



The Maryland-National Capital Park and Planning Commission
 Prince George's County Planning Department
 Development Review Division
 301-952-3530

Note: Staff reports can be accessed at <http://mncppc.iqm2.com/Citizens/Default.aspx>

Detailed Site Plan McDonald's Landover

DSP-19060

REQUEST	STAFF RECOMMENDATION
Raze all existing structures and rebuild an eating and drinking establishment with drive-through service.	APPROVAL with conditions

Location: On the north side of MD 214 (Central Avenue) at its intersection with Brightseat Road.	
Gross Acreage:	1.17
Zone:	M-U-I/D-D-O/M-I-O
Dwelling Units:	N/A
Gross Floor Area:	4,540 sq. ft.
Planning Area:	72
Council District:	05
Election District:	13
Municipality:	N/A
200-Scale Base Map:	210NE08
Applicant/Address: McDonald's Corp 6301 Rockledge Drive, Suite 1100 Bethesda, MD 20817	
Staff Reviewer: Adam Bossi Phone Number: 301-780-8116 Email: Adam.Bossi@ppd.mncppc.org	



Planning Board Date:	07/09/2020
Planning Board Action Limit:	07/09/2020
Staff Report Date:	06/24/2020
Date Accepted:	04/30/2020
Informational Mailing:	10/17/2019
Acceptance Mailing:	04/20/2020
Sign Posting Deadline:	06/09/2020

The Planning Board encourages all interested persons to request to become a person of record for this application. Requests to become a person of record may be made online at http://www.mncppcapps.org/planning/Person_of_Record/. Please call 301-952-3530 for additional information.

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THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION

PRINCE GEORGE'S COUNTY PLANNING BOARD

STAFF REPORT

SUBJECT: Detailed Site Plan DSP-19060
McDonald's Landover

The Urban Design staff has reviewed the detailed site plan for the subject property and presents the following evaluation and findings leading to a recommendation of APPROVAL with conditions, as described in the Recommendation section of this report.

EVALUATION CRITERIA

The detailed site plan was reviewed and evaluated for conformance with the following criteria:

- a. The requirements of the 2010 *Approved Subregion 4 Master Plan and Sectional Map Amendment*;
- b. The requirements of the Prince George's County Zoning Ordinance for the Mixed Use-Infill (M-U-I) and Development District Overlay (D-D-O) Zones;
- c. The requirements of the 2010 *Prince George's County Landscape Manual*;
- d. The requirements of the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance;
- e. The requirements of the Prince George's County Tree Canopy Coverage Ordinance; and
- f. Referral comments.

FINDINGS

Based upon the analysis of the subject application, Urban Design staff recommends the following findings:

- 1. **Request:** This detailed site plan (DSP) requests to raze the existing McDonald's eating and drinking establishment with drive-through service and replace it with a new 4,540-square-foot McDonald's eating and drinking establishment with drive-through service.

2. Development Data Summary

	EXISTING	PROPOSED
Zone(s)	M-U-I/D-D-O/M-I-O	M-U-I/D-D-O/M-I-O
Use(s)	Eating and Drinking Establishment with Drive-Through	Eating and Drinking* Establishment with Drive-Through
Acres	1.17	1.17
Lots	4	4
Parcels	0	0
Square Footage/GFA	4,362 (to be razed)	4,540

Note: * Per Subregion 4 Master Plan and SMA Exemption 1 on page 488, the legally existing buildings, structures, and use are exempt from the development district standards. This exemption also allows that as long as the legally existing use (eating and drinking establishment with full drive-through service) is not discontinued for more than 180 days, in accordance with Section 27-241(c) of the Zoning Ordinance, it is not subject to the development district standards or site plan review. Therefore, the use is not the subject of this DSP, as it is legally existing, and the applicant intends to discontinue the use for less than 180 days. The use is not permitted in the M-U-I/D-D-O Zone, therefore, if the use does discontinue for more than 180 days, the applicant will be required to apply for a new DSP, including a request to permit the use on the property, which will be subject to District Council review and approval.

Other Development Data

Parking Requirements

Description	Rate per Section 27-568	Required	Provided
Eating or Drinking Establishment (including drive-through service or carry out)	69 seats @ 1 space/3 seats	23	20
Gross Floor Area (excluding areas used exclusively for storage or patron seating, and any exterior patron service area)	1,598 sq. ft. @ +1 space/50 sq. ft.	32	26
Handicap-Accessible Spaces	1 per 25 spaces	2	2
Total Spaces		44*	48

Note: * Per the D-D-O Zone (pages 554-555), the minimum surface parking spaces shall be 80 percent of the minimum required parking, as stated in Section 27-568(a) of the Zoning Ordinance.

Bicycle Spaces per the D-D-O

Required (1 space per 20 parking spaces)	3
Provided	3

Loading Spaces	
Required (1 space for <3,000 sq. ft.)	1
Provided	1

3. **Location:** The subject site is located on the north side of MD 214 (Central Avenue) at its intersection with Brightseat Road in Planning Area 72, Council District 5. The site is zoned Mixed Use-Infill (M-U-I) and is within a Development District Overlay (D-D-O) Zone established by the 2010 *Approved Subregion 4 Master Plan and Endorsed Sectional Map Amendment* (Subregion 4 Master Plan and SMA).
4. **Surrounding Uses:** The subject property is bounded to the north by single-family detached residential development in the Rural Residential Zone, to the east by the right-of-way of Brightseat Road, to the south by the right-of-way of MD 214, and to the west by a commercial office building in the M-U-I and D-D-O Zones.
5. **Previous Approvals:** The existing McDonald’s on the subject site was established as a permitted use when the property was zoned Commercial Office (C-O). Subsequently, the Prince George’s County Zoning Ordinance was revised and no longer permitted fast food restaurants in the C-O Zone and the operation was certified as a nonconforming use in 1987 by Special Exception SE-3805. This SE also allowed for expansion of the existing use to include a drive through, increased patron seating, increased parking, and the addition of a vestibule to the building. On June 2, 1994, the Prince George’s County Planning Board approved Revision of Site Plan ROSP-3805 and Variance Special Exception VSE-3805 to address the widening of MD 214 and its impact on the site’s associated setback requirement and landscape strip along its southern frontage.
6. **Design Features:** The existing McDonald’s eating and drinking establishment includes seating for 150 patrons and 69 parking spaces. The subject DSP proposes to raze the existing building and construct a McDonald’s new eating and drinking establishment with drive-through service. The proposed building will be 178 square feet larger than the existing structure, and the new site design sets the building in a south-central location on-site, with its length parallel to MD 214. Site access will be maintained in two of three existing locations: from Brightseat Road in the northeast corner and from MD 214 in the southwest corner. The existing eastern driveway to MD 214 will be removed. Existing sidewalks within the rights-of-way of MD 214 and Brightseat Road are to be retained. Two sidewalk connections to the new McDonald’s building are provided at the southeast corner of the site, one to Brightseat Road and the other to MD 214. Special paving is shown on crosswalks at the drive-through lane to increase the visibility of these pedestrian crossings. An outdoor seating area is located on the east side of the building and a decorative fence with brick columns is provided along the southern and eastern periphery of the site.

A total of 48 parking spaces are provided to the north and west of the building, which slightly exceeds the required number of spaces. A single loading space is located to the southwest of the building, with screening provided by the proposed decorative fencing and

streetscape landscaping. An existing 6-foot-high wooden fence along the northern property line will be retained and provides a level of screening between the site and abutting residential properties. The existing fenced trash enclosure will be replaced with a new one in the same location. The signage program for the site retains an existing pylon sign, adjacent to the intersection of Brightseat Road and MD 214. A series of small directional signs are provided throughout the site. Three building-mounted logo and identity signs, totaling approximately 61 square feet, are also included. A photometric plan provided with the DSP shows sufficient exterior lighting for the development.

The proposed one-story, 19-foot-high, McDonald's building is 4,540 square feet and includes a double drive through. The building is a single-story structure with ample fenestration provided for the dining area. Façades are to be primarily clad with brick veneer and Hardie plank siding in a complimentary color scheme of gray, brown, and black. Aluminum canopy systems are provided over entry areas and drive-through service windows. Access for drive-through service is at the northwest corner of the building, wraps around the southern façade of the building where service windows are located, and exits to the parking lot to the northeast of the building. A bypass lane is provided to the south of the drive-through lane.

The overall redevelopment plan seeks to continue the long-established use of the site as a McDonald's eating and drinking establishment with drive-through service with a modern building and associated features.

COMPLIANCE WITH EVALUATION CRITERIA

7. **2010 Approved Subregion 4 Master Plan and Sectional Map Amendment and the standards of the Development District Overlay (D-D-O) Zone:** The Subregion 4 Master Plan and SMA defines long-range land use and development guidelines and establishes a D-D-O Zone for five focus areas where the establishment of vibrant, pedestrian-friendly, mixed-use environments is desired. Design standards and guidelines for the D-D-O Zone are intended to shape the public realm within applicable areas. All new development, redevelopment, and rehabilitation of existing structures within the D-D-O Zone shall comply with the general intent and goals of the development district standards. The subject site is in the D-D-O Zone focus area for Central Avenue between Norair Avenue and Brightseat Road Redevelopment and the proposal to redevelop the site is subject to the D-D-O Zone standards and guidelines. The proposed project is in general conformance with the applicable standards and guidelines. Where the project is not in conformance, amendments have been requested, as discussed below.

Requests to Amend Development District Standards

In accordance with Section 27-548.25(c), Site Plan Approval, of the Zoning Ordinance, if the applicant so requests, the Planning Board may apply development standards, which differ from the approved development district standards. Given conditions unique to this DSP, the applicant has requested to amend specific D-D-O Zone standards, as follows (all page numbers reference the Subregion 4 Master Plan and SMA):

- **General Site Standards and Guidelines: Secondary Facade (page 538) -** Secondary façades of buildings must not have long expanses of blank wall. Architectural elevations submitted with the DSP show the northern and western

façades include long expanses of blank wall. The applicant has proffered to revise elevations to include appropriate articulation and a condition has been included in the Recommendation section to require such, prior to certification of the DSP. Therefore, an amendment will not be necessary.

- **Building Envelope Standards and Guidelines, B. Mixed-Use, Commercial and Institutional Types (page 540)** - These requirements are applicable to commercial buildings such as the proposed development. The build-to line requirement is 18 feet from the back of curb along MD 214 and Brightseat Road. The DSP provides for variable build-to lines of approximately 66 feet to 68 feet from MD 214, and approximately 120 feet to 127 feet from Brightseat Road, to accommodate a drive-through lane and a vehicular bypass lane. Side and rear yard setback standards are 0 to 10 feet. The building is in a central location on the site, greater than 10 feet from the side and rear lot lines. A minimum of 80 percent of the site's frontage is required to be occupied by building façade. The proposed building will occupy only 34 percent of the site's frontage along MD 214.

Given the scope of the proposed project and narrowness of the site, conformance with these requirements is not possible and the site design provided is appropriate for the intended use. Therefore, staff recommends that the Planning Board **approve** these amendment requests.

- **Street Type Standards and Guidelines, A. Typical Primary Mixed-Use Street: Sidewalk (page 542)** - Six-foot tree pit areas and eight-foot-wide dooryards are required. Given the layout of the site, the provision of a traditional dooryard space is not feasible. The presence of existing subsurface and overhead utilities along MD 214 and Brightseat Road do not allow for the provision of tree pits. The DSP proposes to maintain an existing sidewalk and 5-foot-wide grass strip along each roadway. The DSP also provides a 10-foot-wide landscape strip with a decorative fence and plantings between the sidewalks and drive-through aisle. These design features provide a similar function to the required tree pits and dooryard and are an appropriate alternative given the presence of utilities and the site's use. Therefore, staff recommends that the Planning Board **approve** this amendment request.
- **Architectural Standards and Guidelines, A. General Architectural Framework (page 546)** - Two standards for commercial buildings require articulation of building façades longer than 60 feet and require rear elevations be designed with an inviting appearance with similar features as the main entrance. Architectural elevations submitted with the DSP do not fully address these requirements. The applicant has proffered to revise elevations, prior to certification of the DSP, to show conformance with the intent of the requirements. A condition has been included in the Recommendation section for the submission of updated elevations. Therefore, an amendment will not be necessary.
- **Architectural Standards and Guidelines, A. General Architectural Framework (page 546)** - A general architectural framework standard requires buildings be sited to reinforce the build-to line and define the street wall. As previously noted, the proposed building location does not conform to the build-to line standard. Given the scale and intended use of the proposed building, the build-to line provided is

appropriate for this project. Therefore, staff recommends that the Planning Board **approve** this amendment request.

- **Architectural Standards and Guidelines, D. Fenestration, Commercial (page 547)** - A minimum of 60 percent of the ground story façades of commercial buildings is required to be comprised of windows. The proposed McDonald's building provides 33 percent fenestration on the east façade (facing Brightseat Road), 13 percent on the south façade (facing MD 214), 1.2 percent on the west façade, and 3.2 percent on the north façade. Most windows are located around the public dining area. Given the building's use, much of its interior space is occupied by kitchen, storage, and equipment space for which windows are not traditionally provided. While the percentage of window space provided is significantly less than the requirement, appropriate fenestration is provided where needed for this use. Therefore, staff recommends that the Planning Board **approve** this amendment request.
- **Landscaping Standards and Guidelines, B. Street Trees (page 551)** - Street trees are required to be planted a minimum of 35 feet on center along planting strips or tree grates. The presence of subsurface and overhead utilities along the site's frontage on MD 214 and Brightseat Road prohibits the provision of street trees, in conformance with this requirement. The DSP provides for eight shade trees and a continuous line of shrub plantings in a landscape strip between the sidewalk and internal drive aisles. This alternative proposal meets the intent of the street tree requirement to the degree feasible. Therefore, staff recommends that the Planning Board **approve** this amendment request.
- **Landscaping Standards and Guidelines, D. Dooryard Area (page 552)** - Commercial frontages are required to be comprised primarily of hardscape and include planters and street furnishings. The DSP provides an outdoor seating area with furnishings, hardscape, and plantings adjacent to the east side of the building. It would not be practical for these features to be provided in the dooryard area. The location and type of features provided is appropriate for this development and harmonious with the intent of the requirement. Therefore, staff recommends that the Planning Board **approve** this amendment request.
- **Parking and Loading Standards and Guidelines, A. General (page 554)** - Proposed buildings must meet the frontage occupancy requirements for the perimeter of a block when considering the parking requirements. The proposed project does not conform with the frontage occupancy requirements of the D-D-O Zone, as previously noted. However, the site design is appropriate for the scale of development and intended use, with no parking between the building and the frontage. Parking is otherwise provided, in conformance with parking and loading standards. Therefore, staff recommends that the Planning Board **approve** this amendment request.
- **Parking and Loading Standards and Guidelines, D. Loading and Service Areas (page 556)** - Loading areas must be in the interior of a block, behind buildings. This DSP provides a single loading space adjacent to the drive-through lane at the southwest corner of the proposed building. This location is not interior to the site but is as far as practical from the abutting residential development to the north and

is appropriate for this site. Therefore, staff recommends that the Planning Board **approve** this amendment request.

8. Prince George's County Zoning Ordinance: The DSP application has been reviewed for compliance with the requirements of the M-U-I Zone; Part 10C, Military Installation Overlay (M-I-O) Zone; and the requirements of the D-D-O Zone of the Zoning Ordinance:

a. Section 27-546.19(c), Site Plans for Mixed Uses, of the Zoning Ordinance requires that:

(c) A Detailed Site Plan may not be approved unless the owners shows:

- 1. The site plan meets all approval requirements in Part 3, Division 9;**
- 2. All proposed uses meet applicable development standards approved with the Master Plan, Sector Plan, Transit District Development Plan, or other applicable plan;**

The site plan does not meet all the applicable site design guidelines and development district standards of the Subregion 4 Master Plan and SMA, as discussed in Finding 7. Where development district standards were not met, the applicant has requested amendments.

- 3. Proposed uses on the property will be compatible with one another;**
- 4. Proposed uses will be compatible with existing or approved future development on adjacent properties and an applicable Transit or Development District, and;**

The legal, nonconforming, single-use of the site as an eating and drinking establishment with drive-through service has existed on the site for 40+ years and is proposed to be continued. Therefore, it can be assumed to be compatible with the existing development on adjacent properties.

- 5. Compatibility standards and practices set forth below will be followed, or the owner shows why they should not be applied:**

(A) Proposed buildings should be compatible in size, height, and massing to buildings on adjacent properties;

The adjacent properties to the north are single-family detached and the property to the west is a multistory office building. The proposed building is an appropriate size and scale for its use and is compatible with existing development in the MD 214 corridor.

(B) Primary facades and entries should face adjacent streets or public walkways and be connected by on-site walkways, so pedestrians may avoid crossing parking lots and driveways;

The primary façades of the building face north toward the on-site parking lot and east to Brightseat Road. The southern façade of the building, facing MD 214, includes the drive through. An outdoor eating area and eastern entrance to the building are connected by internal sidewalks to existing sidewalks on Brightseat Road and MD 214. Where pedestrian crossings could not be avoided, crosswalks are provided. Special paving is shown for drive-through lane crosswalks to further define the shared travel space close to the building.

(C) Site design should minimize glare, light, and other visual intrusions into and impacts on yards, open areas, and building facades on adjacent properties;

The photometric plan provided with the application indicates that the proposed lighting design will minimize glare, light, and visual intrusion into nearby properties and buildings.

(D) Building materials and color should be similar to materials and colors on adjacent properties and in the surrounding neighborhoods, or building design should incorporate scaling, architectural detailing, or similar techniques to enhance compatibility;

The materials and colors selected to face the proposed building are compatible with those utilized in similar scale developments. The materials proposed include brick veneer and Hardie planks in complimentary muted color tones. The applicant has proffered to revise the building design to include additional architectural details, in conformance with D-D-O Zone standards, as discussed in Finding 7.

(E) Outdoor storage areas and mechanical equipment should be located and screened to minimize visibility from adjacent properties and public streets;

The DSP does not propose outdoor storage areas and includes appropriate screening of mechanical equipment. Rooftop equipment will be behind a parapet wall and an outdoor transformer is screened with landscaping to minimize visibility from adjacent properties and public streets.

- (F) Signs should conform to the applicable Development District Standards or to those in Part 12, unless the owner shows that its proposed signage program meets goals and objectives in applicable plans; and**

The signage program provided by this DSP conforms with the D-D-O Zone standards.

- (G) The owner or operator should minimize adverse impacts on adjacent properties and the surrounding neighborhood by appropriate setting of:**

- (i) Hours of operation or deliveries;**

The applicant did not indicate the proposed hours of operation or deliveries for the development. However, the single loading space is provided as far as practical from the adjacent residential development to the north to minimize potential adverse impacts.

- (ii) Location of activities with potential adverse impacts;**

The loading space, trash enclosure, and drive-through service have been located on the site to minimize potential adverse impacts to the adjacent residential properties to the north. Adequate queuing space is provided for the drive-through service to limit impacts to access driveways to the property and adjacent public roadways.

- (iii) Location and use of trash receptacles;**

The existing trash corral is proposed to be replaced with a modern trash enclosure in approximately the same location in the northwest quadrant of the site.

- (iv) Location of loading and delivery spaces;**

One loading and delivery space is provided in the southwest quadrant of the site, as far as practical from adjoining residential uses to the north. The location of the loading space does not impact on-site circulation.

(v) Light intensity and hours of illumination; and

The photometric plan included with the DSP confirms that there are minimal adverse impacts on adjacent properties and the surrounding neighborhood from the proposed building.

(vi) Location and use of outdoor vending machines.

No outdoor vending machines are proposed by this DSP.

- b. The subject application is in the M-I-O Zone for height associated with Joint Base Andrews and is subject to Section 27-548.54, Requirements for Height, of the Zoning Ordinance. The proposed building has a maximum height of approximately 19 feet. This is below the height of nearby buildings and is estimated to be well below the applicable requirement for height for the site. However, a condition has been included in the Recommendation section for the applicant to submit calculations for certification per Section 27-548.54(e)(2)(B).
- c. Section 27-548.25(b) requires that the Planning Board find that the site plan meets the applicable development district standards to approve a DSP. As discussed in Finding 7 above, this DSP requests multiple amendments to applicable D-D-O Zone standards. Staff believes that the requested amendments to the development standards address specific site design challenges and modernize the site for continuation of its legal use as an eating and drinking establishment with drive-through service. The DSP generally conforms to the overall intent of the design criteria and would not substantially impair implementation of the Subregion 4 Master Plan and SMA.
9. **2010 Prince George’s County Landscape Manual:** The Subregion 4 Master Plan and SMA states that the provisions of the 2010 *Prince George’s County Landscape Manual* (Landscape Manual) for Section 1.3 for Alternative Compliance, and Sections 4.2, 4.3, and 4.7 do not apply within the D-D-O Zone. Therefore, the proposed development is only subject to the requirements of Section 4.4 and Section 4.9 of the Landscape Manual. Staff has reviewed the submitted landscape plans and determined that they conform with the requirements of these sections.
10. **Prince George’s County Woodland and Wildlife Habitat Conservation Ordinance:** The site has a Woodland Conservation Exemption Letter, S-014-2019, issued on July 25, 2019, and is exempt from demonstrating conformance with the Woodland and Wildlife Habitat Conservation Ordinance.
11. **Prince George’s County Tree Canopy Coverage Ordinance:** The subject site is in the M-U-I Zone, and a 10 percent tree canopy coverage (TCC) requirement applies, per Section 25-128(b) of the County Code. The site’s 1.17-acre area requires 0.12 acre, or 5,097 square feet, to be provided in TCC. The DSP provides 5,750 square feet of TCC, satisfying this requirement.

- 12. Referral Comments:** The subject application was referred to the concerned agencies and divisions. The referral comments are summarized, as follows, and are incorporated herein by reference:
- a. **Historic**—In a memorandum dated May 1, 2020 (Stabler and Smith to Bossi), it was noted that the subject property does not contain and is not adjacent to any Prince George’s County historic sites or resources. This proposal will not impact any historic sites, historic resources, or known archeological sites. A Phase I archeology survey is not recommended.
 - b. **Community Planning**—In a memorandum dated June 8, 2020 (Tariq to Bossi), the Community Planning Division provided an analysis of the subject DSP’s conformance with the standards and guidelines of the Subregion 4 Master Plan and SMA. Several recommendations were noted and are reflected in revisions to the DSP. In addition, as recommended, a condition has been included to clarify the proposed development conforms with the M-I-O Zone height requirement.
 - c. **Transportation**—In a memorandum dated June 16, 2020 (Ryan to Bossi), the Transportation Planning Section noted that proposed site access, circulation, and parking are adequate. There are no issues and from the standpoint of transportation, it is determined that this plan is acceptable and meets the finding required for a DSP, as described in the Zoning Ordinance.
 - d. **Trails**—In a memorandum dated June 16, 2020 (Ryan to Bossi), staff concluded that the multimodal transportation, site access, and circulation layout and features proposed by the DSP are acceptable and generally consistent with site design guidelines. Recommendations were provided for the addition of driveway crosswalks and details regarding bicycle racks.
 - e. **Environmental Planning**—In a memorandum dated June 8, 2020 (Schneider to Bossi), the Environmental Planning Section noted that the site has a Natural Resources Inventory Equivalency Letter (NRI-094-2019) which was issued on July 25, 2019. The site also has an approved Stormwater Management (SWM) Concept Plan 25146-2019-00 that is valid until October 16, 2022. The approved concept plan is consistent with the DSP. No unsafe soils containing Christiana complexes or Marlboro clays are associated with this site.
 - f. **Prince George’s County Fire/EMS Department**—In an email dated June 1, 2020 (Reilly to Bossi) the Fire Department noted they have no comments on the DSP.
 - g. **Prince George’s County Department of Permitting, Inspections and Enforcement (DPIE)**—In a memorandum dated May 22, 2020 (Giles to Bossi), DPIE indicated that the DSP is consistent with the approved SWM Concept Plan 25146-2019-00 and that they have no objections to the proposed project.
 - h. **Prince George’s County Police Department**—In a memorandum dated May 14, 2020 (Contic to Planner Coordinator, Urban Design), the Police Department noted that they have no comments on the DSP.

- i. **Prince George’s County Health Department**—In a memorandum dated June 1, 2020 (Kpadeh to Bossi), the Health Department provided comments regarding construction-time noise and dust controls. Comments were also provided regarding the number of fast-food options in the area and ties to obesity, as well as regarding potential air quality impacts associated with car idling and drive-through service usage.
 - j. **Maryland State Highway Administration (SHA)**—In an email dated May 15, 2020 (Woodroffe to Bossi), SHA provided preliminary comments on the DSP and noted that more comprehensive comments would be provided with an SHA tracking number once detailed engineering plans are provided to SHA.
 - k. **Washington Suburban Sanitary Commission (WSSC)**—In attachments to an email dated May 13, 2020 (Leeth to Bossi), WSSC provided standard comments on the subject DSP. WSSC’s requirements will be enforced through their separate permitting process.
- 13.** The subject application adequately takes into consideration the requirements of the D-D-O Zone and the Subregion 4 Master Plan and SMA. The amendments to the development district standards required for this development would benefit the development and the development district, as required by Section 27-548.25(c), and would not substantially impair implementation of the sector plan.

As required by Section 27-285(b) of the Zoning Ordinance, the DSP represents a reasonable alternative for satisfying the site design guidelines of Subtitle 27, Part 3, Division 9, of the Prince George’s County Code, without requiring unreasonable cost and without detracting substantially from the utility of the proposed development for its intended use.

- 14.** Per Section 27-285(b)(4), which became effective on September 1, 2010, a required finding for approval of a DSP is as follows:

(4) The Planning Board may approve a Detailed Site Plan if it finds that the regulated environmental features have been preserved and/or restored in a natural state to the fullest extent possible.

No regulated environmental features are located on the subject site; thus, preservation or restoration on-site is not applicable.

RECOMMENDATION

Based upon the foregoing evaluation and analysis, the Urban Design staff recommends that the Planning Board adopt the findings of this report and:

- A. APPROVE the alternative development district standards for:
 - 1. **Building Envelope Standards and Guidelines, B. Mixed-Use, Commercial and Institutional Types (page 540)** - To allow for variable build-to lines of approximately 66 feet to 68 feet from MD 214 and approximately 120 feet to 127feet from Brightseat Road; to allow side and rear yard setbacks greater than

10 feet, as provided on the DSP; and to allow a frontage occupancy of 34 percent on MD 214.

2. **Street Type Standards and Guidelines, A. Typical Primary Mixed-Use Street: Sidewalk (page 542)** - To provide a 10-foot-wide landscape strip with a decorative fence and plantings between sidewalks and the drive-through lane, in lieu of required tree pits and dooryard.
 3. **Architectural Standards and Guidelines, A. General Architectural Framework (page 546)** - To allow for the building not to be sited to reinforce the build-to line, as provided on the DSP.
 4. **Architectural Standards and Guidelines, D. Fenestration, Commercial (page 547)** - To allow for fenestration of 33 percent on the east façade, 13 percent on the south façade, 1.2 percent on the west façade and 3.2 percent on the north façade.
 5. **Landscaping Standards and Guidelines, B. Street Trees (page 551)** - To provide eight shade trees and a continuous line of shrub plantings in a landscape strip between the sidewalk and internal drive aisle, in lieu of street trees.
 6. **Landscaping Standards and Guidelines, D. Dooryard Area (page 552)** - To provide an outdoor seating area with furnishings, hardscape, and plantings adjacent to the east side of the building, in lieu of within the dooryard area.
 7. **Parking and Loading Standards and Guidelines, A. General (page 554)** - To allow for 34 percent frontage occupancy, with no parking between the building and the frontage.
 8. **Parking and Loading Standards and Guidelines, D. Loading and Service Areas (page 556)** - To allow for a loading area in the southwest quadrant of the site, instead of interior to the site.
- B. APPROVE Detailed Site Plan DSP-19060 for McDonald's Landover, subject to the following conditions:
1. Prior to certification, the applicant shall revise the plans as follows or provide the specified documentation:
 - a. Provide a detail for the bike racks, which shall be inverted U-style or a style that provides two points for contact for each bicycle.
 - b. Provide a continental style crosswalk across the driveway from Brightseat Road, unless modified by the Prince George's County Department of Permitting, Inspections and Enforcement, with written correspondence.
 - c. Provide a continental style crosswalk across the driveway from MD 214 (Central Avenue), unless modified by the Maryland State Highway Administration, with written correspondence.

- d. Revise architectural elevations to provide additional articulation and visual interest on the northern and western façades, in accordance with applicable development district standards.
- e. Provide Military Installation Overlay Zone height calculations to demonstrate conformance with Section 27-548.54(e)(2)(B) of the Prince George's County Zoning Ordinance.

ITEM: 7

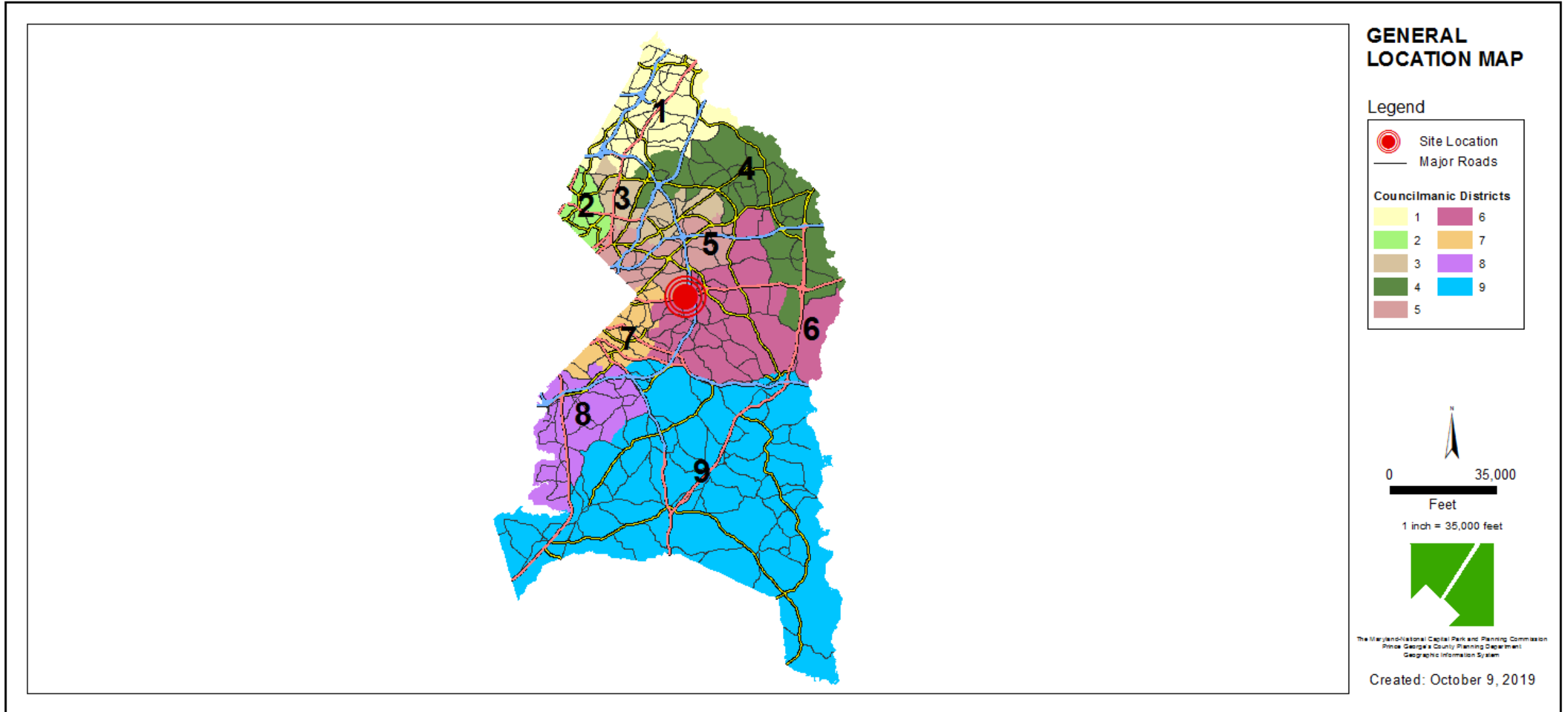
CASE: DSP-19060

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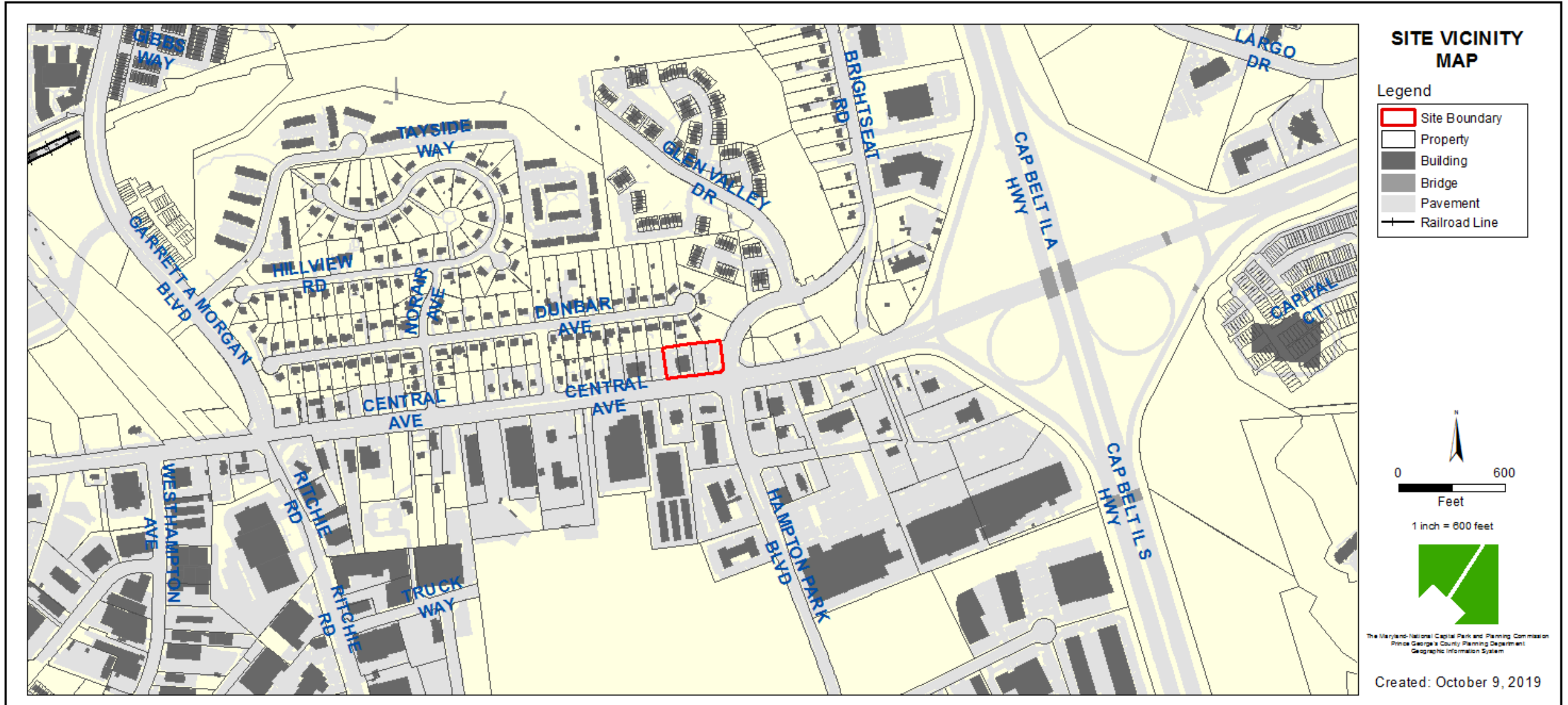
THE PRINCE GEORGE'S COUNTY PLANNING DEPARTMENT



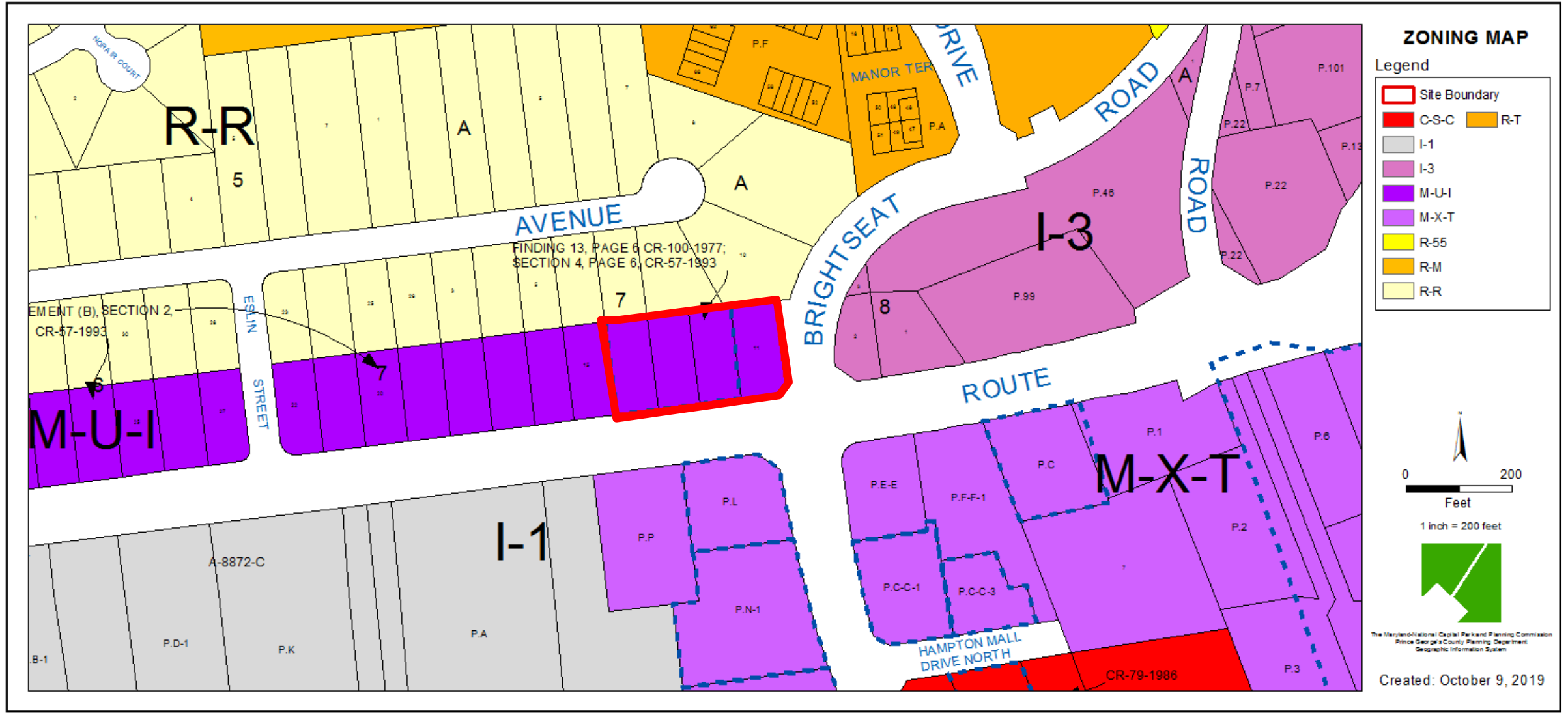
GENERAL LOCATION MAP



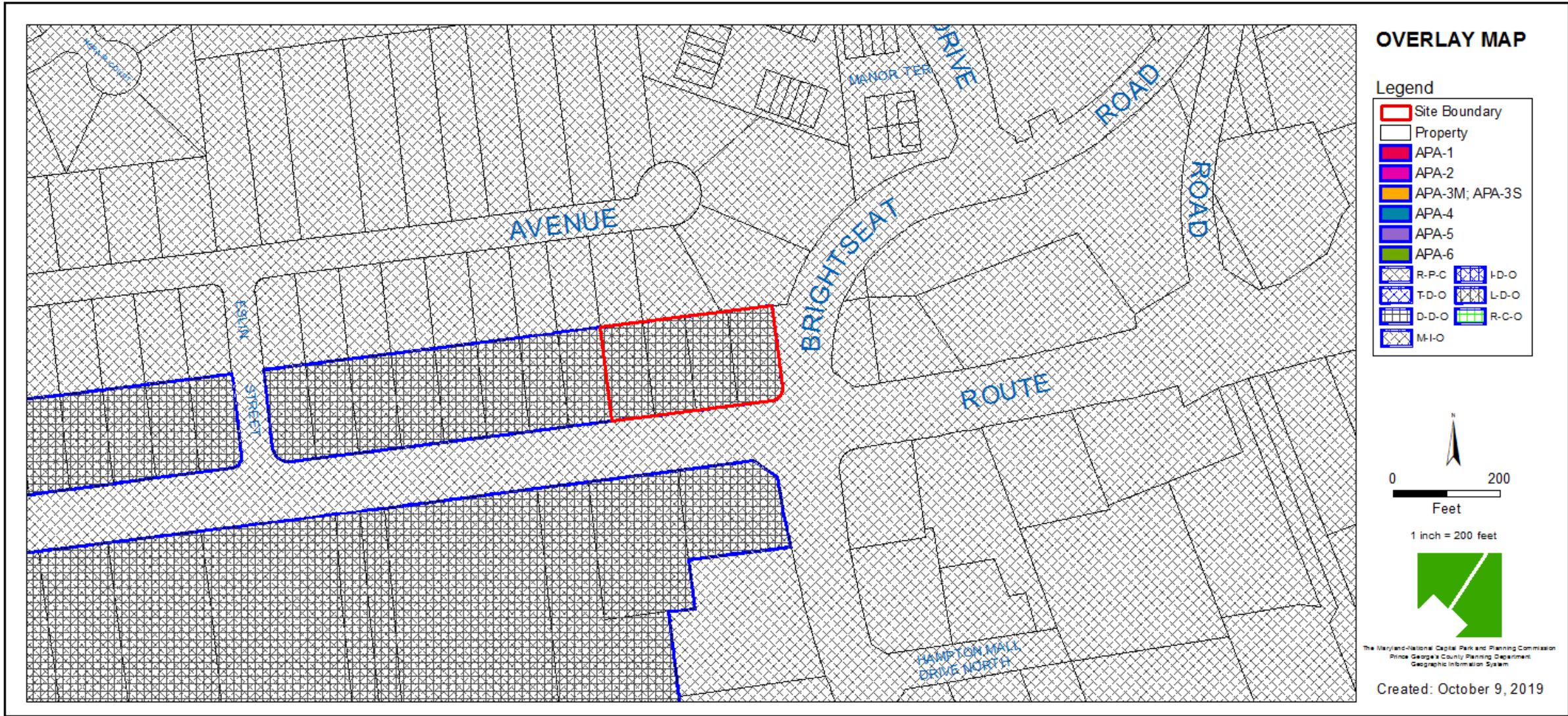
SITE VICINITY



ZONING MAP



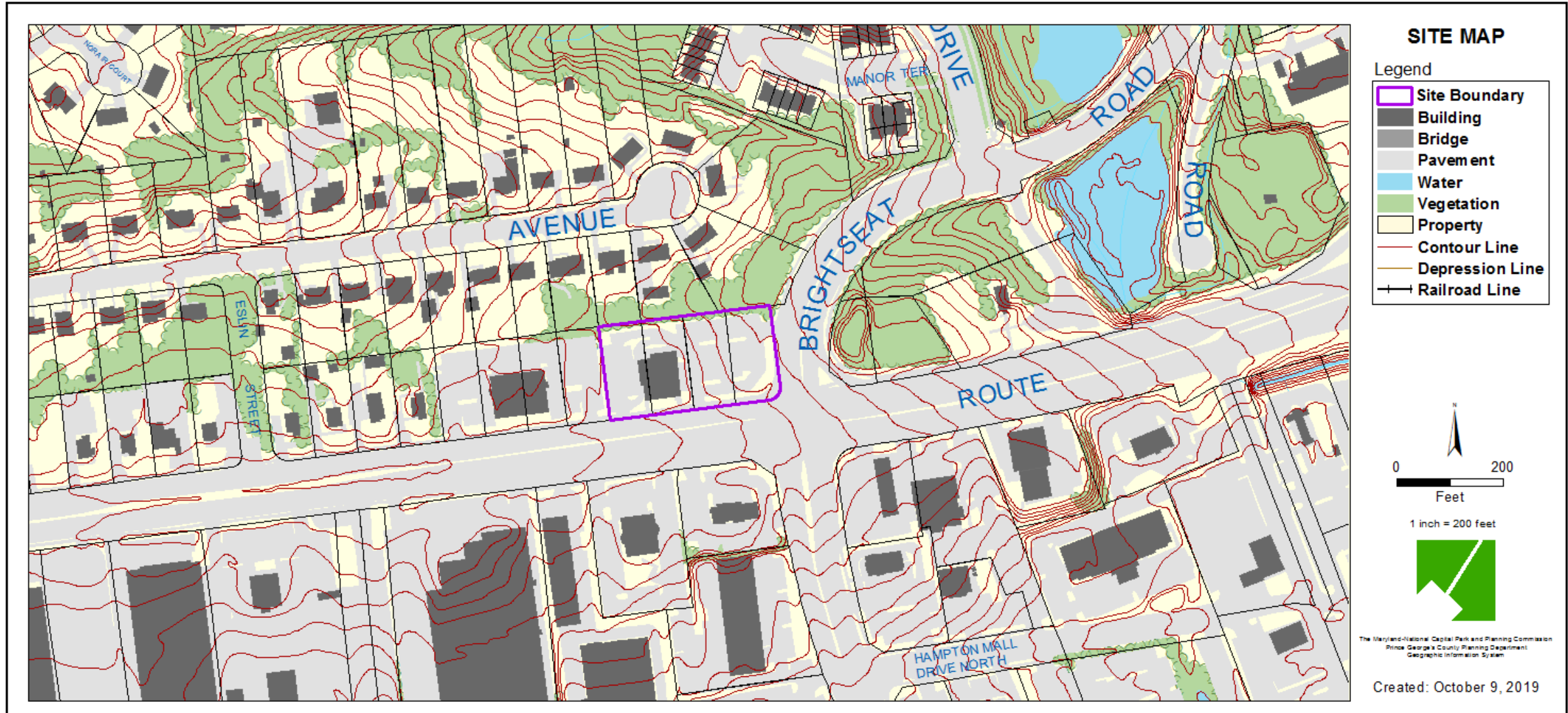
OVERLAY MAP



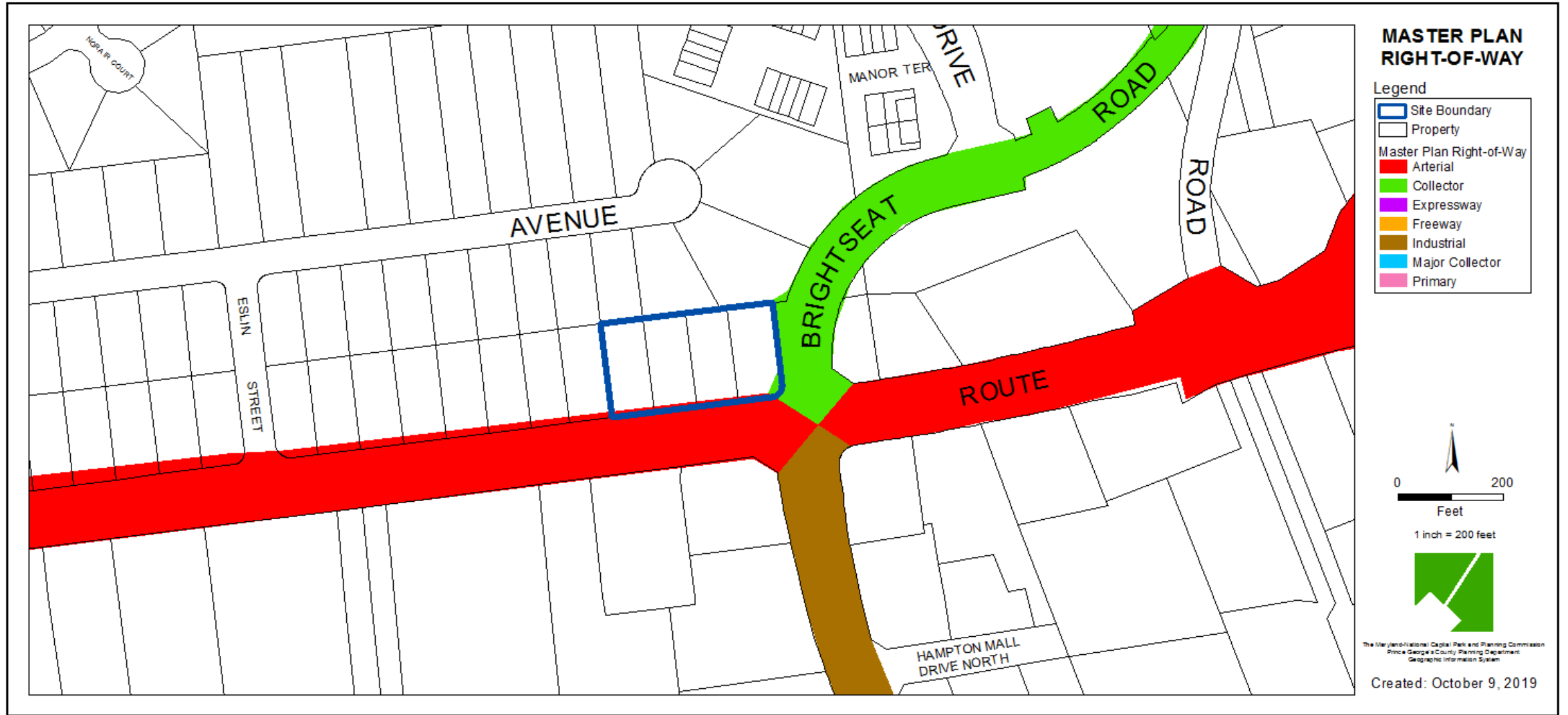
AERIAL MAP



SITE MAP



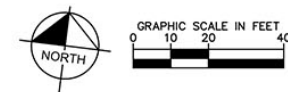
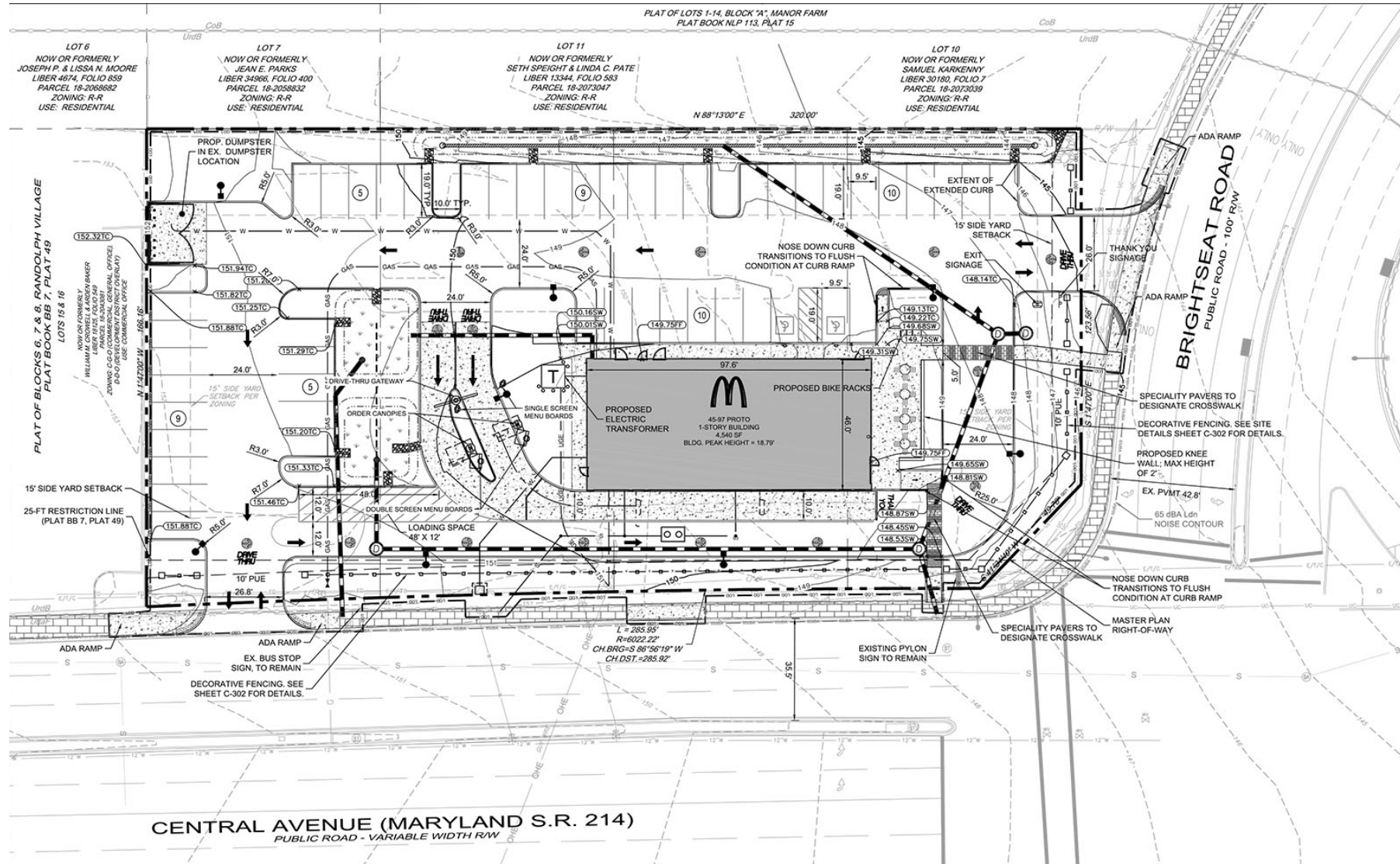
MASTER PLAN RIGHT-OF-WAY MAP



BIRD'S-EYE VIEW WITH APPROXIMATE SITE BOUNDARY OUTLINED



SITE PLAN



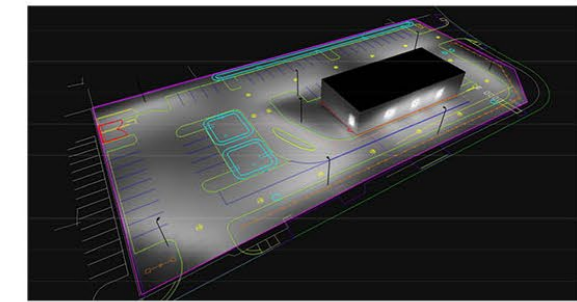
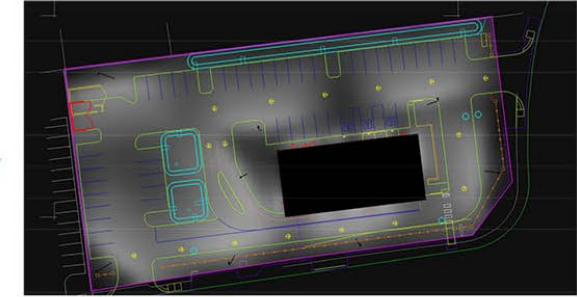
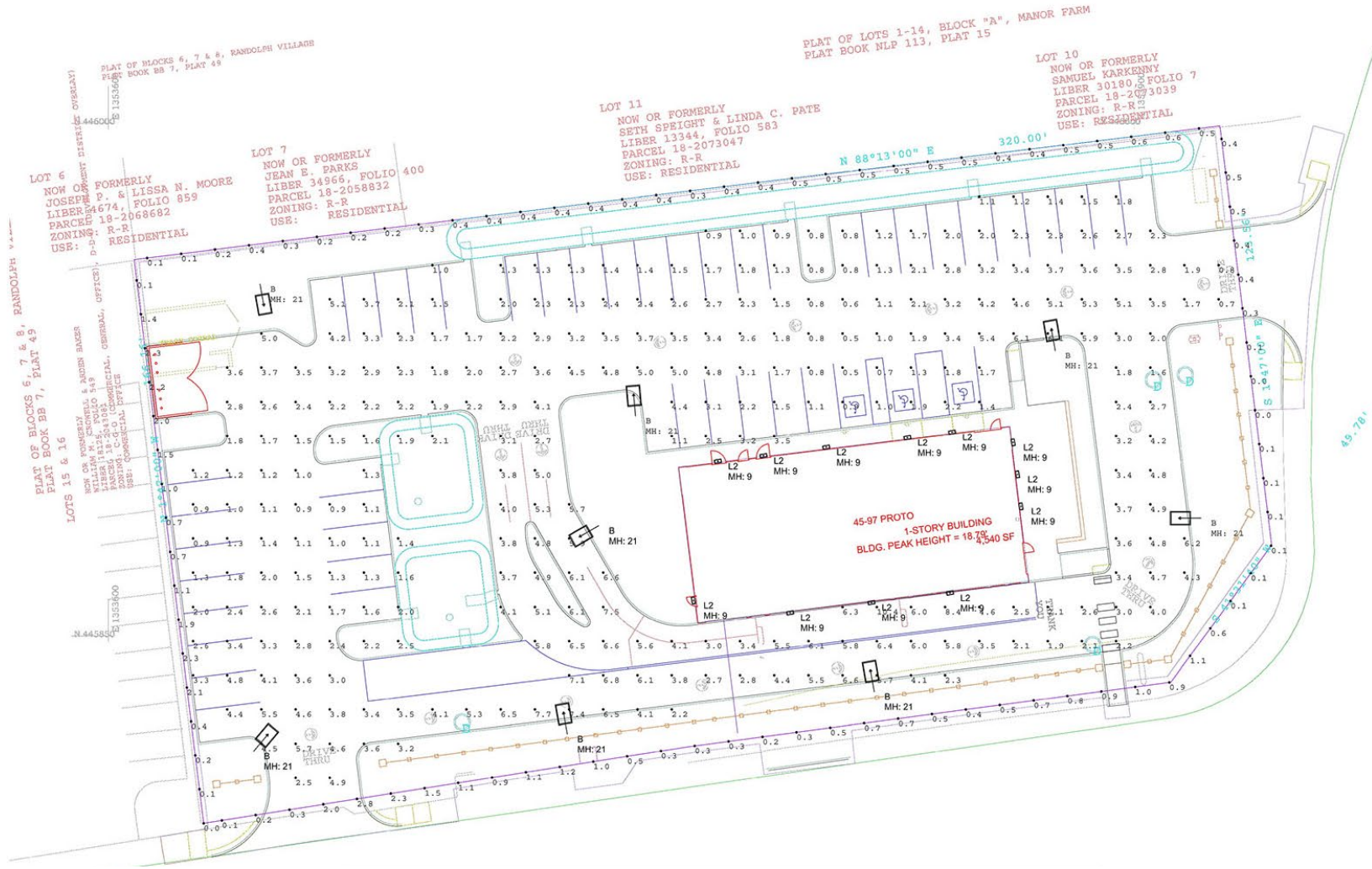
RENDERED LANDSCAPE PLAN



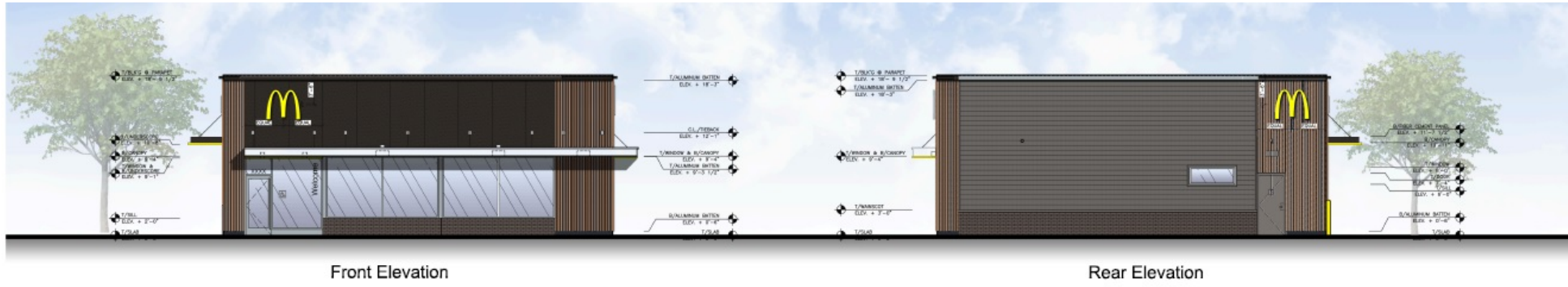
PHOTOMETRIC PLAN

NOTES:

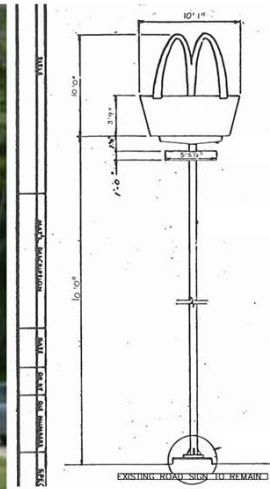
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- 2. DISTANCE BETWEEN READINGS: 10'



ELEVATIONS



SIGNAGE

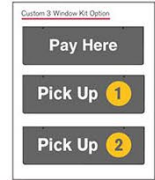
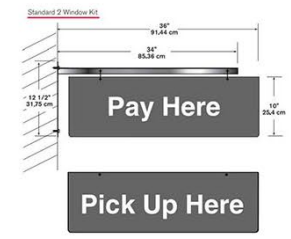


Welcome Sign



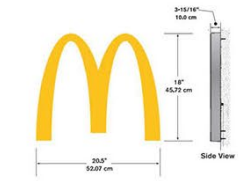
Everbrite, LLC, 315 Marion Ave, South Milwaukee, WI 53172 P: 888-857-4078 F: 877-430-7363 www.everbrite.com

NextGen Window Position Signs



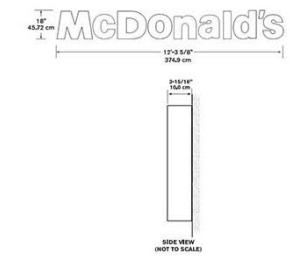
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18\"/>



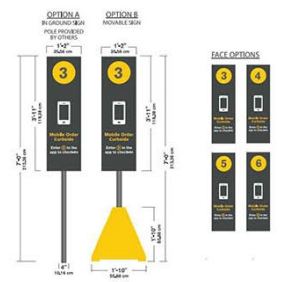
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NextGen 18\"/>



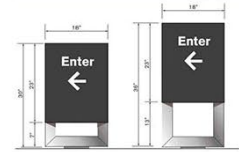
Everbrite, LLC, 315 Marion Ave, South Milwaukee, WI 53172 P: 888-857-4078 F: 877-430-7363 www.everbrite.com

Mobile Ordering Curb Signage



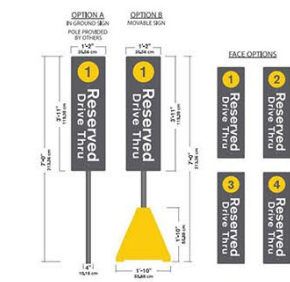
Everbrite, LLC, 315 Marion Ave, South Milwaukee, WI 53172 P: 888-857-4078 F: 877-430-7363 www.everbrite.com

Next Gen 3 Sq. Ft. Directionals



Everbrite, LLC, 315 Marion Ave, South Milwaukee, WI 53172 P: 888-857-4078 F: 877-430-7363 www.everbrite.com

Pull Forward Sign



Everbrite, LLC, 315 Marion Ave, South Milwaukee, WI 53172 P: 888-857-4078 F: 877-430-7363 www.everbrite.com



STATEMENT OF JUSTIFICATION
McDonald's Landover
DSP-19060

APPLICANT: McDonald's Corp
6301 Rockledge Drive, Suite 1100
Bethesda, MD 20817

CORRESPONDENT: Daniel F. Lynch, Esq.
McNamee Hosea
6411 Ivy Lane, Suite 200
Greenbelt, Maryland 20770
(301) 441-2420 Voice
(301) 982-9450 Fax
dlynch@mhlawyers.com

REQUEST: Detailed Site Plan in accordance with Sections 27-548.25 and 27-285(b) the Zoning Ordinance and amendments to the District Development Standards in accordance with Section 27-548.26(b) of the Zoning Ordinance.

I. DESCRIPTION OF PROPERTY

1. Addresses –8710 Central Avenue, Hyattsville, Maryland 20785
2. Proposed and Use –Eating and drinking establishment with drive-through service
3. Election District – 113
4. Lots – 11, 12, 13 and 14
5. Total Area – 1.17 acres
6. Tax Map – 67/C4
7. Location – Located on the north side of Central Avenue at its intersection with Brightseat Road
8. Zoned – M-U-I (with DDOZ overlay)
9. Owners –McDonald's Corp

10. Zoning Map –210NE08

11. Incorporated Area – None

II. NATURE OF REQUEST

The applicant is proposing to raze and rebuild the existing McDonald’s restaurant with a new and modern restaurant. The applicant is proposing a 4,540 square foot store with 69 seats. There will be 48 parking spaces located to the north and west of the restaurant building. A dual ordering point drive-through is being provided. The applicant is also providing an outdoor dining area that have a pedestrian connection to the existing sidewalk located along Central Avenue. The applicant is also provide a decorative fence along its street frontage. The decorative columns associated with this fence will incorporate brick to match the brick used in the restaurant building.

The subject property is currently accessed via two right-in/right-out driveways on Central Avenue and one right-in/right-out access drive on Brightseat Road. As shown on the Detailed Site Plan, the applicant is proposing to maintain one access driveway on Central Avenue and one access driveway on Brightseat Road. One access driveway on Central will be closed.

II. PROCEDURAL HISTORY

The McDonald’s restaurant was established as a permitted eating and drinking establishment in the C-O Zone. Subsequent to its establishment, the Zoning Ordinance was amended to define fast food restaurants and to further provide that fast food restaurants would not be permitted in the C-O Zone. The McDonald’s restaurant use was certified as non-conforming in 1987. Special Exception SE-3805 was approved for the purpose of permitting an alteration of the nonconforming use on the property. As part of that approval, the applicant was permitted to add a drive-through cashier window, increase seating, add a vestibule and increase parking. SE-3805 was approved by the Zoning Hearing Examiner on December 21, 1987. On June 2, 1994, the Planning Board approved ROSP-3805 and VSE-3805 for the purpose of addressing the impact of the widening of Central Avenue on the required 10 foot setback and 4.2 landscape strip. As demonstrated on the existing conditions plan, the site has been developed in conformance with SE-3805 and the subsequent revision. The existing restaurant is contains 150 seats and is serviced by 69 parking spaces.

III. CRITERIA FOR APPROVAL

Section 27-285. Planning Board Procedures.

(b) Required findings.

- (1) The Planning Board may approve a Detailed Site Plan if it finds that the plan represents a reasonable alternative for satisfying the site design guidelines, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use;

COMMENT: The plan does represent a reasonable alternative for satisfying the site design guidelines. The site design guidelines are found in the Sector Plan, the intent of which is to regulate the design and character of the Central Avenue focal area. The intent of these standards and guidelines is to ensure the creation of vibrant urban neighborhoods with the focal areas.

IV. M-U-I/D-D-O ZONE REQUIREMENTS

In approving a detailed site plan in the DDOZ, Section 27-548.25(b) provides that the Planning Board shall find that the site plan meets applicable Development District Standards. In addition, Section 27-548(c) provides that if the applicant so requests, the Planning Board may apply development standards, which differ from the Development District Standards, most, recently approved or amended by the District Council, unless the Sectional Map Amendment text specifically provides otherwise. The Planning Board shall find that the alternate Development District Standards will benefit the development and the Development District and will not substantially impair implementation of the Master Plan, Master Plan Amendment, or Sector Plan.

The eating and drinking establishment with drive-through service was in operation on the subject property on March 2, 2010, the date of the adoption of the *Subregion 4 Master Plan and Sectional Map Amendment*, and the use is therefore a legal nonconforming use.

7. Nonconforming buildings, structures, and uses. Restoration or reconstruction of a nonconforming building or structure, or a certified non-conforming use, is exempt from the development district standards and from site plan review if it meets the requirements of Section 27-243(a)(1) of the Zoning Ordinance.

Except for improvements listed in “8. General”, below, a property may not expand a certified nonconforming use or a use or a structure that was lawful on the date of the SMA approval but does not conform to the development district standards, unless a detailed site plan is approved with findings that the expansion is compatible with adjacent uses and meets the goals of the master plan.

“8” provides:

8. General. The following are exempt from the development district standards and site plan review if the existing or proposed use is permitted:

- a. Permits for alternation or rehabilitation, with no increase of the existing gross floor area
- b. Canopies
- c. Fences of six feet in height or less for rear and side yards and made of wood or masonry (not concrete block) are exempt
- d. Decks
- e. Ordinary maintenance
- f. Changes in occupancy
- g. Changes in ownership

The proposed redevelopment as outlined in Section II above exceeds the improvements set forth in “8” and therefore detailed site plan approval is required. In addition, once the structure is removed

from the subject property and the use ceases to operate, the applicant will have 180 days to complete construction and reoccupy the property before the nonconforming use status is relinquished under Section 27-241 of the Zoning Ordinance.

As noted above, in approving a detailed site plan in the DDOZ, Section 27-548.25(b) provides that the Planning Board shall find that the site plan meets applicable Development District Standards. In addition, Section 27-548(c) provides that if the applicant so requests, the Planning Board may apply development standards, which differ from the Development District Standards

Sheets 310 and 311 of the Detailed Site Plan contain a compliance chart relative to the District Development Standards. The applicant complies with those standards (See full analysis attached hereto as Exhibit A) except as noted below:

General Site Standards and Guidelines

Build-to line—The build-to line is the line to which buildings are required to be built on a lot (no setback is permitted from it, except as noted in the guidelines). Consequently, multifamily, mixed-use, and nonresidential buildings, located within the DDOZ areas, must be located along a continuous street wall defined by the build-to line (see the residential, commercial, and industrial sections of the Building Envelope Standards for specific standards)

COMMENT: The build-to line along Central Avenue is 18 feet from the back of curb. The proposed restaurant building is set back 28 feet from the back of curb. The applicant requests an amendment to the build-to line development standard.

Mixed-Use, Commercial and Institutional Types

A. Building Heights

Principal Building – 4 stories
Accessory Buildings – 2 stories
Ground Floor – 14 feet
Upper Floors – 8 feet

B. Lot Dimensions – Lot Width; 30 feet

C. Lot Occupancy – Building Lot Coverage; 75% Max.

D. Build-to line and Setbacks

Central Avenue - 18 feet from back of curb.
Brightseat Road - 18 feet from back of curb.

E. Encroachments

Awnings and Canopies; 4 ft depth min.
Bay Window; 3 ft. max.
Balcony; 3 ft.

Other Architectural Elements; Permitted

COMMENT: The build-to line for commercial uses is 18 feet and the maximum setback is 10 feet. The proposed McDonald's building is set back 28 feet from back of curb and centrally located within the site due to drive-thru width and bypass lane. The applicant explored a number of options to the site layout, but due to the layout of the proposed building as well as McDonald's standards relative to internal traffic flow and drive-through queuing, it is unable to meet the build-to line requirement. For example, the applicant did explore locating the drive-through on the interior side of the building, but layout would require passenger side pick up at the drive through window. In addition, if the building is rotated with the front facing Central Avenue, the applicant would be prevented from providing 360 degree vehicular circulation and would also be prevented from providing an adequate queuing area for the drive-through. It is necessary to the operation of the drive-through component that a bypass lane be provided adjacent to the drive-through lane. This bypass lane not only allows for the free flow of vehicles in a one-way direction around the site, but also allows vehicles to exit the drive-through lane. As an alternative, the applicant is proposing an outdoor dining area along the east side of the building. The applicant is also proposing a pedestrian connection between that eating area and the sidewalk along Central Avenue. The applicant believes that this outdoor eating area and pedestrian connection will create an attractive, lively and inviting pedestrian friendly environment. The applicant believes that this alternative to the District Development Standards will be a benefit to this development since it will address underlying limitations of the drive-through use and provide for a safe circulation pattern throughout the site. The applicant also contends that the outdoor eating area and pedestrian connection implement the District Development Plan's intent by creating a public space on the property and also creating a more pedestrian friendly environment than exists today and therefore in compliance with the overall intent of this standard.

The applicant is proposing a 1 story building that is 18.79 feet in height. The building occupies less than 75% of the property and the lot is 285± feet in width. The applicant is not proposing any encroachments into the build-to line area. That being said, with the exception of the build-to line standard, the Detailed Site Plan complies with the standards cited above.

As to the frontage occupancy of 80%, the proposed building occupies 34% of the property frontage on Central Avenue and the applicant is requesting an amendment to this standard. The subject property is very wide for a single retail use and compliance with this standard for a single retail use is not feasible. In order to create a larger presence along the street frontage, the applicant is proposing a fence and decorative posts along its frontage. The applicant believes that this fence will create a decorative street wall and will improve the overall aesthetics of this site and make the street frontage along Central Avenue more pedestrian friendly.

Street Type Standards

Typical Primary Mixed Use Street (Central Avenue)

Sidewalk

1. Six foot tree pit area

2. **Four to six foot clear walkway**
3. **Eight foot door yard**

Street

1. **4 travel lanes at 11 feet each**
2. **2 bicycle lanes at 4 feet each**
3. **2 parking lanes at 8 feet each**
4. **Minimum 12 ft. median (if used)**

Typical Primary Mixed Use Street (Brightseat Road)

Sidewalk

1. **Six foot tree pit area**
2. **Four to six foot clear walkway**
3. **Eight foot door yard**

Street

1. **4 travel lanes at 11 feet each**
2. **2 bicycle lanes at 4 feet each**
3. **2 parking lanes at 8 feet each**

COMMENT: The 6-foot wide tree boxes along both Central Avenue and Brightseat Road are not viable given that there are underground telecommunication utilities located 4.5 feet from the curb in these areas. In addition, there are overhead wires located 6 to 7 feet from the curb. These existing conditions inhibit the applicant's ability to comply with this standard and therefore the applicant is requesting an amendment to allow the existing conditions (5 foot sidewalk and 5 foot grass strip) to remain in place. The applicant is proposing to add a 10 foot landscape strip between the grass strip and proposed drive aisle so as to screen the proposed drive-through from the street. The applicant is also proposing to construct a decorative fence in the landscape strip. As noted above, the applicant is also proposing to install a decorative fence within that landscape strip area to create a street wall. The applicant believes that the streetscape proposed for this property will not only create a pedestrian friendly environment but will be an attractive addition to this area of Central Avenue.

As for the street standards, the applicant is only required to provide road frontage improvements, which do not include the construction of a full street section and median. The Prince George's County Department of Permitting Inspections and Enforcement and the State Highway Administration have informed applicant that bicycles lanes will not be permitted on Brightseat Road and Central Avenue.

Architectural Standards and Guidelines

A. General Architectural Framework

2. Buildings shall be sited to reinforce the build-to line and define the street wall. Buildings shall shift in massing and have variations in height, profile, and roof form, while maintaining the formal relationship of building placement to the public street frontage.

COMMENT: Again, the applicant is requesting an amendment to this standard since it requires an 18-foot build-to line. The applicant has located the building as close to Central Avenue as feasible given the nature of the use and the applicant's design requirements. The applicant is proposing a fence along its frontage to help create the street wall and is also proposing an outdoor eating area with a pedestrian connections to the sidewalks along Central Avenue and Brightseat Road. . Not only will this combination create visual interest at this corner but will help activate a pedestrian friendly environment by creating a pedestrian gathering place at this very busy intersection along Central Avenue.

D. Fenestration

3. Fenestration shall comprise a minimum of 60% of ground story facades of mixed-use, commercial, and institutional building types and a minimum of 30% on the upper stories of these building types.

COMMENT: 33% of the north elevation (facing Brightseat Road), 14% of the east elevation, 1.2% of the south elevation and 3.2% of the west elevation (facing Central Avenue) contain windows. The applicant is therefore requesting an amendment to this standard. The areas of the building that contain windows are the public areas while these areas that don't contain windows are occupied by coolers, storage and equipment and therefore are not conducive to windows. The applicant is proposing to install spandrel glass in these areas in order to give the appearance of windows and therefore comply with the intent of the standard.

Landscape Standards

B. Street Trees

1. Street trees, which must be of a drought-resistant native species, shall be planted on average a minimum of 35 ft. on center along planting strips or in tree grates of street rights-of-way (Please refer to the *Prince George's County Landscape Manual* for acceptable street tree species). Where necessary, spacing allowances may be made to accommodate curb cuts, fire hydrants, and other infrastructure elements. However, at no location may the spacing exceed 35 ft. on center.

COMMENT The applicant's ability to comply with the installation of street trees that are spaced less than 35 feet on center is not possible given that there are above ground and underground utilities located along the Central Avenue frontage of the property. The applicant is therefore requesting an amendment to this standard.

Parking and Loading Standards

A. General

4. Buildings must meet the frontage occupancy requirements for the perimeter of a block when considering the parking requirements.

COMMENT: The applicant is requesting an amendment to the frontage occupancy requirements, but the parking located to the north of the building and closest to Central Avenue will be screened by the fence and landscape treatment the applicant is proposing along the Central Avenue frontage of the property and therefore will help enhance the pedestrian friendly environment the applicant is creating along this portion of Central Avenue.

C. Loading and Service Areas

1. All loading and service areas must be located in the interior of a block behind buildings.

COMMENT: The loading area is not located behind the building. The applicant explore every possible alternative, but was restricted in locating the loading area behind the building pursuant to Section 27-579(b) which provides:

(b)

No portion of an exterior loading space, and no vehicular entrances to any loading space (including driveways and doorways), shall be located within fifty (50) feet of any Residential Zone (or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan).

The subject property adjoins residential property along its entire northern property line and this limited the applicant's ability to located the loading space and access driveway to any area behind the proposed building. The applicant has located the loading space to an area that complies with Section 27-579(b) and also complies with the screening requirements set forth in 4.4 of the Landscape Manual. Thea applicant is therefore requesting an amendment to this standard.

Overall, the applicant believes that the Detailed Site Plan and the amendments to the District Development Standard requested herein comply with the Policy and Goals of the Subregion 4 Sector Plan. The subject property is located is located in Opportunity Site 3 and the goals and policies of that opportunity site are as follows:

Goals

- Determine the best mix of land uses, densities, and development/design features needed to create a unique sense of place for the established communities of Zone 1 and Subregion 4.
- Develop a hierarchy of commercial-serving areas that balance the needs of residents, transit riders, and regional traffic.

- Reinforce the sustainability of existing communities by promoting commercial mixed-use development and public realm improvements that enhance the quality of life for residents and people in the immediate surrounding area.
- Establish a higher design standard for commercial development and a wider variety and higher-quality businesses throughout the area.
- Attract higher quality, desirable uses to the area that meet current community needs or provide new opportunities for the area.
- Reinforce and strengthen connectivity between living and commercial areas and centers.
- Provide the necessary infrastructure to enhance the pedestrian environment.

Policies

1 Develop a new character and image for Central Avenue that is inviting to pedestrians

- Provide infill and redevelopment opportunities on Central Avenue that create a coherent street image.
- Create development standards that promote new urbanist principles and encourage new infill to occur closer to and consistently along the build-to line.
- Provide for streetscape improvements that promote a pedestrian-friendly environment.

2 Improve pedestrian circulation and create an appealing streetscape character

- Design and install cohesive and visually interesting sidewalk paving patterns and include site furnishings, lighting, and plantings that invite pedestrians.
- Develop and promote “green street” design standards.
- Establish and enforce higher maintenance standards.
- Create and enforce unified sign design standards for existing and new businesses.

The redevelopment of the McDonald’s restaurant on the subject property is consistent with the Goals and Policies of the District Development Plan. The applicant is proposing to improve the pedestrian system along its Central Avenue and Brightseat Road frontages by providing additional trees in the tree pit area and by providing a 10’ landscape strip and decorative fencing between the sidewalk and its internal drive aisle. In addition, the applicant is proposing an outdoor eating area and a pedestrian connection from Central Avenue in order to activate the corner of Central and Brightseat Road. In addition, the revised layout proposes the closure of one access driveway onto Central Avenue which will not only reduce the opportunity for pedestrian and vehicular conflicts but from an aesthetic standpoint, allow for additional improvements to the streetscape.

27-546.19 (c) Site Plans for Mixed Uses.

In addition to the specific District Development Standards, a Detailed Site Plan may not be approved unless the owner shows:

- (1) The site plan meets all approval requirements in Part 3, Division 9;

COMMENT: As noted above, this Detailed Site Plan complies with the approval requirements set forth in Part 3, Division 9.

- (2) All proposed uses meet applicable development standards approved with the Master Plan, Sector Plan, Transit District Development Plan, or other applicable plan;

COMMENT: The applicant is requesting amendments to the District Development Standards in conformance with Section 27-548(c) of the Zoning Ordinance.

- (3) Proposed uses on the property will be compatible with one another;

COMMENT: The applicant is not proposing a mix of uses on the property and therefore this criterion is not applicable.

- (4) Proposed uses will be compatible with existing or approved future development on adjacent properties and an applicable Transit or Development District; and

COMMENT: The subject property adjoins single-family residential to the north and an office use to west. The McDonald's restaurant has been operated from the property for approximately 40 years without any apparent impact on the surrounding uses. The applicant is proposing to bring the site into compliance with not only current retail standards, but also into compliance with local, state and federal regulations that were not in place at the time this restaurant was first established, such as stormwater management. Although the applicant is increasing the square footage of the building, the increase is only 178 square feet and will have little or no impact on the single-family homes on the adjoining properties. On the other hand, the applicant is proposing to reduce the number of access driveways on Central Avenue, improve the overall flow of the drive-through component, increase the amount of landscaping located along the northern property line (where it adjoins the residential uses) and provide a decorative streetscape that will improve the overall appearance of the property. The applicant therefore contends that the development as proposed in this detailed site plan will be compatible with the existing and future development in the DDOZ.

- (5) Compatibility standards and practices set forth below will be followed, or the owner shows why they should not be applied:

- (A) Proposed buildings should be compatible in size, height, and massing to buildings on adjacent properties;

COMMENT: The applicant is proposing a one story, 4,540 square foot building, 178 square feet larger than the existing building on the property. This building is located 80 feet from the northern property line that adjoins the single-family residential dwellings and 28 feet from the property line on Central Avenue. The office building to the west is located approximately 64 feet from the northern property line and 26 feet from the southern property line on Central

Avenue. The proposed layout of the McDonald's restaurant building is similar to that which occurs on the property to the west and given the fact that the building will be located, ___ feet further away from the residential properties, it should be more compatible that the existing conditions.

- (B) Primary facades and entries should face adjacent streets or public walkways and be connected by on-site walkways, so pedestrians may avoid crossing parking lots and driveways;

COMMENT: The front entrance to the building faces Brightseat Road. The applicant is proposing a seating area that will be located between the front entrance and Brightseat Road. In addition, the applicant is proposing a pedestrian connection leading from this seating area to Central Avenue. The crossing area will be constructed of decorative pavers that will visually help direct pedestrian traffic.

- (C) Site design should minimize glare, light, and other visual intrusions into and impacts on yards, open areas, and building facades on adjacent properties;

COMMENT: The applicant is enhancing the landscape buffer located along the northern property line where it adjoins the single-family residential homes. The buffer will also contain a 6-foot high fence that will provide additional screening of this retail commercial. Vehicles entering and located in the drive-through queuing area will be facing south and east away from the single-family residential homes. In other words, the vehicle headlights will be directed away from the residential structures. The applicant is also proposing to install lighting in the parking area that is designed to address the lighting needs of the operation and provide for a safe environment for its customers while not spilling off site onto the adjoining properties.

- (D) Building materials and color should be similar to materials and color on adjacent properties and in the surrounding neighborhoods, or building design should incorporate scaling, architectural detailing, or similar techniques to enhance compatibility;

COMMENT: The surrounding area is development with eclectic styles of buildings, materials and colors. There is no specific architectural trend or building material that seems to have been established in the community. The applicant is proposing a McDonald's restaurant building that is a standard example of one of the franchise's newer architectural models and has a contemporary appearance. The design is of a one-story, hardie plank and brick veneer building with a flat roof. The building is finished predominantly with hardie plank. Brick veneer is proposed mostly in the water table along the base of the building. Decorative aluminum battans are located in each corner of the building. The dining room and main entrance areas oriented towards the intersection of Landover Road and Brightseat Road. Metal canopies are proposed above the dining room windows.

- (E) Outdoor storage areas and mechanical equipment should be located and screened to minimize visibility from adjacent properties and public streets;

COMMENT: The applicant is not proposing outdoor storage areas and the mechanical equipment will be located on the roof of the proposed building and screened by the parapet.

- (F) Signs should conform to applicable Development District Standards or to those in Part 12, unless the owner shows that its proposed signage program meets goals and objectives in applicable plans; and

COMMENT: The TDDP provides:

Guidelines Intent Appropriately sized and placed signage shall enhance and contribute to the architectural character of buildings within the DDOZ.

A. General

1. Building signs shall be constructed of quality materials. Only essential information, such as the name, business type, company logo, street address, phone number, and hours of operation of the business or retail establishment may be displayed on the front of the building.
2. The placement of signs shall be integrated into the overall architectural design of the building. The materials, color, style, and size of a sign shall be coordinated with the architectural features of the building.

The applicant is proposing to maintain the existing freestanding sign located at the corner of Landover Road and Brighseat Road. In addition, the applicant is proposing 60.6 square feet of building mounted signage which consists of 3 “Golden Arch” logos (14 square feet in size) and one “McDonald’s” logo (18.6 square feet in size). The applicant believes this signage is appropriate in size and compliments the building architecture rather.

- (G) The owner or operator should minimize adverse impacts on adjacent properties and the surrounding neighborhood by appropriate setting of:

- (i) Hours of operation or deliveries;
- (ii) Location of activities with potential adverse impacts;
- (iii) Location and use of trash receptacles;
- (iv) Location of loading and delivery spaces;
- (v) Light intensity and hours of illumination; and
- (vi) Location and use of outdoor vending machines.

COMMENT: The applicant has designed the site to minimize adverse impacts while addressing this retail operation. As indicated above, the drive-through component of the use is designed to minimize the impact on the adjoining residential community. In addition, it has been designed to provide a queuing area that is sufficient in size so that it does not affect the access driveways to the property. The loading area will be located more than 50 ' from the residential properties and the trash corral is located adjacent to the commercial office use as opposed to the residential properties. Overall, the applicant has designed this site to address its needs while at the same time minimizing the impact on the surrounding community.

IV. CONCLUSION

The applicant, McDonald's Corp, is seeking approval of this Detailed Site Plan to allow it to reconstruct McDonald's restaurant on the abovementioned property. As part of this application, the McDonald's is requesting amendments to the District Development Standards contained in the Subregion 4 District Development Plan. The applicant believes that this Detailed Site Plan and amendments meet the standards of the approved District Development Plan for Subregion 4 and therefore the applicant respectfully requests approval of DSP-19060.

Respectfully submitted,

MCNAMEE HOSEA

By:


Daniel F. Lynch

	Development District Standard	Comply? Y/N	Alternative Standard/Comments
General Site Standards and Guidelines			
	1. Building Orientation: The main entrance to a building must face the street. Buildings facing public plazas, parks, or open spaces must also have their main entrance oriented toward the public space.	Yes	
	2. Build-to Line: The build-to line is the line to which buildings are required to be built on a lot (no setback is permitted from it, except as noted in the guidelines). Consequently, multifamily, mixed-use, and nonresidential buildings, located within the DDOZ areas, must be located along a continuous street wall defined by the build-to line (see the residential, commercial, and industrial sections of the Building Envelope Standards for specific standards).	No	Build-to line for commercial use is 18'. Building sits at 28' from back of curb due to drive-thru width. Amendment requested
	3. Dooryard: The area between the public right-of-way and the build-to line is the dooryard area. The dooryard area commonly contains landscaping, building encroachments, fencing, and street furniture that helps to define a streetscape outside of the right-of-way. Each property within the residential, commercial, and industrial areas of the subregion should have a combination of the elements described above in order to ensure a lively streetscape.	Yes	
	4. Encroachments: Stoops, porches, bay windows, trim, eaves, arcades, balconies, chimneys, awnings, signs, and other architectural embellishments may encroach beyond the build-to line, so long as clear pedestrian passage through the public space is not impeded (see the residential, commercial, and industrial sections for specific standards of the Building Envelope Standards and Architectural Standards and Guidelines).	Yes	
	5. Frontage Occupancy: In order to ensure that buildings spatially define streets and the public realm but still allow for building articulation, a percentage of a primary building façade must be located on the build-to line. A portion of a building façade may step back two feet from the build-to line and still count toward the frontage occupancy, so long as portions of the facade are placed on the build-to line.	No	Build-to line for commercial use is 18'. Building sits at 28' from back of curb due to drive-thru width Amendment Requested.
	6. Lead Walks: Paved walkways connecting building entry ways and courtyards to a public sidewalk must be at least four feet in width.	Yes	
	7. Off-Street Parking: Surface parking lots and structures must be provided to satisfy parking demand. However, off-street parking, to the extent possible, should be strategically located either behind buildings or behind a masonry wall attached to a primary façade to minimize its visual impact on the urban environment. Service drives with drop offs and parallel parking may be allowed if room is available and as long as they do not interfere with clear pedestrian passage along the public space.	Yes	
	8. Primary Façade: All façades (including the front and side elevations on a corner lot) parallel to or clearly visible from a street must be treated as a primary elevation.	Yes	
	9. Secondary Façade: A façade, which does not directly face a street or public plaza, park, or open space shall be considered a secondary façade. Secondary facades must be articulated and must not have long expanses of blank wall.	Yes	The elevations will be revised prior to certification of the DSP.
	10. Sidewalks: Public sidewalks shall be at least four feet in width to allow for clear pedestrian passage.	Yes	

	11. Utility Connections and Service Areas: Site utility structures and service areas, such as trash enclosures, should not be visible from the streets. If these features must be placed near the public street or other spaces, they shall be screened from direct public view.	Yes	
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Building Standards for Mixed-Use, Commercial, and Institutional Types:

A. BUILDING HEIGHTS			
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	Principal Building: 4 Stories max. (60 ft.)	Yes	
	Accessory Building: 2 Stories max. (25 ft.)	N/A	No accessory building
	Ground Floor: 14 ft. min.	Yes	
	Upper Floor(s): 8 ft. min.	N/A	No upper floors
	B. LOT DIMENSIONS		
	Lot Width: 30 ft. min.	Yes	
	C. LOT OCCUPATION		
	Building Lot Coverage: 75 percent max.	Yes	
	D1. BUILD-TO LINE AND SETBACKS (CENTRAL AVE)		
	D.1 Build-to Line: 18 ft. from the back of curb	No	Building not sited on build-to line. Amendment requested
	D.2 Side Setback min./max.: 0 ft./10 ft.	No	Amendment requested
	D.3 Rear Setback min./max.: 0 ft./10 ft.	No	Amendment requested
	Frontage Occupancy: 80 percent min.	No	Combined lot is too wide to reasonably front 80%. The applicant is proposing 34%.Amendment requested
	D2. BUILD-TO LINE AND SETBACKS (BRIGHTSEAT RD)		
	D.1 Build-to Line: 18 ft. from the back of curb	No	Building not sited on build-to line. Amendmment requested
	D.2 Side Setback min./max.: 0 ft./10 ft.	No	Amendment requested
	D.3 Rear Setback min./max.: 0 ft./10 ft.	No	Amendment requested
	Frontage Occupancy: 80 percent min.	No	Combined lot is too wide to reasonably front 80%.The applicant is proposing 34% Amendment requested.
	E. ENCROACHMENTS		
	Awnings and Canopies: 4 ft. depth min.	Yes	
	Bay Window: 3 ft. max.	Yes	
	Balcony: 3 ft. min.	N/A	No balcony.
	Other Architectural Elements: Permitted	Yes	

Typical Primary Mixed-Use Street (Central Ave)			
	SIDEWALK		
	1. 6 foot tree pit area	No	Tree pit not provided Amendment requested
	2. 4-6 foot clear walkway	Yes	5 foot sidewalk provided
	3. 8-foot dooryard	Yes	5 foot grass strip provided. Amendment requested
	STREET		
	1. 4 travel lanes at 11 ft. each	Yes	These standards and not applicable in that the applicant is not constructing any streets as part of this DSP. However, the existing conditions comply with standard relative to the 4 travel lanes.
	2. 2 bicycle lanes at 4 ft. each	No	SHA and DPIE
	3. 2 parking lanes at 8 ft. each	No	Street standards are not applicabel
	4. Minimum 12 ft. median (if used)	No	Street Standards are not applicable.

Typical Secondary Street (Brightseat Rd)			
	SIDEWALK		
	1. 6 ft. tree planting strip	No	Amendment requested.
	2. 4-6 ft. clear walkway	Yes	
	3. 8 ft. dooryard	Yes	
	STREET		
	1. 4 travel lanes at 11 ft. each	N/A	Street standards are not applicable.
	2. 2 bicycle lanes at 4 ft. each	No	Insufficient room in ROW to construct
	3. 2 parking lanes at 8 ft. each	N/A	Street standards are not applicable.
Typical Tertiary Residential Street			
	SIDEWALK		
	1. 6 ft. tree planting strip	N/A	Project site does not front a tertiary residential street
	2. 6 ft. sidewalk	N/A	Project site does not front a tertiary residential street
	3. 10 ft. dooryard	N/A	Project site does not front a tertiary residential street
	STREET		
	1. 2 travel lanes at 11 ft. each	N/A	Project site does not front a tertiary residential street
	2. 1 parking lane at 8 ft. each	N/A	Project site does not front a tertiary residential street
Typical Alley or Lane			
	ALLEY		
	1. 16 ft. paved lane (may accommodate two-way traffic at slow speeds)	N/A	Project site does not front an alley or lane
	2. Two aprons at 7 ft. in length	N/A	Project site does not front an alley or lane
	3. No parking along the paved lanes.	N/A	Project site does not front an alley or lane
Architectural Standards:			
	A. GENERAL ARCHITECTURAL FRAMEWORK		

	1. Where redevelopment occurs within or in close proximity to existing neighborhoods, new construction shall complement style of existing surrounding buildings	Yes	
	2. Buildings shall be sited to reinforce the build-to line and define the street wall. Buildings shall shift in massing and have variations in height, profile, and roof form, while maintaining the formal relationship of building placement to the public street frontage.	No	Building not sited on build-to line. Amendment requested
	3. Encroachments that provide for exterior outdoor living spaces, such as porches, balconies, and/or roof terraces, shall be encouraged.	Yes	
	B. FAÇADE ARTICULATION		
	1. Buildings shall emphasize first story and primary entrances with pedestrian-friendly architectural features. Structures greater than one-story shall be organized to have a clear base, middle, and cap to the form of the building.	Yes	
	2. Commercial buildings with long façade shall be articulated through some combination of massing, fenestration, building openings, materials, and color.	Yes	Applicant will submit revised elevations that conform to standard prior to certification.
	3. New residential structures with blank walls exceeding 40 linear feet shall be provided.	N/A	Non-residential

	4. Rear elevations to commercial buildings serve as important secondary entrances to businesses. These rear entrances shall be designed to create an inviting appearance that is recognizable and related to the primary façade of the business in terms of both massing and material.	Yes	The applicant will submit revised elevations that conform to the standard prior to certification.
	C. MATERIALS		
	1. Building facades shall be composed of one dominant facing material and one or two additional accent materials	Yes	
	2. The dominant material should comprise a minimum of 60% of any elevation. No building should have more than 3 facing materials in addition to glass.	Yes	
	3. Primary building facades shall be faced with quality materials, such as masonry brick, natural stone, architectural precast stone or concrete, and cementitious clapboard siding	Yes	
	4. Synthetic stucco and aluminum and vinyl siding shall not be permitted as a dominant building material. These materials shall only be permitted on the attached garage area of single-family residences and on side or rear facades not facing a street.	Yes	
	5. Reflective glass, unfinished and painted concrete masonry units, and unfinished pressure treated wood products are prohibited	Yes	
	6. Wood, glass reinforced fiber concrete, synthetic wood products, fiber cement trim materials, and moulded millwork shall be permitted as trim material	Yes	
	D. FENESTRATION		
	COMMERCIAL		
	1. The proportions of windows and alignment and rhythm of windows should replicate the façade design of the immediate surrounding architecture	Yes	
	2. Windows should be primarily rectilinear in shape.	Yes	
	3. Fenestration shall comprise a minimum of 60% of ground story facades of mixed-use, commercial, and institutional building types and a minimum of 30% on the upper stories of these building types.	No	Amendment requested.
	4. Glazing shall be at least 80% transparent.	Yes	
	RESIDENTIAL		
	1. Windows shall have a vertical proportion	N/A	Non-residential
	2. Fenestration shall comprise a minimum of 30% of a façade area.	N/A	Non-residential
	Architectural elements		
	AWNINGS/CANOPIES		

	1. Awnings shall have a metal structure or metal structure covered with a durable, commercial grade fabric, canvas, or similar material having a matte finish.	Yes	
	2. If a façade is divided into distinct structural bays, such as masonry piers, awnings shall be placed within the vertical elements, rather than overlap them.	Yes	
	3. Awnings shall not extend more than 6 ft. beyond the face of a building.	Yes	
	4. Awnings may have a front skirt; however it shall not be scalloped.	Yes	
	5. Awnings shall not be backlit.	Yes	
	BALCONIES		
	1. Balconies shall be designed to complement the overall building façade and be proportionate to the building's window and door openings.	N/A	No balcony.

	2. Exposed pressure treated wood balconies shall not be permitted. Wooden balconies shall be painted, stained, or clad with an approved decorative fascia material.	N/A	No balcony.
	COLUMNS AND PIERS		
	1. Columns and piers break down the massing of buildings and should be considered to define individual storefronts and bays on longer building facades.	N/A	No columns or piers.
	DOORWAYS AND ENTRIES		
	1. Main entrance to a building or storefront should be emphasized with architectural features.	Yes	
	2. Commercial storefront entries may be recessed.	Yes	
	3. Service entrances to a building shall be located at the rear of a building.	Yes	
	4. Entrances to upper story uses shall be located along the street but should have a secondary character to the main entry.	N/A	No upper story.
	DOOR HOODS, PORCHES, AND STOOPS		
	1. Door hoods, porches, and stoops shall be compatible with the architecture of the building.	Yes	
	2. Porches shall be a minimum of 6 ft. in depth. Stoops shall be a minimum of 4 ft. wide and no greater than 8 ft. in depth.	Yes	
	ROOF TREATMENT AND PARAPET WALLS		
	1. Roofs may be gabled, hipped, or flat. The particular type of roof on a building should complement its architecture and that of the architecture of the surrounding area	Yes	
	2. A minimum roof pitch of 1:2 shall be required for a sloped roof.	Yes	
	3. Buildings with flat roofs shall have cornices and the roof shall be enclosed by a parapet wall. The parapet wall must be a minimum of 42 inches high.	Yes	
	4. Dormers, chimneys, and other architectural embellishments, such as cupolas and towers, may extend above the roof line.	Yes	
	5. Side gabled roofs with expanses greater than 40 linear feet should be articulated. Articulation may consist of dormers, front gables, cupolas, hipped roofs, or other architectural embellishment.	Yes	
	ROOFTOP UTILITIES		
	1. Visible rooftop utilities are prohibited. All roof mounted equipment shall be screened and painted to blend with the approved roof color.	Yes	
	2. Equipment shall be grouped and arranged in an orderly manner behind one screen.	Yes	
	SECURITY GATES ON COMMERCIAL BUILDINGS		
	1. Exterior security gates or burglar bars on windows and doors are prohibited. Additionally, rolled up security doors shall not be permitted.	Yes	
	2. Alternative means of building security including safety glass, lighting, and electronic surveillance should be considered in the place of security bars and roll-up doors over ground floor windows and doors.	Yes	
	3. Security screens and doors should be attractive and complement the buildings on which they are installed.	Yes	
	SHUTTERS		
	1. The dimension of shutters must fit the dimensions of the window opening to which they are applied.	Yes	
	2. Shutters shall be wood, vinyl, or metal.	Yes	

	3. The use of shutters and awnings on the same window shall be prohibited.	Yes	
STAIR TOWERS AND FIRE ESCAPES			
	1. Stair towers or fire escapes shall be allowed only on the side or rear of a building.	N/A	No upper story.
	2. Stair towers should be semi-enclosed or enclosed with walls, glass, railings and/or a roof structure and compatible to the main building in proportion, style, and materials.	N/A	No upper story.
Signage Standards and Guidelines:			
A. General			
	1. Building signs shall be constructed of quality materials. Only essential information, such as the name, business type, company logo, street address, phone number, and hours of operation of the business or retail establishment may be displayed on the front of the building	Yes	
	2. The placement of signs shall be integrated into the overall architectural design of the building. The materials, color, style, and size of a sign shall be coordinated with the architectural features of the building.	Yes	
	3. Signs for multitenant buildings shall be coordinated in terms of design, placement, size, materials, and colors.	N/A	Single tenant building.
	4. Temporary signs, flashing or blinking signs, internally illuminated box signs, roof signs, and billboards are prohibited. However, external lighting of signs and signs consisting of individual characters shall be permitted.	Yes	
B. Board signs			
	1. Buildings shall be designed to include a "signage zone" above the business or retail use. Typically, this zone is located above the lintel of the window of the ground level storefront.	Yes	
	2. All business and/or retail buildings shall have front and/or rear entry signage that is oriented toward the pedestrian.	Yes	
	3. The individual tenant signage for buildings with multiple tenants shall be located at approximately the same height to create a unified look.	N/A	Single tenant building.
C. Window signs			
	1. Window signs, including letters and logos, shall not obscure views into the business or retail establishment and shall not occupy more than 25% of the total window area in which the sign is located	Yes	
	2. Window signs shall generally be centered within a primary storefront display window, doorway, or an overhead transom window	Yes	
	3. There shall be a limit of one window sign per ground level building entry	Yes	
D. BLADE AND BANNER SIGNS			
	1. Blade signs shall be no more than 16 sq. ft. in area and shall allow for an 8 ft. minimum clearance above the finished grade of the sidewalk.	N/A	No blade signs.
	2. Signs attached to buildings shall not project more than 40 in. from the vertical plane of the wall to which they are attached.	N/A	No blade signs.
	3. Blade signs shall not be attached to awnings/canopies.	N/A	No blade signs.
	4. There shall not be more than one blade sign for each storefront tenant and upper floor business tenant, unless the premise is on a corner lot or has public entrances on two or more public rights-of-way, in which case a set of projecting signs may be erected toward the second public right-of-way.	N/A	No blade signs.
E. AWNING/CANOPY SIGNS			

	1. Awnings/canopies may include logos and/or text. However, they shall not be backlit.	Yes	
	F. TENANT DIRECTORY SIGNS		
	1. A tenant directory sign limited to 6 ft. in height and 18 sq. ft. per face may be placed immediately adjacent to a building in the rear yard area.	N/A	Single tenant building.
	2. Information on the signage should be limited to the name and/or address of the building and list of the tenants at that location.	N/A	Single tenant building.
	3. If used, tenant directory signs must be designed to reflect the architectural style of the primary signs of the building.	N/A	Single tenant building.
	G. SPECIALTY SIGNS		
	1. Specialty signs advertising special events and/or celebrating a holiday shall be allowed, so long as they provide for vehicular and pedestrian clearance.	Yes	
	2. Specialty signs must be affixed to light poles or between buildings and must be designed to withstand the elements	Yes	
	3. Specialty signs must be removed when the special event is completed.	Yes	
Landscaping Standards:			
	A. GENERAL		
	1. All plants shall conform to the American Standard for Nursery Stock as published by the American Association of Nurserymen	Yes	
	2. Plans for development shall include separate landscape plans. The landscape plans shall address all land areas of a lot that are not covered by buildings (including lead walks, planting beds, and turf areas). Landscape plans for multifamily, mixeduse, commercial, institutional, and industrial uses are to be developed or approved by a registered landscape architect and drawn based on a site development plan	Yes	
	3. A plant list shall be included on landscape plans and shall specify the plant species (botanical and common name), size/caliper, spacing, quantity, construction details (for trees, shrubs, evergreens, and street trees), and the method of irrigation and illumination.	Yes	
	4. Plant material should be utilized to provide shade; define outdoor spaces; screen, buffer, or enhance views, help define entrances; and accentuate individual developments.	Yes	
	5. Plantings should help mitigate climatic conditions by shading the southern exposure of buildings when feasible.	Yes	
	6. When properties are developed, stands of existing healthy trees should be preserved to the greatest practical extent. Damaged, decayed, or deceased trees should be removed to protect remaining trees.	Yes	
	7. Landscaping and streetscape amenities, including plantings, lawns, fencing, and furniture, should be used to distinguish public from private space	Yes	
	8. For the intent of the industrial screening and buffering standards and guidelines, the boundary streets shall be referred to as perimeter streets	N/A	The property is not located in an Industrial area.

	<p>9. In industrial areas, plants placed along street frontages and in buffer yard areas are subject to many adverse conditions and are not likely to receive consistent care. Accordingly, plant varieties that require little maintenance and tolerate with conditions as sun, wind, drought, glare...should be selected. If trees or shrubberies die, they must be replaced with a plant material similar to the remaining live screening material.</p>	N/A	The property is not located in an Industrial area.
	B. STREET TREES	Yes	

	<p>1. Street trees, which must be of a drought-resistant native species, shall be planted on average a minimum of 35 ft. on center along planting strips or in tree grates of street rights-of-way (Please refer to the <i>Prince George's County Landscape Manual</i> for acceptable street tree species). Where necessary, spacing allowances may be made to accommodate curb cuts, fire hydrants, and other infrastructure elements. However, at no location may the spacing exceed 35 ft. on center.</p>	No	Amendment requested
	<p>2. At planting, street trees shall be at least 3 in. in diameter and at least 10 ft. in overall height.</p>	Yes	
	C. SHADE AND ORNAMENTAL TREES		
	<p>1. Shade trees shall be planted a minimum of 1 shade tree per 10 surface parking spaces with a minimum of 2 shade trees per landscaped median strip.</p>	Yes	
	<p>2. Surface parking lots shall have no more than 2 bays of parking without a continuous planted median.</p>	Yes	
	<p>3. Two ornamental trees may be substituted for 1 shade tree (or vice versa) in surface parking lots.</p>	Yes	
	D. DOORYARD AREA		
	<p>1. The private frontage area between the public right-of-way and the build-to-line shall be treated in specific ways, depending on the adjacent building type use.</p>	Yes	
	<p>2. Mixed-use, commercial, and institutional private frontages shall be composed primarily of hardscape and may have planters and street furniture</p>	Yes	
	<p>3. Private frontages that are primarily residential in character shall be composed of sod, shade, and ornamental trees, planting beds, and some hardscape</p>	N/A	Non-residential
	E. GROUND COVER		
	<p>1. A minimum of 15% of green area of a commercial, office, retail, business, or industrial parcel or lot shall consist of planting beds with a shrubs, flowers, or ground cover. For building parcels with large undisturbed areas, this requirement will be based on the disturbed area</p>	Yes	
	<p>2. For residential properties, a minimum of 10% of the green area of a lot or parcel shall consist of planting beds with shrubs, flowers, or ground cover.</p>	N/A	Non-residential
	F. OPEN SPACE		
	<p>1. A minimum of 15% of green area of any residential development site must be dedicated for open space. Preferably, the open space should be located around existing mature trees.</p>	N/A	Non-residential
	<p>2. Open space requirements should be used to establish natural greenways that protect and restore wetlands, provide habitats, allow for conservation and growth of natural vegetation, and provide opportunity for pedestrian and bicycle pathways.</p>	N/A	

	3. Urban open spaces and pocket parks should be incorporated into the plan. These open spaces shall be planted with shade and flowering trees, evergreen shrubs, and other appropriate landscaping to provide shade, increase air quality, and treat stormwater, as well as add visual interest along streetscapes.	Yes	There is not open space or urban parks proposed as part of this DSP.
	G. TRAILS/PATHWAYS		
	1. Trails and pathways shall be provided through naturalized public spaces and common areas that circulate throughout, and connect to the public sidewalks.	N/A	The applicant is not proposing Naturalized public spaces or common areas.
	2. Great care shall be given to the placement of trails to ensure that their design will not disturb environmentally sensitive areas.	N/A	The applicant is not proposing any Trails.
	H. LANDSCAPE ELEMENTS		
	STREETSCAPE ENHANCEMENTS		
	1. Streetscape enhancements (including, but not limited to street furniture, planters, trash receptacles, decorative paving, sculpture/artwork, and bus shelters) shall be placed in an appropriate location for all future nonresidential development	Yes	

	2. Placement of streetscape enhancements shall not interfere with clear pedestrian passage in the public space.	Yes	
	3. Color and style of streetscape enhancements shall complement and coordinate with future development.	Yes	
	4. All proposed streetscape enhancements shall be indicated on detailed site plan submittals and shall include information on location, spacing, quantity, construction details, and methods of illumination.	Yes	
	BICYCLE PARKING		
	1. Bicycle parking shall be adequately provided throughout the DDOZ area primarily along primary mixed-use and secondary mixed-use streets for safe and convenient temporary storage.	Yes	
	2. One bicycle parking space shall be provided for every 20 vehicular parking spaces.	Yes	
	3. Bicycle parking shall be located within 50 ft. of main entrances to multifamily, mixed-use, commercial, and institutional buildings, and have direct access to public rights-of-way.	Yes	
	4. Bicycle parking areas shall not obstruct sidewalks or walkways.	Yes	
	5. The color and style of bicycle racks shall complement new streetscape improvements.	Yes	
	FENCES		
	1. Fences, with height between 36 and 42 in. may be permitted in the dooryard area adjacent to the public right-of-way. Privacy fences 6 ft. in height may be placed alongside and near lot lines; however, they shall not be placed in the dooryard area of the lot.	N/A	The applicant is proposing a 42" fence composed of aluminum fencing and masonry posts.
	2. Front yard fencing should complement and match the vernacular of the primary structure and accessory structures on a lot.	N/A	
	3. Fences must either be wood or metal. No plastic, vinyl, or chain-link fences shall be permitted on properties within the DDOZ.	N/A	The proposed fence is composed of aluminum fencing and masonry posts.
	RETAINING WALLS		

	1. Retaining walls may be necessary to grade individual lots or parcels. Such walls shall match or be compatible with the architectural elements of the primary buildings on the lot.	N/A	
	2. Retaining walls at the sidewalk shall have a maximum height of 30 in.	N/A	
	3. Bare, poured concrete, wood posts and timber ties, and modular concrete retaining walls shall not be permitted in a front or side yard visible from a public street.	N/A	
	FENCES AND WALLS IN INDUSTRIAL AREAS		
	1. Fences and walls shall be constructed of durable materials that include pressure-treated wood, masonry, stucco, and decorative metal. The following materials shall not be used for any type of fencing and/or walls: barbed wire, razor wire, unclad cinder block, vinyl cladding, corrugated metal, corrugated fiberglass, sheet metal, wire mesh, nonpressure-treated wood, or chain link.	N/A	Non-industrial
	2. The kickboards (the lower horizontal member of a fence) of pressure treated wood fencing shall be placed at least 2 in. above grade, so as to prevent deterioration. The base of a wooden fence shall not rest at grade.	N/A	Non-industrial
	3. Fences and/or walls shall be required around properties containing open truck and/or open storage facilities in order to ensure that the storage of materials and/or commercial vehicles are not visible at eye level (5 ft.) when standing from the approximate centerline of the public right-of-ways adjacent to the property. Consequently, fences/walls for these types of facilities must be between 6 ft. min. and 8 ft. max.	N/A	Non-industrial
	4. Fences and/or walls may be used as screening material around parking lots. These fences and/or walls should be between 3-4 ft. in height.	N/A	Non-industrial

	LIGHTING		
	1. Lighting shall be provided along public and private streets, alleys and access drives, public open spaces and trails, and in parking areas.	Yes	
	2. Pedestrian-scaled street light fixtures (at heights between 12-14 ft. in height) should be installed in all public spaces at no more than 60 ft. intervals, as measured parallel to the street.	Yes	
	3. Cobra head streetlights shall not be permitted.	Yes	
	4. The style of ornamental poles and luminaires should be coordinated with the appropriate public agencies.	Yes	
	I. ADDITIONAL LANDSCAPING STANDARDS FOR INDUSTRIAL PROPERTIES		
	1. A 15 ft. minimum to 25 ft. maximum landscaped buffer area, as measured from the back of curb, shall be provided along perimeter streets.	N/A	Non-industrial
	2. A 10 ft. minimum landscaped buffer area, as measured from the edge of paving, shall be provided along internal streets.	N/A	Non-industrial
	3. Side and rear bufferyards around a property shall be at least 20 ft. in width.	N/A	Non-industrial
	4. The buffer areas along perimeter streets shall be planted with trees selected from the recommended species of evergreen trees found in Appendix 3, Table A-3(d) of the <i>Prince George's County Landscape Manual</i> . Evergreen trees must be at least 6 ft. in height at the time of planting and must be placed between 8 ft. minimum and 12 ft. maximum on center. Trees may be placed in a double staggered row, so long as the center line of	N/A	Non-industrial

	each tree within the triangular grouping is between 8-12 ft. apart.		
	5. The buffer areas along internal streets shall be planted with evergreen trees recommended from the <i>Prince George's County Landscape Manual</i> . These trees must be at least 6 ft. in height at the time of planting, but may be placed between 12 ft. minimum and 16 ft. maximum on center around the edges of a property instead of 8 ft. minimum to 12 ft. maximum.	N/A	Non-industrial
	6. Side and rear bufferyards shall be planted with the recommended evergreen trees from the <i>Prince George's County Landscape Manual</i> . These trees shall be at least 6 ft. at the time of planting and shall be spaced between 12 ft. minimum and 16 ft. maximum on center. Trees may be placed in a double staggered row, so long as the center line of each tree within the triangular grouping is between 12 and 16 ft. apart.	N/A	Non-industrial
	7. In addition to evergreen trees, shrubs may be provided along perimeter streets, internal streets, and bufferyard areas. If provided, shrubs should be between 2 to 5 ft. in height at the time of planting and should be placed in tightly spaced groupings of at least 3 plants (5 ft. on center). The shrubs should be either evergreen, or if deciduous, have a dense, twiggy growth habit for winter screening and an attractive year-round appearance. Along perimeter streets, every fifth tree may be replaced by a grouping of shrubs, so long as the shrubs are at least 4 ft. in height and do not expose open truck and/or open storage facilities. on an internal street and bufferyard area, every third tree may be replaced by a grouping of shrubs, so long as the shrubs are at least 4 ft in height.	N/A	Non-industrial
	8. Landscaped berms may be allowed along internal street edges or the side and/or rear bufferyard of a property in order to conceal uses within that property from adjacent public rights-of-way and/or properties	N/A	Non-industrial

Parking and Loading Standards and Guidelines:

	A. GENERAL		
	1. The minimum surface parking spaces shall be 80% of minimum required parking and loading as stated by Section 27568(a) of Part 11 of the Zoning Ordinance.	Yes	

	2. The maximum number of surface parking spaces shall be equal to minimum required by parking and loading as stated by Section 27-568(a) of Part 11 of the Zoning Ordinance.	Yes	
	3. The maximum number of structured parking shall be equal to 115% of the minimum required by parking and loading as stated by Section 27-568(a) of Part 11 of the Zoning Ordinance.	N/A	No structured parking.
	4. Buildings must meet the frontage occupancy requirements for the perimeter of a block when considering the parking requirements.	No	Building does not meet frontage occupancy requirements. Amendment requested

	5. All private residential parking (non-multifamily) shall be located at the rear or side of the principal building on a lot in an attached or detached garage, carport, or on a parking pad, and shall be accessed from a landscaped driveway, side street, or alley. Residential garages or carports must be recessed a minimum of 6 ft. from the front building facade of the principal building on the lot.	N/A	Non-residential
	6. Parking pads or tandem parking spaces in private residential areas must not block clear pedestrian passage along the adjacent right-of-way.	N/A	Non-residential
	7. If possible, surface parking lots, structured parking, and loading areas shall be located on the interior of the block or at the rear of the property in the case of commercial, mixed-use, and institutional uses, and shall be accessed from a side street, alley, or entrance drive aisle	Yes	
	8. In industrial areas, surface parking lots may be located on the side of a primary building, so long as the parking lot does not exceed one parking bay and the property is not located along a primary mixed-use street.	N/A	Non-industrial
	9. Cross-access easements between surface parking lots shall be encouraged.	Yes	This DSP does not involve multiple properties and cross access easements between surface parking lots is not feasible at this time.
	10. Parking lots and structure parking shall not dominate the streetscape, obscure building frontages, endanger pedestrians, or overwhelm the visual environment. Curb cuts should be reduced to minimize potential pedestrian/vehicular conflict. When possible, there shall be one curb cut per block.	Yes	
B. SURFACE PARKING LOTS			
	1. Surface parking lots shall be set back from the rear façade of nonresidential, mixed-use, or commercial structures in order to accommodate a landscape planting buffer adjacent to the building and 5 ft. wide walkway adjacent to the parking.	Yes	
	2. A 10 ft. wide by 18 ft. deep landscaped island with shade trees must be provided a maximum of every 10 parking spaces.	Yes	
	3. Surface parking lots with more than two bays of parking shall be divided by a continuous landscaped strip at least 10 ft. in width between the third and fourth bays to accommodate shade trees and other landscape elements. Where possible and appropriate, landscaping strips should be greater than 10 ft. to provide for bio-swailes.	Yes	
	4. Surface parking lots located on the side of a principal building must have screen walls behind the build-to line that connect to the principal building and conceal the parking from the adjacent public space. The walls must be between 3-3.5 ft. in height and must consist of materials similar to the primary facade of the principal building. Additionally, appropriate landscaping should be provided in front of the wall. Chain link and chain link fences with privacy slats are prohibited as a screening material.	Yes	
C. STRUCTURED PARKING			
	1. A parking structure, adjacent to a street or other public space, shall be located at the build-to line and shall have retail/commercial liner uses on the first level fronting the street in order to create a continuous street wall along the sidewalk edge.	N/A	No structured parking.

	2. Parking structure facades visible from the street or other public spaces shall consist of high-quality materials. These facades shall mimic the architecture of the adjacent buildings. Parking structures located on corner lots shall provide street quality architectural facades on both the front and side streets.	N/A	No structured parking.
	3. Parking structures shall be between 2 and 5 stories in height, depending on the site where the garage is located. However, parking garages shall not exceed the height of the surrounding buildings.	N/A	No structured parking.
	4. When the siting of a parking structure creates a gap between the parking structure and the building it services, there shall be a minimum 10 ft. wide landscaped area between the two structures. The sides and rear of parking structures shall be screened from adjacent properties with coniferous plant material.	N/A	No structured parking.
	D. LOADING AND SERVICE AREAS		
	1. All loading and service areas must be located in the interior of a block behind buildings.	No	The loading space is not located behind the building but is screened from Cenral Avenue in conformance with Section 4.4 of the Landscape Manual. Amendment requested.
	2. Dumpsters, exposed storage areas, machinery, service areas, truck loading areas, utility buildings, air conditioning units, and other similar structures shall be screened from the view of neighboring properties and streets with walls made of the same materials, color, or style as the primary building on the lot or parcel. Additionally, landscaping must also be provided around the perimeter of the loading yard.	Yes	
	3. Utility areas separated from the primary building on a lot or parcel shall be enclosed by a 6 ft. high solid wall or fence constructed with materials compatible to the primary building.	Yes	
	4. The front of enclosures shall have steel swing gates for vehicular service access. A separate pedestrian gate must also be provided.	Yes	
	5. A wooden trash enclosure is not permitted.	Yes	
	E. OPEN STORAGE IN INDUSTRIAL AREAS:		
	Open storage, within 20 ft. of the inside portion of a screening wall and/or fence for a property, may not extend above the screening wall/fence.	Yes	
	F. DRIVEWAYS IN INDUSTRIAL AREAS:		
	Driveways and curb cuts to properties must be constructed of permeable or nonpermeable paving material, such as asphalt or concrete. Gravel driveway and curb cuts are not permitted.	Yes	



**INTER-OFFICE MEMORANDUM
PRINCE GEORGE'S COUNTY
POLICE DEPARTMENT**



M E M O R A N D U M

DATE: May 14, 2020
TO: Planning Coordinator, Urban Design Application Section
Development Review Division
FROM: Captain Wendy Contic, Assistant Commander, Planning & Research Division
SUBJECT: DSP-19060 Landover McDonald's

Upon review of the site plans, there are no comments at this time.

June 8, 2020

MEMORANDUM

TO: Adam Bossi, Planner coordinator, Development Review Division

VIA: David A. Green, MBA, Master Planner, Community Planning Division *DAG*

FROM: Maha Tariq, Neighborhood Revitalization Section, Community Planning Division

SUBJECT: **DSP-19060, Landover McDonald's**

FINDINGS

Community Planning Division staff finds that, pursuant to Section 27-548.25(c) of the Zoning Ordinance this applicant requests development standards which differ from the Development District Standards of the -2010 *Approved Subregion4 Master Plan and Sectional Map Amendment and 2004 Approved Sector Plan and Sectional Map Amendment for the Morgan Boulevard and Largo Town Center Metro Areas*. that will benefit the development and will not substantially impair implementation of the Sector Plan.

BACKGROUND

Application Type: Detailed Site Plan in a Development District Overlay Zone

Location: 8710 Central Ave. Hyattsville, MD 20785

Size: 1.32 acres

Existing Uses: MacDonal'd's fast-food restaurant

Proposal: The applicant proposes to demolish existing McDonald's restaurant and rebuild eating and drinking establishment with drive through services. The proposed restaurant will be 4,540 square feet with 69 seats and 48 parking spaces. An outdoor dining area with a pedestrian connection to the existing sidewalk located along Central Avenue is proposed.

GENERAL PLAN, MASTER PLAN, AND ZONING

General Plan: This application is in the Established Communities Growth Policy Area. "Established Communities are most appropriate for context-sensitive infill and low-to-medium density development," (pg.20) The General Plan Generalized Future Land Use is Commercial.

Master Plan: The *2010 Approved Subregion 4 Master Plan and Sectional Map Amendment* recommends Mixed Use Commercial future land use on the subject property.

In addition, the property is located within Opportunity Site 3, in Zone 1 of the Master Plan. The opportunity site is situated between the Morgan Boulevard and Central Avenue/Capital Beltway nodes. The vision is to redevelop the north side of Central Avenue with mixed commercial uses. The plan recommends to place the buildings closer to the street and locate parking on the side or rear of the properties to make the area more pedestrian-friendly.(pg. 308) The following two policies are provided to enhance the pedestrian environment (pg. 308):

Policy 1: Develop a new character and image for Central Avenue that is inviting to pedestrians.

- Provide infill and redevelop opportunities on Central Avenue that create a coherent street image
- Create development standards that promote new urbanist principles and encourage new infill to occur closer to and consistently along build-to-line.
- Provide for streetscape improvements that promote a pedestrian-friendly environment.

Policy 2: Improve pedestrian circulation and create an appealing streetscape character.

- Design and install cohesive and visually interesting sidewalk paving patterns and include site furnishings, lightening, and plantings that invite pedestrians
- Develop and promote "green street" design standards.
- Establish and enforce higher maintenance standards.
- Create and enforce unified sign design standards for existing and new businesses.

Planning Area: PA-72

Community: Landover and Vicinity

Aviation/MIOZ: This application is located within the Military Installation Overlay Zone, Area Label B, App/Dep Clearance (50:1), North-End which has specific height restrictions.

The applicant shall provide maximum height of the proposed structure which conforms to the MIOZ height restrictions.

SMA/Zoning: The subject property is in the Development District Overlay Zone (DDOZ) area which reclassifies its underlying C-O (Commercial-Office) zone to M-U-I (Mixed-Use-Infill) zone as part of the SMA highlighted in *2010 Approved Subregion 4 Master Plan and Sectional Map Amendment* and *2004 Approved Sector Plan and Sectional Map Amendment for the Morgan Boulevard and Largo Town Center Metro Areas*.

DEVELOPMENT DISTRICT MANDATORY STANDARDS

Community Planning Division staff finds that, pursuant to Section 27-548.25(b), this application is not in conformance with the mandatory requirements of the Development District Overlay Zone.

Restoration or reconstruction of nonconforming building or structure, or a certified nonconforming use, is exempt from the Development District Standards and from site plan review if it meets the requirements of Section 27-243(a)(1) of the Zoning Ordinance. The property may not expand, enlarge or relocate a certified non-conforming use or a use or a structure that was lawful on the date of SMA approval but does not conform to the development district standards, unless a detailed site plan is approved with findings that the expansion is compatible with adjacent uses and meets the goal of the masterplan. DDOZ standards apply to the subject property as the proposed area of the restaurant is more than the existing gross floor area.

REQUESTED AMENDMENTS TO DEVELOPMENT DISTRICT STANDARDS

The plan recommends to placing the buildings closer to the street and locate parking on the side or rear of the properties to make the area more pedestrian-friendly.

CPD Response: The applicant should provide revised plans to demonstrate how the proposed design will benefit the development and will not substantially impair implementation of the Sector Plan.

The following amendments to the plan have been requested:

1. Required Build-to-line is 18' (proposed 28')
2. Required Building Frontage Occupancy is 80% (provided 34%)
3. Required Sidewalk is 6-foot, tree pit area, 4-6' walkway, 8' dooryard (provided – maintain existing 5' sidewalk and 5' grass strip, add 10' landscaped strip)

c: Long-range Agenda Notebook

Frederick Stachura, Planning Supervisor, Neighborhood Revitalization Section, Community Planning Division [only for staff in the Neighborhood Revitalization Section]



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
 Upper Marlboro, Maryland 20772
 TTY: (301) 952-4366
 www.mncppc.org/pgco

June 16, 2020

MEMORANDUM

TO: Adam Bossi, Development Review Division

FROM: Benjamin Ryan, Transportation Planning Section, Countywide Planning Division

SUBJECT: **Detailed Site Plan Review for Multimodal Transportation, DSP-19060 Landover McDonald's**

The following detailed site plan (DSP) was reviewed for conformance with the Zoning ordinance, the *Approved Countywide Master Plan of Transportation (MPOT)*, and the 2010 *Approved Subregion 4 Master Plan* and sectional map amendment to provide the appropriate multimodal transportation recommendations.

Detailed Site Plan Number: DSP-19060

Development Case Name: Landover McDonald's

Type of Master Plan Bikeway or Trail

Municipal R.O.W.	<u> </u>	Public Use Trail Easement	<u> </u>
PG Co. R.O.W.	<u> X </u>	Nature Trails	<u> </u>
SHA R.O.W.	<u> X </u>	M-NCPPC - Parks	<u> </u>
HOA	<u> </u>	Bicycle Parking	<u> X </u>
Sidewalks	<u> X </u>	Trail Access	<u> </u>

Detailed Site Plan Background	
Building Square Footage (non-residential)	4,540 SF
Number of Units (residential)	N/A
Abutting Roadways	MD 214 (Central Avenue), Brightseat Road
Abutting or Nearby Master Plan Roadways	MD 214 (Central Avenue, A-32), Brightseat Road (C-412), Hampton Park Boulevard (I-413), I-95 / I-495
Abutting or Nearby Master Plan Trails	Planned Bike Lane: Central Avenue, Brightseat Road
Proposed Use(s)	Commercial – Fast Food Restaurant
Zoning	M-U-I
Centers and/or Corridors	Central Avenue Corridor
Prior Approvals on Subject Site	N/A

Background

The 1.17-acre M-U-I Zoned property is located at the northwest quadrant of the intersection of Central Avenue and Brightseat Road. This application proposes to raze the current McDonald's restaurant and replace it with a 4,540 square-foot McDonald's restaurant.

Previous Conditions of Approval

This development case does not have any binding prior approvals germane to multimodal transportation.

Existing Conditions and proposed sidewalk and bicycle infrastructure

The submitted plans show sidewalks along Central Avenue and Brightseat Road. An internal sidewalk which surrounds the building is proposed. A pedestrian connection has been provided adjacent to the southeast corner of the building, which will connect a new outdoor eating area directly to sidewalks along Central Avenue and to the entrance of the building. An additional pedestrian connection has been provided adjacent to the northeast corner of the building, which will provide pedestrians with a direct connection to Brightseat Road. The applicant intends to close one point of vehicle entry along Central Avenue which will be replaced with new sidewalks. Proposed bicycle racks have been displayed near the entrance of the building.

Review of Master Plan Compliance

This development case is subject to the 2009 *Approved Countywide Master Plan of Transportation*, which recommends the following facilities:

- Planned bike lanes along Central Avenue and Brightseat Road

Comment: MD 214 (Central Avenue) fronts the subject site to the south, and the full dedication of this road has already been provided. The Maryland State Highway Administration (SHA) can require the construction of the master plan recommended street section, with the included bicycle facility, as appropriate, or the bicycle lanes may be installed by the State Highway Administration (SHA) as part of a future roadway repaving or capital improvement project.

Brightseat Road also fronts the subject site to the east. No additional right-of-way is being sought with this application. The Prince George's County Department of Permits, Inspections, and Enforcement (DPIE) can require the construction of the master plan recommended bicycle lane along Brightseat Road as appropriate, or the bicycle lane may be installed by the Department of Public Works & Transportation (DPW&T) as part of a future roadway repaving or capital improvement project.

The MPOT provides policy guidance regarding multimodal transportation and the Complete Streets element of the MPOT recommends how to accommodate infrastructure for people walking and bicycling:

Policy 2: All road frontage improvements and road capital improvement projects within the Developed and Developing Tiers shall be designed to accommodate all modes of transportation. Continuous sidewalks and on-road bicycle facilities should be included to the extent feasible and practical.

Policy 4: Develop bicycle-friendly roadways in conformance with the latest standards and guidelines, including the 1999 AASHTO *Guide for the Development of Bicycle Facilities*.

Policy 5: Evaluate new development proposals in the Developed and Developing Tiers for conformance with the complete streets principles.

Comment: The applicant has included a bicycle rack to be installed along the north side of the building. Staff recommend that an Inverted-U style bicycle rack or a similar style of bicycle rack that can provide two points of contact to support and secure a bicycle be used. A sidewalk network is shown which will provide pedestrian access along both the frontage of Central Avenue and Brightseat Road. Additionally, pedestrian connections using highly visible specialty pavers to Central Avenue and Brightseat Road have are proposed. Staff recommend continental crosswalks at the both vehicle entrance points. Crosswalks that are highly visible support separating the pedestrian and vehicular circulation, pursuant to Section 27-274.(a)(2)(C)(ix).

The MPOT and the area master plan classify Central Avenue as an arterial road. The current roadway section in front of the subject site to the south is a nine-lane road. The westbound portion features three through lanes. The east bound portion, as it approaches its intersection with Brightseat Road, features three through lanes, two center-turn lanes, and one free right-turn lane.

The MPOT and the area master plan classify Brightseat Road as a collector road. The current roadway section which fronts the subject site to the east is a seven-lane road. The southbound portion, as it approaches its intersection with Central Avenue, features one through lane, two center-turn lanes, and one free right-turn lane. The northbound portion features two through lanes and one additional right-hand lane, which originates as a free right-turn lane from Central Avenue.

The Transportation Recommendations Section of the 2010 *Approved Subregion 4 Master Plan* and sectional map amendment makes the following recommendations:

- Identify priority sidewalk corridors to parks, schools, Metro stations, and other activity centers where sidewalk construction is necessary to meet existing pedestrian needs. (p.233)
- Improve bicycle facilities around Metro stations in Subregion 4. Facilities needed include bicycle racks, lockers, and striping for designated bike lanes. (p.233)

Comment: The Morgan Boulevard Metrorail Station is approximately 0.70 miles northwest of the project site and the Largo Town Center Metrorail Station is approximately 0.8 miles northeast of the project site. The subject's use of existing sidewalks and bicycle parking support further multimodal transportation in the area. As noted above, bicycle lanes can be provided by the roadway permitting and operating agencies.

Site Access and Circulation

The property is currently improved with a McDonald's restaurant and three vehicle entry points, two of which are on Central Avenue and one on Brightseat Road. The western vehicle point along Central Avenue is an exit lane and the eastern vehicle point along Central Avenue is an entry lane. The vehicle point on Brightseat Road allows for vehicle entry and exit. This project proposes to remove the vehicle entry lane on Central Avenue and replace it with sidewalks while turning the western vehicle point into a combined entry and exit lane. Parking is being proposed on the northern and western sides of the building which aligns with the reconfigured vehicle entrance points.

The proposed layout displays two drive-through lanes adjacent to the northwest corner of the proposed building where customers can place orders. The drive-through lanes combine into one lane and follow a southeastern route as drivers approach the pick-up window along the south facing side of the restaurant. A loading area is proposed to the south of the drive-through lane fronting Central Avenue.

Comment: Staff conclude that proposed site access, the parking lot, and circulation are designed in accordance with Sections 27-283 and 27-274.

The subject application proposes 48 parking spaces and one loading space. The minimum requirements, per Section 27-568 for an eating or drinking establishment use of this size is 55 parking spaces and one loading space. However, the property falls within the 2010 *Approved Subregion 4 Master Plan* and sectional map amendment, and is thus held to Development District Overlay (D-D-O) standards, which reduces the minimum number of parking spaces to 80% of what is required by Section 11 of the Zoning Ordinance.

Comment: Staff conclude that the proposed number of parking spaces are in accordance with Section 27-568 per Development District Overlay standards.

Conclusion

Based on the findings presented above, staff conclude that the multimodal transportation site access and circulation of this plan is acceptable, consistent with the site design guidelines pursuant to Section 27-283, and meets the findings required by Section 27-285(b) for a detailed site plan for transportation purposes if the following condition is met:

1. Prior to certification of the detailed site plan, the applicant, or the applicant's heirs, successors, and/or assignees shall revise the plans to provide:
 - a. Detail sheet showing specification of bike racks (Inverted U-bicycle rack or a style that provides two points for contact for each bicycle).
 - b. Continental style crosswalk crossing the driveway along Brightseat Road unless modified by the Department of Permits, Inspections, and Enforcement (DPIE) with written correspondence.
 - c. Continental style crosswalk crossing the driveway along Central Avenue unless modified by the Maryland State Highway Administration (SHA) with written correspondence.



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

Countywide Planning Division
Environmental Planning Section

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
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301-961-3650

June 8, 2020

MEMORANDUM

TO: Adam Bossi, Planner Coordinator, Urban Design Section, DRD

VIA: Megan Reiser, Supervisor, Environmental Planning Section, CWPD

FROM: Chuck Schneider, Planner Coordinator, Environmental Planning Section, CWPD

SUBJECT: **Detailed Site Plan DSP-19060; Landover - McDonalds**

The Environmental Planning Section (EPS) has reviewed Detailed Site Plan, DSP-19060, received by the Countywide Planning Division on April 30, 2020. The Environmental Planning Section recommends approval of the application with no conditions.

The site has a Natural Resources Inventory Equivalency Letter (NRI-094-2019) which was issued on July 25, 2019 and a Woodland Conservation Exemption Letter (S-014-2019) issued on July 25, 2019.

The site is primarily developed with a building and paved surface parking areas. No woodland or regulated environmental features are located on this site. According to the US Department of Agriculture (USDA) Natural Resource Conservation Service (NRCS) Web Soil Survey (WSS), the site contains only Urban land-Collington-Wist complex and Udorthents soils. No unsafe soils containing Christiana complexes or Marlboro clays are associated with this site. This site is not located within a Sensitive Species Protection Review Area (SSPRA) based on a review of the SSPRA GIS layer prepared by the Maryland Department of Natural Resources Natural Heritage Program (DNR NHP). The site is located within the Environmental Strategy Area 1 of the Regulated Environmental Protection Areas Map as designated by *Plan Prince George's 2035 Approved General Plan (2014)*. There is Evaluation Area located along the northern property line within the maintained landscape areas, as mapped in the *Countywide Green Infrastructure Plan of the Approved Prince George's Resource Conservation Plan (May 2017)*.

The site has an approved Stormwater Management Concept Plan #25146-2019-00 that is in conformance with the current code and is valid until October 16, 2022. The approved concept plan is consistent with the detail site plan.

No additional Information is required. The Environmental Planning Section Recommends approval of DSP-19060.



Division of Environmental Health/Disease Control

Date: June 1, 2020

To: Adam Bossi, Development Review Division, M-NCPPC

From: ^{AK} Amend Kpadeh, Environmental Health Specialist, Environmental Engineering/Policy Program

Re: DSP-19060, Landover McDonald's

The Environmental Engineering/Policy Program of the Prince George's County Health Department has completed a desktop health impact assessment review of the detailed site plan submission for the Landover McDonald's and has the following comments/recommendations:

1. The property is located in the recharge area for the Patuxent River Watershed, a surface water supply that flows into the Chesapeake Bay. The Patuxent River Watershed provides recreational opportunities for Prince George's County residents. This Watershed also serves as an integral part of the water quality health of the Chesapeake Bay, an economic engine of Maryland. The applicant should implement strategies to facilitate the return of precipitation to the Watershed to protect the sustainability of the surface water resource through the use of pervious pavers, appropriate landscaping and apropos storm-water management structures.
2. Idling vehicles release fumes that are hazardous to health. Air pollutants released from an idling vehicle include carbon monoxide, fine particulate matter, nitrogen oxides, ozone and benzene among others. These air pollutants can lead to health problems such as asthma, bronchitis, irregular heartbeat, nonfatal heart attacks, and a weakened immune system among other long-term and short-term adverse health effects. Maryland State Transportation Code Section 22-402 prohibits a vehicle engine from operating for more than five consecutive minutes when the vehicle is not in motion. The applicant proposes a Side-by-Side Drive-Thru window which will reduce time spent in a drive-thru window by about two minutes. The reduction in wait time and a reduction in idling time will be a positive health benefit due to the reduction of air pollutants released into the air by vehicles using the drive-thru. However, the applicant should demonstrate that there is not a bottleneck once the vehicles move from the two lane ordering system to a single lane order payment and pick-up window, especially during the peak hours. In addition, the applicant anticipates an increase in drive-thru use due to the convenience of the tandem ordering system. An increase in drive-thru use and a bottleneck at the payment and pick-up window will increase the cumulative amount of air



Angela Alsobrooks
County Executive

Environmental Engineering/Policy Program
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pollutants released into the air. The applicant should design the entire drive-thru system to minimize idle time and total wait time.

3. There are at least seven existing carry-out and convenience store food facilities within a ½ mile radius of this location. Research has found that people who live near an abundance of fast-food restaurants and convenience stores compared to grocery stores and fresh produce vendors, have a significantly higher prevalence of obesity and diabetes.
4. During the demolition/construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.
5. During the demolition/construction phases of this project, no noise should be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.

If you have any questions or need additional information, please contact me at 301-883-7658 or adkpadeh@co.pg.md.us.

May 18, 2020

MEMORANDUM

TO: Adam Bossi, Urban Design, Development Review Division
FROM: Michelle Hughes, Permit Review Section, Development Review Division
SUBJECT: Referral Comments for DSP-19060, Landover McDonald's

1. The parking schedule should be updated to indicate accessible spaces are included in total parking required.
2. The dimensions of the accessible parking spaces shall be demonstrated on the site plan.
3. If the outdoor seating is seasonal no parking is required and a note should be demonstrated on the site plan.
4. All Landscape Schedules should be provided per all applicable schedules shown in the Landscape Manual.
5. A note should be added to the Landscape Plan indicating it is in compliance with SE-3805 and any subsequent revision.
6. The height of sign should measure from the finished grade at the base of the sign to the top of the sign.
7. A table should be shown indicating all sign details required and provided.

May 1, 2020

MEMORANDUM

TO: Adam Bossi, Urban Design Review, Development Review Division

VIA: Howard Berger, Supervisor, Historic Preservation Section, Countywide Planning Division

FROM: Jennifer Stabler, Historic Preservation Section, Countywide Planning Division
Tyler Smith, Historic Preservation Section, Countywide Planning Division

SUBJECT: DSP-19060 Landover McDonald's

The subject property comprises 1.32 acres located on the north side of Central Avenue at its intersection with Brightseat Road. The subject application proposes razing all existing structures and rebuilding an eating and drinking establishment with drive-thru service. The subject property is Zoned D-D-O and M-U-I.

A search of current and historic photographs, topographic and historic maps, and locations of currently known archeological sites indicates the probability of archeological sites within the subject property is low. The subject property does not contain and is not adjacent to any Prince George's County Historic Sites or resources. This proposal will not impact any historic sites, historic resources or known archeological sites. A Phase I archeology survey is not recommended. Historic Preservation staff recommend approval of DSP-19060 Landover McDonald's with no conditions.

From: [Bossi, Adam](#)
To: [PGCReferrals](#)
Subject: FW: DSP-19060 Landover McDonald's
Date: Thursday, June 11, 2020 11:59:01 AM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
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[image015.png](#)
[image016.png](#)

Importance: High

Gentlemen,

Can you please add the email below from Kwesi to the referral file for DSP-19060? Thank you!

Adam Bossi

Planner Coordinator | Urban Design Section | Development Review Division

 THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
Prince George's County Planning Department
14741 Governor Oden Bowie Drive, Upper Marlboro, MD 20772
301-780-8116 | adam.bossi@ppd.mncppc.org



From: Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>
Sent: Friday, May 15, 2020 10:00 AM
To: Bossi, Adam <Adam.Bossi@ppd.mncppc.org>
Subject: RE: DSP-19060 Landover McDonald's
Importance: High

Adam,

Thank you providing the referral.

An Access Permit will be required for the proposed work in the State right of way.

Detailed engineering plans and all supporting documents should be submitted to SHA for review. In the meantime, I have the following preliminary comments:-

- A Plan review checklist should be included with the submittal, ensuring that all checked items are clearly shown on the plans. The link to download the checklist is : <https://www.roads.maryland.gov/ohd2/Plan-check-list.pdf>.
- Storm drain computations should be included with the submittal
- It looks like the easternmost driveway is going to be closed; this should be clearly shown and called out on the plans.
- The westernmost driveway, and pedestrian ramps, bus stop and sidewalks along the property frontage are not ADA compliant and will all need to be reconstructed to meet ADA criteria.
- Dimensions should be provided for the reconstructed sidewalk width (5ft min.) and for the entrance radii.
- Typical sections should be provided at the entrance and at the sidewalk.
- The applicable ADA ramps standard(s) should be called out on the plan for each reconstructed ramp.
- Copies of SHA Standards should be removed from the plans and listed on the title sheet in the format shown below:

The following standards (construction and temporary traffic control) are required for this project (list them out as shown below):

- a. MD-xxx.xxx – Name of standard
- b. MD-xxx.xxx – Name of standard

For all standards referred to on the plans the contractor must go to the Book of Standards which will have the most current version. The Book of Standards can be accessed at:

<http://apps.roads.maryland.gov/businesswithsha/bizStdsSpecs/desManualStdPub/publicationonline/ohd/bookstd/index.asp>

All items are to be constructed in accordance with the current version of the referenced standard at the time of construction.

- Temporary Traffic Control Typical (TTCT) should be provided (and listed as shown above) to show how traffic will be maintained during construction of improvements in the State right of way.

These are preliminary comments. More comprehensive comments (and an SHA Tracking number) will be provided once detailed engineering plans and supporting documents are submitted for review. Please feel free to forward the comments to the Applicant and their engineer; they can contact me if they have questions.

It looks like there is uncompleted work at the ramps at the MD 214/ Brightseat Rd intersection. I'm not certain if this is an SHA project, but I will look into it.

Thanks, Kwesi

Kwesi Woodroffe
Regional Engineer
District 3 Access Management
MDOT State Highway Administration
KWoodroffe@mdot.maryland.gov
301-513-7347 (Direct)
1-888-228-5003 – toll free
9300 Kenilworth Avenue,
Greenbelt, MD 20770
<http://www.roads.maryland.gov>



From: Bossi, Adam <Adam.Bossi@ppd.mncppc.org>
Sent: Friday, May 15, 2020 8:48 AM
To: Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>
Subject: RE: DSP-19060 Landover McDonald's

Hi Kwesi,

Sorry about that. It must have been an oversight. The Dropbox link to the DSP documents from DSP-19060 for Landover McDonalds is below. Just for quick orientation purposes – the site is located on the corner of Central Ave. and Brightseat Road, a short distance west of the on/off ramps to the beltway. Talk to you at SDRC a little later this morning.

This is an EPlan ACCEPTANCE referral for **DSP-19060, Landover McDonald's**. This case was officially accepted as of today, April 30, 2020. SDRC is scheduled for: **May 15, 2020**

Please submit ALL comments to Adam Bossi(email attached). Click on the hyperlink to view the case:

<https://www.dropbox.com/sh/edufzinu9sgbpiy/AAAhbOpqnRYOiG9IsTL7UZr-a?dl=0> .

Adam

Adam Bossi

Planner Coordinator | Urban Design Section | Development Review Division

 THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
Prince George's County Planning Department
14741 Governor Oden Bowie Drive, Upper Marlboro, MD 20772
301-780-8116 | adam.bossi@ppd.mncppc.org



From: Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>

Sent: Friday, May 15, 2020 8:44 AM
To: Bossi, Adam <Adam.Bossi@ppd.mncppc.org>
Subject: Re: DSP-19060 Landover McDonald's
Importance: High

Good morning Andrew,

I did not receive a referral for the subject project, so I can't comments at this time.

Please forward the referral to me and I will provide comments.

Thanks, Kwesi

Kwesi Woodroffe
Regional Engineer
District 3 Access Management
MDOT State Highway Administration
KWoodroffe@mdot.maryland.gov
301-513-7347 (Direct)
1-888-228-5003 – toll free
9300 Kenilworth Avenue,
Greenbelt, MD 20770
<http://www.roads.maryland.gov>



From: [Reilly, James V](#)
To: [Bossi, Adam](#)
Cc: [PGCReferrals](#)
Subject: FW: EPlan RE-referral for DSP-19060, LANDOVER McDONALD's (PB) via DROPBOX (SDRC Comments)
Date: Monday, June 1, 2020 9:55:26 AM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)
[image009.png](#)
[DSP-19060_D COVER.pdf](#)
Importance: High

Good Morning Adam,

The Fire/EMS Department has reviewed the provided referral for DSP-19060 Landover McDonald's. We have no comments at this time. Regards. Jim

James V. Reilly
Contract Project Coordinator III



Office of the Fire Marshal
Division of Fire Prevention and Life Safety
Prince George's County Fire and EMS Department
6820 Webster Street, Landover Hills, MD 20784
Office: 301-583-1830
Direct: 301-583-1838
Cell: 240-508-4931
Fax: 301-583-1945
Email: jvreilly@co.pg.md.us

From: ePlan [mailto:ePlan@ppd.mncppc.org]
Sent: Friday, May 29, 2020 7:54 AM
To: Smith, Tyler; Stabler, Jennifer; Hall, Ashley; Brake, Michelle; Henderson, Tamika; Franklin, Judith; Green, David A; Masog, Tom; Barnett-Woods, Bryan; Dixon, June; Chaconas, Sheila; Brooke E. Larman; Fields, Ernest; Reiser, Megan; Shoulars, Katina; Reilly, James V; sltoth@co.pg.md.us; Lane Dillon ; Richards, Dorothy A.; Gaskins, Tabitha; De Guzman, Reynaldo S.; Giles, Mary C.; Snyder, Steven G.; Abdullah, Mariwan; Formukong, Nanji W.; Tayyem, Mahmoud; Yuen, Steven; Contic, Wendy M.; Thweatt, Susan W.; Adepoju, Adebola O.; erigby@sha.state.md.us; #dsgintake@wsscwater.com; jchandler@hyattsville.org; PLANNING@hyattsville.org; Kate Powers; Thompson, Ivy
Cc: Bossi, Adam; Kosack, Jill; Hunt, James; Summerlin, Cheryl; Grigsby, Martin; Conner, Sherri
Subject: EPlan RE-referral for DSP-19060, LANDOVER McDONALD's (PB) via DROPBOX (SDRC Comments)
Importance: High

CAUTION: This email originated from an external email domain which carries the additional risk that it may be a phishing email and/or contain malware.

All,

This is a EPlan RE-referral for **DSP-19060, Landover McDonald's**. The **DUE DATE** for this RE-referral is: **June 8, 2020**

Please submit ALL comments to Adam Bossi(email attached). Click on the hyperlink to view the case: <https://www.dropbox.com/sh/bpzglajej8nlhp5/AADRhk-CVt4VAhspex8g04ja?dl=0> .

(Note: RE-referral is dated 5-28-2020 Revised in the Dropbox link)

Thank you...

Donald R. Townsend

Senior Planning Technician | Development Review Division

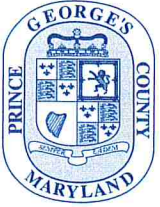
THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION
Prince George's County Planning Department

14741 Governor Oden Bowie Drive, Upper Marlboro, MD 20772

301-952-4688 | donald.townsend@ppd.mncppc.org

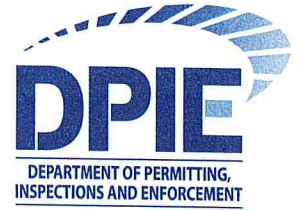


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Angela D. Alsobrooks
County Executive

THE PRINCE GEORGE'S COUNTY GOVERNMENT
Department of Permitting, Inspections and Enforcement
Site/Road Plan Review Division



MEMORANDUM

May 22, 2020

TO: Adam Bossi, Urban Design Section
Development Review Division, M-NCPPC

FROM: Mary C. Giles, P.E., Associate Director
Site/Road Plan Review Division, DPIE *Mary C. Giles* 5/21/2020

RE: Landover McDonalds
Detailed Site Plan No. DSP-19060

CR: Central Avenue (MD 214)

CR: Brightseat Road

In response to the Detailed Site Plan No. DSP-19060 referral, the Department of Permitting, Inspections and Enforcement (DPIE) offers the following:

- The property is located at 8710 Central Avenue (MD 214), at the north side of MD 214 at its intersection with Brightseat Road. Brightseat Road is a County-maintained roadway. The applicant shall dedicate additional right of way and improve Brightseat Road along the frontage, to comply with current requirements.
- The applicant is proposing to raze all existing structures and rebuild an eating and drinking establishment with drive-thru service.
- DSP-19060 is consistent with Concept Number 25146-2019, issued on October 16, 2019. DPIE has no objection to the proposed Detailed Site Plan.
- This memorandum incorporates the Site Development Plan Review pertaining to Stormwater Management (County Code 32-182(b)). The following comments are provided pertaining to this approval phase:
 - a) Final site layout, exact impervious area locations are shown on plans.
 - b) Exact acreage of impervious areas has not been provided.
 - c) Proposed grading is shown on plans.
 - d) Stormwater volume computations have not been provided.

9400 Peppercorn Place, Suite 230, Largo, Maryland 20774
Phone: 301.636.2060 ♦ <http://dpie.mypgc.us> ♦ FAX: 301.925.8510

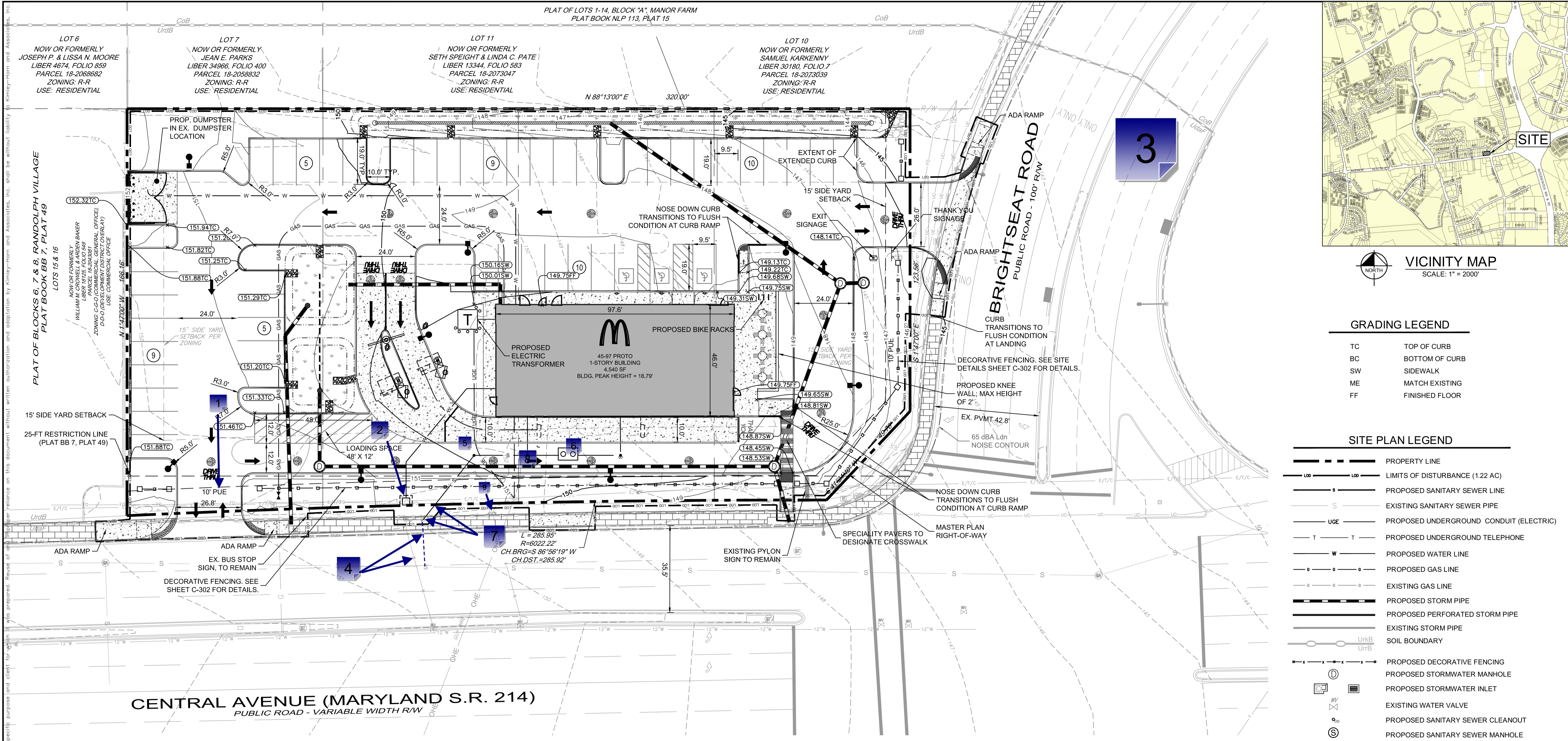
Adam Bossi
May 22, 2020
Page 2

- e) Erosion/sediment control plans that contain the construction sequence, and any phasing necessary to limit earth.
- f) Disturbances and impacts to natural resources, and an overlay plan showing the types and locations of ESD devices and erosion and sediment control practices are not included in the submittal.
- g) A narrative in accordance with the code has not been provided.
- h) The applicant shall provide items (a-g) at the time of filing final site permits.

If you have any questions or require additional information, please contact Mr. Steve Snyder, P.E., the District Engineer for the area, at (301) 883-5710.

MCG:DJK:dar

cc: Steve Snyder, P.E., District Engineer, S/RPRD, DPIE
Yonas Tesfai, P.E., Engineer, S/RPRD, DPIE
Dana Karzoun, Engineer, S/RPRD, DPIE
McDonalds Corporation, 6903 Rockledge Drive, Suite 1100,
Bethesda, MD 20817
McNamee & Hosea, 6411 Ivy Lane, Suite 200, Greenbelt,
Maryland 20770

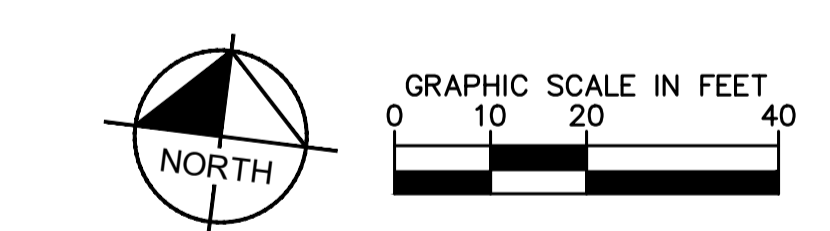


GRADING LEGEND

TC	TOP OF CURB
BC	BOTTOM OF CURB
SW	SIDEWALK
ME	MATCH EXISTING
FF	FINISHED FLOOR

SITE PLAN LEGEND

---	PROPERTY LINE
---	LIMITS OF DISTURBANCE (1.22 AC)
---	PROPOSED SANITARY SEWER LINE
---	EXISTING SANITARY SEWER PIPE
---	PROPOSED UNDERGROUND CONDUIT (ELECTRIC)
---	PROPOSED UNDERGROUND TELEPHONE
---	PROPOSED WATER LINE
---	PROPOSED GAS LINE
---	EXISTING GAS LINE
---	PROPOSED STORM PIPE
---	PROPOSED PERFORMED STORM PIPE
---	EXISTING STORM PIPE
---	SOIL BOUNDARY
---	PROPOSED DECORATIVE FENCING
---	PROPOSED STORMWATER MANHOLE
---	PROPOSED STORMWATER INLET
---	EXISTING WATER VALVE
---	PROPOSED SANITARY SEWER CLEANOUT
---	PROPOSED SANITARY SEWER MANHOLE
---	PROPOSED GREASE TRAP
---	EXISTING SANITARY SEWER MANHOLE
---	WATER BEND
---	TEST PIT
---	PROPOSED PARKING SPACES
---	PROPOSED CONCRETE PAVEMENT
---	PROPOSED STORMWATER FACILITY
---	PROPOSED BUILDING
---	PROPOSED LOADING AREA
---	EXISTING CONCRETE SIDEWALK
---	PROPOSED LIGHT



PARKING AND LOADING SCHEDULE

Description	Rate	Required	Provided
Parking	Eating or Drinking Establishment (including drive-through service or carryout)	1 space / 3 seats	23
	1,598 SF Net Floor Area (Excluding any areas used exclusively for storage or patron seating, and any exterior patron service area)	+ 1 space / 50 sf	32
Accessible	1 per 25 spaces	3	3
DDO Zoning	Minimum provided parking is 80% of required per Part 11 of Zoning Ordinance	-20%	
Total number of parking spaces		44	48
Loading	1,598 SF Net Floor Area	One Loading space for < 3,000 SF (33' x 12')	1
Bicycle	DDO requirement per Subregion 4 Master Plan and Sectional Map Amendment	1 space per 20 vehicle parking spaces	3

NOTE
 FOR LOCATION OF UTILITIES CALL 8-1-1 OR 1-800-257-7777 OR LOG ON TO www.ca811.com
 http://www.missutility.net
 48 HOURS IN ADVANCE OF ANY WORK IN THIS VICINITY
 INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATION OF THE MAINS BY DIGGING TEST PITS BY HAND AT ALL UTILITY CROSSINGS WELL IN ADVANCE OF THE START OF EXCAVATION.

THIS BLOCK IS FOR OFFICIAL USE ONLY

QR label certifies that this plan meets conditions of final approval by the Planning Board, its designee or the District Council.

M-NCPPC APPROVAL

PROJECT NAME:
PROJECT NUMBER:

For Conditions of Approval see Site Plan Cover Sheet or Approval Sheet
 Revision Numbers must be included in the Project Number

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PROFESSIONAL CERTIFICATION: I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. _____ EXPIRATION DATE _____

KHA PROJECT: 110366028 DATE: 3/19/2020 SCALE: AS SHOWN DESIGNED BY: TCF DRAWN BY: AY CHECKED BY: RM

DETAILED SITE PLAN

MCDONALD'S REBUILD PREPARED FOR MCDONALD'S CORPORATION

PRINCE GEORGE'S COUNTY, MD DSP-19060

SHEET NUMBER: **C-200**
 SHEET 4 OF 12

DSP-19060_Backup 47 of 55

1 - PUE

Created by: Rufus Leeth
On: 05/11/2020 09:06 AM

It is highly recommended the planning of the proposed improvements (dry utilities, water, sewer, drainage, ESD's, fencing, etc.) are properly designed to ensure the restrictions imposed by the PUE are not infringed upon.

Changes to the design of the proposed improvements may impede the ability to the meet all standards and specifications.

Note WSSC standards regarding PUE's in relation to water and sewer and ensure all are met.

----- 0 Replies -----

2 - Outside meter

Created by: Rufus Leeth
On: 05/11/2020 09:13 AM

Not allowed inside a PUE. Will need to be relocated.

----- 0 Replies -----

3 - Existing WSSC Water and Sewer

Created by: Rufus Leeth
On: 05/11/2020 09:15 AM

Properly show and label all existing WSSC mains per our guidelines.

See comments regarding work over existing mains, PCCP guidelines, easement requirements, and other pertinent information as cited in the comments on Sheet 2 of 2 of the Survey.

----- 0 Replies -----

4 - House Connections

Created by: Rufus Leeth
On: 05/11/2020 09:18 AM

Show all existing and proposed connections.

Ensure all standards and policies are maintained for such.

----- 0 Replies -----

5 - Sewer

Created by: Rufus Leeth
On: 05/11/2020 09:19 AM

WSSC requires manholes for all changes in direction on gravity sewer for mains 6" and larger.

Ensure the standards are satisfied.

----- 0 Replies -----

6 - Grease Trap

Created by: Rufus Leeth
On: 05/11/2020 09:20 AM

Refer to WSSC Regulatory Service's Divisions standards and polices regarding grease traps.

----- 0 Replies -----

7 - WSSC Sewer Houce Connection (SHC)

Created by: Rufus Leeth
On: 05/11/2020 09:21 AM

Verify where the SHC ends and ensure WSSC's standards and policies are satisfied.

----- 0 Replies -----

8 - Separation Between Drainage and Sewer

Created by: Rufus Leeth
On: 05/11/2020 09:22 AM

Ensure WSSC's standards are met.

----- 0 Replies -----

9 - Existing Fire Hydrant

Created by: Rufus Leeth
On: 05/11/2020 09:23 AM

A WSSC Developer Relocation Permit will be required to make adjustments to the existing fire hydrant

----- 0 Replies -----

3

LEGEND OF SYMBOLS

Contour Line	65	Setback Line	—	Benchmark	⊕	Grease Trap	⊕	Water Valve	⊕
Water Line	—	Traffic Signal Light	⊕	Turn Arrows	↔	Cleanout	⊕	Gas Valve	⊕
Gas Line	—	Accessible parking Symbol	♿	Fire Hydrant	⊕	Utility Pole	⊕	Guy Wire	⊕
Overhead Electric, Tele., & Cable TV	E/T/C	Curb Inlet	⊕	Right-of-Way	R/W	Sign	⊕	Speaker Box	⊕
Overhead Telephone	—	Underground Electric	—	Storm Manhole	⊕	Illuminated Signs	⊕	Spot Elevation	⊕
Underground Cable	—	Chain Link Fence	—	Sanitary Manhole	⊕	Trees	⊕	Ballard	⊕
Underground Fiber Optic	—	Storm Sewer	—	Electric Vault	⊕	Menu Board	⊕	Flagpole	⊕
Wood Fence	—	Sanitary Sewer	—	Electric Vault	⊕	Brick	⊕	MD State Plane Grid Marks	⊕

ZONING DISTRICT R-R (RURAL RESIDENTIAL)

ZONING DISTRICT R-R (RURAL RESIDENTIAL)

PLAT OF BLOCKS 6, 7 & 8, RANDOLPH VILLAGE
PLAT BOOK BB 7, PLAT 49

PLAT OF LOTS 1-14, BLOCK "A", MANOR FARM
PLAT BOOK NLP 113, PLAT 15

LOT 6
NOW OR FORMERLY
JOSEPH P. & LISSA N. MOORE
LIBER 4674, FOLIO 859
PARCEL 18-2088682

LOT 7
NOW OR FORMERLY
JEAN E. PARKS
LIBER 34866, FOLIO 400
PARCEL 18-2058832

LOT 11
NOW OR FORMERLY
SETH SPEIGHT & LINDA C. PATE
LIBER 13344, FOLIO 583
PARCEL 18-2073047

LOT 10
NOW OR FORMERLY
SAMUEL KARKENNY
LIBER 30180, FOLIO 7
PARCEL 18-2073039

PLAT OF BLOCKS 6, 7 & 8, RANDOLPH VILLAGE
PLAT BOOK BB 7, PLAT 49

LOT 16

LOT 15

ZONING DISTRICT M-U-1 (MIXED USE-INFLLL)
ALSO D-D-0 (DEVELOPMENT DISTRICT OVERLAY)

PARCEL ONE OF TITLE COMMITMENT
NOW OR FORMERLY
MCDONALD'S CORPORATION
LIBER 4826, FOLIO 942
PLAT BOOK BB 7, PLAT 49
PARCEL I.D. A18-0004

PARCEL TWO OF TITLE COMMITMENT
NOW OR FORMERLY
MCDONALD'S CORPORATION
LIBER 4826, FOLIO 942
PLAT BOOK BB 7, PLAT 49
PARCEL I.D. A18-0004

PARCEL THREE OF TITLE COMMITMENT
NOW OR FORMERLY
MCDONALD'S CORPORATION
LIBER 4826, FOLIO 944
PARCEL 18-2065795

1-STORY BLOCK BUILDING
"MCDONALD'S RESTAURANT"
ZONING DISTRICT M-U-1
(MIXED USE-INFLLL)
ALSO D-D-0 (DEVELOPMENT DISTRICT OVERLAY)

TOTAL AREA = 1.173 ACRES
= 51,099.77 SQ. FT.

30' FRONT YARD SETBACK PER ZONING
HEIGHT OF BUILDING = 18.06'

25' RESTRICTION LINE
(PLAT 89 7, PLAT 49)

80.0' THESE SPACES RESERVED FOR DRIVE-THRU ORDERS

POINT OF BEGINNING
AS-SURVEYED LEGAL DESCRIPTION

L=285.95'
R=6022.22'
CH.BRG.=S 86°56'19" W
CH.DST.=285.92'

CENTRAL AVENUE (MARYLAND S.R. 214)
PUBLIC ROAD
VARIABLE WIDTH R/W

BRIGHTSEAT ROAD 100'

1

2

6

CURB INLET W/MANHOLE
RIM=152.21'
INV.=148.91' (15" RCP-N)

CURB INLET W/MANHOLE
RIM=151.60'
INV.=NOT KNOWN

CURB INLET W/MANHOLE
RIM=148.16'
INV.=143.78' (15" RCP-S)

CURB INLET W/MANHOLE
RIM=142.48'
INV.=UNKNOWN

CURB INLET W/GRATE
RIM=141.20'
INV.=134.05' (30" RCP-SW)

CURB INLET W/GRATE
RIM=144.51'
INV.=133.60' (30" RCP-S)

CURB INLET W/GRATE
RIM=143.38'
INV.=135.93' (30" RCP-NE)

STORM MANHOLE
RIM=143.65'
INV.=136.49' (30" RCP-SW)

STORM MANHOLE
RIM=143.65'
INV.=136.49' (30" RCP-N)

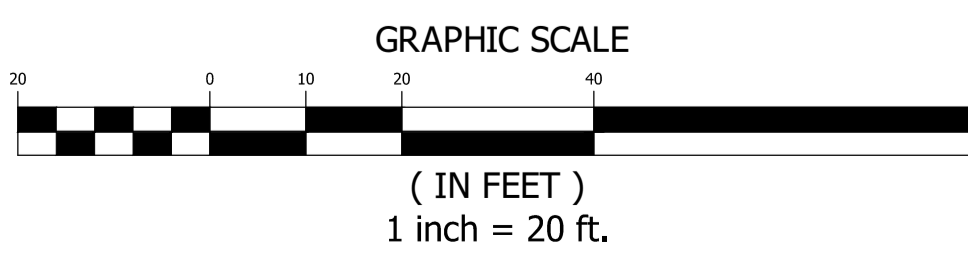
Sanitary Manhole
RIM=141.52'
INV.=NOT KNOWN

Sanitary Manhole Bolted Shut
RIM=145.08'
INV.=NOT KNOWN

STORM MANHOLE
RIM=147.73'
INV.=142.34' (15" RCP-NW)

STORM MANHOLE
RIM=142.21'
INV.=142.21' (30" RCP-NE)

STORM MANHOLE
RIM=142.35'
INV.=142.35' (15" RCP-S)



SERIAL NUMBER:
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CONSTRUCTION PHASE AND 10 WORKING
DAYS IN DESIGN STAGE - STOP CALL
PENNSYLVANIA ONE CALL SYSTEM, INC.
1-800-242-1776

REV.	DATE	DESCRIPTION
1	09/24/18	ORIGINAL SUBMITTAL
2	09/24/18	ADDED TITLE COMMITMENT NO. & DATE

MIDM
MILLFRIED, DIDIANO, & MOX, LLC
Site Planners, Engineers, Surveyors
8851 Kind Drive
Pittsburgh, PA 15237
Ph.: (724) 934-2810 Fax: (724) 934-2811
www.midm.com

McDonald's Restaurant L/C 19-0142
8710 Central Avenue
County of Prince Georges
State of Maryland
ALTA/NPS LAND TITLE SURVEY



SCALE:
1"=20'
DATE:
09/24/18
DRAWN BY:
LB
CHECKED BY:
HGM

SHEET NUMBER:
2 OF 2
PROJECT NO. 7842

1 - -WSSC Plan Review Comments

Created by: Dagoberto Beltran
On: 05/07/2020 10:17 AM

Plan # DSP-19060
McDonald's Landover - 8710 Central Ave

----- 0 Replies -----

2 - - WSSC Standard Comments for all plans

Created by: Dagoberto Beltran
On: 05/07/2020 10:19 AM

1. WSSC comments are made exclusively for this plan review based on existing system conditions at this time. We will reevaluate the design and system conditions at the time of application for water/sewer service.
2. Coordination with other buried utilities:
 - a. Refer to WSSC Pipeline Design Manual pages G-1 and G-2 for utility coordination requirements.
 - b. No structures or utilities (manholes, vaults, pipelines, poles, conduits, etc.) are permitted in the WSSC right-of-way unless specifically approved by WSSC.
 - c. Longitudinal occupancy of WSSC rights-of-way (by other utilities) is not permitted.
 - d. Proposed utility crossings of WSSC pipelines or rights-of-way that do not adhere to WSSCs pipeline crossing and clearance standards will be rejected at design plan review. Refer to WSSC Pipeline Design Manual Part Three, Section 3.
 - e. Failure to adhere to WSSC crossing and clearance standards may result in significant impacts to the development plan including, impacts to proposed street, building and utility layouts.
 - f. The applicant must provide a separate Utility Plan to ensure that all existing and proposed site utilities have been properly coordinated with existing and proposed WSSC facilities and rights-of-way.
 - g. Upon completion of the site construction, utilities that are found to be located within WSSCs rights-of-way (or in conflict with WSSC pipelines) must be removed and relocated at the applicants expense.
3. Forest Conservation Easements are not permitted to overlap WSSC existing or proposed easements. Potential impacts to existing Forest Conservation Easements (due to proposed water and/or sewer systems) must be reviewed and approved by County staff.
4. Unless otherwise noted: ALL extensions of WSSCs system require a request for Hydraulic Planning Analysis and need to follow the System Extension Permit (SEP) process. Contact WSSC's Permit Services Section at (301-206-8650) or visit our website at <https://www.wsscwater.com/business--construction/developmentconstruction-services.html> for requirements. For information regarding connections or Site Utility (on-site) reviews, you may visit or contact WSSC's Permit Services Section at (301) 206-4003.

----- 0 Replies -----

3 - DSD Review Comments

Created by: Rufus Leeth
On: 05/07/2020 11:13 AM

DESIGN COMMENTS:

WATER

0This site is currently being served by existing and active water connection(s).

0A large diameter PCCP water pipeline is located within or adjacent to this property. Service connections to this existing PCCP water mains are not permitted.

0Design water service connection(s) to avoid environmental, storm water management facilities, ESD Devices, other utilities, landscaping, tree boxes and structures or paving impacts for future maintenance.

0Existing water mains shown on plan should be labeled with correct pipe size, material and WSSC contract number.

0Show and label easement limits on plan for all existing and proposed water mains.

0Provide proper protection of water supply where water main is below or parallel to sewer main, building drain, sewer house connection or septic field and when pipe crosses other utilities.

0Revise the plan to realign any water pipeline that conflicts with large storm drains, culverts, deep side ditches, etc. Maintain the required horizontal clearances from other utilities, retaining walls, sediment traps, street lights, paving, etc. See WSSC 2017 Pipeline Design Manual Part Three, Section 3; Pipeline Crossings and Clearances.

0There is a 24- inch diameter water main located on or near this property. WSSC records indicate that the pipe material is Prestressed Concrete Cylinder Pipe (PCCP). Prior to submittal for review, it is the applicant's responsibility to test pit the line and determine its exact horizontal and vertical location as well as to verify the type of pipe material. The applicant's engineer is responsible for coordinating with WSSC for monitoring and inspecting test pits for this project.

0Water pipelines 12-inch and smaller must have the greater of: a minimum of 15 feet horizontal separation from any building or dwelling or a 1:1 slope from the bottom of the foundation of the existing or proposed building to the bottom edge of the pipeline trench.

0Water pipelines larger than 12-inch, including PCCP mains, must have a minimum of 25 feet horizontal separation from any building or dwelling. The building must also be outside the WSSC existing or proposed easement.

0Notes for Special Construction Requirements within the Vicinity of Existing PCCP water mains shall be added to all design plans, including the Erosion and Sediment Control Plans.

0Some construction activities may require the shutdown of these larger diameter PCCP water Mains. The shutdown schedule will be determined solely by WSSC and is dependent on the time of year and the coordination of the shutdown with other repairs and maintenance. The Applicant is encouraged to coordinate the timing of the shutdown with WSSC as early as possible and plan accordingly.

0Notes for Special Construction Requirements shall be added to all design plans. See WSSC 2017 Pipeline Design Manual, Part Three, Section 3.i. - Working in the Vicinity of Existing PCCP and Larger Water Mains.

SEWER

0This site is currently being served by existing and active sewer connection(s).

0For sewer pipelines 12-inch and smaller in diameter, provide a minimum separation from a building or dwelling the greater of the following: fifteen (15) feet horizontal separation or a distance on a 1:1 slope from the bottom of the foundation of the existing or proposed building or dwelling to the bottom edge of the pipeline trench

0Existing sewer mains shown on plan should be labeled with correct pipe size, material and WSSC contract number.

0Design sewer service connection(s) to avoid environmental, storm water management facilities, ESD Devices, other utilities, landscaping, tree boxes and structures or paving impacts for future maintenance. See WSSC 2017 Pipeline Design Manual Part Three, Section 3; Pipeline Crossings and Clearances.

0Show and label easement limits on plan for all existing and proposed sewer mains.

0Revise the plan to realign any sewer pipeline that conflicts with large storm drains, culverts, deep side ditches, etc. Maintain the required horizontal clearances from other utilities, retaining walls, sediment traps, street lights, paving, etc. See WSSC 2017 Pipeline Design Manual.

SITE UTILITY

0OUTSIDE METERS - 3-inch and larger meter settings shall be furnished and installed by the utility contractor in an outside meter vault. Show and label vault and required WSSC easement. WSSC prefers an outside meter in a vault, however and indoor meter may be allowed under certain conditions. See WSSC 2019 Plumbing & Fuel Gas Code 111.5.7 & 603.4.1

EASEMENTS

0WSSC easements must be free and clear of other utilities, including storm drain systems, ESD devices, gas, electric, telephone, CATV, etc., with the exception of allowed crossings designed in accordance with the WSSC 2017 Pipeline Design Manual. Landscaping and Hardscaping are also not allowed without approval. Under certain conditions (and by special request) the items listed above may be permitted within the WSSC easement. However, this will be evaluated on a case by case basis and if allowed, will require execution of a special agreement and/or Hold Harmless Agreement between WSSC and the developer.

0WSSCs minimum easement width for a normal (14-inch diameter or less) pipeline (water or sewer at normal depth) is 20-feet. When both water and sewer (normal diameter and depth) are installed in the same easement, the minimum width is 30-feet. Installation of deep or large water/sewer will require additional easement width.

0The minimum horizontal clearance from a building to the outside diameter of a WSSC pipeline is 15-feet. The minimum spacing between adjacent buildings with both water and sewer lines between them must be 40-feet. In some cases where connections, fire hydrants, or deep water/sewer lines are involved, additional easement width is required.

0Balconies or other building appurtenances must not encroach within WSSC easements. Water/Sewer pipeline alignment should maintain a minimum 5-foot horizontal clearance from storm-drain pipeline/structures and other utilities. Review of plan submitted does not meet these requirements.

GENERAL

0Follow WSSC Demolition/Abandonment procedures to obtain a County Raze Permit. Note: Failure to obtain an SDC fixture credit permit inspection prior to the removal of existing fixtures

will result in the issuance of Basic Credit Only. To obtain System Development Charge (SDC) credits for existing plumbing fixtures, an SDC Fixture Count Inspection MUST be completed by a WSSC Regulatory Inspector BEFORE REMOVAL OF FIXTURES OR DEMOLITION of the structure. The inspection requires a permit which can only be obtained through a WSSC Registered Master Plumber. SDC Fixture Credit Procedures are available at the WSSC Permit Services website.

0Any grading change in pipe loading (including but not limited to proposed fill or excavation), adjustment to manhole rims, fire hydrant relocations, placement of access roads or temporary haul roads, temporary sediment control devices, paving construction or construction related activity of any kind over an existing WSSC water or sewer main or within an existing WSSC right-of-way requires advance approval by WSSC. Any proposed public street grade establishment plan (GEP) with an existing WSSC water or sewer main of any size located within the existing or proposed public street right-of-way requires WSSC approval directly on the original GEP prior to approval of the GEP by the County Department of Public Works and Transportation. Any work (design, inspection, repair, adjustment, relocation or abandonment of existing WSSC facilities) is done at the sole expense of the applicant/builder/developer. Contact WSSC Development Services Division for review procedures and fee requirements. See WSSC 2017 Pipeline Design Manual, Part Three, Section 5 & Section 11.

0Show and label all existing nearby water and/or sewer service connections that may be impacted by the proposed development.

0WSSC facilities/structures cannot be located with a public utility easement (PUE) however WSSC pipelines may cross over a PUE. Revise the plan to relocate any pipeline, valve, fire hydrant, meter vault and any other WSSC facilities/structures outside of the PUE.

HYDRAULICS COMMENTS:

GENERAL

0Site Utility System reviews are required for projects with proposed water connections greater than 2-inch or sewer connections greater than 4-inch. Contact the WSSC Permit Services Unit on (301) 206-8650 for submittal requirements or view our website.

WATER

0A 12-inch water main is available to serve the proposed site. Contact the Permit Services Unit at (301) 206-8650 for details regarding applying for service connections or visit our website.

SEWER

0A 8-inch gravity sewer main is available to serve the proposed site. Contact the Permit Services Unit at (301) 206-8650 for details regarding applying for service connections or visit our website.

----- 0 Replies -----

4 - Existing WSSC Facilities

Created by: Rufus Leeth
On: 05/07/2020 11:42 AM

Impacts to existing WSSC facilities will require a Developer Relocations Permit

----- 0 Replies -----

5 - Existing House Connections

Created by: Rufus Leeth
On: 05/07/2020 11:52 AM

All unused connections will need to be abandoned

----- 0 Replies -----

6 - Existing 24" PCCP Main

Created by: Rufus Leeth
On: 05/07/2020 11:53 AM

See comments regarding work in the vicinity of PCCP Mains.

----- 0 Replies -----