complex. According to the Prince George=s County Soil Survey the limitations associated with these soils include impeded drainage, a high water table and flood hazard for the Hatboro silt loam. It should also be noted that this soil is identified as a hydric soil and may be an indicator of wetlands. No Marlboro Clays have been found to occur on or near this site.

A Forest Stand Delineation has been submitted and found to be acceptable. Since this site is subject to the requirements of the Prince George=s County Woodland Conservation Ordinance it is not subject to TDOZ Woodland Conservation requirement S1 which requires afforestation for projects not otherwise subject to the Woodland Conservation Ordinance. A Type I Tree Conservation Plan, TCPI/5/99, has been reviewed and found to generally satisfy the requirements of the Prince George=s County Woodland Conservation Ordinance. The requirements for this site include a Woodland Conservation Threshold of 15% or 1.98 acres and replacement requirements of 1.96 acres for a total requirement of 3.94 acres. Those requirements are being satisfied by 0.09 acres of on-site preservation, 0.22 acres of on-site reforestation and 3.57 acres of off-site preservation. To use off-site preservation to satisfy the requirements for this site the applicant will need to provide 7.14 acres of woodland. The applicant is currently proposing to use land in the Town of University Park as off-site preservation. TCPI/5/99 is approved in conjunction with Preliminary Plat of Subdivision 4-99004 subject to the conditions included in this report.

No other significant environmental constrains have been identified for this property.

4. This subdivision is proposed within Subarea 9 of the Prince George=s Plaza Transit District. The Transit District Development Plan (TDDP) for the Transit District Overlay Zone provides the following required findings for subdivisions within the transit district.
 - a. The Preliminary Plat of Subdivision must be in conformance with all aspects of the TDDP.
 - b. The Preliminary Plat of Subdivision is in general conformance with the approved Conceptual Site Plan (if one is required).
 - c. The development will be adequately served within a reasonable period of time with existing or programmed public facilities either shown in the appropriate Capital Improvement Program or provided as part of the private development.@

With regard to the first required finding, compliance with most of the district-wide and subarea-specific Mandatory Development Requirements applicable to this project will be determined at Detailed Site Plan. However, the approval of this subdivision must not preclude conformance with the following Mandatory Development Requirements applicable to Subarea 9:

- AP7 A trail connection to the hiker-biker trail in the Northwest Branch Stream Valley Park shall be provided.@
- AS1 All Conceptual and Detailed Site Plans shall be submitted to the Fire Department for review and to evaluate whether a medivac landing area location is warranted.@

While not specifically a subdivision issue, staff referred the application to the Fire Department. The Fire Department responded as follows:

AFire/EMS Department would like to have site tentatively allocated for Medical Evaluation (sic) Unit along this TDOZ.@

The applicant should be aware that the Fire Department is looking seriously at this site as a location for a medivac landing area. In compliance with Mandatory Requirement S1 of the *Prince George=s Plaza TDDP*, this issue will be further reviewed at the Detailed Site Plan stage.

With regard to the second finding, no Conceptual Site Plan is required for this property. Adequacy of public facilities (third finding) involves payment of a fee for parking spaces. Adequacy issues and the fee are discussed in the Transportation section of this report.

5. Since the site is commercially zoned, it is exempt from the mandatory dedication requirements of section 24-134 of the Subdivision Regulations. However, the site abuts

the Northwest Branch Stream Valley Park. Two issues present themselves. First, a trail connection, discussed below, will be required. Second, the preliminary plat shows a stormwater outfall onto park property. This is inconsistent with the approved stormwater concept plan. The applicant should be aware that no stormwater will be allowed to encroach park property without prior written consent of the Department of Parks and Recreation.

6. The *Prince George=s Plaza TDDP* recommends that an eight-foot wide asphalt trail connection be provided to link the existing hiker-biker trail in the Northwest Branch Stream Valley Park to East West Highway. The TDDP recommend this trail run from the northeast corner of the property, along the southern portion of the property, to the southwestern corner of the property to meet up with the existing trail. This may prove problematic, given the site=s development constraints. However the trail connection should still be required. The exact location of this trail shall be determined at the Detailed Site Plan stage.
7. The Transportation Planning Division has reviewed the subdivision application. The subject property consists of approximately 13.21 acres of land in the C-S-C zone. The property is located at the southwest quadrant of the MD 410 and Toledo Terrace intersection. The applicant proposes to develop the property as a commercial development in accordance with the regulations of the *Prince George=s Plaza Transit District Overlay Zone (TDOZ)*.

No traffic study was requested of, or was prepared by the applicant. The findings and recommendations outlined below are based upon a review of the Approved *Transit District Development Plan (TDDP) for the Prince George=s Plaza TDOZ*. The TDDP guides the use and development of all properties within its boundaries, and the staff must determine the degree to which the submitted plan conforms to that document.

The TDDP identifies the subject property as Subarea 9 of the TDOZ; there are a total of 15 subareas in the TDOZ (two of which will remain undeveloped). During the preparation of the TDDP, the transportation staff performed an analysis of all road facilities in the vicinity of the TDOZ. This analysis was based on establishment of a transit district-wide cap on the number of additional parking spaces (preferred and premium), that can be constructed or provided in the transit district to accommodate any new development. Pursuant to this concept, the Plan recommends implementing a system of developer contributions to insure adequacy of the transportation facilities, based on the number of additional parking spaces, as long as the authorized total parking limits and their attendant, respective, parking ratios (Tables 5 and 6 of the TDDP) are not exceeded. The collected fee will be applied toward the required number of transportation improvements totaling \$1,562,000, as summarized in Table 4 of the TDDP. These improvements are needed to ensure that the critical roadways and intersections in the transit district will remain adequate and will be operating at or above Level of Service E. Therefore, since the accumulated sum of all approved parking spaces including the total additional parking

proposed by this application does not exceed the parking caps identified in Table 6 of the Prince George's Plaza TDDP, no additional traffic study is required. In addition, the subject property is required to pay a proportional fair share at the time of Detailed Site Plan approval. Provided the applicant pays the required pro-rata share contribution, which will be determined at the time of Detailed Site Plan, adequate transportation facilities for the proposed subdivision in accordance with Sec. 24-124(a) and Section 24-124(a)(5) will exist.

A final determination of conformance with the TDDP is to be made at the time of Detailed Site Plan. For the subject property, the Detailed Site Plan should also address the adequacy of the Vehicular and pedestrian access and on site circulation, and the operation adequacy of the intersection of MD410 with Toledo Terrace.

Dedication of right-of-way along MD 410, which is a planned arterial facility, and Toledo Terrace, which is a planned industrial roadway with 70 foot right-of-way, is acceptable as shown on the submitted plan.

Based on the preceding findings, adequate transportation facilities would exist to serve the proposed subdivision as required under Section 24-124 of the Prince George's County Code if the application is approved the transportation condition included in this report.

Additionally, the State Highway Administration requires several road improvements. These should be further examined at the time of Detailed Site Plan.

8. The property is exempt from the requirements of Section 24-122.02 of the Subdivision Regulations for adequacy of public schools because it is commercially zoned.
9. The Countywide Planning Section has reviewed the subdivision plans for adequacy of public facilities and concluded that existing fire and rescue services are adequate to serve the proposed subdivision. Fire engine and ambulance service will be provided by the Hyattsville Fire Station, Company 1, located at 6200 Belcrest Road. Paramedic service is provided by Brentwood Fire Station, Company 4. Ladder truck service is provided by Riverdale Fire Station, Company 7.
 - a. The existing fire engine service at Hyattsville Fire Station, Company 1, has a service response time of 1.09 minutes, which is within the 3.25 minutes response time guideline.
 - b. The existing ambulance service at Hyattsville Fire Station, Company 1, has a service response time of 1.09 minutes, which is within the 4.25 minutes response time guideline.

- c. The existing paramedic service at Brentwood Fire Station, Company 4, located at 3712 Utah Avenue has a service response time of 4.70 minutes, which is within the 7.25 minutes response time guideline.
- d. The existing ladder truck service at Riverdale Fire Station, Company 7, located at 4714 Queensbury Road has a service response time of 3.22 minutes, which is within the 4.25 minutes response time guideline.

The proposed subdivision will be within the adequate coverage area of the nearest existing fire/rescue facility for engine, ambulance, ladder truck and medic services. These findings are in conformance with the *Adopted and Approved Public Safety Master Plan 1990* and the *Guidelines for the Analysis of Development Impact on Fire and Rescue Facilities*.

- 10. The proposed development is within the service area of the District I-Hyattsville. In accordance with Section 24-122.1 (c) (1) (A) and (B) of the Subdivision Regulations of Prince George's County, the staff concludes that the existing County police facilities will be adequate to serve the proposed Home Depot development. This police facility will adequately serve the proposed subdivision.
- 11. The Prince George=s County Health Department has reviewed the application and offers no comment.
- 12. The Department of Environmental Resources (DER), Development Services Division, has determined that on-site stormwater management is required. A Stormwater Management Concept Plan, # 998003250, has been approved with conditions to ensure that development of this site does not result in on-site or downstream flooding. Development must be in accordance with this approved plan.
- 13. The site has frontage on East West Highway, but direct access to that road is not desirable. Section 24-128(b)(9) of the Prince George=s County Subdivision Regulations allows alternative access by an easement on an adjoining lot when direct access to a road is considered unsafe. In this case, additional access to East West Highway is unsafe; therefore, the use of the existing access by the easement on the adjoining property is appropriate. Prior to signature approval, the preliminary plat needs to be revised to include a note indicating that access is provided pursuant to Section 24-128(b)(9).

BE IT FURTHER RESOLVED, that an appeal of the Planning Board=s action must be filed with Circuit Court for Prince George=s County, Maryland within thirty (30) days following the adoption of this Resolution.

* * * * *

This is to certify that the foregoing is a true and correct copy of the action taken by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the

PGCPB No. 99-78
File No. 4-99004
Page 9

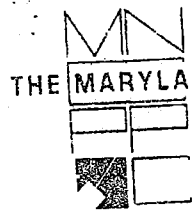
motion of Commissioner McNeill, seconded by Commissioner Dabney, with Commissioners McNeill, Dabney, Boone, Brown and Hewlett voting in favor of the motion, at its regular meeting held on Thursday, May 6, 1999, in Upper Marlboro, Maryland.

Adopted by the Prince George's County Planning Board this 10th day of June 1999.

Trudye Morgan Johnson
Executive Director

By Frances J. Guertin
Planning Board Administrator

TMJ:FJG:JPD:meg



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-3796

PGCPB No. 99-146

File No. SP-99006

RESOLUTION

WHEREAS, the Prince George's County Planning Board is charged with the approval of Detailed Site Plans pursuant to Part 3, Division 9 of the Zoning Ordinance of the Prince George's County Code; and

WHEREAS, in consideration of evidence presented at a public hearing on September 9, 1999, regarding Detailed Site Plan SP-99006 for Home Depot-Hyattsville, the Planning Board finds:

1. The Detailed Site Plan is for a Home Depot building supply store consisting of 117,975 square feet with a 16,309-square-foot garden center on Parcel 9 of the Prince George's Plaza Transit District Overlay Zone. The site consists of 13.21 acres in the C-S-C Zone and is located at the southwest quadrant of the intersection of East West Highway, MD 410 and Toledo Terrace. To the east of the site is the existing Kiplinger's printing facility; to the west is Heurich Community Park, owned and maintained by The Maryland-National Capital Park and Planning Commission; to the south are the existing Metro rail lines; and to the north is MD 410.

Section 27-548.08.(c) Required findings for a Detailed Site Plan in the Transit District Overlay Zone (TDOZ).

The findings required by Section 27-285(b) shall not apply to the T-D-O Zone. Instead, the following findings shall be made by the Planning Board when approving a Detailed Site Plan in the T-D-O Zone:

2. The Transit District Site Plan is in strict conformance with any Mandatory Development Requirements of the Transit District Development Plan;

The Detailed Site Plan is not in strict conformance with the following Mandatory Development Requirements:

Urban Design (p. 28)

- S1 All proposed development/redevelopment shall have a primary pedestrian walkway system that coincides with the street system and provides a connection directly to the Metro. In addition, the secondary and tertiary pedestrian systems shall provide inner block connections through parks, plazas and green areas and have efficient pedestrian circulation.

The applicant has requested a Secondary Amendment to the above requirement. See Finding 7 below.

- S3 All primary and secondary walkways shall be well lighted to a minimum of 1.25 foot candles.**

A proposed trail that connects an existing trail within Heurich Park to the East West Highway streetscape is not lit. The applicant has requested a Secondary Amendment. See Finding 7 below.

- S5 All primary and secondary pedestrian routes shall be constructed using special paving materials. (See Figure 7 for crosswalks.)**

The sidewalk connecting the store to the streetscape along East West Highway should be revised to provide special paving.

Streetscape (p.30)

- S8 All property frontages shall be improved in accordance with Figures 7, 8 and 9 in order to create a visually continuous and unified streetscape.**

The plan should be revised to provide a note that states that street trees will be limbed up to a minimum of six feet. Also, the landscape schedule should be revised to indicate that street trees are required to be 3 1/2 to 4" caliper.

The applicant has requested a Secondary Amendment and the streetscape 130 feet short of the western boundary. See Finding 7 below.

- S13 All major pedestrian crossings, such as crossings through more than three lanes of traffic, shall have a contrasting pavement material. Crossings are to conform to all Road Code standards and conform with Figure 7.**

In accordance with the above requirement, a crosswalk should be provided across the travel lanes of the main entrance road off of East West Highway.

Architecture (p.31)

- S14 Building materials shall be high quality, enduring and distinctive. Exterior building materials such as pre-cast concrete, brick, tile and stone are encouraged. (Also, S 59 for Parcel 9)**

The materials board submitted by the applicant indicates that the building materials proposed for the Home Depot will consist of split-faced block, synthetic stucco (EIFS) material and a standing-seam metal roof. The colors of the building are beige and brown with orange accents, typical of other Home Depot stores. Decorative masonry accent

diamonds, squares and striping are provided regularly along the front elevation for detailing. Metal roofing is orange in color. The garden center is enclosed with decorative masonry piers and green vinyl-coated chain-link fence, 20 feet in height. The applicant has indicated that they are willing to provide a more decorative fencing material around the garden center, similar to that provided at the Bowie Home Depot. This information should be provided prior to certification of the Detailed Site Plan.

Parking and Loading (p.38)

- S17 All parking lots shall, in general, be located behind buildings, and shall not occupy more than 33 percent of the frontage of any subarea along a pedestrian street.**

The parking lot is located in front of the Home Depot building and occupies approximately 70 percent of the frontage of the subarea along East West Highway. The applicant has requested a Secondary Amendment. See Finding 7 below.

- S18 All parking lots shall not extend beyond the "build-to-line" or project beyond the front plane of adjoining buildings.**

A build-to-line is not required for the Home Depot, however, the parking lot projects beyond the front plane of the existing Kiplinger's building to the east. The Kiplinger building is set back substantially from East West Highway with a parking lot in front of their building. It is for this reason that the applicant cannot comply with the above requirement. The applicant has requested a Secondary Amendment. See Finding 7 below.

- S19 All large-surface parking lots (lots with more than 100 spaces) shall be segmented into smaller units, using methods of continuous internal green in conformance with the *Landscape Manual*, Section 4.3(c)(5).**

The applicant had requested a Secondary Amendment for the above requirement. Subsequent to that request, the applicant has revised the plan to be in conformance with the requirement. Therefore, a Secondary Amendment is not needed.

- S23 All surface parking lots shall be screened from view of roadways by the use of both a low, opaque wall and an evergreen hedge (See Figure 7), unless they are providing short-term parking for ten cars or fewer. (Also S58 for Parcel 9.)**

For the most part, this requirement has been met. The applicant has requested a Secondary Amendment for a small parking compound adjacent to the garden center. See Finding 7.

Lighting (p.39)

- S24 All lighting poles, fixture designs, light retention and level of illumination shall be coordinated throughout the transit district to achieve a recognizable design, and be consistent with the streetscape construction drawings provided in Appendix A.

The light fixtures and poles selected by the applicant are not consistent with the streetscape construction drawings provided in Appendix A. Prior to certification of the Detailed Site Plan, the applicant should provide light fixtures and poles that are more attractive in design and are consistent with the drawing provided in Appendix A.

Signage (p.40)

- P2 All development/redevelopment shall have a sign plan approved by the Planning Board at the time of Detailed Site Plan. This plan shall provide the sign location(s), size, color, lettering style, construction details and materials specifications including the method of illumination.

A sign plan has been provided by the applicant which consists of various building-mounted signs and one 25-foot-high pylon sign at the corner of East West Highway and Toledo Terrace. The Landscape Plan provides for no landscaping at the base of the pylon sign. Attractive landscaping should be provided at the base of the pylon sign prior to certification of the Detailed Site Plan. The sign plan satisfies all of the above required specifications and will be entered into the record at the Planning Board hearing.

Bicycle Facilities (p.41)

- S30 All new retail development shall provide four bike racks per 10,000 gross square feet of floor space with each rack holding a minimum of two bikes.

A total of 54 bike racks, with each holding a minimum of two bikes, are required for 134,284 square feet of retail space. The applicant proposes to provide 12 bike racks. The applicant has requested a Secondary Amendment. See Finding 7 below.

Stormwater Management (p.68)

- S32 Prior to final inspection and sign-off of permits by the Sediment/Stormwater or Building Inspector, any storm drain inlets associated with the development and all inlets on the subject subarea shall be stenciled with "Do Not Dump. Chesapeake Bay Drainage." The Detailed Site Plan and the Sediment Control Plan (in the sequence of construction) shall contain this information.

The site plan should be revised to contain this information.

3. **The Transit District Site Plan is consistent with, and reflects the guidelines and criteria contained in the Transit District Development Plan;**

The Transit District Site Plan will be consistent with, and reflect the guidelines and criteria contained in, the Transit District Development Plan when the conditions of approval below are met.

4. **The Transit District Site Plan meets all of the requirements of the Transit District Overlay Zone and applicable regulations of the underlying zones;**

The development data is as follows:

| Zone | C-S-C |
|--|-------------|
| Total Site Area | 13.21 acres |
| Gross Floor Area, Home Depot | 117,975 sf. |
| Gross Floor Area, Garden Center | 16,309 sf. |
| Total | 134,284 sf. |
| Parking Permitted (Preferred Ratio of <4.35 sp/1000sf.): | 584 spaces |
| Parking Provided: | 507 spaces |
| Handicapped Spaces Required: | 7 spaces |
| Van Spaces Required: | 2 space |
| Handicapped Spaces Provided: | 10 spaces |
| Van Spaces Provided: | 2 spaces |
| Loading Spaces Required: | 3 spaces |
| Loading Spaces Provided: | 4 spaces |

5. **The location, size and design of buildings, signs, other structures, open spaces, landscaping, pedestrian and vehicular circulation systems, and parking and loading**

areas maximize safety and efficiency and are adequate to meet the purposes of the Transit District Overlay Zone;

The layout of the development minimizes conflicts between pedestrian and vehicular circulation systems, provides for coordinated architectural building style and materials, provides adequate open space areas for landscaping to screen undesirable views, provides for safe and efficient parking and loading areas, and is adequate to meet the purposes of the TDOZ.

6. **Each structure and use, in the manner proposed, is compatible with other structures in the Transit District and with existing and proposed adjacent development.**

The proposed building is architecturally coordinated in terms of building materials and style and is situated on the site in a manner which is compatible with the surrounding existing and proposed development.

7. The applicant has filed Secondary Amendments for the following District-Wide and Subarea Requirements:

Pedestrian Access and Circulation (p.28)

- S1 All proposed development/redevelopment shall have a primary pedestrian walkway system that coincides with the street system and provides a connection directly to the Metro. In addition, the secondary and tertiary pedestrian systems shall provide inner block connections through parks, plazas and green areas and have efficient pedestrian circulation.

The applicant contends that "Full compliance with Mandatory Development Requirement S1 would require that the applicant construct the streetscape improvements along East West Highway to the westernmost boundary of the property." The applicant has provided a primary pedestrian walkway system that does coincide with the street system. The above requirement does not stipulate that the walkway system must extend along the entire frontage of the property. Therefore, staff is of the opinion that a Secondary Amendment is not necessary for the above requirement.

- S3 All primary and secondary walkways shall be well lighted to a minimum of 1.25 foot candles.

A proposed trail connection from East West Highway to an existing trail within Heurich Park will not be lit. The applicant contends that to provide lighting from East West Highway to the existing trail connection "would encourage pedestrians to leave the sidewalk within the streetscape improvements and into the trail network. The trail network is not lit in the park property, nor are the ball fields or the parking lots of the

park property lit. The applicant does not want to encourage pedestrians to walk behind the building if there is no method to safely negotiate the trail beyond the subject property." Staff agrees with the applicant that to provide lighting for the new portion of the trail when the rest of the park itself is not lit raises safety concerns for potential pedestrians who might be tempted to walk into the park from the streetscape at night. Therefore, staff supports the requested Secondary Amendment for S3.

Streetscape (p. 30)

- S8 All property frontages shall be improved in accordance with figures 7 and 8 in order to create a visually continuous and unified streetscape.**

The applicant is responsible for streetscape improvements along the entire frontage of the property. The applicant requests to modify the streetscape along East West Highway to end the sidewalk approximately 130 feet short of the western property line. The sidewalk would then blend into the trail system which runs through Heurich Community Park. The applicant has agreed to continue the street trees and lighting to the end of the property. There are no other TDOZ properties beyond Subarea 9 to the west. An existing bridge on East West Highway that crosses over the Northwest Branch does not have adequate space for a sidewalk. To carry the streetscape sidewalk to the end of the property would encourage pedestrians to continue to walk along East West Highway where there would be no alternative to cross the bridge, other than to walk in the street itself. Connecting the sidewalk into the trail system offers pedestrians a safe method of getting to and from the residential area on the other side of the Northwest Branch until such time as the bridge is improved to accommodate pedestrian traffic. Therefore, staff supports the requested Secondary Amendment to blend the sidewalk into the trail system, 130 feet short of the western property line.

Parking and Loading (p.38)

- S17 All parking lots shall, in general, be located behind buildings, and shall not occupy more than 33 percent of the frontage of any subarea along a pedestrian street.**

The subject property is triangular in shape, with the widest part of the site along the western property line. Because of this, the building must be located closer to the western portion of the site with parking in front of the building. As a result, more than 33 percent of the frontage along East West Highway is occupied with parking. Staff supports the requested Secondary Amendment to allow greater than 33 percent of the frontage to be occupied by parking.

- S18 All parking lots shall not extend beyond the "build-to-line" or project beyond the front plane of adjoining buildings.**

There are no requirements for a build-to line for retail in Subarea 9. The proposed parking lot does extend beyond the front plane of the adjoining Kiplinger Press Building. That building sets back from the right-of-way approximately 75 feet, with parking located between the street and the building. The bulk of the parking lot for the Home Depot is behind the front plane of the Kiplinger building. Only a small portion of the parking lot projects beyond the front plane of the building. Because of the existing status of the Kiplinger building and associated parking, and because of the unique shape of Subarea 9, requiring the building to be located along the western property line with a triangular-shaped parking compound in front of the building, staff supports the requested Secondary Amendment.

- S19** All large-surface parking lots (lots with more than 100 spaces) shall be segmented into smaller units, using methods of continuous internal green in conformance with the *Landscape Manual*, Section 4.3(c)(5).

The applicant had requested a Secondary Amendment for the above requirement. Subsequent to that request, the applicant has revised the plan to be in conformance with the requirement. Therefore, a Secondary Amendment is not needed.

- S23** All surface parking lots shall be screened from view of roadways by the use of both a low, opaque wall and an evergreen hedge (See Figure 7), unless they are providing short-term parking for ten cars or fewer. (Also S58 for Parcel 9.)

For the most part, this requirement has been met. A brick wall with an evergreen hedge has been provided between the main parking compound and East West Highway. The applicant's request is for a small parking compound located on the north side of the Garden Center. The parking compound will sit approximately six to eight feet above the grade of East West Highway. The compound angles away from the street so that the closest space is 40 feet from the curb, and the farthest space is 95 feet from the curb of East West Highway. The applicant intends to provide an evergreen hedge between the parking compound and the street, but not the low brick wall. Evergreen trees have also been provided in the space between the parking compound and the street. Staff believes that the combined effect of the grade difference, the angle of the parking compound, the evergreen hedge and the evergreen trees will effectively screen the parking compound from the street. Therefore, staff supports the requested Secondary Amendment.

Bicycle Facilities (p. 41)

- S30** All new retail development shall provide four bike racks per 10,000 gross square feet of floor space with each rack holding a minimum of two bikes.

Based on the requirement of four bike racks per 10,000 gross square feet of floor space, the applicant is required to provide 54 bike racks, with each rack holding two bikes. The applicant is requesting relief from the requirement and is proposing to provide 12 bike racks. Given the nature of the type of retail use, staff supports the requested Secondary Amendment.

Section 27-213.06(c)(3)(B), Required Findings for Secondary Amendment of Transit District Development Plan:

- (i) **The requested Secondary Amendment is in compliance with the requirements for the approved Transit District Development Plan as set forth in Section 27-548.08 (c).**

The requested Secondary Amendments are in compliance with the requirement of the TDDP in that the Detailed Site Plan meets all of the applicable regulations of the underlying zone; the building, parking, loading and pedestrian facilities are designed to maximize safety and efficiency; and each structure and use, in the manner proposed, is compatible with other structures and uses in the Transit District, and with the existing and proposed adjacent development.

- (ii) **The requested Secondary Amendment is in conformance with the purposes of the Transit District Overlay Zone.**

The proposed Secondary Amendments are in conformance with the applicable purposes of the TDOZ. Specifically, the amendments to S3 for lighting, S8 for improvements along the property frontage, S17 for parking occupying more than 33 percent the property frontage, S18 for parking extending beyond the front plane of adjoining buildings, S23 for screening of parking, and S30 for the number of bicycles required are necessary to accomplish an efficient design for the proposed development and will therefore provide the necessary flexibility in the design and layout of the buildings and structures to promote a coordinated and integrated development scheme.

- (iii) **The original intent of the Transit District Development Plan element or mandatory requirement being amended is still fulfilled with the approval of the Secondary Amendment.**

Given the nature of the retail use, the unique shape of the site that dictates the building location and associated parking, loading and vehicular and pedestrian circulation, the requested amendments are justified and fulfill the original intent of the Transit District Development Plan.

8. In general, the Detailed Site Plan is in conformance with all applicable sections of the *Landscape Manual*, except for Section 4.7 along the western property line. The follow-

ing application for Alternative Compliance has been recommended for approval by the Planning Director:

Alternative Compliance is requested for Section 4.7, Buffering Incompatible Uses, along the western property line, for Bufferyard #2 and #3.

BACKGROUND:

The subject site is located on the east side of the Northwest Branch Stream Valley Park and south of East West Highway. The applicant is proposing the development of a Home Depot building supply store with a garden center on the subject site. Alternative Compliance is requested in order to reduce the minimum number of plant units required in Bufferyard #2 and #3. A type "C" bufferyard is required.

REQUIRED: Bufferyard #2

Length of bufferyard: 456 linear feet
Building setback: 40 feet
Landscape yard: 30 feet
Plant units:

478 plant units (reduction of 13% for existing woodland from 549 total plant units required.)

PROVIDED:

Building setback: 126 feet
Landscape yard: 30 feet
Plant units:

267 plant units provided in the bufferyard and 347 plant units outside the bufferyard.

JUSTIFICATION OF RECOMMENDATION:

Bufferyard #2 is located along the western property line. A portion of a required stormwater management pond encroaches into the required bufferyard. The applicant was only able to provide 267 plant units within the bufferyard, and the remaining 211 plant units have been provided outside and directly adjacent to the required bufferyard. The applicant has provided 136 more plant units as compensation. The Alternative Compliance Committee believes the applicant's proposal is equal to or better than normal compliance to the requirements of the *Landscape Manual*.

REQUIRED: Bufferyard #3

| | |
|-----------------------|---|
| Length of Bufferyard: | 297 linear feet |
| Building setback: | 40 feet |
| Landscape yard: | 30 feet |
| Plant units: | 344 plant units (reduction of 4% for existing woodlands from 358 total plant units required.) |

PROVIDED:

| | |
|-------------------|--|
| Building setback: | 95 feet |
| Landscape yard: | 30 feet |
| Plant units: | 209 plant units provided in the bufferyard and 143 plant units outside the bufferyard. |

JUSTIFICATION OF RECOMMENDATION:

Bufferyard #3 is located along the western property line. A portion of a required stormwater management pond encroaches into the required bufferyard. The applicant is unable to provide the required plant units in the bufferyard, but is providing the balance of the required plant units directly outside the bufferyard. The applicant has provided eight (8) more plant units as compensation. The Alternative Compliance Committee believes the applicant's proposal is equal to or better than normal compliance to the requirements of the *Landscape Manual*.

RECOMMENDATION:

The Planning Director recommends approval of alternative compliance from Section 4.7 along the western property line for Bufferyard #2 and #3.

Referrals

9. The Countywide Planning Section reviewed the above-referenced plan according to the requirements of the adopted *Prince George's Plaza Transit District Development Plan*. The TDDP requires that all site plans be submitted to the Police Department and Fire Department for their review and evaluation. The staff of the Countywide Planning Section discussed this site plan with police personnel and they have no comments. The Fire Department indicated that the site is adequate for a Medivac landing area. The applicant has provided a Medivac landing area on the site in a location that is suitable to the Fire Department. However, the Fire Department has requested that all parking spaces that have direct access to the travel lane that is also the main access for the medivac landing area be relocated to prevent accidents between emergency vehicles and cars backing into the travel lane.

10. The Natural Resources Division reviewed the Detailed Site Plan (DSP) and recommends the following findings:

Floodplains and wetlands have not been found to occur on this property. An area at the southwest corner of the property has been identified as Waters of the U.S., a stream. This stream will be impacted by the proposed development and was identified as such during the review of the Preliminary Plat of Subdivision. Prior to the issuance on any grading permits which impact this stream or the associated buffer the applicant shall submit to the Natural Resources Division a copy of the appropriate Federal and/or State permits.

There are no Marlboro Clays on this property and there are no significant limitations associated with the soils found on this site. There are no Scenic or Historic Roads located on or adjacent to this property and the site is in Sewer and Water Service categories S-3 and W-3.

This property, which is located on the south side of East West Highway, is located within the noise corridor associated with that roadway. The 65 dBA noise contour has been shown on the Detailed Site Plan as required by the approved TDOZ plan. A Noise Study will not be required for this property since no residential dwellings will be impacted by the proposed development.

A Forest Stand Delineation and Type I Tree Conservation Plan (TCPI/5/99) were reviewed and approved in conjunction with Preliminary Plat of Subdivision 4-99004. The Type II Tree Conservation Plan (TCPII/46/99) for this 13.21-acre property has been reviewed and found to satisfy the requirements of the Prince George's County Woodland Conservation Ordinance. This property has a 15 percent, or 1.98-acre, Woodland Conservation Threshold and a replacement requirement of 1.96 acres for a total requirement of 3.94 acres. This requirement is being satisfied by 0.09 acre of on-site preservation, 0.28 acre of on-site afforestation/reforestation and 3.57 acres of off-site reforestation and forest enhancement in a priority afforestation area. The off-site mitigation is being accomplished along a stream in the Town of University Park (TCPII/85/99). TCPII/46/99 is recommended for approval in conjunction with SP-99006 subject to Condition 1.b. below.

11. The Transportation Planning Division has reviewed the submitted Detailed Site Plan and the requested supplemental traffic operation analysis for the site's main access. The applicant proposes to develop the proposed site as a commercial development in accordance with the regulations of the Prince George's Plaza Transit District Overlay Zone (TDOZ).

The approved Prince George's Plaza Transit District Development Plan (TDDP) guides the use and development of all properties within its boundaries. The findings and

recommendations outlined below are based upon staff evaluation of the submitted Detailed Site Plan and to determine the degree to which it conforms to the TDDP.

During the preparation of the TDDP, staff performed an analysis of all road facilities in the vicinity of the TDOZ. This analysis was based on establishment of a transit district-wide cap on the number of additional parking spaces (preferred and premium) that can be constructed or provided in the transit district to accommodate any new development. Pursuant to this concept, the plan recommends implementing a system of developer contributions to ensure adequacy of the transportation facilities, based on the number of additional parking spaces, as long as the authorized total parking limits and their attendant, respective, parking ratios (Tables 5 and 6 of the TDDP) are not exceeded. The collected fee will be applied toward the required number of transportation improvements totaling \$1,562,000, as summarized in Table 4 of the TDDP. These improvements are needed to ensure that the critical roadways and intersections in the transit district will remain adequate and will be operating at or above Level-of-Service E, as required by the plan. Since the accumulated sum of all approved parking spaces including the total parking proposed by this application does not exceed the recommended parking cap, the review of the submitted detailed site plan will be limited to the determination of the required pro rata fee, the operational adequacy of site's access points, and level of compliance with all applicable transportation-related requirements and design guidelines.

Detailed Site Plan Findings and Comments

The TDDP identifies the subject property as Subarea 9 of the TDOZ. There are a total of 15 subareas in the TDOZ, two of which are designated as open-space and will remain undeveloped. The proposed site consists of approximately 13.21 acres of land in the C-S-C Zone. The property is located at the southwest quadrant of the MD 410 and Toledo Terrace intersection. The proposed application is for a Home Depot Store.

The submitted Detailed Site Plan includes details regarding access, circulation patterns and surface parking layout as well as the layout and location of pedestrian facilities. Pursuant to the adopted and approved TDDP, all new retail development are required to provide four bike racks for each 10,000 gross square feet of floor space with each rack holding a minimum of two bikes. For the proposed 134,248 square feet of retail development, a total of 53 bicycle racks with a minimum capacity for 106 bicycles are needed. The applicant has prepared and submitted a statement of justification in support of several Secondary Amendments to the TDDP requirements, which includes a request for significant reduction in the number of required bicycle storage spaces. It should be noted that for Parcel 6 with a similar retail use with minimum bicycle trip demand, the Planning Board approved a request for 50 percent reduction in the number of required bicycle storage spaces.

The on-site vehicular circulation patterns are acceptable. The submitted site plan shows sidewalks along the site's main access roadway, Editors Park Drive. In lieu of the recommended trail proposed to the south of the subject property, a proposed trail connection along the stream valley on west side of the property to East West Highway and provision of an eight-foot-wide pedestrian walkway along entire frontage of East West Highway would not have significant adverse effects. In addition, a direct pedestrian connection to the suggested eight-foot-wide walkway along East West Highway and as close as possible to the front of the building would improve pedestrian circulation and safety to and from the site. At least two main crosswalks are needed between the parking area and the building entrance and exit locations. Crosswalks need to be provided along all approaches of the East West Highway and Toledo Terrace intersection.

The submitted traffic operation analysis for the intersection of East West Highway and Toledo Terrace suggests that the extension of Toledo Terrace or Kiplinger access drive be widened to four lanes (two outbound and two inbound lanes). The study also recommends widening the turning radii for right-turning vehicles to and from Kiplinger Access Drive to 30 feet. Staff also concurs with the traffic operation analysis findings that the existing 130-foot-long left-turn lane along MD 410 westbound is adequate to accommodate the existing and projected traffic. Assuming a 120-second cycle length and 50-foot-long average vehicle length, for the projected 73 left-turn vehicles the required storage length is 124 feet.

Pursuant to the adopted and approved 1998 PG-TDDP, a cash contribution based on the number of additional new parking spaces proposed to be provided by the applicant is required. This fee is to recover the funds needed for required improvements to the existing and planned transportation facilities. The recommended maximum "Preferred and Premium" total parking caps and parking ratios for each land use type is identified in Tables 5 and 6 of the PG-TDDP. The recommended maximum fees for each parking space allocated under the "Preferred" and "Premium" are \$400.00 and \$800.00, respectively. These fees are expressed in 1998 dollars. For the proposed 134,248 square feet of retail development, the site plan indicates a total of 507 parking spaces, including the 12 handicapped spaces. Since the resulting surface parking ratio for the proposed development of 3.87 spaces per 1,000 square feet of development is less than the recommended maximum preferred ratio of 4.35, the total amount of the applicant's cash contribution will be calculated as \$400.00 X the number of new parking spaces. Based on a detailed parking inventory conducted by the Transportation Planning Division, 186 parking spaces, and not 190 spaces as suggested in the submitted plan, are considered as replacement of previously existed parking spaces. Therefore, the proposed application, if approved as submitted, will reduce the available parking for retail by a total of 321 spaces, and the new preferred parking caps for retail uses and for the Transit District will be set as 529 and 2,426 spaces, respectively.

The 1998 PG-TDDP also authorized the Prince George's Plaza Transportation Demand Management District (TDMD), which requires that each property owner in the district to be a member and participate in the TDMD. The annual TDMD membership fee is \$5.00 for each surface parking space. The annual TDMD membership fee for parking spaces in structures and surface spaces that are permanently reserved for handicapped occupant vehicles, carpools and vanpools are set at a rate of \$2.00 per space. As it is required by the TDDP, the Transportation Planning Division will begin the implementation of the TDMD and its requirements as outlined in the County Zoning Ordinance once the total allocation under the approved preferred parking cap has been reached.

Transportation Staff Conclusions

Based on the preceding findings, the Transportation Planning Division concludes that the proposed Detailed Site Plan as submitted is in conformance and consistent with all applicable Transit District's Mandatory Development Requirements and Site Design Guidelines if Conditions 2 and 3 below are included as a part of any approval of the subject Detailed Site Plan.

12. The Park Planning and Development Division of the Department of Parks and Recreation has reviewed the Detailed Site Plan and recommends approval subject to conditions 4, 5 and 6 below.
13. The Detailed Site Plan is in conformance with the Preliminary Plat of Subdivision, 4-99004.
14. The City of Hyattsville and University Park were sent referrals. It is staff's understanding that both municipalities support the application, but neither has sent written comments.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to Subtitle 27 of the Prince George's County Code, the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission adopted the findings contained herein and APPROVED the Type II Tree Conservation Plan (TCP/46/99), APPROVED Detailed Site Plan SP-99006 for the above-described land, and further approved Alternative Compliance AC-99029, subject to the following conditions:

1. Prior to certification, the Detailed Site Plan shall be revised as follows:
 - a. The sidewalk connecting the store to the streetscape along East West Highway shall be revised to provide special paving.
 - b. Note 17 on the TCP detail sheet shall be corrected to reflect the fee for a violation as \$1.50 per square foot of woodland disturbance, not \$1.29 per square foot.

The TCP shall also be revised to indicate that all plant materials used for the afforestation/reforestation areas shall be no less than one (1) inch caliper.

- c. The plan shall be revised to provide a note that states that street trees will be limbed up to a minimum of six feet.
 - d. The landscape schedule shall be revised to indicate that street trees are required to be 3½ to 4" caliper.
 - e. A crosswalk, in accordance with Figure 7, shall be provided across the travel lanes of the main entrance road off of East West Highway.
 - f. The architectural elevations for the Garden Center shall be revised to provide a more decorative fencing material around the garden center, similar to that provided at the Bowie Home Depot.
 - g. The applicant shall provide light fixtures and poles that are more attractive in design and are consistent with the drawing provided in Appendix A.
 - h. Attractive landscaping shall be provided at the base of the pylon sign at the entrance to the site.
 - i. A note shall be provided stating that any stormdrain inlets associated with the development and all inlets on the subject subarea shall be stenciled with "Do Not Dump, Chesapeake Bay Drainage."
 - j. All parking spaces that have direct access to the travel lane that is also the main access for the medivac landing area, shall be relocated to prevent accidents between emergency vehicles and cars backing into the travel lane.
2. Prior to Certification of the Detailed Site Plan, the applicant, his heirs, successors, and/or assigns shall provide payment of \$128,000 calculated as \$400.00 per parking space X 321 proposed new preferred parking spaces to the Prince George's County Department of Public Works and Transportation (DPW&T) and proof of payment to the Transportation Planning Division of the Prince George's County Planning Department. The required fee is expressed in 1998 dollars and shall be adjusted for inflation at the time of payment, using the latest Federal Highway Administration Construction Cost Index. The collected fee shall be applied toward the construction of the required transportation improvements listed in Table 4 of the 1998 PG-TDDP.
 3. Prior to issuance of any building permit, the following improvements shall be in place, fully bonded or permitted for construction:

- a. Reconstruct Kiplinger Access Drive as a 4-lane divided access roadway from its intersection with MD 410 to the proposed first site access driveway, and as a 4-lane undivided roadway per Prince George's County DPW&T standards to the site's second access point.
 - b. Provision of 30 feet turning radii for the right-turning traffic to and from the Kiplinger Access Drive.
4. The location of the proposed hiker/biker trail on Park Property shall be staked in the field, and approved by Park Planning and Development staff prior to construction.
 5. Prior to the issuance of a building permit, a Recreational Facilities Agreement shall be executed for the trail, and a performance bond posted for the construction of the trail, and to warrant the restoration, repair or improvements for the stormdrain system on park property.
 6. The Liber and Folio of the RFA shall be placed on the Final Plat of subdivision.
 7. Prior to the issuance on any grading permits which impact the stream or the associated buffer in the southwest corner of the site, the applicant shall submit to the Natural Resources Division a copy of the appropriate Federal and/or State permits.

BE IT FURTHER RESOLVED, that an appeal of the Planning Board's action must be filed with the District Council of Prince George's County within thirty (30) days following the adoption of this Resolution.

* * * * *

This is to certify that the foregoing is a true and correct copy of the action taken by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the motion of Commissioner McNeill, seconded by Commissioner Boone, with Commissioners McNeill, Boone, Hewlett and Brown voting in favor of the motion, at its regular meeting held on Thursday, September 9, 1999, in Upper Marlboro, Maryland.

Adopted by the Prince George's County Planning Board this 9th day of September 1999.

APPROVED AS TO LEGAL SUFFICIENCY

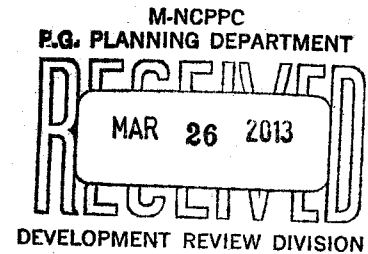
George Johnson
M-NCPPC Legal Department

Date 9/14/99

Trudve Morgan Johnson
Executive Director

By *Frances J. Guertin*
Frances J. Guertin
Planning Board Administrator

TMJ:FJG:GAW:meg



Prince George's County Planning Department
Community Planning Division

301-952-4225
www.mncppc.org

March 22, 2013

MEMORANDUM

TO: Jill Kosack, Senior Planner, Urban Design Section, Development Review Division

VIA: Cynthia Fenton, Acting Planning Supervisor, Community Planning Division *crf*

FROM: Roberto Duke, Planner Coordinator, Community Planning Division

SUBJECT: **DSP-99006-01 McDonald's-Hyattsville**

DETERMINATIONS

- This application is consistent with the 2002 General Plan Development Pattern policies for centers in the Developed Tier.
- This application conforms to the land use recommendations of the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone*.
- The proposed addition of drive-through service is contrary to the goals of the Prince George's Plaza Transit District.

BACKGROUND

Location: The site is located on the south side of East West Highway immediately west of the intersection of East West Highway and Toledo Terrace.

Size: 13.21 acres

Existing Uses: Home Depot and associated parking lot

Proposal: The applicant seeks to develop a McDonald's restaurant along the northeast corner of the Home Depot property and requests an amendment to the use table of the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* to permit the proposed use.

GENERAL PLAN, MASTER PLAN AND SMA

- 2002 General Plan:** This application is located in the Developed Tier and is in a designated Center (Prince George's Plaza).
- The vision for the Developed Tier is a network of sustainable, transit-supporting, mixed-use, pedestrian-oriented, medium- to high-density neighborhoods.
- The vision for Centers is mixed residential and nonresidential uses at moderate to high densities and intensities, with a strong emphasis on transit-oriented development.
- Master Plan:** 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone*
- Planning Area/Community:** Planning Area 68/Hyattsville
- Land Use:** The 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* does not identify future land uses for the subject property within Subarea 9. However, the purpose statement provided on page 114 reads: "To provide a retail, service and office uses. A mixed-use development should be considered in the future for this property given its close proximity to the Metro Station." The land uses are determined by uses permitted in the underlying zone.
- Environmental:** Refer to the Environmental Planning Section referral for comments on the environmental element of the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* and the 2005 *Countywide Green Infrastructure Plan*.
- Historic Resources:** None identified
- Transportation:** The property fronts East West Highway which is identified in the 2009 *Approved Countywide Master Plan of Transportation* as a four- to six-lane Arterial roadway (A-15) with a 100 to 120 foot right-of-way between New Hampshire Avenue and Pennsy Drive.
- Public Facilities:** The subject property is located approximately 500 feet from Pathways Schools Norwood at Hyattsville and a little less than a quarter mile from the Nicholas Orem Middle School.
- Parks & Trails:** A future bike lane is identified by the 2009 *Approved Countywide Master Plan of Transportation* along East West Highway. The subject property is also located a little less than a quarter mile from the Heurich Community Park.

SMA/Zoning: The 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* retained the subject property in the C-S-C Zone.

PLANNING COMMENTS

The application is consistent with the development pattern policies of the 2002 *Prince George's County Approved General Plan* for regional centers in the Developed Tier. The proposed restaurant will contribute to the development of a mixed residential and nonresidential center proximate to the Prince George's Plaza Metro station.

Although the 1998 *Approved Transit District Development Plan for the Prince George's Plaza Transit District Overlay Zone* does not specifically identify the intended future land uses for parcels within the plan area, it links the allowed/desired uses to the zone and provides a purpose statement found on page 114 of the plan that calls for retail, service and office uses with consideration of mixed-used development in the future. In this case, the zoning of the property is C-S-C, and the permitted uses are generally in conformance with that zone, emphasizing commercial retail and office uses.

The transit district development plan does not permit eating and drinking establishments with drive-through service in the C-S-C Zone. The applicant must request an amendment to the development to allow the requested use which will require the approval of the District Council.

While the applicant should be commended for proposing an innovative drive-through configuration that strives to preserve the primary pedestrian network along East West Highway, the drive-through component by its very nature is contrary to the goals for the Prince George's Plaza Transit District to create a pedestrian-friendly environment along East West Highway (Page 14). The transit district development plan emphasizes the importance of pedestrian access and circulation (see page 28) and identifies the public rights-of way as the location of the primary pedestrian system and focus of pedestrian connectivity. The parking area and drive-through for the proposed McDonald's have been designed to be less suburban in character and more pedestrian-friendly and walkable. The proposed restaurant is sited with the longer side of the building close to the East West Highway right-of way. The parking lot, the drive-through, and the associated circulation for the two functions are located behind the building. The main entrance will connect with an existing six-foot sidewalk adjacent to East West Highway. An eight-foot high screen wall and landscaped plantings will conceal the loading area from public view along the street. The facades of the building will be articulated with brick, cultured stone, and metal roof panels which will provide visual interest along the public right-of-way. Should the amendment be approved, the relationship between the drive-through queuing area and parking drive aisles should be clarified by the applicant to ensure that there are no issues with vehicular stacking and egress during peak operating hours. Additionally, the portion of the East West Highway elevation near the service doors should have greater façade articulation to help break up the blank wall areas.

Although the planned configuration of the McDonalds is less suburban and promotes a more pedestrian-friendly environment, staff finds any potential amendment to the development plan to permit drive-through service would be detrimental to the transit district goals and desired development pattern. However, should this application be amended or conditioned to preclude the drive-through component the proposed restaurant building would help reinforce the street-wall and pedestrian environment and would contribute positively to the transit district.

DSP 99006-01 McDonald's-Hyattsville

March 22, 2013

Page 2

c: Ivy Lewis, Division Chief, Community Planning Division
Long-range Agenda Notebook

J:\Referrals-DRD\DSP-99006-01 Revised_rduke.doc



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION


Prince George's County Planning Department
Countywide Planning Division, Transportation Planning Section

(301) 952-3680
www.mncppc.org

April 4, 2013

MEMORANDUM

TO: Jill Kosack, Urban Design Section, Development Review Division

FROM:  Tom Masog, Transportation Planning Section, Countywide Planning Division

SUBJECT: DSP-99006/01, McDonald's (Prince George's Plaza)

The Transportation Planning Section has reviewed the site plan noted above. The overall subject site consists of approximately 13.21 acres of land in the C-S-C Zone within the area of the *Approved Transit District Development Plan (TDDP) for the Prince George's Plaza Transit District Overlay Zone (TDOZ)*. The property is located in the southwest quadrant of the MD 410 and Toledo Terrace. It is located in Subarea 9 of the TDDP. The applicant proposes to construct a 4,197 square foot eating and drinking establishment (fast-food restaurant) with drive-through service on 1.68 acres of land.

Review Comments: Use Within TDOZ

In September 1999 PGCPB No.99-146 approved a detailed site plan (DSP-99006) for a retail store of 117,975 square feet in Parcel 9 of the TDOZ. The proposed restaurant would be located adjacent to and east of the existing retail building.

The enactment of CB-56-2011 and the resulting amendment of Sec.27-107.01 of the Zoning Ordinance by the District Council essentially combined fast-food and drive-in restaurants with the more general category termed eating or drinking establishments. The use table in the TDDP, however, distinguishes between a fast-food restaurant and other types of eating and drinking establishments, and greatly limits the circumstances in which a fast-food restaurant can be located. In general, the fast-food restaurant, following the guidance in the TDDP use table, can only be located within another building or located as an accessory to a recreation facility. Given that the TDDP is intended to provide a set of development regulations unique from the general provisions of Subtitle 27, it is unclear how CB-56-2011 affects the interpretation of the use table in the TDDP.

Given that fast-food or drive-in restaurants are no longer defined or recognized separately in Subtitle 27, it is believed that enforcing a distinction between these and other eating and drinking establishments within the TDDP would be difficult. In reviewing the TDDP, that document attempted to limit fast-food restaurants by placing them in locations where the provision of drive-through service would be difficult if not impossible. Therefore, while the transportation staff is inclined to support the placement of a fast-food restaurant on the subject site, it is believed that the inclusion of drive-through service is inconsistent with the TDDP. This contention is based on the goals of the transit district, including the creation of a pedestrian-friendly environment. It is also based on the general goals of the Transit District Overlay (T-D-O) Zone given in Section 27-548.03. Furthermore, the "Transportation Review Guidelines, Part 1" document includes a checklist to assist in the determination of excellent or successful transit-oriented development (TOD). The checklist, which is based on review of similar checklists and sets of regulations around the United States, indicates that lack (or non-provision) of drive-through facilities is a positive characteristic.

The existing retail store consisting of 117,975 square feet with a 16,309 square-foot garden center on Parcel 9 was approved with 507 parking spaces on 13.21 acres of land. The TDOZ required a maximum parking of 4.35 spaces per 1,000 square feet or 18 parking spaces for the proposed use. The applicant is providing 64 parking spaces, an excess of 46 parking spaces. Combined, there will be net loss of 19 parking spaces proposed by this site plan leaving 488 parking spaces remaining within Parcel 9. Given that parking spaces will be eliminated under this site plan, the site is not subject to payment of an impact fee under either preferred or the premium parking caps, as defined in the TDDP.

Site Plan Review Comments

One of the required findings for a Detailed Site Plan within the TDOZ is that the buildings, pedestrian and vehicular circulation systems and parking/loading areas maximize safety and are adequate to meet the purposes of the TDOZ. The site plan shows the fast food restaurant adjacent to MD 410. The proposed drive-through service is located directly south and adjacent to the building. This creates a conflict between vehicles and pedestrians seeking to enter the building. Virtually all of the restaurant's patrons must cross the drive-through service lane in order to enter the proposed fast food restaurant, including the handicapped patrons using the designated parking spaces.

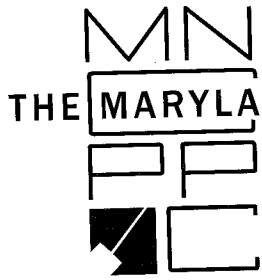
The configuration of the drive-through service lane and the potential queuing creates issues with persons seeking to enter or exit the parking on the site. Even the queuing of two or three cars at the entrance to the drive-through service lane, where the menu board is located and where orders are taken, would block much of the site's parking. This area requires additional signage even if drive-through service is maintained in order to lessen the likelihood of queued vehicles blocking parking.

These operational issues provide another justification for the elimination of the drive-through service. Given the site constraints, the plan would be much more functional for all users, including delivery vehicles, if drive-through service were eliminated on the plan.

Beyond these issues, the site plan is acceptable. Access is acceptable. It shall be noted that the Maryland State Highway Administration (SHA) has requested that a traffic study accompany the site plan. Aside from the fact that issues of off-site transportation adequacy are generally not reviewed at the time of detailed site plan, the specific requirements of this TDDP greatly reduce the need for adequacy review at any stage of development provided that the development proposal conforms to the TDDP. For these reasons, no traffic study has been requested or reviewed. However, by this statement the applicant shall be made aware that SHA has requested a study, and that agency may compel the provision of a study by the applicant in the event that any permits from SHA are needed to implement this use.

Recommendation

In summary, the Transportation Planning Section finds that the subject application does generally conform to the TDDP and other prior approved plans. It is determined that while the use is acceptable at this location, the provision of the drive-through service (a) poses issues with the TDDP and the general goals of the T-D-O Zone, (b) is not consistent with excellent TOD, (c) creates conflicts with patrons entering the building, and (d) results in queuing issues affecting vehicles entering and leaving, and should be eliminated from the plan.



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-4366
www.mncppc.org/pgco

March 29, 2013

MEMORANDUM

TO: Jill Kosack, Urban Design
VIA: Whitney Chellis, Subdivision Section *WC*
FROM: Quynn Nguyen, Subdivision Section *QN*
SUBJECT: Referral for McDonald's-Hyattsville, DSP-99006-01

The subject property is known as Parcel A, located on Tax Map 41 in Grid E-2, within the C-S-C Zone and is 13.21 acres. The site is currently improved with a 117,782-square-foot Home Depot store built in 2000. The applicant has submitted a revised detailed site plan for the construction of 4,197-square-foot McDonald's restaurant at the eastern portion of the existing parking lot of the Home Depot.

The site is the subject of the approved Preliminary Plan of Subdivision 4-99004 and the resolution was adopted by the Prince George's County Planning Board on June 10, 1999 (PGCPB Resolution No. 99-78). The resolution of approval (PGCPB Resolution No. 99-78) contains eleven conditions. The following conditions in bold relate to the review of this application:

1. **Development of this subdivision shall be in compliance with the approved Type I Tree Conservation Plan (TCP I/5/99). The following note shall be placed on the Final Plat of Subdivision:**

"Development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCP I/5/99), or as modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy."

Conformance of this DSP to the approved Type I Tree Conservation Plan (TCPI/5/99) should be reviewed and determined by Environmental Planning Section. The above note is placed as Note 1 on the record plat VJ 188-33 for the subject site.

2. **A Type II Tree Conservation Plan shall be approved in conjunction with the Detailed Site Plan.**

Conformance of this DSP to the approved Type II Tree Conservation Plan should be reviewed and determined by Environmental Planning Section.

3. **Development of this site shall be in conformance with the approved stormwater concept plan, Concept 998003250.**

The General Note 12 on the DSP states that the property has an approved stormwater management concept plan 2259-2011-00. The approved and valid date of the stormwater management plan should be added to the General Note 12.

4. **Prior to signature approval of the preliminary plat:**

- a. **The Type I TCP worksheet shall be revised to reflect no clearing of woodlands in the 100-year floodplain.**
- b. **A note shall be added to the preliminary plat identifying the approved stormwater concept plan number and approval date.**
- c. **A note shall be added indicating that access is provided pursuant to Section 24-128(b)(9).**

The preliminary plan has been signature approved and the Parcel A was record in Plat Book VJ 188-33 on November 19, 1999.

5. **In addition to the typical review, the Detailed Site Plan shall examine:**

- a. **The possible location of a site for a Medical Evacuation Unit.**
- b. **The alignment of the trail connection.**
- c. **Road improvements required by the State Highway Administration.**
- d. **Stormwater management outfalls onto park property.**

6. **At the time of Detailed Site Plan, the applicant shall demonstrate conformance to the requirements and guidelines for the subject subarea as defined in the Transit District Development Plan (TDDP) for the Prince Georges Plaza Transit District Overlay Zone, as well as general conformance to all other regulations in the same document. If the proposed parking in addition to already approved total parking exceeds that which is allowed by the TDDP, the applicant shall submit new studies as required by staff in order to determine any additional facilities needed to support the planned development.**

Conformance to Condition 5 and 6 should be reviewed and determined by Urban Design Section.

Parcel A was recorded in Plat Book VJ 188-33 on November 19, 1999. The record plat contains 5 notes and the following notes in bold relate to the review of this application:

4. **This plat is subject to a recreation facilities agreement recorded in Liber 13466 at Folio 335.**

The recorded recreation facilities agreement indicates that the developer is to construct an 8-foot-wide asphalt trail and stone veneer on headwall. The construction of the recreation facilities are to be completed prior to issuance of the certificate of occupancy. The Home Depot store was built in 2000 and is currently occupied. Completion of the recreation facilities agreement should be determined by Department of Park and Recreation and Permit Section.

5. **This plat is subject to an access easement, in accordance with Section 24-128(b)(9) of the Subdivision Regulations, Prince George's code, recorded in Liber 13408 at Folio 634.**

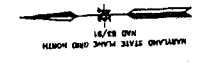
The DSP is not proposing any access on to East West Highway, MD Route 410. The proposed McDonald's restaurant will be using the existing access location of the Home Depot onto the access easement. No new access locations are being proposed with this DSP. Applicant should provide evidence that the existing access easement is acceptable for additional traffic from the proposed McDonald's restaurant.

The bearings, distances, and PUE on the site plan are consistent with the record plat. However, the DSP has some inconsistencies that need to be address. The DSP should be revised with the following technical corrections:

1. Show and label the entire acreage for Parcel A
2. Clearly show and label the access easement Liber 13408 Folio 634 as reflected on the record plat VJ 188-33.
3. Clearly show and label the PEPCO easement Liber 6909 Folio 325 as reflected on the record plat VJ 188-33.

The DSP-99006-01 is in substantial conformance with the approved Preliminary Plan 4-99004 and record plat if the above comments have been addressed. Failure of the site plan and record plat to match will result in the grading and building permits being placed on hold until the plans are corrected. There are no other subdivision issues at this time.

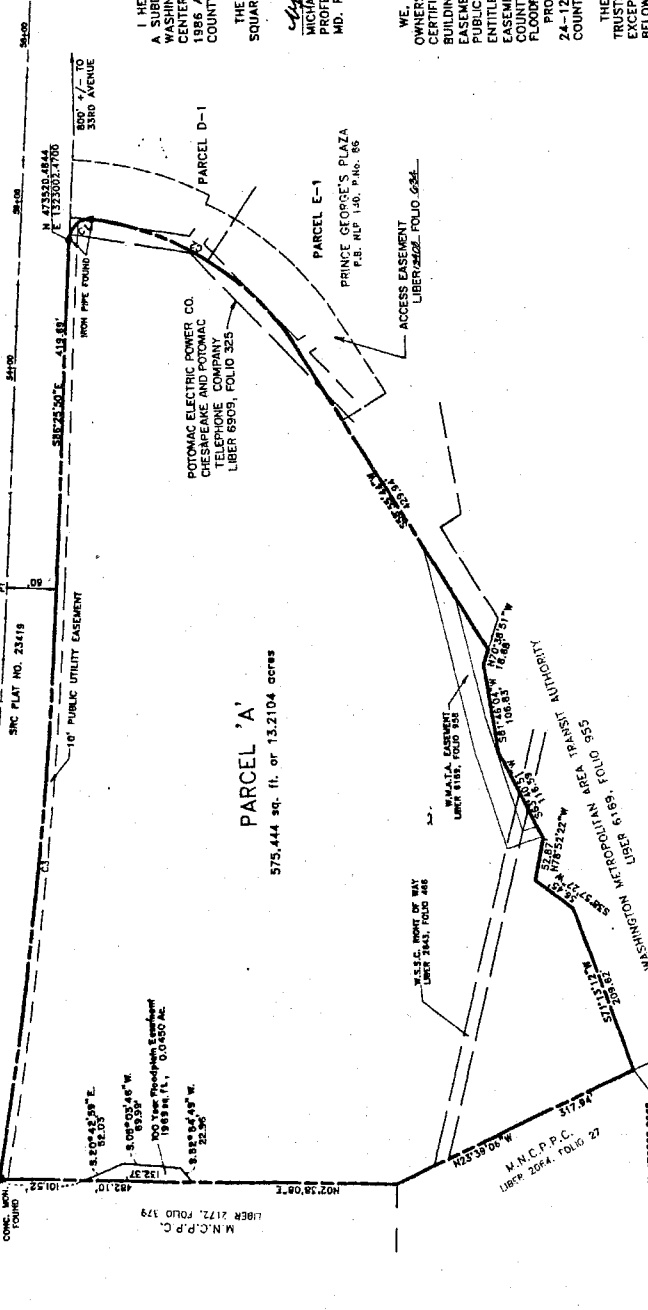
VF 188-33



| CURVE | RADIUS | LENGTH | CHORD | BEARING | DELTA |
|-------|---------|--------|--------|---------|---------|
| C1 | 25.00 | 4.029 | 2.733 | 85.41 | 31.589 |
| C2 | 34.367 | 7.149 | 4.613 | 83.229 | 50.513 |
| C3 | 37.0258 | 7.4489 | 4.8730 | 82.5558 | 52.0400 |

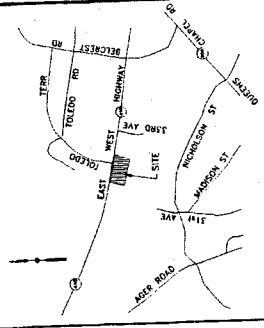
CON'TE COMPANY
L-4529 F. 955

EAST-WEST HIGHWAY (MD RTE 410)
(120' W/M)



PARCEL 'A'
575,444 sq. ft. or 13.2104 acres

VICINITY MAP
SCALE 1"=2000'



SURVEYOR'S CERTIFICATE

I HEREBY CERTIFY THAT THE PLAN SHOWN HEREON IS CORRECT; THAT IT IS A SUBDIVISION OF ALL OF THE RECORDS ACQUIRED BY THE KPLINGER WASHINGTON EDITORS, INC. OF DELAWARE CORPORATION, FROM PRINCE GEORGE COUNTY, MARYLAND, BY DEED DATED DECEMBER 18, 1983, AND RECORDED AMONG THE LAND RECORDS OF PRINCE GEORGE'S COUNTY, MARYLAND IN LIBER 5507 AT FOLIO 982.

THE TOTAL AREA INCLUDED IN THIS PLAN OF SUBDIVISION IS 575,444 SQUARE FEET OR 13.2104 ACRES OF LAND.

Michael B. DeLoe
MICHAEL B. DELOE
PROFESSIONAL LAND SURVEYOR
MD. REG. NO. 11033

DATE
9/14/99

OWNER'S DEDICATION

WE, THE KPLINGER WASHINGTON EDITORS, INC., A DELAWARE CORPORATION, OWNERS OF THE LAND SHOWN HEREON AND DESCRIBED IN THE SURVEYOR'S CERTIFICATE, HEREBY ADOPT THIS PLAN OF SUBDIVISION, ESTABLISH THE MINIMUM BUILDING RESTRICTION LINES AND ESTABLISH TEN (10) FEET WIDE PUBLIC UTILITY EASEMENTS (P.U.E.) ADJACENT, PARALLEL, CONCENTRIC AND CONTIGUOUS TO ALL PUBLIC STREETS SHOWN HEREON SUBJECT TO THE TERMS OF A DOCUMENT ENTITLED "DECLARATION OF TERMS AND PROVISIONS FOR PUBLIC UTILITIES EASEMENTS" RECORDED AMONG THE LAND RECORDS OF PRINCE GEORGE'S COUNTY, MARYLAND IN LIBER 3703 AT FOLIO 748. AND ESTABLISH THE 100-YEAR FLOODPLAIN EASEMENT AS SHOWN HEREON.

THERE ARE NO SUITS, ACTIONS AT LAW, LEASES, MORTGAGES OR TRUSTS AFFECTING THE PROPERTY INCLUDED IN THIS PLAN OF SUBDIVISION EXCEPT A CERTAIN MORTGAGE, AND ALL PARTIES IN INTEREST HERETO HAVE BELOW INDICATED THEIR ASSENT.

BY: *Stephen B. Roedel*
STEPHEN J. BRODRICK
VICE PRESIDENT / REAL ESTATE

WITNESS: *Douglas J. Celley*
DOUGLAS J. CELLEY
VICE PRESIDENT / REAL ESTATE

WE HEREBY ASSENT TO THIS PLAN OF SUBDIVISION

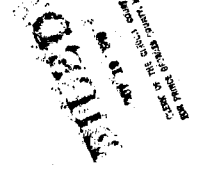
ROBERT M. CRAMP, JR.
TRUSTEE

PARCEL A
HOME DEPOT PRINCE GEORGE'S PLAZA
CHILLUM (17TH) ELECTION DISTRICT
PRINCE GEORGE'S COUNTY, MARYLAND
SCALE: 1"=100' DATE: JULY, 1999

GPI
GREENMAN-PEDERSEN, INC.
14522 GREENWAY DRIVE, SUITE 100, LABEL, MD. 20706
WASH., DC (301) 476-2772 FAX: (301) 476-3005
www.gpi.com

NOTES:

- DEVELOPMENT IS SUBJECT TO RESTRICTIONS SHOWN ON THE APPROVED TYPE I TREE CONSERVATION PLAN (TOP 1/5/99), OR AS MODIFIED BY THE TYPE II TREE CONSERVATION PLAN, AND PRECLUDES ANY DISTURBANCE OR INSTALLATION OF STRUCTURE WITHIN SPECIFIC AREAS. FAILURE TO COMPLY WILL MEAN A VIOLATION OF AN APPROVED TREE CONSERVATION PLAN AND WILL MAKE THE OWNER SUBJECT TO MITIGATION UNDER THE WOODLAND CONSERVATION/TREE PRESERVATION POLICY.
- DEVELOPMENT OF THIS SITE SHALL BE IN CONFORMANCE WITH THE APPROVED STORM WATER MANAGEMENT CONCEPT PLAN NO. 990603250, AS APPROVED OR AS SUBSEQUENTLY AMENDED.
- DEVELOPMENT OF THIS PROPERTY MUST CONFORM TO THE SITE PLAN WHICH WAS APPROVED BY THE PRINCE GEORGE'S COUNTY PLANNING BOARD ON 9/9/99, 91-39006, OR AS AMENDED BY ANY SUBSEQUENT REVISIONS THERETO.
- THIS PLAN IS SUBJECT TO A RECREATION FACILITIES AGREEMENT RECORDED IN LIBER 29462 AT FOLIO 302.
- THIS PLAN IS SUBJECT TO AN ACCESS EASEMENT, IN ACCORDANCE WITH SECTION 24-128(b)(9) OF THE SUBDIVISION REGULATIONS, PRINCE GEORGE'S CODE, RECORDED IN LIBER 28423 AT FOLIO 254.



APPROVAL OF THIS PLAN IS PREDICATED UPON PUBLIC WATER AND SEWER BEING AVAILABLE PRIOR TO CONSTRUCTION.
DEPARTMENT OF ENVIRONMENTAL RESOURCES
PRINCE GEORGE'S COUNTY, MARYLAND
APPROVED ON 9-27-99
Henry Lawrence
DIRECTOR OF ENVIRONMENTAL RESOURCES

FOR PUBLIC WATER AND SEWER ONLY
THE MARIAND NATIONAL CAPITAL PARK AND PLANNING COMMISSION
PRINCE GEORGE'S COUNTY PLANNING BOARD
APPROVED: October 14, 1999
Stromas S. Suetterlin
ASSISTANT SECRETARY

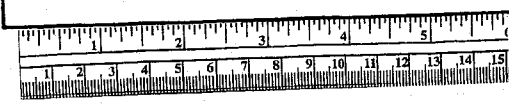
PRELIMINARY PLAN No. 4-99004 207 NE 3

RECORDED: 11-19-99
PLAT BOOK: VI-188
PLAT NO. 33

MDCPPC RECORD FILE NO. 5-99137

PRINCE GEORGE'S COUNTY PLANNING BOARD
CHAIRMAN: *James C. 238 (F-308)*
SECRETARY: *James C. 238 (F-308)*
SOURCE: SL 928E

10.7.99





THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

Prince George's County Planning Department
Countywide Planning Division, Transportation Planning Section

(301) 952-3680
www.mncppc.org

MEMORANDUM

March 7, 2013

TO: Jill Kosack, Urban Design Section, Development Review Division
VIA: *DM* Tom Masog, Transportation Planning Section, Countywide Planning Division
FROM: *DJ* Dan Janousek, Transportation Planning Section, Countywide Planning Division
SUBJECT: DSP-99006-01 McDonald's - MD 410 Hyattsville



Type of Master Plan Bikeway or Trail

| | | | |
|-------------------|---------|---------------------------|---------|
| Municipal R.O.W.* | _____ | Public Use Trail Easement | _____ |
| PG Co. R.O.W.* | _____ | Nature Trails | _____ |
| SHA R.O.W.* | X _____ | M-NCPPC Parks | _____ |
| HOA | _____ | Bicycle Parking | X _____ |
| Sidewalks | X _____ | Trail Access | _____ |

*If a Master Plan Trail is within a city, county, or state right-of-way, an additional two to four feet of dedication may be required to accommodate construction of a trail.

Review Comments

This analysis is limited to the review of a Transit District Site Plan for the provision of basic, safe and adequate pedestrian and bicycle facilities, and adequate access to transit services via the transportation circulation system.

The property is situated in the approved parking area of the existing Home Depot on University Boulevard (MD 410). To the west is the Northwest Branch Stream Valley Trail. To the east, within walking distance, is the Prince George's Plaza Metro Station. The site is close to the Nicholas Orem Middle School and a new elementary school. The applicant's property is contained in the "Prince George's Plaza District" as described in the TDDP. It is within "Subarea 9" and subject to the Mandatory Development Requirements described in the TDDP. It is further contained within the "West Hyattsville and Prince George's Plaza Transit District Overlay Zones" (CR-32-1997). The applicant's proposal is in strict conformance with the mandatory development requirements of the TDDP as described within the *Prince George's Plaza Approved Transit District Development Plan (TDDP) for the Transit District Overlay* (June 1998). This TDDP is a regulating tool for the properties in the West Hyattsville and Prince George's Plaza Transit District Overlay Zones (CR-32-1997).

The proposal is for an eating and drinking establishment that provides food or beverage for consumption on or off premise. It is proposed to be developed freestanding, on a pad site, and it includes a drive-through service.

Zoning Ordinance Requirements

Section 27-548.08 Site Plan

The applicant has submitted the required information, specifically a general description of the proposed pedestrian system that appears to be adequate for the intended use. The applicant's proposal conforms with the required information to be submitted with a Detailed Site Plan as described in Section 27-546(b)(1) of the Zoning Ordinance. A statement regarding the "description of the relationship between vehicular and pedestrian circulation systems" is required site plan content.

In this regard, and in their Statement of Justification, the applicant states that the landscape plan *"incorporates pedestrian safety into its design. Clear lines of site for both automobiles and pedestrians are provided so as to allow for safe travel throughout Subarea 9."*

Staff agrees with this assertion. No new curb cuts are proposed along MD 410. The proposed building is located along East West Highway (MD 410). The proposal includes a vehicular access via the existing MD 410 access drive at the Home Depot. A direct sidewalk connection to the existing sidewalks and streetscape along East West Highway is shown on the plans. The sidewalks and the pedestrian zone appear to be adequate to meet the purposes of the Transit District Overlay Zone as required by 27-548.08(c). The streetscape contains pedestrian scaled lighting and other amenities and is in strict conformance with the TDDP's District Wide Mandatory Development Requirements.

Further, the applicant's proposal shall be in conformance with the requirements of the T-D-O (Transit District Overlay) Zone. The applicant's proposal is consistent with the "Regulations" contained within Section 27-548-06(e), which requires that the "pedestrian system within a Transit District shall be oriented toward serving the Metro station, as well as other development within the District."

Technical staff has reviewed the applicant's proposal. The design of the building and drive through layout orients sidewalks towards MD 410 in order to provide access to the nearby Prince George's Plaza Metro Station. The sidewalks are wide enough for a bicycle to use, and bicycles can ride within the limits of MD 410. The site is close to the access to the Northwest Branch Trail and the proposal conforms to the TDDP's District Wide and Subarea 9 Mandatory Development Requirements that are described in the TDDP.

Zoning Ordinance Sections 27-548.08(c)(1)(A) thru 27-548.08(c)(1)(C) requires that a Transit District Development Plan "be in strict conformance with any mandatory development requirements of the TDDP". It also requires that it be "consistent with, and reflects the guidelines and criteria for development" contained in, the TDDP, and that it "meets all the requirements of the Transit District Overlay Zone". The Detailed Site Plan is also known as the "Transit District Site Plan".

District Wide Mandatory Development Requirements for Pedestrian Access and Circulation S1 thru S6

The TDDP's Urban Design section contains mandatory Development Requirements for Pedestrian Access and Circulation. The following is a technical staff analysis of the mandatory development requirements and the guidelines and criteria for development that contained in the TDDP as they relate to the pedestrian and bicycle system and access to transit services and area land use. The requirements and guidelines begin on page 27 of the TDDP.

S1 Primary Pedestrian Walkway

Analysis: The proposal will not conflict with this Mandatory Development Requirement or the overall design character of the area, and it can contribute to a strong pedestrian network. The proposal shows a primary walkway system on the Conceptual Site Plan that contains sidewalks and sidewalk amenities that are consistent with the TDDP. The sidewalk system connects directly to the Prince George's Plaza Metro Station. Internal sidewalks and walk areas are proposed and appear to be adequate.

S2 Walkways Through Parking Lots

Analysis: The sidewalk locations provided appear to be adequate for the intended use and they do not conflict with this Mandatory Development Requirement. There are no walkways on the proposal that would extend through any proposed parking lots to link the building to the Home Depot building, which is not adjacent to the proposed building. The applicant states that they will address further "walkway design" at the time of detailed site plan.

S3 Well Lit Walkways

Analysis: The application states that all walkways will be lighted to a minimum 1.25 foot candles. The proposed walkway lighting does not conflict with this Mandatory Development Requirement.

S4 Direct, Safe Pedestrian Links Between The Land Uses, Walkways, and The Metro Station

Analysis: The proposed sidewalks connecting to the existing sidewalks and streetscape features on MD 410 appear to be adequate. The location will provide direct linkages between the transit district uses and ultimately to the Metro station.

S5 & S6 Special Paving Materials

Analysis: The special paving materials are provided on side and to the connection to MD 140.

It should be noted that brick crosswalks exist today along MD 410. However, SHA does not currently construct brick or allow brick in crosswalks. SHA is replacing bricks with a stamped concrete to look like brick material. Stamped concrete is more durable than brick and it will have a smoother surface if constructed properly.

District Wide Pedestrian Access and Circulation Site Design Guidelines G1, G2 (page 30)

G1 Minimize Vehicular and Pedestrian Conflicts

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It minimizes vehicular and pedestrian conflicts by providing the existing streetscape on East West Highway and by minimizing the number of curb cuts along roadways.

G2 Barrier-Free Pedestrian Link(s)

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It indicates that barrier-free pedestrian walkways will be provided.

Landscape Planting and Design S7 (page 30)

S7 Landscape Screens and Buffers, and Pedestrian Safety

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It does not appear to contain any landscape screens or buffers that would lessen the safety of pedestrian walkways.

District Wide Streetscape mandatory Development Requirements P1, S8 (page 30)

P1 Streetscape Improvements

Analysis: The applicant will be responsible for any new streetscape improvements. This is enforced through the permitting process.

S8 thru S9 Visually Continuous and Unified Streetscape

Analysis: In terms of the pedestrian and bicycle circulation system, the applicant proposes a visually unified and continuous streetscape. There are no interruptions in the proposed pedestrian zone landscaping, pedestrian ways or lighting.

S13 Major Pedestrian Crossings

Brick crosswalks exist today along MD 410. The applicant is not proposing and changes to the existing crosswalks. SHA does not currently construct brick or allow brick in crosswalks. SHA is replacing bricks with a stamped concrete to look like brick material. Stamped concrete is more durable than brick and it will have a smoother surface if constructed properly.

Landscape Site Design Guidelines G3 (page 30)

G3 Landscape Screening

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It does appear to contain any landscaping at this time that would buffer parking areas.

District Wide Mandatory Development Requirements for Streetscape P1, S8, S9, S10, S11, S12, S13 (pages 30-31)

P1 Streetscape Improvements – Phasing

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It provides the existing streetscape on East West Highway.

S8 Visually Continuous Streetscape

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It contains a continuous streetscape along East West Highway and the adjacent streets.

S9 Streetscape Elements

Analysis: The proposal does not conflict with this Mandatory Development Requirement. It provides the

existing streetscape on East West Highway.

S10 Traffic Lights

Analysis: The proposal does not conflict with this Mandatory Development Requirement. The applicant states that there are no new traffic lights proposed with this application.

S11 Limbing Street Trees

Analysis: The proposal does not conflict with this Mandatory Development Requirement. The applicant states that trees will be pruned in the future according to the specifications of the TDDP.

S12 Tree Pits

Analysis: The proposal does not conflict with this Mandatory Development Requirement. The applicant states that tree pits will comply with County standards during construction.

S13 Street Crossings

Analysis: The proposal does not conflict with this Mandatory Development Requirement. The applicant states that "major" street crossings will conform to the County's standards. The proposal provides the existing streetscape on East West Highway.

Figure 8 (Page 32)

Analysis: The Street Sections Diagram indicates that a pedestrian zone, 40 feet wide, is required along MD 410. The proposal indicates that a typical streetscape approximately 40 feet wide will be provided, and it shows streetscape locations and widths that conform with the TDDP diagrams and details, including appendix A.

Bicycle Facilities Mandatory Development Requirements S29, S30 (page 41)

S29 & S30 Bicycle Parking

Analysis: The proposal does not currently conform with this Mandatory Development Requirement. The applicant proposes that bicycle parking was approved via the prior approval for Home Depot. However, bicycle parking will need to be added to the proposed eating and drinking establishment.

The TDDP requires that "All new retail development shall provide four bicycle racks per 10,000 square feet of floor space with each rack holding 2 bicycle parking spaces." (page 41). There is 4,197 gross square feet of development is proposed. But even with less than 10,000 square feet of floor space proposed, staff recommends that two additional bicycle parking spaces be provided close to the building entrance of the eating and drinking establishment in a concrete base.

Bicycle Facilities Site Design Guidelines G48, G49, G50, G51 (Page 41)

G48, G49, G50, G51 Bicycle Parking, Lighting and Visibility

Analysis: The proposal does not conform with this Mandatory Development Requirement. Staff

recommends that two additional bicycle parking spaces be provided close to the building entrance of the eating and drinking establishment in a concrete base to satisfy this requirement.

Pedestrian Facilities Mandatory Development Requirements P20 (page 62)

P20 Developers Shall Provide Continuous Sidewalks

Analysis: The proposal conforms with this requirement and the proposal will connect to the continuous sidewalk system that exists along MD 410.

Public Facilities (page 77-79)

The TDDP section title "Public Facilities" provides a summary of goals, objectives classifications and improvements for "Pedestrian/Bicycle/Multiuse Trail Network".

Analysis: The proposal conforms with this requirement in general, and it is connecting to the continuous sidewalk system that exists along MD 410. In terms of pedestrian and bicycle facilities, the application conforms to the TDDP requirements and guidelines, except for bicycle parking. Staff recommends that two additional bicycle parking spaces be provided close to the building entrance of the eating and drinking establishment in a concrete base.

The TDDP that depicts "proposed trails" on page 79 indicates that there is a proposed trail along the southern property line of the subject property that would connect the park system trail to the metro station.

Today, this trail connection is via MD 410. This is because in 2009, the District Council approved and adopted the *Approved Countywide Master Plan of Transportation*. This plan indicates that the preferred bicycle facility along MD 410 is an on-road bicycle lane, reflecting the current project concept. SHA has not indicated when or if a bicycle lane will be constructed on MD 410. The wide sidewalk along MD 410 functions as a shared use path. It can be used by both bicycle enthusiasts and those people who have less skill to access the Metro station from the surrounds.

Subarea 9 Mandatory Development Requirement P85 (Trail Connection)

P85 Trail Connection to Northwest Branch Stream Valley Park & Trail

Subarea 9 mandatory development Requirement number P85 requires that "A trail connection to the hiker biker trail in the Northwest Branch Stream Valley Park shall be provided."

Analysis: The trail access connects to Heurich Park via the sidewalk along MD 410. This local connection to the Northwest Branch Stream Valley Park and trail system is located on the south property line of the subject where a decorative retaining wall is located for the Home Depot development.

The applicant's proposal does not include the TDDP's proposed trail connection as it is depicted on the map on page 79, but it is not required and cannot be implemented as depicted in the TDDP because of land use and topography constraints.

In 2009, the District Council approved and adopted the *Approved Countywide Master Plan of Transportation*. This new plan indicates that the preferred bicycle facility along MD 410 is an on-road bicycle lane. Again, the wide sidewalk along MD 410 functions as a shared use path. It can be used by both bicycle enthusiasts and those people who have less skill to access the Metro station from the

surrounds.

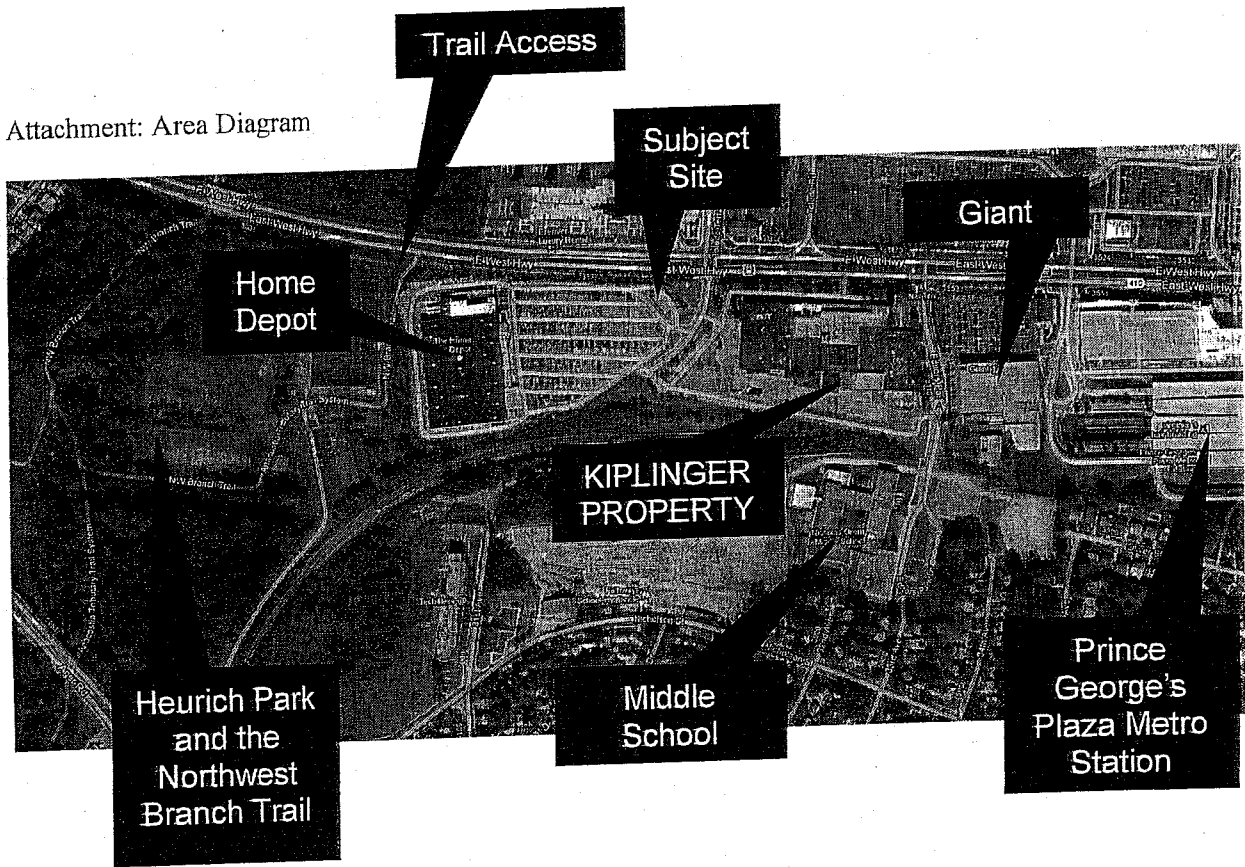
Conclusion

Based on the preceding analysis, the proposed pedestrian walkways, lighting, and bicycle parking do not conflict with the *Prince George's Plaza Approved Transit District Development Plan for the Transit District Overlay* (June 1998). The site will be conveniently located along East West Highway and will be accessible to the Prince George's Plaza Metro Station and the Northwest Branch Trail.

Staff recommends that the following condition be considered:

1. Install two u-shaped bicycle parking spaces close to the main entrance to the building anchored into a concrete base. Prior to certification, the detailed site plan shall show two u-shaped bicycle parking spaces near the main entrance to the building. Details of the bicycle parking and signage shall be provided on the detailed site plan. A bicycle parking area sign (MUTCD D4-3) shall be erected at the parking location (see MUTCD Part 9, Traffic Control for Bicycle Facilities, Section 9B.23.).

Attachment: Area Diagram





The Maryland-National Capital Park and Planning Commission
 14741 Governor Oden Bowie Drive, Upper Marlboro, MD 20772 301-952-3530
 Development Review Division - 301-952-3749 (fax)

**** REFERRAL REQUEST ****

Date: 2/14/2013
 To: PARKS DEPT. REY PALFREY
 From: URBAN DESIGN-JILL KOSACK
 Subject: DSP-99006-01, McDONALD'S - HYATTSVILLE

Received 2/15
 DPR J/K

IDENTIFICATION OF MAJOR ISSUES DUE DATE*: 3/1/2013
 *Note: E-mail any major issues/problems to the reviewer by the above date.

SUBDIVISION REVIEW COMMITTEE DATE: 3/1/2013

REFERRAL DUE DATE: 3/18/2013

Full Review of New Plan Revision of Previously Approved Plan
 Limited or Special Review Plans/Documents Returned for Second Review Following Revision by Applicant
 NOTE: This case is being reviewed at: Planning Board level OR Planning Director level
 COMMENTS: PROPOSED MCDONALDS IN EXISTING PARKING LOT.

 Related Cases: _____

REFERRAL REPLY COMMENTS:
 NO COMMENTS N/A FOR PARKS
 Paul J. [Signature] 3/6/13

NOTE: IF YOU HAVE NO COMMENTS, PLEASE INDICATE ABOVE AND FORWARD OR FAX TO THE REVIEWER'S ATTENTION.



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-4366
www.mncppc.org/pgco

March 20, 2013

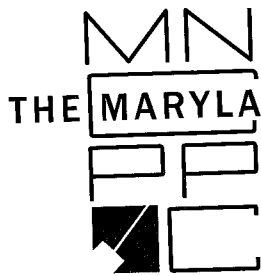
MEMORANDUM

TO: Jill Kosack, Urban Design

FROM: Brooke Larman, Permit Review Section, Development Review Division BEL

SUBJECT: Referral Comments for DSP-99006-01, McDonald's-Hyattsville

1. The use of a Fast Food Restaurant for a proposed McDonald's at an existing Home Depot site is not a permitted use in the Prince George's Plaza TDDP book. You will need to require Council review to amend the TDDP use table.
2. Until the use of a Fast Food Restaurant is permitted at this location then no further review or referral comments will be generated at this time.



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-4366
www.mncppc.org/pgco

Countywide Planning Division
Environmental Planning Section
301-952-3650

March 11, 2013

MEMORANDUM

TO: Jill Kosack, Master Planner, Urban Design Section
VIA: Katina Shoulars, Supervisor, Environmental Planning Section
FROM: Chuck Schneider, Planner, Environmental Planning Section
SUBJECT: McDonalds-Hyattsville (formerly Home Depot); **DSP-99006-01**

The Environmental Planning Section (EPS) has reviewed the above referenced Detail Site Plan (DSP) stamped as received on February 14, 2013. Verbal comments were provided in a Subdivision Development Review Committee meeting on March 1, 2013. The Environmental Planning Section recommends approval of DSP-99006-01 subject to the required revisions found at the end of this memorandum.

Background

The Environmental Planning Section previously reviewed and approved Natural Resource Inventory Equivalency Letter NRI-EL-009-11/NRI-084-11 and Tree Conservation Plan TCPII-046-99. The Equivalency Letter expires on April 13, 2016.

The subject property is located in the Prince George's Plaza Transit District Overlay Zone. The current application proposes to construct a new McDonald's restaurant in the existing parking lot.

Grandfathering

The site is not subject to the environmental regulations in Subtitles 25 and 27 that became effective on September 1, 2010.

Site Description

The 13.21 acre McDonald's -Hyattsville (formerly Home Depot) site is located on the southwest corner of the East-West Highway and Toledo Terrace intersection. A review of the available information indicates that no wetlands or streams or floodplain are found to occur on the subject project area. The predominant soil found to occur according to the USDA NRCS Web Soil Survey is Russett-Christian-Urban land complex soil series. According to available information, Marlboro clay does not occur on or

in the vicinity of this site. According to the Sensitive Species Project Review Area (SSSPRA) map received from the Maryland Department of Natural Resources Natural Heritage Program, there are no rare, threatened, or endangered species found to occur on or near this property. The site drains to the west towards Northwest Branch and is part of Anacostia watershed. According to PGAtlas.com, this site is not within the designated network of the Approved Countywide Green Infrastructure Plan. The site has frontage on East-West Highway, a master planned arterial road. East-West Highway is not designated a scenic or historic road. The property is in the Developing Tier as reflected in the adopted General Plan.

Master Plan Conformance

The approved Transit District Development Plan (TDDP) for Prince George's Plaza (June 1998) for this area contains Mandatory Development requirements and guidelines that are required to be evaluated with this application. The mandatory requirements in the TDDP are indicated by a "P" or an "S." The requirements must be completely reflected by and incorporated into this CSP. The site design guidelines are criteria for development to be used in the review of this application. The text in **BOLD** is the text from the transit district development plan that are environmental nature and the plain text provides comments on the plan conformance.

Stormwater – Mandatory Development Requirements

P25 – Any Development shall provide for water quality and quantity control in accordance with all Federal, State and County regulations. Bio-retention or other innovative water quantity or quality methods shall be used where deemed appropriate.

The site has a Stormwater Management Concept approval letter (Concept approval # 28828-2011-00). The associated plan has not been submitted. The approval letter states that bio-retention is required. It is unclear if the approval is based on the proposed concept submitted with this application. The associated concept plan needs to be submitted for review. It should be noted, that because this site is a redevelopment project, water quantity control is not required.

During the SDRC meeting on March 1, 2013, the applicant stated that a revised Stormwater Management Concept plan consistent with the Proposed CSP layout would be submitted.

Recommended Condition: Prior to certification of the Design Site Plan, a copy of the existing approved Stormwater Management Concept plan and letter shall be submitted.

P26 – Where stormwater management cannot be provided for existing developed properties, a mandatory 15 percent green space requirement shall be provided. The green space can be incorporated into the mandatory 10 percent afforestation required if it occurs on the actual property.

A copy of the stormwater concept plan is needed to address this requirement. See P25.

S31 - At the time of Detail Site Plan, the number of trash cans and locations shall be shown on the plan. Trash receptacles should be placed in strategic locations to prevent litter from accumulating in and around the proposed development.

This requirement has partially been met with this submission. The Detail Site Plan identifies an area on the west side of the proposed structure as a trash corral. There is no information to say how many dumpsters are located in this corral area. Individual trash receptacles must be strategically located to

prevent litter accumulation. In the Statement of Justification the applicant states that this plan complies with S-31. The Landscape Plan provided does not show the locations of the proposed individual trash receptacles within the parking area.

Recommended Condition: Prior to approval of the detailed site plan, a copy of the landscape plan shall show how many trash receptacles are within the corral area and where proposed individual trash receptacles will be located throughout the proposed parking on the McDonald's site.

S32 – Prior to the final inspection and sign off of permits by the Sediment/Stormwater or Building Inspector, and storm drain inlet associated with the development and all inlets on the subject subarea shall be stenciled with “Do Not Dump, Chesapeake Bay Drainage.” The Detailed Site Plan and the Sediment Control Plan (in the sequence of construction) shall contain this information.

A sediment and erosion control plan is not required with this submission; this stenciling information would be included on these plans.

Recommended Conditions: Prior to approval of the detail site plan, the DSP shall include notes and a detail regarding the stenciling of applicable storm drain inlets with “Do Not Dump – Chesapeake Bay Drainage” with the submission. Prior to the certification of the DSP, a copy of the sediment and erosion control plan containing notes and details regarding the same stenciling shall be submitted.

Woodland Conservation - Mandatory Development Requirements

S33 – Afforestation of at least 10 percent of the gross tract shall be required on all properties within the Prince George's Plaza Transit District currently exempt from the Woodland Conservation and Tree Preservation Ordinance. Afforestation shall occur on-site or within the Anacostia watershed in Prince George's County, with priority given to riparian zones and nontidal wetlands, particular within the Northwest Branch Sub-watershed.

The site is not exempt from the Woodland Conservation Ordinance. The site has an approved Type II Tree Conservation Plan (TCPII-046-99).

100 Year Floodplain - Mandatory Development Requirements

P28 – Any new development or reconstruction of existing development shall be in conformance with the Prince George's County Floodplain Ordinance.

P29 – No development within the 100 year floodplain shall be permitted without the express written consent of the Prince George's County Department of Environmental Resources.

P30 – If the development is undergoing subdivision, approval of a variation request shall be obtained for proposed impacts to the floodplain.

The site is not proposing a subdivision. According to the approved TCPII, the site contains a small area of 100-year floodplain; however, but the proposed development activity will not impact this feature.

Nontidal Wetlands - Mandatory Development Requirements

P31 – If impacts to nontidal wetlands are proposed, a Maryland Corps of Engineers Joint Permit Application shall be required and, where required, issuance of the permit.

P32 – If impacts to nontidal wetlands are proposed, a State Water Quality Certification pursuant to Section 401 of the Clean Water Act shall be required from the Maryland Department of the Environment.

The site does not contain areas of wetlands.

Noise Impacts - Mandatory Development Requirements

P33 – Each Preliminary Plat, Conceptual and/or Detailed Site Plan shall show a 65dBA (Ldn) noise contour based upon average daily traffic volumes at LOS E. Upon plan submitted, the Natural Resource Division shall determine if a noise study is required based on the delineation of the noise contour.

P34 – If it is determined by the Natural Resource Division that a noise study is required, it shall be reviewed and approved by the Natural Resource Division prior to approval of any Preliminary Plat of Subdivision, Conceptual and/or Detailed Site Plan. The study shall use Traffic volumes at LOS E and include examination of appropriate mitigation techniques and the use of acoustical design techniques. Furthermore, a typical cross-section profile of noise emission from the road to the nearest habitable structure is required.

Policies contained in the General Plan call for the reduction of adverse noise impacts to meet State of Maryland noise standards. Noise is generally regulated along roads with a classification of arterial or higher for residential uses. The project proposes to construct a restaurant facility in an existing paved parking lot area. No residential use is proposed with this new development application and no noise impact studies are required.

Conformance with the Countywide Green Infrastructure Plan

The *Approved Countywide Green Infrastructure Plan* indicates that none of the property is within or near the designated network.

Environmental Review

As revisions are made to the plans submitted the revision boxes on each plan sheet shall be used to describe what revisions were made, when, and by whom.

1. A Natural Resource Inventory Equivalency letter, NRI-090-11, in conformance with the environmental regulations was issued on April 25, 2011 and submitted with the current application. The site has less than 10,000 square feet of woodlands and no regulated environmental features will be impacted as part of the proposed application.

Comment: No additional information is required concerning a NRI.

2. The property is subject to the 1993 Prince George's County Woodland Conservation and Tree Preservation Ordinance (WCO) because the site has a previously approved Tree Conservation Plan. A Tree Conservation Plan (TCPII-046-99) was approved for the site on August 12, 1999 and the site has been developed in conformance with that approval. The previous Tree Conservation Plan had an overall requirement of 3.94 acres that was met with 0.22 acres of reforestation, 3.57 acres of off-site woodland credits and 0.09 acres of on-site woodland preservation.

The proposed development is in conformance with the existing TCPII because no change to the previously approved limit of disturbance. Because this is a major addition to the subject site; revise the TCPII to show the proposed McDonald's restaurant facility and parking lot revisions.

Required Conditions: Prior to the certification of the detail site plan, the TCPII-046-99 shall be revised as follows:

- a. Show the proposed building and parking lot layout for the proposed application
- b. Show the previous TCP approval and add "01" to the approval block.
- c. Have the revised plan signed and dated by the qualified professional preparing the plan.

Summary of Recommended Information and Conditions: The Environmental Planning Section recommends approval of DSP-99006-1 subject to the following conditions:

Recommended Conditions:

1. Prior to certification of the detail site, the TCPII-046-99 shall be revised as follows:
 - a. Show the proposed building and parking lot layout for the proposed application.
 - b. Show the previous TCP approval and add "01" to the approval block.
 - c. Have the revised plan signed and dated by the qualified professional preparing the plan.
2. Prior to approval of the detail site plan, the DSP shall include notes and a detail regarding the stenciling of storm drain inlets with "Do Not Dump - Chesapeake Bay Drainage" with the submission. Prior to the certification of the DSP, a copy of the sediment and erosion control plan containing notes and details regarding the same stenciling shall be submitted.
3. Prior to approval of the detailed site plan, a copy of the landscape plan shall show how many trash receptacles are within the corral area and where proposed individual trash receptacles will be located throughout the proposed parking on the McDonald's site.
4. Prior to certification of the detail site plan, submit a copy of the existing approved Stormwater Management Concept plan and letter consistent with the proposed layout.

If you have any questions concerning these comments, please contact me at 301-952-3650 or by e-mail at alwin.schneider@ppd.mncppc.org.

ACS:acs

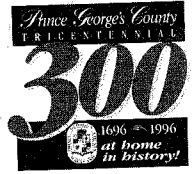


Rushern L. Baker, III
County Executive

PRINCE GEORGE'S COUNTY GOVERNMENT



Department of Public Works and Transportation
Office of Engineering



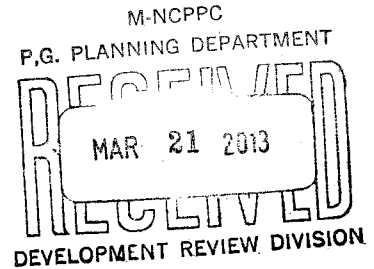
MEMORANDUM

March 12, 2013

TO: Jill Kosack, Urban Design Section
Development Review Division, N-NCPPC

FROM: *[Signature]* Dawit Abraham, P.E., Associate Director
Office of Engineering, DPW&T

RE: McDonald's Hyattsville
Detailed Site Plan No. DSP-99006-01



In response to the Detailed Site Plan No. DSP 99006/01 referral, the Department of Public Works and Transportation (DPW&T) offers the following:

- The property is located on the southside of East-West Highway (MD 410), at its intersection with Toledo Terrace. MD 410 is a State-maintained road; therefore, coordination with the Maryland State Highway Administration (SHA) is necessary. The site has no frontage on any County-maintained roadway.
- All storm drainage systems and facilities are to be in accordance with DPW&T's Specifications and Standards.
- A soils investigation report which includes subsurface exploration and a geotechnical engineering evaluation for the proposed building may be required.
- The proposed Detailed Site Plan is not consistent with approved Stormwater Management Concept Plan #2259-2011, dated April 19, 2011. (The micro bio-retentions proposed in the Concept has been eliminated in the Detailed Site Plan.)

If you have any questions or require additional information, please contact Ms. Elizabeth McKinney, the District Engineer for the area, at (301) 883-5710.

DA:MA:dar

cc: Elizabeth McKinney, District Engineer, EISD, OE, DPW&T
 Mariwan Abdullah, Engineer, EISD, OE, DPW&T
 Ikem Nwolisa, Engineer, EISD, OE, DPW&T
 Robin Jones, Engineering Technician, EISD, OE, DPW&T
 McDonalds Corp, 6903 Rockledge Drive, Rockville, MD 20817
 Bohler Engineering, 16900 Science Drive, Suite 104, Bowie,

Maryland 20715
 Inglewood Centre 3 9400 Peppercorn Place, Suite 420
 (301) 883-5710 FAX (301) 925-8510

Largo, Maryland 20774
 TDD (301) 985-3894



**PRINCE GEORGE'S COUNTY
POLICE DEPARTMENT**

MEMORANDUM

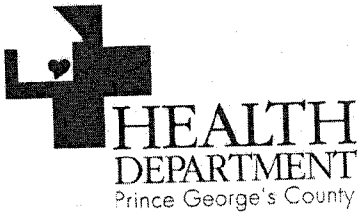
DATE: February 25, 2013

TO: Jill Kosack, Urban Designer
Urban Design Section
Development Review Division

FROM: Pfc. Christopher Wood #3288
Prince George's County Police Department
Community Services Division

SUBJECT: **DSP-9906-01 McDonald's - Hyattsville**

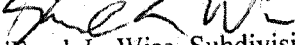
After visiting the site and reviewing the plans there are no CPTED related issues at this time.



Division of Environmental Health

Date: March 1, 2013

To: Jill Kosack, Urban Design, MNCPPC

From:  Frank L. Wise, Subdivision Review Specialist, Environmental Engineering Program

Re: DSP-99006-01, McDonald's-Hyattsville

The Environmental Engineering Program of the Prince George's County Health Department has completed a health impact assessment review of the detailed site plan submission for McDonald's-Hyattsville, and has the following comments/recommendations:

1. The photometric plan reflects what appear to be areas of excessive light levels on the site, but does not extend far enough to include light levels at the adjacent property lines. There is an increasing body of scientific research suggesting that artificial light pollution can have lasting adverse impacts on human health. The photometric plan should be revised to indicate that all proposed exterior light fixtures will be shielded, fixed in the full cut-off position and located so as to minimize light trespass caused by spill light onto adjacent properties.
2. There are 7 existing carry-out/convenience store food facilities and 2 markets/grocery stores within a ½ mile radius of this location. Research has found that people who live near an abundance of fast-food restaurants and convenience stores compared to grocery stores and fresh produce vendors, have a significantly higher prevalence of obesity and diabetes.
3. During the demolition/construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.
4. During the demolition/construction phases of this project, no noise should be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.

If you have any questions or need additional information, please contact me at 301-883-7651 or flwise@co.pg.md.us

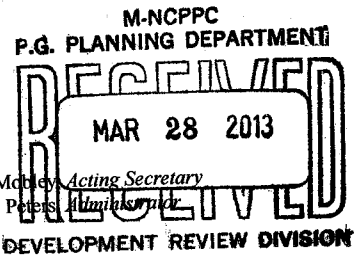


Southern L. Baker III
County Executive

Environmental Engineering Program
Largo Government Center
9201 Basil Court, Suite 318, Largo, MD 20774
Office 301-883-7681, Fax 301-883-7266, TTY/STS Dial 711
www.princegeorgescountymd.gov/health



Martin O'Malley, Governor
Anthony G. Brown, Lt. Governor



Darrell B. McTeeley, Acting Secretary
Melinda B. Peters, Administrator

MARYLAND DEPARTMENT OF TRANSPORTATION

March 25, 2013

Ms. Jill Kosack
Development Review Division
Maryland-National Capital Park and
Planning Commission
14741 Governor Oden Bowie Drive,
Upper Marlboro, Maryland 20772

RE: **Prince George's County**
MD 410
Mc Donald's - Hyattsville
SHA Tracking No.: 13APPG013XX
County Tracking No.: DSP-99006-01
Mile Post: 1.89

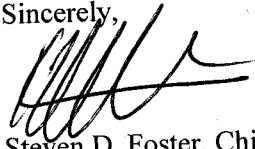
Dear Ms Kosack:

The State Highway Administration (SHA) is in receipt of the Detailed Site Plan submittal, received on February 28, 2013, proposing the Mc Donald's - Hyattsville development in Prince George's County.

The State Highway Administration (SHA) has completed the review and has no objection to plan acceptance as access to this site is from Toledo Terrace, a county road. All work is subject to the permitting process and requirements of Prince George's County. However, we would like the applicant to submit seven (7) copies of a Traffic Impact Study (TIS) for review in order to determine if any off-site improvements will be required. If it is concluded that off site improvements are required to MD 410, an access permit will be required from this office.

If you have any questions, or require additional information, please contact Mr. Kwesi Woodroffe at 410-545-8771, by using our toll free number in Maryland only at 1-800-876-4742 (x8771) or via email at KWoodroffe@sha.state.md.us.

Sincerely,


for Steven D. Foster, Chief
Access Management Division

SDF/JWR/KSW

- cc: HD Development of Maryland, Inc. - P.O. Box 105842, Atlanta, GA 30348-5842
- Mr. Pranoy Choudhury, SHA - Access Management Division
- Mr. Nick Driban, SHA - Access Management Division
- Mr. Matthew Jones (mjones@bohlereng.com), Bohler Engineering - 16701 Melford Blvd., Suite 310, Bowie, MD 20715
- Mr. Joe Katzenberger, SHA - Access Management Division
- Mr. Gary Kilfeather, Mc Donald's - 6903 Rockledge Dr., Suite 1100, Rockville, MD 20817
- Ms. Kate Mazzara, SHA District 3 Project Development

DETAILED SITE PLAN

FOR

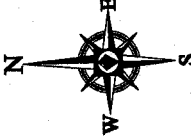


LOCATION OF SITE

3301 EAST-WEST HIGHWAY
TAX MAP 41, GRID E2, PART OF PARCEL A
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MD 20782



LOCATION MAP
COMPILED BY: BOHLLER ENGINEERING
DATE: 11-15-02



1

2

3

STANDARD ABBREVIATIONS

| SYMBOL | DESCRIPTION | SYMBOL | DESCRIPTION | SYMBOL | DESCRIPTION |
|--------|-------------------|--------|----------------------|--------|----------------------|
| AS | ASBESTOS | PS | PRESSURE | SA | STEEL ANGLE |
| AW | ALUMINUM WINDOW | PT | POINT | SB | STAINLESS STEEL BOLT |
| B | BUILDING | RA | RAILROAD | SD | STEEL DRUM |
| CA | CONCRETE | RR | RAILROAD | SE | STEEL END |
| CB | CONCRETE BLOCK | RS | RAILROAD SLEEPER | SH | STEEL HANGING |
| CC | CONCRETE CURB | RT | RAILROAD TIE | SI | STEEL I-BEAM |
| CD | CONCRETE DRIVEWAY | SA | STEEL ANGLE | SK | SKY |
| CE | CONCRETE | SB | STAINLESS STEEL BOLT | SL | SLAB |
| CF | CONCRETE FOOTING | SC | STEEL CHANNEL | SM | STEEL MESH |
| CG | CONCRETE | SD | STEEL DRUM | SN | STEEL NUT |
| CH | CHIMNEY | SE | STEEL END | SO | STEEL O-RING |
| CI | CONCRETE | SH | STEEL HANGING | SP | STEEL PIPE |
| CM | CONCRETE MASONRY | SI | STEEL I-BEAM | SQ | SQUARE |
| CO | CONCRETE | SK | SKY | SR | STEEL ROD |
| CP | CONCRETE | SL | SLAB | SS | STEEL SHEET PILING |
| CQ | CONCRETE | SM | STEEL MESH | ST | STEEL TIE |
| CR | CONCRETE | SN | STEEL NUT | SW | STEEL WELDED |
| CS | CONCRETE | SO | STEEL O-RING | TX | TRANSFORMER |
| CT | CONCRETE | SP | STEEL PIPE | U | UNDERGROUND |
| CU | CUPOLA | SQ | SQUARE | V | VENT |
| CV | CONCRETE VALVE | SR | STEEL ROD | W | WALL |
| CC | CONCRETE | SS | STEEL SHEET PILING | X | CROSS |

STANDARD DRAWING LEGEND

| SYMBOL | DESCRIPTION | SYMBOL | DESCRIPTION |
|--------|-------------|--------|-------------|
| ○ | CONCRETE | ⊕ | ELECTRIC |
| □ | CONCRETE | ⊗ | ELECTRIC |
| ⊙ | CONCRETE | ⊘ | ELECTRIC |
| ⊠ | CONCRETE | ⊡ | ELECTRIC |
| ⊡ | CONCRETE | ⊣ | ELECTRIC |
| ⊢ | CONCRETE | ⊤ | ELECTRIC |
| ⊥ | CONCRETE | ⊦ | ELECTRIC |
| ⊧ | CONCRETE | ⊨ | ELECTRIC |
| ⊩ | CONCRETE | ⊪ | ELECTRIC |
| ⊫ | CONCRETE | ⊬ | ELECTRIC |
| ⊭ | CONCRETE | ⊮ | ELECTRIC |
| ⊯ | CONCRETE | ⊰ | ELECTRIC |
| ⊱ | CONCRETE | ⊲ | ELECTRIC |
| ⊳ | CONCRETE | ⊵ | ELECTRIC |
| ⊶ | CONCRETE | ⊷ | ELECTRIC |
| ⊸ | CONCRETE | ⊹ | ELECTRIC |
| ⊺ | CONCRETE | ⊻ | ELECTRIC |
| ⊼ | CONCRETE | ⊽ | ELECTRIC |
| ⊾ | CONCRETE | ⊿ | ELECTRIC |
| ⊿ | CONCRETE | ⊺ | ELECTRIC |
| ⊻ | CONCRETE | ⊼ | ELECTRIC |
| ⊽ | CONCRETE | ⊾ | ELECTRIC |
| ⊿ | CONCRETE | ⊿ | ELECTRIC |

CONTACT INFORMATION

REFERENCES

- 1. ALL INFORMATION IS TAKEN FROM THE RECORD DRAWINGS AND SURVEY DATA PROVIDED BY THE OWNER.
- 2. THE LOCATION OF THE SITE IS SHOWN ON THE ATTACHED LOCATION MAP.
- 3. THE EXISTING UTILITIES ARE SHOWN ON THE ATTACHED UTILITY MAP.
- 4. THE PROPOSED UTILITIES ARE SHOWN ON THE ATTACHED UTILITY MAP.
- 5. THE EXISTING STRUCTURES ARE SHOWN ON THE ATTACHED SITE PLAN.
- 6. THE PROPOSED STRUCTURES ARE SHOWN ON THE ATTACHED SITE PLAN.
- 7. THE EXISTING ROADS ARE SHOWN ON THE ATTACHED SITE PLAN.
- 8. THE PROPOSED ROADS ARE SHOWN ON THE ATTACHED SITE PLAN.

UTILITIES

ALL UTILITIES ARE TO BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH THE MARYLAND DEPARTMENT OF TRANSPORTATION AND HIGHWAYS (DOT) SPECIFICATIONS.

BOHLLER ENGINEERING

PROJECT MANAGER: [Name]

PROJECT: [Name]

DATE: [Date]

SCALE: [Scale]

NOT APPROVED FOR CONSTRUCTION

DETAILED SITE PLAN

LOCATION OF SITE: 3301 EAST-WEST HIGHWAY, PRINCE GEORGE'S COUNTY, HYATTSVILLE, MD 20782



BOHLLER ENGINEERING

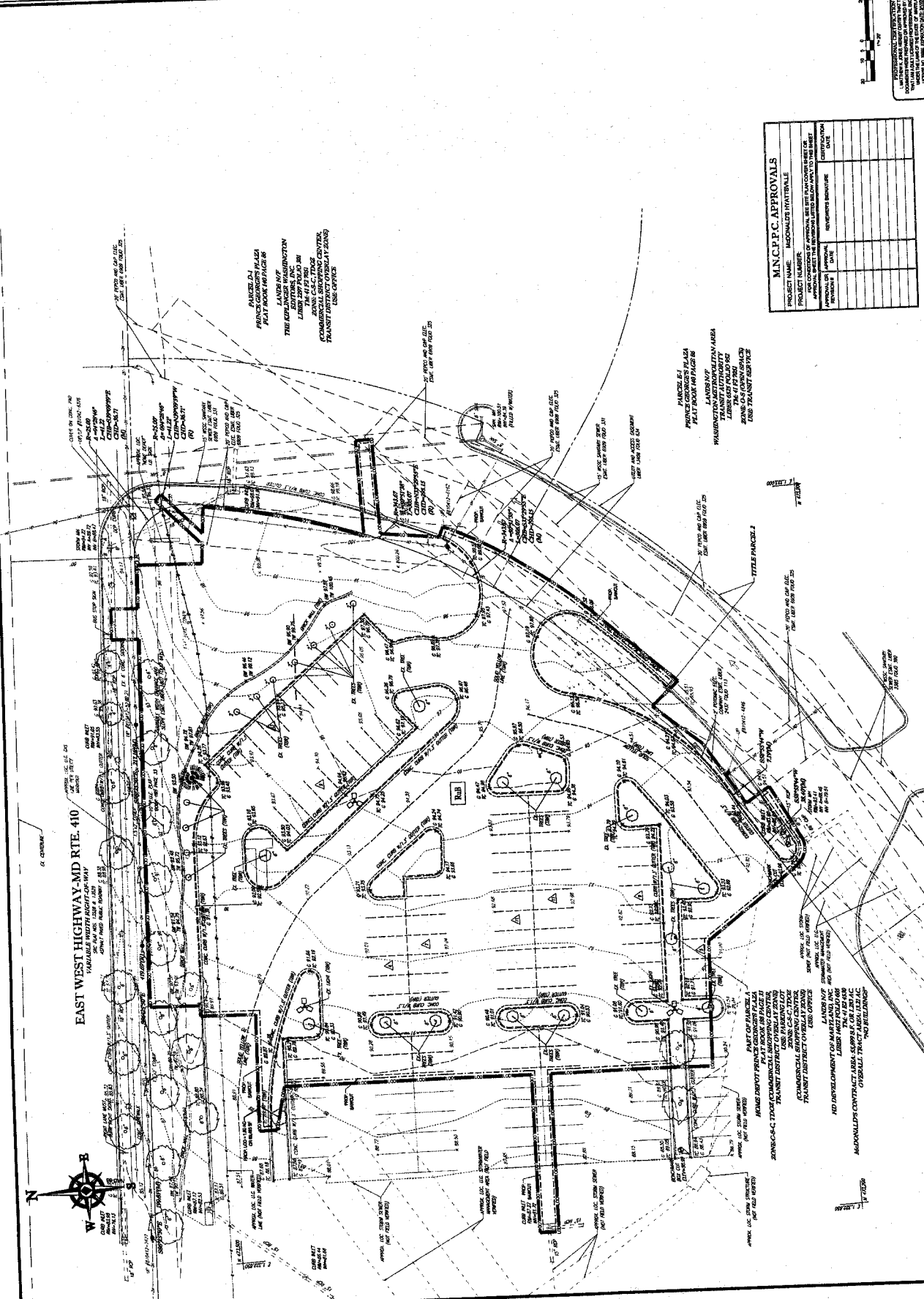
18701 MELFORD BOULEVARD, SUITE 310, BOWIE, MARYLAND 20715

PHONE: (301) 809-4500, FAX: (301) 809-4501

WWW.BOHLERENGINEERING.COM

CONTACT: MATTHEW K. JONES

|  <p>BOHLER ENGINEERING</p> <p>ENGINEERING ARCHITECTS PROJECT MANAGERS ENVIRONMENTAL CONSULTANTS 1000 W. WASHINGTON ST. SUITE 100 HYATTSVILLE, MD 20884 TEL: (301) 281-1100 FAX: (301) 281-1101 WWW.BOHLER-ENG.COM</p> | <p>REVISIONS</p> <table border="1" style="width:100%; border-collapse: collapse;"> <tr> <th>REV#</th> <th>DATE</th> <th>COMMENTS</th> <th>BY</th> <th>CHK</th> </tr> <tr> <td>1</td> <td>10/20/11</td> <td>ISSUED FOR PERMITS</td> <td>MM</td> <td>MM</td> </tr> </table> | REV# | DATE | COMMENTS | BY | CHK | 1 | 10/20/11 | ISSUED FOR PERMITS | MM | MM |  <p>NO SMOKING</p> | <p>NOT PERMITTED FOR CONSTRUCTION</p> <p>THIS PLAN IS NOT TO BE USED FOR CONSTRUCTION UNLESS APPROVED BY THE LOCAL AGENCIES.</p> | <p>DETAILED SITE PLAN</p> <p>PROJECT: M LOCATION OF SITE: 3301 EAST WEST HIGHWAY, PRINCE GEORGES COUNTY, HYATTSVILLE, MD 20884</p> | <p>BOHLER ENGINEERING</p> <p>1000 W. WASHINGTON ST. SUITE 100 HYATTSVILLE, MD 20884 TEL: (301) 281-1100 FAX: (301) 281-1101 WWW.BOHLER-ENG.COM</p> | <p>MARTZANO ENGINEERS</p> <p>1000 W. WASHINGTON ST. SUITE 100 HYATTSVILLE, MD 20884 TEL: (301) 281-1100 FAX: (301) 281-1101 WWW.MARTZANO-ENG.COM</p> | <p>EXISTING/DEMOLITION PLAN</p> <p>SHEET NUMBER: 4</p> <p>OF 11</p> |
|---|---|--------------------|------|----------|----|-----|---|----------|--------------------|----|----|---|--|---|--|--|---|
| REV# | DATE | COMMENTS | BY | CHK | | | | | | | | | | | | | |
| 1 | 10/20/11 | ISSUED FOR PERMITS | MM | MM | | | | | | | | | | | | | |



MNCP.P.C. APPROVALS

| PROJECT NAME | APPROVAL DATE | APPROVAL TYPE | APPROVED BY | DATE |
|------------------------|---------------|---------------|-------------|------|
| MCDONALD'S HYATTSVILLE | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

BOHLER ENGINEERING
 CIVIL & CONSTRUCTION ENGINEERS
 2500 UNIVERSITY AVENUE, SUITE 100
 PRINCE GEORGE, VIRGINIA 22916
 TEL: 804-731-1000 FAX: 804-731-1001
 WWW.BOHLERENGINEERING.COM

REVISIONS

| NO. | DATE | BY | COMMENT |
|-----|----------|----|---------|
| 1 | 08/01/14 | AK | FINAL |

NOT ADOPTED FOR CONSTRUCTION

DETAILED SITE PLAN

MCK JONES

LOCATION OF SITE
 3301 EAST WEST HIGHWAY
 PRINCE GEORGE'S COUNTY
 HYATTSVILLE, MD 20886

BOHLER ENGINEERING
 2500 UNIVERSITY AVENUE, SUITE 100
 PRINCE GEORGE, VIRGINIA 22916
 TEL: 804-731-1000 FAX: 804-731-1001
 WWW.BOHLERENGINEERING.COM

MCK JONES

AMENDED SITE PLAN

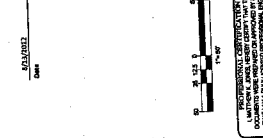
SHEET NUMBER: **5**

- NOTES:**
- SEE SUPPLEMENTAL SHEET FOR ALL UTILITIES.
 - PROPERTY LINES ARE SHOWN IN RED.
 - CONSTRUCTION OF APPROVED PROJECTS SHALL BE IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
 - SEE SHEET 10 FOR MASSING WITH NEIGHBORING PROJECTS.
 - NO SITE.

Tree Canopy Coverage Schedule for Sec. 22.128

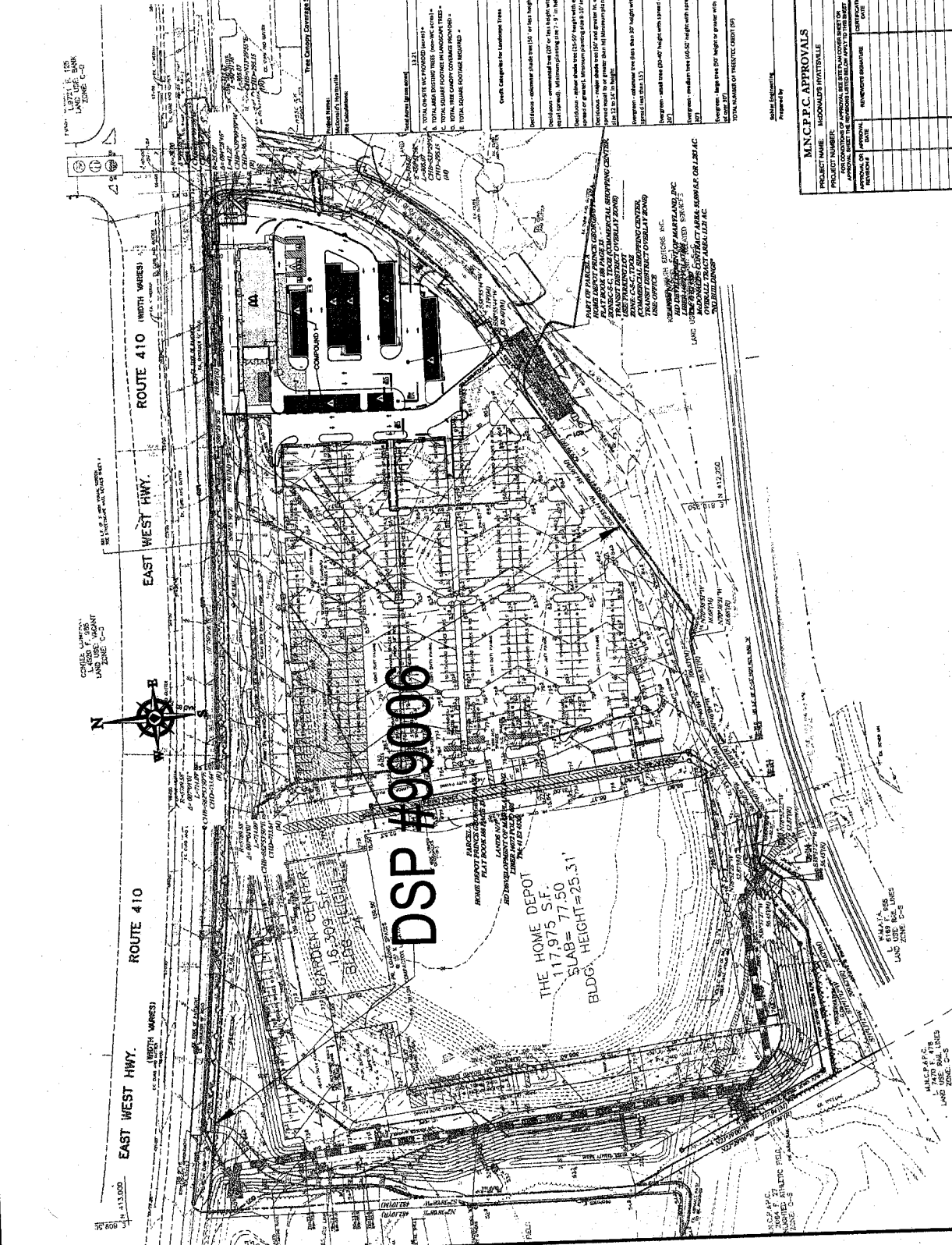
| Tree Species | Species | % of Tree Canopy | Area |
|----------------------------------|---------|------------------|------------|
| Deciduous | 13.3 | 15.0 | 133 |
| Evergreen | 13.3 | 15.0 | 133 |
| TOTAL TREE CANOPY COVERED | | | 266 |

| Tree Species | Species | % of Tree Canopy | Area |
|----------------------------------|---------|------------------|------------|
| Deciduous | 13.3 | 15.0 | 133 |
| Evergreen | 13.3 | 15.0 | 133 |
| TOTAL TREE CANOPY COVERED | | | 266 |



MN.C.P.C. APPROVALS

| | |
|-----------------|-----------------------|
| PROJECT NAME: | MCK JONES HYATTSVILLE |
| PROJECT NUMBER: | |
| DATE: | |
| APPROVED: | |
| APPROVED: | |
| APPROVED: | |
| APPROVED: | |



SCALE: 1" = 40'-0"

LAND USE: BULK LINES
 LAND ZONE: C-3

1 - - WSSC Plan Review Comments

Created by: Alicia Edwards
On: 02/27/2013 01:43 PM

WSSC Plan Review Comments
DSP-99006-01 McDonald's - Hyattsville

2 - - WSSC Plan Review Fee Due

Created by: Alicia Edwards
On: 02/27/2013 01:44 PM

The required WSSC plan review fee of \$1100.00 is outstanding. Invoice #2013-158 has been created and will be sent to the applicant.

3 - - WSSC Standard Comments for all Plans

Created by: Alicia Edwards
On: 02/27/2013 01:44 PM

1. WSSC comments are made exclusively for this plan review based on existing system conditions at this time. We will reevaluate the design and system conditions at the time of application for water/sewer service.
2. Coordination with other buried utilities:
 - a. Refer to WSSC Pipeline Design Manual pages G-1 and G-2 for utility coordination requirements.
 - b. No structures or utilities (manholes, vaults, pipelines, poles, conduits, etc.) are permitted in the WSSC right-of-way unless specifically approved by WSSC.
 - c. Longitudinal occupancy of WSSC rights-of-way (by other utilities) is not permitted.
 - d. Proposed utility crossings of WSSC pipelines or rights-of-way that do not adhere to WSSCs pipeline crossing and clearance standards will be rejected at design plan review. Refer to WSSC Pipeline Design Manual Part Three, Section 3.
 - e. Failure to adhere to WSSC crossing and clearance standards may result in significant impacts to the development plan including, impacts to proposed street, building and utility layouts.
 - f. The applicant must provide a separate Utility Plan to ensure that all existing and proposed site utilities have been properly coordinated with existing and proposed WSSC facilities and rights-of-way.
 - g. Upon completion of the site construction, utilities that are found to be located within WSSCs rights-of-way (or in conflict with WSSC pipelines) must be removed and relocated at the applicants expense.
3. Forest Conservation Easements are not permitted to overlap WSSC existing or proposed easements. Potential impacts to existing Forest Conservation Easements (due to proposed water and/or sewer systems) must be reviewed and approved by County staff.
4. Unless otherwise noted: ALL extensions of WSSCs system require a request for Hydraulic Planning Analysis and need to follow the System Extension Permit (SEP) process. Contact

WSSCs Development Services Center at (301-206-8650) or visit our website at [www.wsscwater.com/Development Services](http://www.wsscwater.com/Development%20Services) for requirements. For information regarding connections or Site Utility (on-site) reviews, you may visit or contact WSSCs Permit Services at (301) 206-4003.

4 - WSSC Design Comments

Created by: Ann Russell
On: 03/13/2013 11:26 AM

1. This site is currently being served by an existing, active water connection - 99OS0121 serving the Home Depot.
2. Align water and sewer lines to avoid environmental, storm water management facilities, ESD devices, other utilities, landscaping, tree boxes and structures or paving impacts for future maintenance. See WSSC Design Manual C-3.1
3. Existing mains shown on plan should be labeled with correct pipe size, material and WSSC contract number.
4. Show right-of-way limits on plan for all existing water and sewer mains.
5. Provide proper protection of water supply where water main is below or parallel to sewer main, building drain, sewer house connection or septic field and when pipe crosses other utilities.
6. Revise the plan to realign any water pipeline that conflicts with large storm drains, culverts, deep side ditches, etc. Maintain the required horizontal clearances from other utilities, retaining walls, sediment traps, street lights, paving, etc. See WSSC Design Manual C-3.1
7. All buildings shall have a backflow containment device installed on the outlet side of the water meter, prior to any water uses within the premise, as cited in Section 502.3 of the WSSC Plumbing & Fuel Gas Code. Backflow preventers shall be maintained by the owner as cited in Section 102.3.9.
8. The minimum horizontal clearance from a building to the outside diameter of a pipeline is 15-feet. The minimum spacing between adjacent buildings with both water and sewer lines between them must be 40-feet. In some cases where connections, fire hydrants, or deep water/sewer lines are involved, additional easement width is required.
9. Any grading, change in pipe loading (including but not limited to proposed fill or excavation), adjustment to manhole rims, fire hydrant relocations, placement of access roads or temporary haul roads, temporary sediment control devices, paving construction or construction related activity of any kind over an existing WSSC water or sewer main or within an existing WSSC right-of-way requires advance approval by WSSC. Any proposed public street grade establishment plan (GEP) with an existing WSSC water or sewer main of any size located within the existing or proposed public street right-of-way requires WSSC approval directly on the original GEP prior to approval of the GEP by the County Department of Public Works and Transportation. Any work (design, inspection, repair, adjustment, relocation or abandonment of existing WSSC facilities) is done at the sole expense of the applicant/builder/developer. Contact WSSC Relocations Unit at (301) 206-8672 for review procedures and fee requirements. See WSSC Design Manual, C-5.1 and Part Three, Section 11.
10. WSSC facilities/structures cannot be located within a public utility easement (PUE) however WSSC pipelines may cross over a PUE. Revise the plan to relocate any pipeline, valve, fire hydrant, meter vault and any other WSSC facilities/structures outside of the PUE.

5 - Retaining Wall

Created by: Ann Russell
On: 03/12/2013 09:50 AM

Water under retaining wall will require special design considerations.

6 - Conflict with Screening Wall

Created by: Ann Russell
On: 03/12/2013 09:57 AM

Proposed water is under screen wall. There should be at least 5 feet of separation and the line of influence will need to be evaluated. Water should have a 15 foot clearance from building.

Consider connection to relocated water on other side of building with shorter connection to McDonald's.

7 - Meter and WSSC easement may be required

Created by: Ann Russell
On: 03/13/2013 11:36 AM

An outside meter and WSSC easement may be required. This will be determined during the Site Utility review.

8 - Hydraulics Comments

Created by: Ann Russell
On: 03/12/2013 04:21 PM

1. Site Utility System reviews are required for projects with proposed water connections greater than 2-inch or sewer connections greater than 4-inch. Contact the WSSC Permit Services Unit on (301) 206-4003 for submittal requirements or view our website.

2. An 8-inch gravity sewer main is available to serve the proposed site. Contact the Permit Services Unit at 301-206-4003 for details regarding applying for service connections or visit our website.

3. The proposal to connect to the existing Site Utility water main serving the Home Depot will require abandonment of some existing Site Utility water main. Show limits of abandonment on plan.

Kosack, Jill

From: James Chandler <jchandler@hyattsville.org>
Sent: Thursday, April 04, 2013 10:00 AM
To: Kosack, Jill
Cc: Adams, Steven
Subject: Re: RE: Mcdonalds DSP-99006/01

Jill,

The motion to oppose will be placed on our consent agenda (unanimous) for April 15th, therefore please include in your staff report that the following:

1. The City of Hyattsville does not support the applicant's request for an amendment to the table of uses to permit eating and drinking establishments with drive-through service, as this request is inconsistent with the pedestrian and transit oriented intent of the Prince George's Plaza Transit District Development Plan. If the applicant requires a drive-through as part of the development, than the City is opposed to this proposed detailed site plan.
2. The City is supportive of the applicant's proposed compliance with landscape manual and tree canopy.

Jim Chandler, LEED AP
Director
Community & Economic Development
City of Hyattsville
4310 Gallatin Street
Hyattsville Maryland, 20781
(301) 985-5013

Disclaimer: The information contained in this communication may be confidential, is intended only for the use of the recipient named above, and may be legally privileged. If the reader of this message is not the intended recipient, you are hereby notified that any dissemination, distribution, or copying of this communication, or any of its contents, is strictly prohibited. If you have received this communication in error, please re-send this communication to the sender and delete the original message and any copy of it from your computer system. Thank you.

On 4/4/13 9:56 AM, "Kosack, Jill" wrote:

Jim – I am the reviewer on this case. My staff report needs to be mailed out by next Thursday, the 11th, for the PB hearing on April 25th. Therefore, the staff report will say that no official response had been received from Hyattsville. When I receive the City's response letter, I will put it in the PB's books as late back-up and it will then be incorporated in the final resolution on the case. Please plan to be at the hearing on the 25th if possible to formally present the City's position.

Thanks,

Jill

From: Fields, Meika
Sent: Thursday, April 04, 2013 9:26 AM
To: Kosack, Jill
Subject: FW: Mcdonalds DSP-99006/01

FYI

From: James Chandler [<mailto:jchandler@hyattsville.org>]
Sent: Thursday, April 04, 2013 9:25 AM
To: dlynch@mhlawyers.com; jchandler@hyattsville.org
Cc: Adams, Steven; Fields, Meika
Subject: Re: Mcdonalds DSP-99006/01

Dan,

On Monday evening, the Council stated that they were not in support of this proposal and that they did not feel it is necessary for you to provide them with a presentation. They will be formally opposing the request to amend the table of uses to permit a restaurant with drive-through on April 15th.

Jim Chandler, LEED AP
Director
Community & Economic Development
City of Hyattsville
4310 Gallatin Street

Marc Tartaro
Mayor

Elaine Stookey
Acting City Administrator



April 16, 2013

Elizabeth M. Hewlett, Chairman
M-NCPPC Planning Board
County Administration Building
14741 Governor Oden Bowie Drive
Upper Marlboro, MD 20772

CITY COUNCIL

WARD 1

Candace B. Hollingsworth
Eric Wingard

WARD 2

David Hiles, Vice President
Shani N. Warner

WARD 3

Matthew D. McKnight, President
Timothy P. Hunt

WARD 4

Paula J. Perry
Carlos Lizanne

WARD 5

Ruth Ann Frazier

POLICE DEPARTMENT

Non-Emergency
301/985-5060
Emergency
301/985-5050

PUBLIC WORKS

301/985-5032

COMMUNITY & ECONOMIC
DEVELOPMENT

301/985-5013

RECREATION & THE ARTS

301/985-5020

RE: DSP-99006-01 - McDonald's Hyattsville

Dear Chairman Hewlett,

On Monday, April 15, 2013, the City Council voted unanimously to oppose the proposal to construct a McDonald's restaurant with a drive-through within the Prince George's Plaza Transit District Overlay Zone (TDOZ).

The City of Hyattsville does not support the applicant's request for an amendment to the table of uses to permit eating and drinking establishments with drive-through service. The request is inconsistent with the pedestrian and transit oriented intent of the Prince George's Plaza Transit District Development Plan and will compromise the walkability and long-term development within the TDOZ.

The City continues to maintain the position that restaurants with drive-through service are not an appropriate land-use within this TDOZ. If the applicant requires a drive-through as part of the development the City is opposed to this proposed detailed site plan.

We thank you for your consideration of these comments and we look forward to your decision regarding this matter.

Sincerely,

Marc Tartaro
Mayor

cc: City Council
Steve Adams, Urban Planning Supervisor, M-NCPPC
Honorable Will Campos, County Council, District 3
Jill Kosack, M-NCPPC
Dan Lynch, Attorney, McNamee, Hosea, Jernigan, Kim, Greenan and Lynch, P.A.

CITY OF HYATTSVILLE

4310 Gallatin Street, Hyattsville, MD 20781 | Tel 301/985-5000 | Fax 301/985-5007 | www.hyattsville.org

011

AGENDA ITEM # 12
PGCPB MEETING OF 4/25/2013

PRINCE GEORGE'S COUNTY PLANNING BOARD
OFFICE OF THE CHAIRMAN
RECEIVED 04/22/13 LOG NO. C13042201
DISTRIBUTION Planning AA
Jessica Jones

Harrison, LaTasha

From: PGCPB
Sent: Monday, April 22, 2013 2:47 PM
To: Hewlett, Elizabeth; Jones, Jessica; Proctor, Marie; Kratka, Leigh; Piret, Fern; Hirsch, Alan; Harrison, LaTasha; Hutchison, Lorrie
Subject: FW: DSP-99006-01 MCDONALD'S - HYATTSVILLE / Regular Agenda Item # 1694

Prince George's County Planning Department
Office of the Director
Rec'd **APR 23 2013** Log No Same
Distribution FP, AD, Log

From: Christina Kim
Sent: Monday, April 22, 2013 2:46:40 PM (UTC-05:00) Eastern Time (US & Canada) Original to DRD
To: PGCPB
Subject: DSP-99006-01 MCDONALD'S - HYATTSVILLE / Regular Agenda Item # 1694

Hello-

I live across the street from the proposed McDonalds on East West Highway and Toledo Terrace. There is a McDonalds located inside the food court at Prince Georges Plaza and one right down the street at the corner of East West Highway and Route 1. We do not need another McDonalds at this proposed location. I feel like this intersection is very congested already because of cars turning left and right from Toledo Terrace without having dedicated turn lights. I would not mind having something built there that is healthy and nutritious, but we absolutely do not need another McDonalds here.

Christina Kim
240-447-1455
3300 East West Hwy, Apt 351
Hyattsville, MD 20782

PGCPB
PLANNING DEPARTMENT
RECEIVED
APR 23 2013
RECEIVED
DEVELOPMENT REVIEW DIVISION
Page 119