



The Maryland-National Capital Park and Planning Commission
Prince George's County Planning Department
Development Review Division
301-952-3530

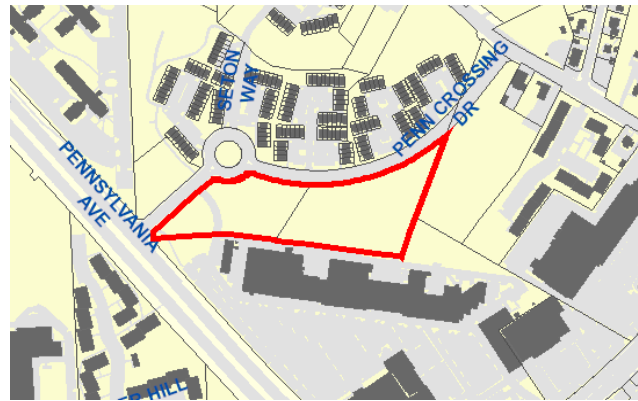
Note: Staff reports can be accessed at <http://mncppc.iqm2.com/Citizens/Default.aspx>

Conceptual Site Plan Penn Place 1

CSP-87128-02

REQUEST	STAFF RECOMMENDATION
<p>This case was continued from the Planning Board hearing date of March 16, 2023 to April 6, 2023.</p> <p>Development of 168 multifamily units and 767 square feet of commercial space</p>	<p>With conditions recommended herein:</p> <ul style="list-style-type: none"> • APPROVAL of Conceptual Site Plan CSP-87128-02 • APPROVAL of Type 1 Tree Conservation Plan TCP1-017-2022

Location: On the south side of Penn Crossing Drive, approximately 800 feet west of its intersection with Pennsylvania Avenue	
Gross Acreage:	7.54
Zone:	RMF-48
Zone Prior:	M-X-T
Reviewed per prior Zoning Ordinance:	Section 27-1903(c)
Dwelling Units:	168
Gross Floor Area:	182,767 sq. ft.
Planning Area:	75A
Council District:	07
Municipality:	N/A
Applicant/Address: Northern Real Estate Urban Ventures 1401 New York Avenue, NE Washington, DC 20002	
Staff Reviewer: Tom Burke Phone Number: 301-952-2739 Email: thomas.burke@ppd.mncppc.org	



Planning Board Date:	04/06/2023
Planning Board Action Limit:	Waived
Staff Report Date:	03/01/2023
Date Accepted:	01/05/2023
Informational Mailing:	03/16/2022
Acceptance Mailing:	12/30/2022
Sign Posting Deadline:	02/14/2023

The Planning Board encourages all interested persons to request to become a person of record for this application. Requests to become a person of record may be made online at http://www.mncppcapps.org/planning/Person_of_Record/. Please call 301-952-3530 for additional information.

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THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION

PRINCE GEORGE'S COUNTY PLANNING BOARD

STAFF REPORT

SUBJECT: Conceptual Site Plan CSP-87128-02
Type 1 Tree Conservation Plan TCP1-017-2022
Penn Place 1

The Urban Design staff has completed the review of the subject application and appropriate referrals. The following evaluation and findings lead to a recommendation of APPROVAL with conditions, as described in the Recommendation section of this report.

EVALUATION CRITERIA

This property is within the Residential, Multifamily-48 (RMF-48) Zone; however, this application is being reviewed and evaluated in accordance with the prior Prince George's County Zoning Ordinance, pursuant to Section 27-1903(c) of the Zoning Ordinance, which allows an application for a project of any type for properties in the Mixed Use-Transportation Oriented (M-X-T) Zone to be reviewed and approved under the prior Zoning Ordinance. This conceptual site plan was reviewed and evaluated for conformance with the following criteria:

- a. The requirements of the prior Prince George's County Zoning Ordinance, specifically for the M-X-T Zone, and the site design guidelines;
- b. The requirements of the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance;
- c. The requirements of other site plan related regulations; and
- d. Referral comments.

FINDINGS

Based upon the evaluation and analysis of the subject application, the Urban Design staff recommend the following findings:

1. **Request:** This application requests approval of a conceptual site plan (CSP) for a mixed-used development consisting of 168 multifamily units and 767 square feet of commercial space.

2. Development Data Summary:

	EXISTING	PROPOSED
Zone	RMF-48	Prior M-X-T
Use(s)	Vacant	Residential/ Commercial
Site Acreage	7.54	7.54
Gross Floor Area (GFA) (sq. ft.)		
Commercial/Retail	-	767
Residential	-	182,000
Total GFA (sq. ft.)		182,767
Total Residential Dwelling Units	-	168

Floor Area Ratio (FAR) in the M-X-T Zone

Base Density Allowed	0.40 FAR
Total FAR Permitted	0.40 FAR
Total FAR Proposed	0.12 FAR

3. **Location:** This property is located on the south side of Penn Crossing, approximately 800 feet west of its intersection with MD 4 (Pennsylvania Avenue), in Planning Area 75A, Council District 7.
4. **Surrounding Uses:** The site is bounded to the east by an undeveloped parcel in the Residential, Multifamily-20 (RMF-20) Zone; to the south by a commercial/retail center in the Commercial, General Office (CGO) Zone; to the west by MD 4, with multifamily in the RMF-20 Zone beyond; and to the north by townhomes in the Residential, Multifamily-48 (RMF-48) Zone.
5. **Previous Approvals:** This property was part of a larger 35.99-acre tract of land which was placed in the Mixed Use-Transportation Oriented (M-X-T) Zone, pursuant to adoption of the *Suitland-District Heights and Vicinity Sectional Map Amendment (SMA)* in 1986.

On March 24, 1988, the Prince George’s County Planning Board approved CSP-87128 (PGCPB Resolution No. 88-126) for the overall 35.99-acre property, inclusive of this parcel. The Board approved up to 550,000 square feet of residential with 550 multiplex units, 100,000 square feet of office or industrial uses, 6,000 square feet of retail space, and 6,000 square feet of day care, subject to 12 conditions. CSP-87128-01 was approved by the Board on December 12, 1991 (PGCPB Resolution No. 91-451) to reduce the 550 multiplex units to 200 townhouse units, with five conditions.

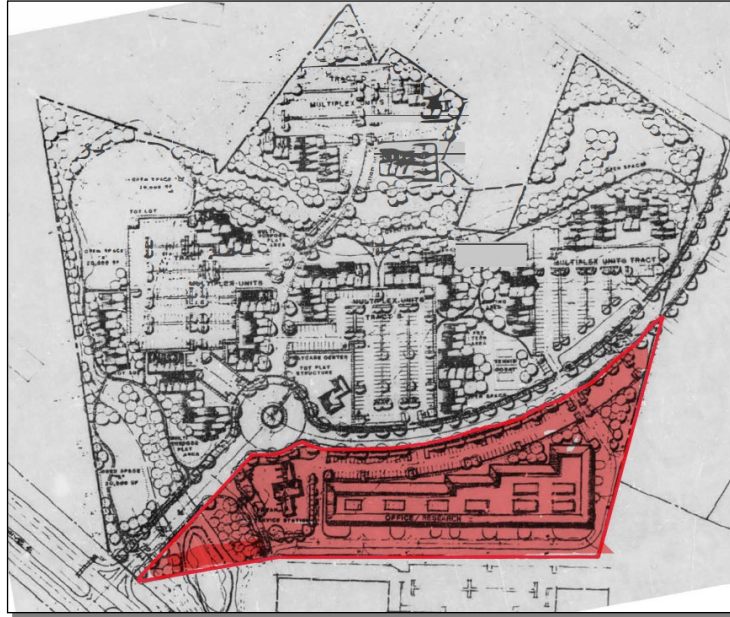


Figure 1: Original CSP with Subject Site in red

On November 29, 2021, the Prince George’s County District Council approved Resolution CR-136-2021, the Countywide Sectional Map Amendment (CMA), which reclassified the subject property from the M-X-T Zone to the RMF-48 Zone, effective April 1, 2022.

6. **Design Features:** The applicant proposes to develop the property as a mixed-use development project consisting of a maximum of 168 residential units, separated into four buildings, containing 42 units each, and 767 square feet of commercial space. The site will be accessed from Penn Crossing Drive, a fully improved roadway. The applicant proposes two points of vehicular access to the site, both of which originate along Penn Crossing Drive. These access points will be further evaluated with the preliminary plan of subdivision (PPS) and detailed site plan (DSP) applications. The conceptual design shows the buildings fronting on Penn Crossing Drive, with the parking concealed in the rear of the buildings.

The 767 square feet of commercial space is proposed on the first floor of one of the proposed residential buildings and it will be the offices of Project Community Capital (PCC). PCC is a social capital platform that connects people in affordable housing communities with jobs. PCC bridges the social capital of employment decision-makers of companies with individuals in affordable housing communities. PCC ensures that subcontractors who have socio-economic goals can meet their requirements by connecting them with individuals who are “ready-to-work.” The illustrative plan provided below shows the multifamily buildings, the location of the commercial component, and the proposed stormwater management devices.

Given the scale, configuration, and unit type of the proposed development, there are ample opportunities for sustainable and green building techniques. The applicant should apply those techniques, as practical, at time of the DSP. A condition has been included in the Recommendation section of this report requiring the applicant to provide details on sustainable site and green building techniques that will be used in this development, at the time of the DSP.

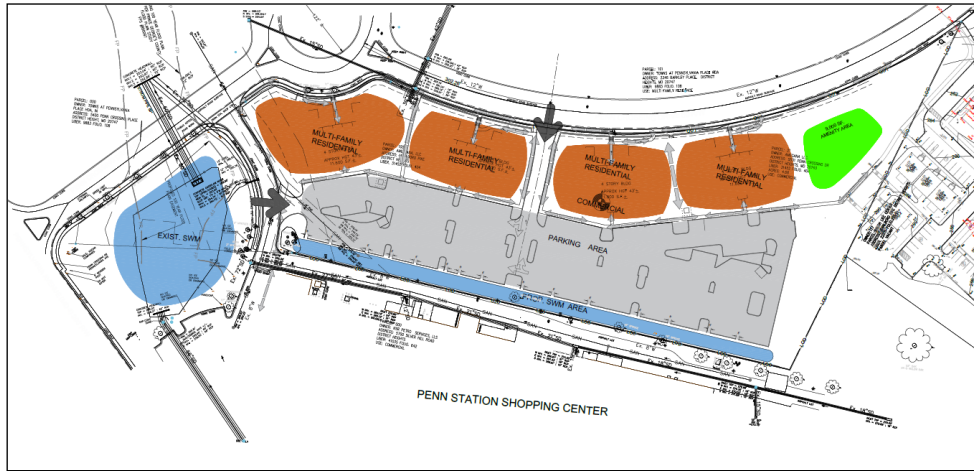


Figure 2: Conceptual Site Plan

Recreational facilities are proposed on-site with this application and are represented as a 9,000-square-foot amenity area shown in green in Figure 2, above. The details of the amenity area were not provided with this application and will be evaluated with the PPS and DSP applications. The Prince George’s County Department of Parks and Recreation (DPR) supports the provision of on-site recreational facilities to fulfill the mandatory dedication of parkland requirement; however, staff recommend that the amenity area be more centrally located within the community. A condition to evaluate alternative locations for the amenity area is provided herein.

COMPLIANCE WITH EVALUATION CRITERIA

7. **Prince George’s County Zoning Ordinance:** This CSP has been reviewed for compliance with the requirements of the M-X-T Zone and site design guidelines of the prior Zoning Ordinance.
 - a. The subject application is in conformance with the requirements of Section 27-547, Uses Permitted, of the prior Zoning Ordinance, which governs uses in all mixed-use zones, as follows:
 - (1) The proposed residential and commercial uses are permitted in the prior M-X-T Zone. Per Footnote 7 of the Table of Uses, the maximum number and type of dwelling units should be determined at the time of CSP approval. Therefore, development of this property would be limited to the numbers and types, as proposed in this CSP, that cannot exceed 168 multifamily dwelling units.
 - (2) Section 27-547(d) provides standards for the required mix of uses for sites in the M-X T Zone, as follows:
 - (d) **At least two (2) of the following three (3) categories shall be included on the Conceptual Site Plan and ultimately present in every development in the M-X-T Zone. In a Transit District**

Overlay Zone, a Conceptual Site Plan may include only one of the following categories, provided that, in conjunction with an existing use on abutting property in the M-X-T Zone, the requirement for two (2) out of three (3) categories is fulfilled. The Site Plan shall show the location of the existing use and the way that it will be integrated in terms of access and design with the proposed development. The amount of square footage devoted to each use shall be in sufficient quantity to serve the purposes of the zone:

- (1) Retail businesses;**
- (2) Office, research, or industrial uses;**
- (3) Dwellings, hotel, or motel.**

This CSP proposes two types of uses, as required, including residential and commercial uses. These proposed uses, in the amount shown, satisfy the mixed-use requirement of Section 27-547(d).

- b. Section 27-548 of the prior Zoning Ordinance, M-X-T Zone regulations, establishes additional standards for development in this zone. The CSP's conformance with the applicable provisions is discussed, as follows:

(a) Maximum floor area ratio (FAR):

- (1) Without the use of the optional method of development—0.40 FAR**
- (2) With the use of the optional method of development—8.0 FAR**

A FAR of 0.12 is proposed in this CSP. The optional method of development, which allows additional FAR on top of the base 0.40 FAR, was not utilized with this application. This will be evaluated further, at the time of DSP.

(b) The uses allowed in the M-X-T Zone may be located in more than one (1) building, and on more than one (1) lot.

The applicant proposes to include the uses on the prior M-X-T-zoned property, in multiple buildings, on a single lot.

(c) Except as provided for in this Division, the dimensions for the location, coverage, and height of all improvements shown on an approved Detailed Site Plan shall constitute the regulations for these improvements for a specific development in the M-X-T Zone.

This requirement is not applicable, since this application is for a CSP. Subsequent DSP approvals will provide regulations for development on this property.

(d) Landscaping, screening, and buffering of development in the M-X-T Zone shall be provided pursuant to the provisions of the

Landscape Manual. Additional buffering and screening may be required to satisfy the purposes of the M-X-T Zone and to protect the character of the M-X-T Zone from adjoining or interior incompatible land use.

The development is subject to the requirements of the 2010 *Prince George's County Landscape Manual* (Landscape Manual). Additional buffering and screening may be required to satisfy the purposes of the M-X-T Zone, and to protect the character of the M X-T Zone from adjoining and interior incompatible land uses, at the time of DSP.

- (e) **In addition to those areas of a building included in the computation of gross floor area (without the use of the optional method of development), the floor area of the following improvements (using the optional method of development) shall be included in computing the gross floor area of the building of which they are a part: enclosed pedestrian spaces, theaters, and residential uses. Floor area ratios shall exclude from gross floor area that area in a building or structure devoted to vehicular parking and parking access areas (notwithstanding the provisions of Section 27-107.01). The floor area ratio shall be applied to the entire property which is the subject of the Conceptual Site Plan.**

The FAR for the proposed development, of up to 182,767 square feet on the 7.54-acre property, is 0.12. This will be refined further, at the time of DSP, relative to the final proposed gross floor area of the buildings, in conformance with this requirement.

- (f) **Private structures may be located within the air space above, or in the ground below, public rights-of-way.**

There are no private structures within the air space above, or in the ground below, public rights-of-way, as part of this project.

- (g) **Each lot shall have frontage on, and direct vehicular access to, a public street, except lots for which private streets or other access rights of way have been authorized pursuant to Subtitle 24 of this Code.**

This CSP shows two access points on Penn Crossing Drive, a public street.

- (h) **Townhouses developed pursuant to a Detailed Site Plan for which an application is filed after December 30, 1996, shall be on lots at least one thousand two hundred (1,200) square feet in size, and shall have at least sixty percent (60%) of the full front facades constructed of brick, stone, or stucco. In addition, there shall be no more than eight (8) townhouses per building group, except where the applicant demonstrates to the satisfaction of the Planning Board or District Council, as applicable, that more than eight (8) dwelling units (but not more than ten (10) dwelling units) would create a more attractive living environment or would be more environmentally sensitive. In no**

event shall the number of building groups containing more than eight (8) dwelling units exceed twenty percent (20%) of the total number of building groups in the total development. The minimum building width in any continuous, attached group shall be eighteen (18) feet, and the minimum gross living space shall be one thousand two hundred and fifty (1,250) square feet. For the purposes of this Subsection, gross living space shall be defined as all interior building space except the garage and unfinished basement or attic area. The minimum lot size, maximum number of units per building group and percentages of such building groups, and building width requirements and restrictions shall not apply to townhouses on land any portion which lies within one-half (½) mile of an existing or planned mass transit rail station site operated by the Washington Metropolitan Area Transit Authority and initially opened after January 1, 2000. In no event shall there be more than ten (10) dwelling units in a building group and no more than two (2) building groups containing ten (10) dwelling units. For purposes of this section, a building group shall be considered a separate building group (even though attached) when the angle formed by the front walls of two (2) adjoining rows of units is greater than forty-five degrees (45°). Except that, in the case of a Mixed-Use Planned Community, there shall be no more than eight (8) townhouses per building group, except when the applicant demonstrates to the satisfaction of the Planning Board or District Council, as applicable, that more than eight (8) dwelling units (but not more than ten (10) dwelling units) would create a more attractive living environment or would be more environmentally sensitive. In no event shall the number of building groups containing more than eight (8) dwelling units exceed twenty percent (20%) of the total number of building groups in the total development. The minimum building width in any continuous, attached group shall be eighteen (18) feet, and the minimum gross living space shall be one thousand two hundred and fifty (1,250) square feet. For the purposes of this Subsection, gross living space shall be defined as all interior building space except the garage and unfinished basement or attic area. Garages may not dominate the streetscape. Garages that are attached or incorporated into the dwelling shall be set back a minimum of four (4) feet from the front façade and there shall not be more than a single garage, not to exceed ten (10) feet wide, along the front façade of any individual unit. Garages may be incorporated into the rear of the building or freestanding in the rear yard and accessed by an alley. Sidewalks are required on both sides of all public and private streets and parking lots. At the time of Detailed Site Plan, the Planning Board or the District Council may approve a request to substitute townhouses, proposed for development as condominiums, in place of multifamily dwellings that were approved in a Conceptual Site Plan approved prior to April 1, 2004. Such substitution shall not require a revision to any previous plan approvals. Further, at the time of Detailed Site Plan for a Mixed-Use Planned Community, the Planning Board or the District Council may approve modifications to these regulations so long as the

modifications conform to the applicable regulations for the particular development.

Townhouse uses are not proposed on this site.

- (i) The maximum height of multifamily buildings shall be one hundred and ten (110) feet. This height restriction shall not apply within any Transit District Overlay Zone, designated General Plan Metropolitan or Regional Centers, or a Mixed-Use Planned Community.**

The height limit of the proposed multifamily buildings will be further evaluated with the DSP.

- (j) As noted in Section 27-544(b), which references property placed in the M-X-T Zone through a Sectional Map Amendment approved after October 1, 2006, and for which a comprehensive land use planning study was conducted by Technical Staff prior to initiation, regulations for Conceptual or Detailed Site Plans (such as, but not limited to density, setbacks, buffers, screening, landscaping, height, recreational requirements, ingress/egress, and internal circulation) should be based on the design guidelines or standards intended to implement the development concept recommended by the Master Plan, Sector Plan, or the Sectional Map Amendment Zoning Change and any referenced exhibit of record for the property. This regulation also applies to property readopted in the M-X-T Zone through a Sectional Map Amendment approved after October 1, 2006 and for which a comprehensive land use planning study was conducted by Technical Staff prior to initiation of a concurrent Master Plan or Sector Plan (see Section 27-226(f)(3) of the Zoning Ordinance).**

This property was part of a larger 35.99-acre tract of land which was placed in the M-X-T Zone, pursuant to adoption of the SMA in 1986.

- c. The subject application has been reviewed for conformance with the requirements of Section 27-546(d) of the prior Zoning Ordinance, which requires additional findings for the Planning Board to approve a CSP in the M-X-T Zone, as follows:

- (1) The proposed development is in conformance with the purposes and other provisions of this Division;**

The proposed development is in conformance with the purposes of the M-X-T Zone. For example, one purpose of the M-X-T Zone is to promote the orderly development of land in the vicinity of major intersections, to enhance the economic status of the County. The proposed development, consisting of residential and commercial uses, will provide increased economic activity in the commercial centers surrounding the intersection of MD 4 and Silver Hill Road. As the development site is adjacent to these commercial centers, it will allow for a reduction of the number and distance of automobile trips, by constructing residential and nonresidential uses near each other. This CSP promotes the purposes of the M X-T Zone and

contributes to the orderly implementation of the 2014 *Approved Plan Prince George's 2035 General Plan* (Plan 2035). In addition, one of the purposes of the M X-T Zone is to create compact, mixed-use, and walkable communities that emphasize pedestrian experience with active street fronts. This CSP contains a small commercial component within one of the multifamily buildings, and is designed with all of the buildings oriented toward the street, with parking designated behind the buildings to enhance the pedestrian experience.

- (2) For property placed in the M-X-T Zone through a Sectional Map Amendment approved after October 1, 2006, the proposed development is in conformance with the design guidelines or standards intended to implement the development concept recommended by the Master Plan, Sector Plan, or Sectional Map Amendment Zoning Change;**

This property was part of a larger 35.99-acre tract of land, which was placed in the M-X-T Zone, pursuant to adoption of the SMA in 1986. The site can be developed in a manner to support Plan 2035 and the 2009 *Approved Marlboro Pike Sector Plan and Sectional Map Amendment* (Marlboro Pike Sector Plan and SMA) goals by providing a mix of uses and a safe, attractive, walkable, vibrant community where people live, work, shop, dine, and recreate.

A continuous network of sidewalks exists along this property's road frontage. Design details, to ensure that this community meets these criteria and contributes to safe pedestrian access to area parks, retail, commercial, and institutional destinations, will be evaluated with the PPS and DSP.

- (3) The proposed development has an outward orientation which either is physically and visually integrated with existing adjacent development or catalyzes adjacent community improvement and rejuvenation;**

The details of the orientation are not fully available at the time of CSP; however, based on conceptual plans provided, the proposed development will be outwardly oriented, with the multifamily units oriented toward Penn Crossing Drive. Visually, this community will be integrated with existing surrounding communities and may serve to catalyze development on other undeveloped RMF-48, RMF-20, and CGO zoned properties nearby, contributing to the vibrancy of the community with development directed toward the road frontage, by maintaining the existing network of sidewalks and being in close proximity to area amenities.

- (4) The proposed development is compatible with existing and proposed development in the vicinity;**

The surrounding areas are developed with various auto-oriented, commercial, and residential uses. The CSP is visually integrated with existing and future uses through the use of existing streets and pedestrian systems, which will be further illustrated at the time of DSP review.

- (5) **The mix of uses, arrangement and design of buildings and other improvements, and provision of public amenities reflect a cohesive development capable of sustaining an independent environment of continuing quality and stability;**

The development is proposed as four multifamily buildings, with a small commercial component integrated within one of the buildings. This community is proposed on an existing fully improved public street and it is this project's close proximity to existing commercial services that would promote an independent environment of continuing quality and stability. The location and design of the buildings will be further evaluated at the time of DSP review.

- (6) **If the development is staged, each building phase is designed as a self-sufficient entity, while allowing for effective integration of subsequent phases;**

The applicant indicated, in the statement of justification, that phasing this development is not anticipated.

- (7) **The pedestrian system is convenient and is comprehensively designed to encourage pedestrian activity within the development;**

This requirement will be evaluated in detail at the time of PPS and DSP; however, the CSP shows that the multifamily buildings will be placed along the road frontage, with an existing sidewalk and lighting network in place.

- (8) **On the Detailed Site Plan, in areas of the development which are to be used for pedestrian activities or as gathering places for people, adequate attention has been paid to human scale, high quality urban design, and other amenities, such as the types and textures of materials, landscaping and screening, street furniture, and lighting (natural and artificial); and**

The above finding is not applicable because the subject application is a CSP. Further attention should be paid to the design of pedestrian and public spaces, at the time of DSP.

- (9) **On a Conceptual Site Plan for property placed in the M-X-T Zone by a Sectional Map Amendment, transportation facilities that are existing; that are under construction; or for which one hundred percent (100%) of construction funds are allocated within the adopted County Capital Improvement Program, or the current State Consolidated Transportation Program, will be provided by the applicant (either wholly or, where authorized pursuant to Section 24-124(a)(8) of the County Subdivision Regulations, through participation in a road club), or are incorporated in an approved public facilities financing and implementation program, will be adequate to carry anticipated traffic for the proposed development. The finding by the Council of adequate**

transportation facilities at the time of Conceptual Site Plan approval shall not prevent the Planning Board from later amending this finding during its review of subdivision plats.

The property was placed in the M-X-T Zone by an SMA. A traffic study has been prepared which indicates that transportation facilities will be adequate to carry anticipated traffic for the proposed development; however, an adequacy determination will be made at the time of PPS and will be evaluated, based on the proposed uses with the application.

- (10) On the Detailed Site Plan, if more than six (6) years have elapsed since a finding of adequacy was made at the time of rezoning through a Zoning Map Amendment, Conceptual Site Plan approval, or preliminary plat approval, whichever occurred last, the development will be adequately served within a reasonable period of time with existing or programmed public facilities shown in the adopted County Capital Improvement Program, within the current State Consolidated Transportation Program, or to be provided by the applicant (either wholly or, where authorized pursuant to Section 24-124(a)(8) of the County Subdivision Regulations, through participation in a road club).**

The above finding is not applicable because the subject application is a CSP. This requirement will be evaluated at the time of DSP for this project.

- (11) On a property or parcel zoned E-I-A or M-X-T and containing a minimum of two hundred fifty (250) acres, a Mixed-Use Planned Community including a combination of residential, employment, commercial and institutional uses may be approved in accordance with the provisions set forth in this Section and Section 27-548.**

The subject property is 7.54 acres and does not meet the above acreage requirement. Furthermore, this CSP does not propose development of a mixed-use planned community. Therefore, this requirement is not applicable.

- d. The CSP is in conformance with the applicable site design guidelines contained in Section 27-274 of the prior Zoning Ordinance. The proposed development concept provides a mix of new multifamily housing, townhouses, and commercial/retail uses designed to front on roadways. A connected circulation system for vehicles and pedestrians is proposed. Detailed designs of all buildings, site infrastructure, features, and amenities will be further reviewed at the time of DSP.
- e. In accordance with Section 27-574 of the prior Zoning Ordinance, the number of parking spaces required in the M-X-T Zone is to be calculated by the applicant and submitted for Planning Board approval, at the time of DSP. Detailed information regarding the methodology and procedures to be used, in determining the parking ratio, is outlined in Section 27-574(b). At the time of DSP review, demonstration of adequacy of proposed parking, including visitor parking and loading configurations, will be required for the development.

8. **Prince George's County Woodland and Wildlife Habitat Conservation Ordinance:** The site is subject to the provisions of the Woodland and Wildlife Habitat Conservation Ordinance (WCO) because the property is greater than 40,000 square feet in size and it contains more than 10,000 square feet of existing woodland. This project is subject to the WCO and the Environmental Technical Manual (ETM). Type 1 Tree Conservation Plan TCP1-017-2022 has been submitted with the subject application and requires minor revisions, to be found in conformance with the WCO.

The site contains a total of 5.82 acres of woodlands and no wooded floodplain. The site has a woodland conservation threshold of 15 percent, or 1.13 acres. The TCP1 proposes to clear 5.08 acres of woodland, resulting in a total woodland conservation requirement of 2.69 acres. The woodland conservation requirement is proposed to be met with 0.19 acre of on-site preservation, 0.41 acre of reforestation, and 2.09 acres of off-site credits.

Although this site has been part of several entitlement reviews, future applications should continue to look for opportunities to provide additional areas of preservation and reforestation. This area of the County is generally underserved, in terms of green spaces, and providing additional green space on-site promotes the goals of the *Countywide Green Infrastructure Plan* of the *Approved Prince George's County Resource Conservation Plan: A Countywide Functional Master Plan*.

When calculated by staff, the woodland conservation requirement is 2.69 acres. Based on the proposed conservation and afforestation, the applicant requires 2.09 acres of off-site credits. The worksheet shall be verified, prior to signature approval of the TCP1.

Technical revisions are required to the TCP1, prior to certification of the CSP, in conformance with the conditions provided herein.

9. **Other site plan related regulations:** Additional regulations are applicable to the site plan review that usually require detailed information, which can only be provided at the time of DSP. The discussion provided below is for information only:
- a. **Prince George's County Tree Canopy Coverage Ordinance**—Subtitle 25, Division 3, the Tree Canopy Coverage Ordinance, requires a minimum percentage of tree canopy coverage (TCC) on projects that require a grading permit. Properties that are zoned M-X-T are required to provide a minimum of 10 percent of the gross tract area in TCC. This site is 7.54 acres and, therefore, requires 0.75 acre of TCC. Conformance with the requirements of the Tree Canopy Coverage Ordinance will be ensured, at the time of DSP.
 - b. **2010 Prince George's County Landscape Manual**—This development, in the prior M X T Zone, will be subject to the requirements of the Landscape Manual, at the time of DSP. Specifically, the site is subject to Section 4.1, Residential Requirements; Section 4.2, Requirements for Landscape Strips along Streets; Section 4.3, Parking Lot Requirements; Section 4.4, Screening Requirements; Section 4.6, Buffering Development from Streets; Section 4.7, Buffering Incompatible Uses; Section 4.9, Sustainable Landscaping Requirements, and Section 4.10, Street Trees Along Private Roads, of the Landscape Manual.

10. Referral Comments: The subject application was referred to the concerned agencies and divisions. The referral comments are summarized as follows, and incorporated herein by reference:

- a. **Community Planning**—In a memorandum dated February 6, 2023 (Bishop to Burke), the Community Planning Division provided an evaluation of goals and strategies in the Plan 2035 and the Marlboro Pike Sector Plan, summarized as follows:

Plan 2035 places this application in the Established Communities Growth Policy Area, which are most appropriate for context-sensitive infill and low- to medium-density development. Plan 2035 recommends maintaining and enhancing existing public services (police and fire/EMS), facilities (such as libraries, schools, parks, and open space), and infrastructure in these areas (such as sidewalks) to ensure that the needs of existing residents are met.

The sector plan recommends enhancing streetscape conditions to encourage pedestrian activity that will help Marlboro Pike evolve into a thriving, community-oriented main street.

The sector plan recommends the use of crime prevention through environmental design principles to create an attractive and safe community and, if incorporated early in the design process, these strategies can be made compatible and seamless with the overall site development pattern.

- b. **Subdivision Review**—In a memorandum dated February 13, 2023 (Heath to Burke), the Subdivision Section noted that there are no prior final plats of subdivision recorded for this property. Final plats of subdivision will be required, subsequent to approval of this CSP amendment and following approval of the new PPS and DSP, before any building permits may be approved for development of this site. The Subdivision Section provided the following comments:

- (1) The property is located adjacent to MD 4, which is designated as a master plan expressway. A Phase I noise study was submitted with this CSP and will be required with the PPS, to demonstrate that any planned outdoor recreation areas and the residential dwelling units are not impacted by noise. While the CSP depicts the concept layout of multifamily buildings and the location of on-site recreational facilities, these will be finalized at the time of DSP, at which time Phase II noise studies will be required. Mitigation will be required for all exterior noise-sensitive areas exposed to traffic noise levels above 65 dBA Ldn, to ensure traffic noise is reduced to not higher than that level for the affected area. All dwelling units exposed to noise levels above 65 dBA Ldn must achieve an interior noise level no higher than 45 dBA Ldn.
- (2) The prior Prince George's County Subdivision Regulations require that all residential lots and parcels, adjacent to existing or planned roadways of expressway, freeway, or higher classification, shall be platted with a minimum depth of 300 feet, which is shown to be met by the subject proposal.

- (3) The CSP identifies locations for the proposed on-site recreational facilities. The adequacy of any on-site recreational facilities to satisfy the mandatory dedication of parkland requirement, in accordance with the Subdivision Regulations, will be evaluated at the time of PPS. Recreational facilities should include a mix of active and passive recreation, indoor and outdoor, for all seasons and age groups.
- (5) The CSP proposes two parcels for 168 multifamily dwelling units and 767 square feet of commercial development. The development is proposed to be served by two driveways. The lotting and circulation pattern, and any required right-of-way dedication, will be reviewed further with the PPS application.

c. **Transportation Planning**—In a memorandum dated February 22, 2023 (Ryan to Burke), the Transportation Planning Section provided the following comments:

This property fronts Penn Crossing Drive along the north side of the proposed development. The 2009 *Countywide Master Plan of Transportation* (MPOT) and the Marlboro Pike Sector Plan and SMA do not contain any right-of-way recommendations for Penn Crossing Drive.

A small portion of frontage is along Pennsylvania Avenue (MD 4, E-3). The MPOT and sector plan recommend this portion of Pennsylvania Avenue as a 4- to 6-lane expressway, with an ultimate right-of-way of 200 feet.

The extent and limits of the recommended master plan right-of-way, along the property's Pennsylvania Avenue frontage, are not shown on the latest CSP submission. Though master plan compliance is evaluated at the time of the PPS, staff request that the CSP show the limits of any recommended right-of-way that impacts the site. As a condition of approval, staff request that the applicant update the CSP to show the extent and limits of the ultimate right-of-way, along the subject property's frontage of Pennsylvania Avenue.

This development case is subject to the MPOT, which recommends the following facilities:

Planned Side Path: Pennsylvania Avenue

The MPOT provides policy guidance regarding multimodal transportation and the Complete Streets element recommends how to accommodate infrastructure for people walking and bicycling.

Policy 1: Provide standard sidewalks along both sides of all new road construction within the Developed and Developing Tiers.

Policy 2: All road frontage improvements and road capital improvement projects within the Developed and Developing Tiers shall be designed to accommodate all modes of transportation.

Continuous sidewalks and on-road bicycle facilities should be included to the extent feasible and practical.

Policy 4: Develop bicycle-friendly roadways in conformance with the latest standards and guidelines, including the 1999 AASHTO Guide for the Development of Bicycle Facilities.

Policy 5: Evaluate new development proposals in the Developed and Developing Tiers for conformance with the complete streets principles.

This portion of Marlboro Pike, on which the property fronts, also falls within the 2013 *Approved Subregion 6 Master Plan and Sectional Map Amendment*, which makes similar recommendations.

Bicycle facilities, along Pennsylvania Avenue and Penn Crossing Drive, will be addressed at the PPS and DSP stage of development. In addition, to facilitate the MPOT policies regarding site-connected active transportation networks and complete streets, staff recommend that the site is served by an interconnected network of on-site bicycle and pedestrian facilities. The exact location and details of all pedestrian and bicycle facilities shall be provided, at the time of DSP.

- d. **Environmental Planning**—In a memorandum dated February 13, 2023 (Kirchhof to Burke), the Environmental Planning Section (EPS) provided an analysis of the CSP and TCP1, and comments are summarized, as follows:

Grandfathering

The project is subject to the environmental regulations contained in Subtitle 25 and prior Subtitles 24 and 27 of the Prince George’s County Code that came into effect on September 1, 2010, because the application will require a new PPS.

Site Description

This 7.54-acre site is located just west of the MD 4 (Pennsylvania Avenue) and Penn Crossing Drive intersection. The site is bounded to the north by Penn Crossing Drive and to the south by an existing shopping center, which has frontage on MD 4. A review of the available information indicates that no streams or wetlands occur on the property. Steep slopes occur on the property, along the site edges. There is no potential forest interior dwelling species habitat mapped on-site. According to information obtained from the Maryland Department of Natural Resources, Natural Heritage Program, there are no rare, threatened, or endangered species on or in the vicinity of this property. The site has frontage on MD 4, which is identified as a master plan freeway and is a traffic noise generator. The property is not adjacent to any roadways indicated as scenic or historic. The site is located within Environmental Strategy Area 1 (formerly the Developed Tier) of the Regulated Environmental Protection Areas Map and is in the Established Communities area of the Growth Policy map, as designated by Plan 2035. The CSP is shown on the Generalized Future Land Use map as Mixed-Use (Plan 2035). According to the Green Infrastructure Plan, the site contains evaluation areas.

Environmental Review

Natural Resources Inventory/Environmental Features

An approved Natural Resources Inventory (NRI-132-2021) was submitted with the application. The site does not contain regulated environmental features (REF), such as steep slopes, streams, or wetlands. The site does not contain any specimen trees. No additional information regarding the NRI is required, at this time.

Stormwater Management

An unapproved site development concept plan was submitted with the current application. The submittal of an approved stormwater concept letter and plan will be required for subsequent development review applications. No further information pertaining to stormwater management is required, at this time.

- e. **Historic Preservation Section**—In a memorandum dated February 7, 2023 (Stabler, Smith, and Chisholm to Burke), the Historic Preservation Section evaluated the site and determined that the site does not contain, and is not adjacent to, any designated Prince George’s County historic sites or resources. The site was previously graded. A Phase I archeology survey will not be recommended.
 - f. **Prince George’s County Department of Parks and Recreation (DPR)**—In a memorandum dated February 13, 2023 (Thompson to Burke), DPR supports the applicant’s proposal to provide on-site recreational facilities, including outdoor active and passive amenities, to fulfill the dedication of parkland requirement.
 - g. **Prince George’s County Department of Permitting, Inspections and Enforcement (DPIE)**—As of the writing of this staff report, DPIE did not provide comments.
 - h. **Prince George’s County Police Department**—As of the writing of this staff report, the Police Department did not provide comments.
 - i. **Prince George’s County Fire/EMS Department**—As of the writing of this staff report, the Fire/EMS Department did not provide comments.
 - j. **Prince George’s County Health Department**—In a memorandum dated January 23, 2023 (Adepoju to Burke), the Health Department provided an evaluation of area food resources and comments regarding noise and dust during the construction phases. These comments will be required to be placed on the DSP, as conditioned herein.
11. As required by Section 27-276(b)(1) of the prior Zoning Ordinance, if approved with the conditions below, the CSP represents a reasonable alternative for satisfying the site design guidelines, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use.
12. Section 27-276(b)(4) of the prior Zoning Ordinance requires that, for approval of a CSP, the regulated environmental features on-site have been preserved and/or restored in a natural state, to the fullest extent possible, in accordance with the requirements of Section 24-130(b)(5) of the prior Subdivision Regulations. The approved Natural Resources

Inventory (NRI-132-2021) identifies no primary management area or REF, such as streams or wetlands, on-site and no impacts are requested with this application. Therefore, regulated environmental features have been preserved and/or restored, to the fullest extent possible.

RECOMMENDATION

Based upon the foregoing evaluation and analysis, the Urban Design staff recommend that the Planning Board adopt the findings of this report and APPROVE Conceptual Site Plan CSP-87128-02 and Type 1 Tree Conservation Plan TCP1-01-2022 for Penn Place 1, subject to the following conditions:

1. Prior to certification of the conceptual site plan, the following revisions shall be made, or information shall be provided:
 - a. Show the limits of the ultimate right-of-way along the subject property's frontage of MD 4 (Pennsylvania Avenue).
 - b. The type 1 tree conservation plan shall be revised, as follows:
 - (1) Revise the woodland conservation worksheet to indicate 2.09 acres of off-site credits.
 - (2) Correct the Soils table to be consistent with the approved natural resources inventory and indicate Beltsville-Urban land complexes and Udorthents-Urban land complex as the soils on-site.
 - (3) Revise General Note 8 to state "This site is located adjacent to Pennsylvania Avenue, a designated Star-Spangled Banner scenic byway."
 - (4) Revise General Note 9 to state "This property is adjacent to Pennsylvania Avenue which is classified as a freeway."
 - (5) Revise General Note 10 to state "This site is not grandfathered by CB-27-2010, Section 25-119(g)."
2. At the time of preliminary plan of subdivision (PPS), the applicant shall
 - a. Explore opportunities to provide the amenity area at a more central location within the community.
 - b. Submit a new and approved Traffic Impact Study Scoping Agreement, as part of a new traffic impact study, to evaluate transportation adequacy as part of the PPS application.
 - (1) The Traffic Impact Study Scoping Agreement shall include all site access connections, consistent with the approved Conceptual Site Plan, CSP-87128-02.

- c. Submit an operational and queuing analysis using *The Highway Capacity Manual* methodology for the proposed site access driveways along Penn Crossing Drive.
3. At the time of detailed site plan (DSP), the applicant shall:
 - a. Provide details on sustainable site and green building techniques that will be used in this development.
 - b. Submit a parking analysis to determine the parking rate for the proposed development, which examines both the residential and commercial/retail uses, in accordance with Section 27-574 of the prior Prince George's County Zoning Ordinance.
 - c. Submit a bicycle and pedestrian plan which displays the details, location, and extent of an interconnected network of on-site bicycle and pedestrian facilities.
 - d. The following notes shall be placed within the general notes of the DSP:

“During the construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.”

“During the construction phases of this project, noise should not be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.”
4. Prior to issuance of any permits or acceptance of the detailed site plan, whichever comes first, the stormwater concept plan shall be approved and submitted with the Type 2 tree conservation plan.

PENN PLACE 1

Conceptual Site Plan

TCP1-017-2022

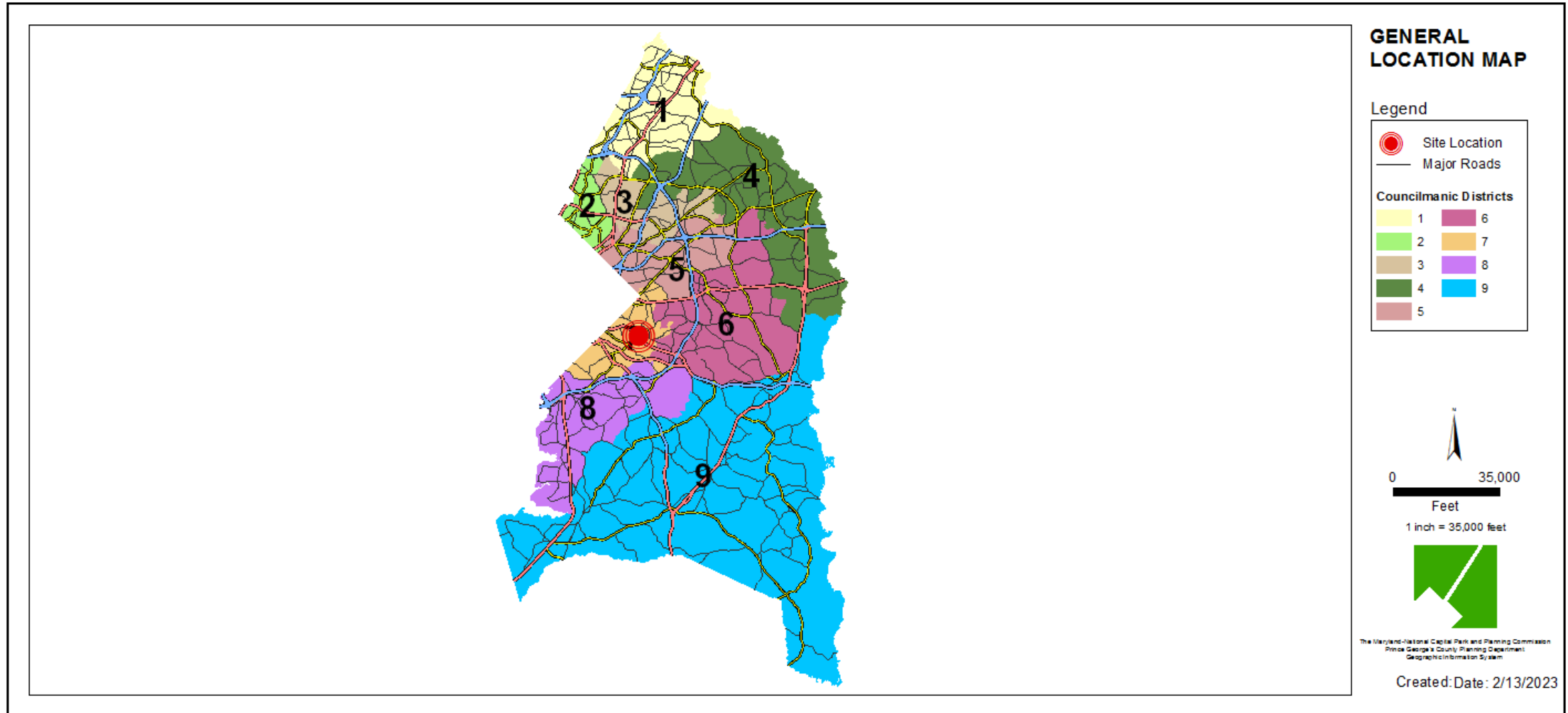
Staff Recommendation: APPROVAL with conditions



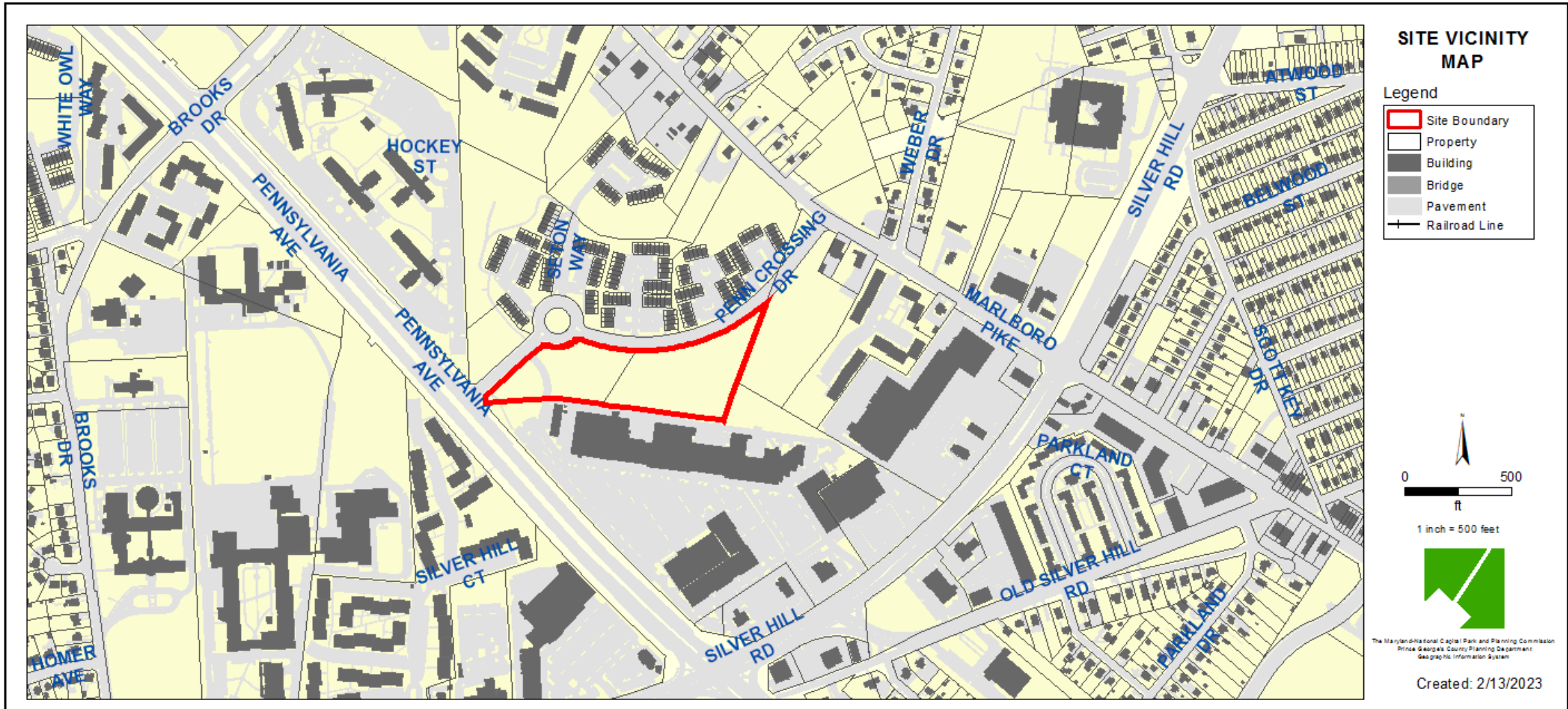
GENERAL LOCATION MAP

Council District: 07

Planning Area: 75A



SITE VICINITY MAP

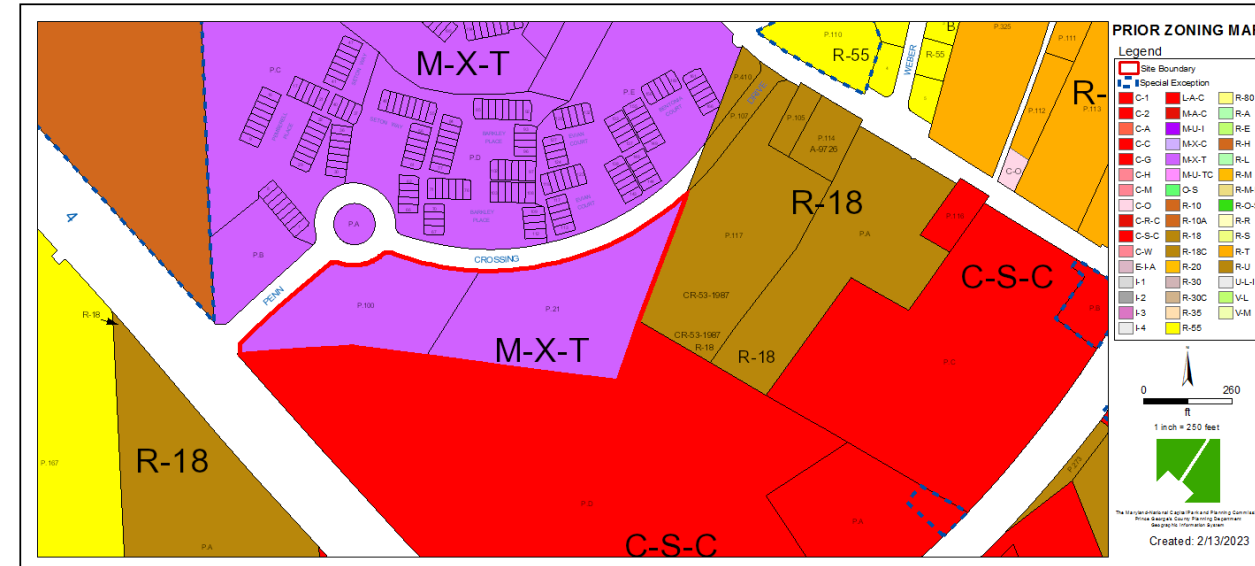
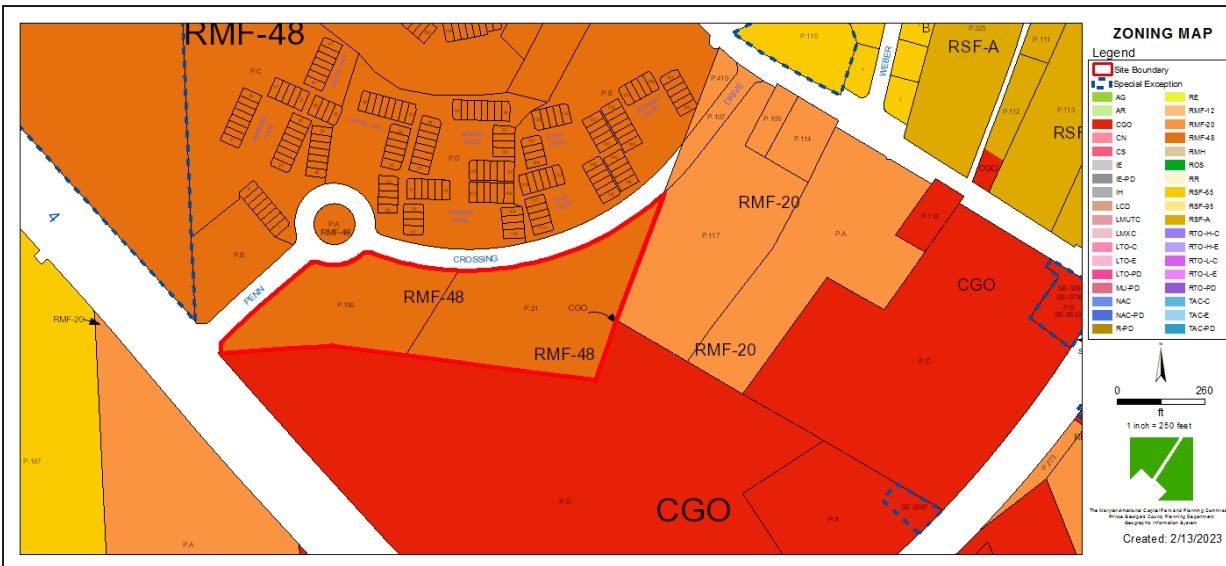


ZONING MAP (CURRENT & PRIOR)

Property Zone: RMF-48
 Prior Property Zone: M-X-T

CURRENT ZONING MAP

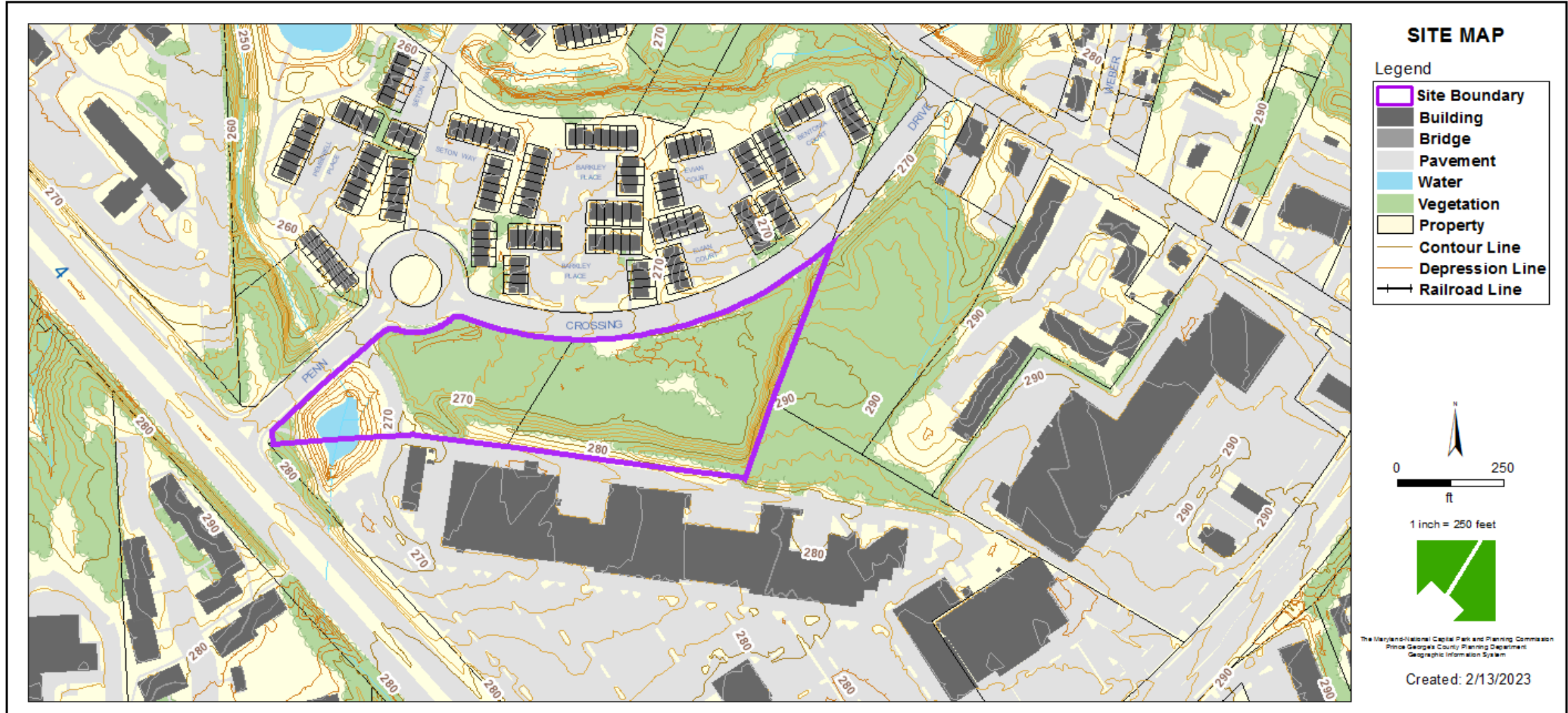
PRIOR ZONING MAP



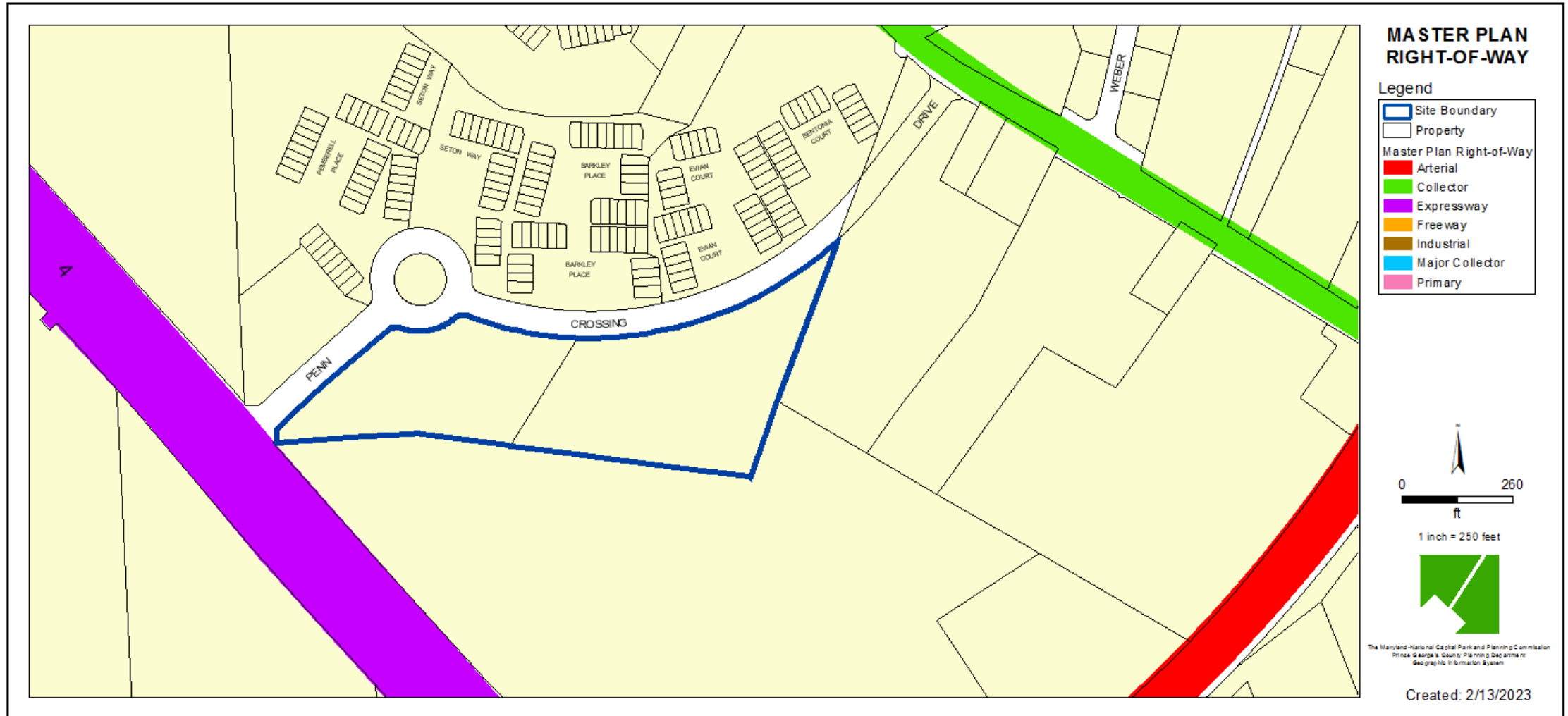
AERIAL MAP



SITE MAP



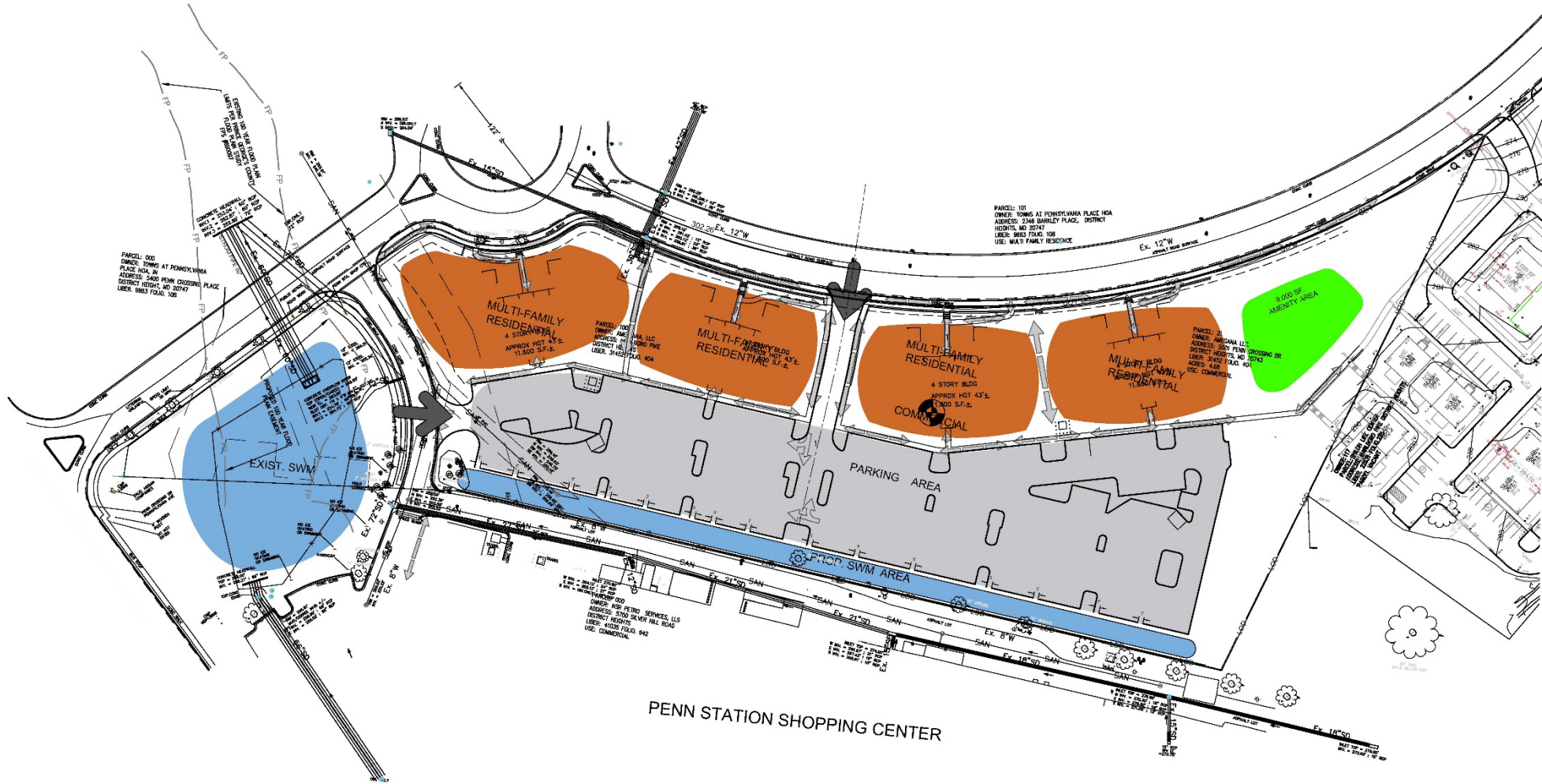
MASTER PLAN RIGHT-OF-WAY MAP



ORIGINAL CONCEPTUAL SITE PLAN AREA



CIRCULATION & USE EXHIBIT



STAFF RECOMMENDATION

- APPROVAL with conditions
- APPROVAL with conditions

Conceptual Site Plan CSP-87128-02 Type 1 Tree Conservation Plan TCP1-017-2022

[Major/Minor] Issues:

- None

Applicant Required Mailings:

- Informational Mailing: March 16, 2022
- Acceptance Mailing: December 30, 2022

STATEMENT OF JUSTIFICATION
PENN PLACE
AMENDMENT OF CONCEPTUAL SITE PLAN
CSP-87128-02
APRIL 28, 2022
REVISED JUNE 14, 2022

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STATEMENT OF JUSTIFICATION
PENN PLACE
CONCEPTUAL SITE PLAN CSP-87128-02

1.0 INTRODUCTION/OVERVIEW/ORIENTATION

Suitland Penn, Inc. ("Applicant") files this amendment to Conceptual Site Plan CSP-87128 for approximately 7.54 acres of land located on the south Side of Penn Crossing Drive in District Heights Maryland. Penn Crossing Drive extends within and intersects with Pennsylvania Avenue (MD 4) and Marlboro Pike. The property which is the subject of this application consists of two parcels of land which are unplatted. These parcels are more particularly described as Parcels 21 and 100 on Tax Map 81, which were conveyed to the Applicant by deed dated April 9, 2021 and recorded among the Land Records of Prince George's County at Book 45722 Page 506 (the "Subject Property"). Parcel 21 contains 4.68 acres and Parcel 100 contains 2.86 acres.

The Applicant proposes to construct a mixed-use development containing 168 multifamily dwelling units and 767 square feet of commercial space (in addition to the existing commercial use of the property previously developed). The multifamily dwelling units will be in four buildings, each containing 42 units. The

Subject Property is zoned RMF-48, but was previously zoned, and the Conceptual Site Plan was approved, in the M-X-T Zone.

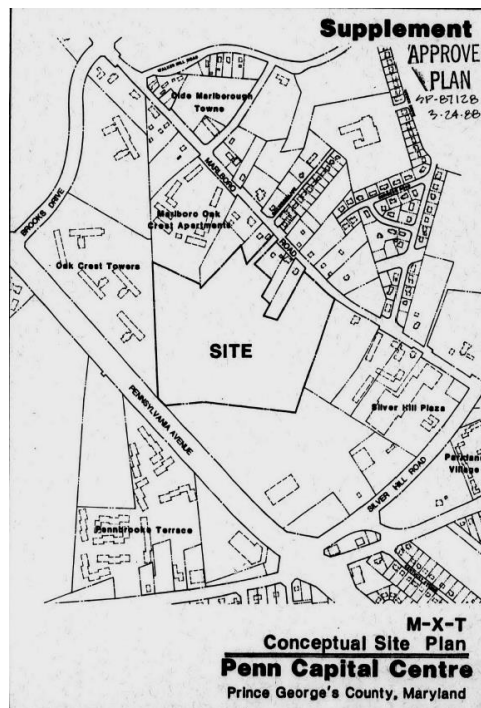
As noted above, the Subject Property is located on the south side of Penn Crossing Drive. Across Penn Crossing Drive from the Subject Property is the Towns at Pennsylvania Place townhome community. As discussed in greater detail below, this development constitutes the remainder of a larger area of land included in the initial CSP being amended. Abutting the Subject Property to the south is the Penn Station Shopping Center, currently zoned CGO but previously zoned C-S-C. The western boundary of the Subject Property fronts on Pennsylvania Avenue (MD 4). Abutting the Subject Property to the east is land in the RMF-20 zone (formerly R-18). The property zoned RMF-20 is owned by the Shiloh Abundant Life Center, but is under contract to an entity affiliated with the Applicant in this case. It is the intention of the Applicant to construct additional multifamily units on the adjoining property compatible with the development proposed on the Subject Property.

The application in this case is being filed pursuant to the provisions of the M-X-T zone. The Applicant evaluated the ability to process the application under the provisions of the RMF-48 zone. However, the proposed multifamily dwellings are four stories in height and served by an elevator. Pursuant to the Neighborhood Compatibility Standards in the new Zoning

Ordinance, multifamily dwellings constructed across from existing townhouses (even though those townhouses are part of the same development) cannot exceed three stories or 35 feet in height. The proposed development cannot meet this limitation and therefore the Applicant has elected to proceed under the provisions of the M-X-T zone.

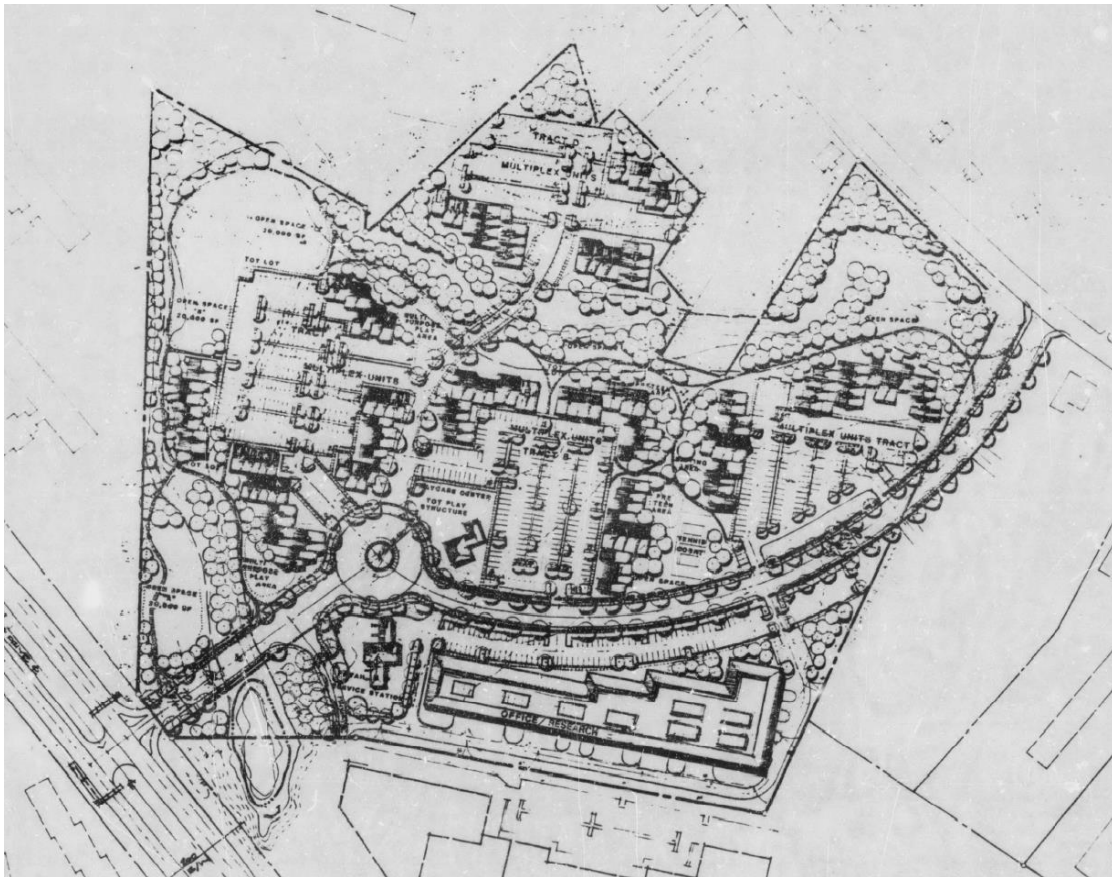
2.0 ZONING HISTORY AND SUMMARY OF DEVELOPMENT PROPOSAL

As noted above, the Subject Property is being developed under the provision of the M-X-T Zone. The Subject Property was part of a larger 35.99-acre tract of land which was placed in the M-X-T zone pursuant to the adoption of the Suitland-District Heights and Vicinity Sectional Map Amendment in 1986.



In September, 1987, Conceptual Site Plan application CSP-87128 was filed. The land included in the CSP is depicted above. The

CSP proposed the development of 500,000 to 550,000 square feet of residential (500 to 550 multiplex dwelling units); 75,000 to 100,000 square feet of building comprised of office/research/industrial uses; a 4,000 to 6,000 square foot retail/service station complex; and a 4,000 to 6,000 square foot day care center. The initial development concept is shown below:



As can be seen on the concept plan, the area north of Penn Crossing Drive was proposed for the 500-550 multiplex dwelling units and a daycare center. The land on the south side of Penn Crossing Drive was proposed for a stormwater management facility and access roadway to the (at that time proposed) commercial

shopping center, a gas station, and an office/research building to serve as a buffer between the proposed shopping center and the residential component.

CSP-87128 proposed a range of development from 583,000 square feet to 662,000 square feet, or an FAR of .37 to .42. This proposed development included 80,000-105,000 square feet of non-residential development.

Following the approval of the CSP, two subsequent applications were filed and approved for the project, Preliminary Plan 4-91126 and DSP-92024. The development approved pursuant to these applications was 200 townhouse units on the north side of Penn Crossing Drive. This project, known as the Towns at Pennsylvania Place, has since been constructed. No commercial gross floor area was constructed—only the joint stormwater management facility and access driveway to the shopping center were constructed on the Subject Property. The Subject Property was never platted and none of the commercial square footage was constructed. As reflected in the Resolution approving DSP-92024 (PGCPB No. 92-191(C), a total of 200 townhouse units were approved, for an overall FAR of .25.

As now configured, the Subject Property is not visible from Pennsylvania Avenue and is located on a roadway which serves only the existing townhouse development. A gas station and large commercial complex would not be compatible with the

existing townhouse development. The Applicant desires to instead build a predominantly residential project with a small commercial component (as required by Section 27-547(d) of the Zoning Ordinance) to promote neighborhood compatibility. This proposed commercial component will be addressed in greater detail below. The proposed buildings would be constructed close to Penn Crossing Drive with parking to the rear of the units. This is done for two reasons. The multifamily units serve as a more appropriate buffer between the now existing retail shopping center and the existing townhouses than a gas station and office/research complex. Locating the parking to the rear allows for a more attractive streetscape for the existing residents and provides a buffer for the shopping center from the proposed new units and the commercial component. Access to the parking will be from two curb cuts, one from the driveway entering the commercial shopping center and the other from Penn Crossing Drive. The parking will be convenient to the buildings it serves.

3.0 ANALYSIS OF CONFORMANCE WITH THE CONDITIONS OF CSP-87128

Conceptual Site Plan CSP-87128 was approved subject 12 conditions. Although this application proposes an amendment to CSP-87128, the proposed revisions conform to each of the

conditions applicable to the original approval, as addressed below.

- 1. Vehicular access to Pennsylvania Avenue is subject to any requirements established by State Highway Administration.**

COMMENT: The vehicular access to Pennsylvania was approved with the development of the townhouses and is now an existing street known as Penn Crossing Drive.

- 2. The width of the right-of-way entering the site from Marlboro Pike and the amount of right of way and physical development of the traffic circle shall be determined prior to the approval of the Preliminary Plat of Subdivision.**

COMMENT: The width of the right of way entering the side and the physical development of the traffic circle was determined at the time the Preliminary Plan of Subdivision was approved. A 60' wide right of way was required and deeded to Prince George's County.

- 3. A minimum of 50' undisturbed buffer should be maintained alongside the streams that traverse the property. This buffer should be expanded to include 100-year floodplain, adjacent non-tidal wetlands, steep slopes and erodible soils. This condition shall apply to those stream channels not requiring stabilization (to be determined at the time of preliminary plat of subdivision).**

COMMENT: Stream buffers were established at the time of the preliminary plat of subdivision. The Subject Property is not impacted by any streams or any buffers related to the streams.

- 4. A 100-year floodplain study approval shall be approved by DER prior to submission of a preliminary plat of subdivision.**

COMMENT: The limits of the 100-year floodplain were addressed at the time of the preliminary plan of subdivision. No floodplain impacts the Subject Property.

5. A Stormwater Concept Plan shall be approved by DER prior to submission of a preliminary plat of subdivision.

COMMENT: The development of the Subject Property will require approval of a new Stormwater Concept Plan approved under current regulations. An application has been filed and is under review.

6. A Forest Stand Delineation is required prior to the submission of a Preliminary Plat for review and approval by the Natural Resources Division.

COMMENT: A Natural Resource Inventory was prepared in conjunction with this application, as was a Type 1 Tree Conservation Plan.

7. A soils study is required for review and approval by SCD, DER and Natural Resources Division prior to submission of a Detailed Site Plan.

COMMENT: A soils study was reviewed at the time of the initial Detailed Site Plan. Should additional soils information be required, this can be provided at the time of the Detailed Site Plan filed pursuant to this CSP application.

8. All recreation facilities shall be constructed in accordance with the Parks and Recreation Facilities Guidelines.

COMMENT: All recreation facilities will be constructed in accordance with the Parks and Recreation Facilities Guidelines.

9. The central circulation trail system shall be a minimum 6 foot wide hard surface trail, with the feeder connecting a minimum four-foot-wide hard surface trail.

COMMENT: The trail system was established at the time of the prior subdivision and Detailed Site Plan reviews. The proposed development will construct sidewalks along Penn Crossing Drive which will provide access to the trail system previously established.

10. The design of the stormwater management system shall encourage the multi-use nature of the facility.

COMMENT: The stormwater management facility has been constructed under prior approvals in accordance with this condition.

11. The design of the Detailed Site Plan shall minimize the impact of negative on- and off-site visual features.

COMMENT: The design of the Detailed Site Plan will be addressed at that time.

12. The first Detailed Site Plan submitted for any structural development shall include for review a signage and street furniture package that will become applicable to the entire project.

COMMENT: The townhouses were constructed with brick monument signs that identify the development as "Penn Place on the Avenue". Any signage or street furniture proposed with the development of the Subject Property will be designed to be compatible with the existing signage.

4.0 ANALYSIS OF CONFORMANCE WITH SECTION 27-547(d)

Section 27-547(d) provides that at least two (2) of three (3) listed use categories shall be included on the Conceptual Site Plan and ultimately present in every development in the M-X-T Zone. Further, the Site Plan shall show the location of the existing use and the way that it will be integrated in terms of access and design with the proposed development. The amount of square footage devoted to each use shall be in sufficient quantity to serve the purposes of the zone. The three listed use categories are 1) retail businesses; 2) office, research, or industrial uses; or 3) dwellings, hotel, or motel.

As noted above, the Penn Crossing development has already been improved with 200 townhouses. No commercial component has yet been constructed, nor has any commercial development been proposed in the 30 years since DSP-92024 was approved. The Applicant submits that the developing the Subject Property with retail or office space, as contemplated 35 years ago, is not compatible with the now entrenched residential community. This is particularly true of the gas station and convenience store that was once proposed. The Applicant further submits that a small commercial component designed to serve the needs of the community in which it is located is sufficient to serve the purposes of the zone, given its proximity to and physical connection to a major shopping center.

In this instance, the proposed commercial component will be the offices of Project Community Capital ("PCC"). PCC is a social capital platform that connects people in affordable housing communities with jobs. PCC bridges the social capital of employment decision-makers of companies with individuals in affordable housing communities. PCC ensures that subcontractors who have socio-economic goals can meet their requirements by connecting them with individuals who are "Ready-To-Work".

Once the project has started construction, the PCC Team commences its agency-level implementation of PCC. Agency-level implementation seeks to place individuals from the community in employment opportunities outside of construction as well to help create an entry-level opportunity and access to a career path.

PCC is in the process of building out its Social Capital Hub space in multiple markets. PCC is operating in Cleveland, Ohio where it will build a dedicated amenity building that will be the home of the flagship PCC office. PCC will also build an office at the Parkway House Apartments in Washington, D.C. - where NREUV has a 100-unit property under contract.

The PCC Social Capital Hub Services will promote ongoing engagement and connections to the community, with both residents that live at Penn Place Apartments and that reside in the larger community. This will happen at the PCC Social Capital Hub located on the project site once the project is complete. PCC

will provide connections to employment opportunities, provide small business consulting, and implement its entrepreneurship accelerator - Heighten Then Hustle™. The proposed development is a substantially more compatible with the existing development both in terms of function and scale.

5.0 ANALYSIS OF CONFORMANCE WITH THE REQUIREMENTS OF A CONCEPTUAL SITE PLAN

As noted above, a conceptual site plan is already approved for the Subject Property, but is proposed to be amended through this application. The Applicant wishes to remove uses approved in the initial Conceptual Site Plan (the gas station and daycare center), retain a reduced amount of commercial office as the commercial component and add multifamily residential to the approved uses. Section 27-279(b) provides that when amending an approved conceptual site plan, "all requirements for the filing and review of an original Conceptual Site Plan shall apply to an amendment. The Planning Board shall follow the same procedures and make the same findings." To approve a Conceptual Site Plan, the Planning Board must make certain findings, which are set forth in Section 27-276(b) of the Zoning Ordinance. Section 27-276(b) (2) and (b) (3) are not applicable because the Subject Property is not a Mixed-Use Planned Community or a Regional Urban Community. Sections 27-276(b) (1) and (b) (4) are applicable to this application and will be addressed below.

Section 27-276(b) (1) provides as follows:

"The Planning Board may approve a Conceptual Site Plan if it finds that the Plan represents a most reasonable alternative for satisfying the site design guidelines without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use. If it cannot make this finding, the Planning Board may disapprove the Plan."

COMMENT: The Applicant submits that the proposed CSP amendment for Penn Place does represent a most reasonable alternative for satisfying the site design guidelines. As noted above, the Subject Property is located directly across from existing single family attached homes and adjacent to an existing commercial shopping center. Due to the existing stormwater management facility and entrance to the abutting shopping center, the Subject Property has no visibility from MD 4 and is not well located to sustain a larger commercial complex. While Penn Crossing Drive is dedicated as a 70-foot-wide right-of-way from MD 4 to the existing traffic circle, along the majority of the property frontage the road is only 60 feet wide. This is typically not sufficient for a commercial development. Constructing multifamily dwellings is an appropriate transitional use. The concept plan envisions that the proposed buildings will face the street to screen the parking from the existing residents. This layout also allows sufficient distance from the commercial center to ensure compatibility with that use. The commercial component, while small in size, was

intentionally selected to be a benefit to both the existing and proposed community.

The Site Design Guidelines are contained in Section 27-274. These Site Design Guidelines address General matters, Parking, Loading and Circulation, Lighting, Views, Green Area, Site and Streetscape Amenities, Grading, Service Areas, Public Spaces, Architecture and Townhouses. Many of these Site Design Guidelines are most appropriately addressed at the time of Detailed Site Plan or are inapplicable. Those that are relevant are addressed below.

Section 27-274(a)(1) General. The proposed plan should promote the purposes of the Conceptual Site Plan. The purposes of Conceptual Site Plans are listed in Section 27-272. The General Purposes include providing for development in accordance with the Master Plan and helping fulfill the purposes of the zone in which the land is located. As noted above, the Subject Property was placed in the M-X-T zone by a Sectional Map Amendment and partially developed. The development constructed was less dense than initially contemplated. The property has remained in the M-X-T zone for many years, and this application will complete the development. The reduction in the intensity of the proposed development from that originally contemplated is appropriate given the lower density of the existing development and the reduction in need for traditional office and commercial

space in the current market. The existing shopping center continues to satisfy the day-to-day commercial needs of the existing and proposed development and additional commercial space is not appropriate at this time.

The Specific Purposes of Conceptual Site Plans include explaining the relationships between the proposed uses and illustrating approximate locations of building and parking. The proposed CSP fulfills these specific purposes. The proposed multifamily residential is separated from the existing commercial uses by the parking area and the existing townhouses are screened from the parking by the proposed buildings. The CSP also shows the approximate location of buildings, parking and the commercial component which will be provided on site.

Section 27-274(a)(2) Parking, Loading and circulation.

General guidance is given regarding the location of parking and loading facilities. The proposed residential buildings are being oriented such that they front on Penn Crossing Drive with the parking behind the proposed buildings. This is consistent with the guideline to place parking lots to the rear or sides of structures. The parking is located as near as possible to the uses they serve, which is important in rental housing. The pedestrian circulation on site is safe and efficient. The residents will have easy access to their units, with parking

being provided in close proximity. This layout also provides a sufficient setback from the abutting commercial development.

Section 27-274(a)(3) Lighting. Lighting will be addressed in greater detail at the time of DSP, but the lighting provided will illuminate important on-site elements, such as the entrances, recreational areas and pedestrian pathways.

Section 27-274(a)(4) Views. The proposed development is oriented across from an existing townhome community. The orientation of the proposed development is important to protect the attractiveness of the streetscape. Locating the buildings close to the street, rather than placing parking along Penn Crossing Drive, allows for a pedestrian friendly streetscape. The location of the proposed residential buildings will establish a street edge which help to orient the residential toward the townhouses with the goal being to integrate the uses in a manner which will help fulfill the purposes of the M-X-T zone. The design protects both the existing and proposed residential uses from the abutting commercial area.

Section 27-274(a)(5) Green Area. Green area will be provided on site and will be accentuated by elements such as landscaping and recreational facilities. Street furniture will be addressed at the time of DSP. The property also has easy pedestrian access to the abutting shopping center.

Section 27-274(a)(6) Site and streetscape amenities. Site and streetscape amenities will be addressed in greater detail at the time of DSP.

Section 27-274(a)(7) Grading. Portions of the Subject Property were graded at the time that the streets and stormwater management facility were constructed. On-site grades for the proposed development necessary to tie into the existing street grades are shown.

Section 27-274(a)(8) Service areas. The service areas for the residential component will be minimal. The Conceptual Site Plan shows the conceptual location for a loading space and a trash enclosure, but more detail will be provided at the time of DSP.

Section 27-274(a)(9) Public spaces. The main public space associated with the residential component will be the amenity area which is proposed along the eastern end of the Subject Property.

Section 27-274(a)(10) Architecture. The architecture will be addressed at the time of DSP.

Section 27-274(a)(11) Townhouses and three family dwellings. This consideration is inapplicable to the proposed DSP as there are no townhouses or three family dwellings proposed.

Section 27-276(b)(4) provides as follows:

"The plan shall demonstrate the preservation and/or restoration of the regulated environmental features in a natural state to the fullest extent possible in accordance with the requirement of Subtitle 24-130(b) (5)."

In this case, a preliminary plan of subdivision will be required as the Subject Property was not previously platted.

Preservation of regulated environmental features will be addressed at that time. However, the Natural Resources Inventory indicates that no regulated environmental features are present on the Subject Property. Therefore, this finding can be made by the Planning Board.

In addition to the above findings, the Planning Board must make the findings set forth in Section 27-546(d) (1)-(11), which are related specifically to the M-X-T zone. Each of the subsections will be set forth below, with a corresponding comment, which provides as follows:

(1) The proposed development is in conformance with the purposes and other provisions of this Division;

COMMENT: The purposes of the M-X-T Zone are set forth in Section 27-542 of the Zoning Ordinance. There are a total of 10 purposes, several of which are promoted by this CSP. For example, one purpose of the M-X-T zone is to promote orderly development of land in the vicinity of major interchanges and major intersections so that these areas will provide an expanding source of desirable living opportunities for its citizens. The intersection of Silver Hill Road and MD 4 is a

major intersection and providing new multifamily housing in this area, easily accessible to existing commercial, will provide a desirable living opportunity. Another purpose is to encourage an appropriate horizontal and vertical mix of land uses which blend together harmoniously. As discussed above, there is a substantial existing commercial shopping center immediately adjacent to the Subject Property that is connected to this development. Adding a substantially larger commercial component would not enhance the existing commercial and would not be compatible with the existing townhouses. The proposed amendment to the CSP is a more appropriate mix of land uses which blend together harmoniously. While the proposed amendment to the CSP reduces the scope of the originally envisioned commercial component, one purpose of the M-X-T zone is to permit a flexible response to the market and promote economic vitality and investment. The CSP was approved over 35 years ago, and the development which ultimately occurred, when combined with the market that exists today, dictates that the types of uses initially envisioned for the Subject Property are either not appropriate or viable. The proposed development adds quality residential development in a manner which is compatible with the built environment. Finally, the purposes of the M-X-T zone are implemented by the fact that the Subject Property is within easy walking distance to the abutting Penn Station Shopping Center

and implements the approved Master Plan by reducing automobile use by locating a mix of residential and non-residential uses in proximity to one another, encouraging a 24 hour environment, and creating functional relationships among individual uses with a distinctive visual character and identity.

Each of the purposes discussed above is promoted by the CSP, which contributes to the implementation of the overall Master Plan and General Plan.

(2) For property placed in the M-X-T Zone through a Sectional Map Amendment approved after October 1, 2006, the proposed development is in conformance with the design guidelines or standards intended to implement the development concept recommended by the Master Plan, Sector Plan, or Sectional Map Amendment Zoning Change;

COMMENT: As stated above, the Subject Property was placed in the M-X-T zone in 1986. Thus, this finding is inapplicable to the Subject Property.

(3) The proposed development has an outward orientation which either is physically and visually integrated with existing adjacent development or catalyzes adjacent community improvement and rejuvenation;

COMMENT: The proposed development's outward orientation will physically and visually integrate with existing adjacent development. The proposed buildings are designed to line Penn Crossing Drive to create an attractive urban edge consistent which is compatible with the townhouses across the street. This will encourage the visual integration of the residential component with the existing neighborhood.

(4) The proposed development is compatible with existing and proposed development in the vicinity;

COMMENT: The existing development in the vicinity is a mix of commercial and residential uses. As discussed above, the property lies between existing commercial and existing single family attached development. The Subject Property will allow for the introduction of moderate density multifamily residential uses to support the existing commercial uses and complement the existing single family attached development. For these reasons, the proposed development will be compatible with both existing and proposed development in the vicinity.

(5) The mix of uses, arrangement and design of buildings and other improvements, and provision of public amenities reflect a cohesive development capable of sustaining an independent environment of continuing quality and stability;

COMMENT: As discussed in detail above, a large commercial component with 75,000 to 100,000 square feet of retail is not necessary to create the type of mixed use environment envisioned by the Zoning Ordinance. That mixed use environment already exists because the Subject Property is connected to the adjacent commercial shopping center. Thus, a small commercial component focused on meeting the needs of the existing and proposed community is all that is necessary to satisfy the requirements of the Zoning Ordinance. The arrangement and design of the

buildings is the most critical factor to ensuring that this infill development is sensitive to the built environment in which it is proposed. Accomplishing this will ensure the creation of a cohesive development capable of sustaining an independent environment of continuing quality and stability.

(6) If the development is staged, each building phase is designed as a self-sufficient entity, while allowing for effective integration of subsequent phases;

COMMENT: The development envisioned was staged in that the commercial component initially envisioned for the Subject Property never proved to be viable or appropriate. The amended uses proposed in this application will not be staged, but will be developed into a self-sufficient entity which will complement the original community.

(7) The pedestrian system is convenient and is comprehensively designed to encourage pedestrian activity within the development;

COMMENT: The existing road was improved with sidewalks and lighting which is being preserved and additional on-site pedestrian connections will be provided.

(8) On the Detailed Site Plan, in areas of the development which are to be used for pedestrian activities or as gathering places for people, adequate attention has been paid to human scale, high quality urban design, and other amenities, such as the types and textures of materials, landscaping and screening, street furniture, and lighting (natural and artificial); and

COMMENT: The current plan is a CSP. This issue will be addressed at the time of DSP.

(9) On a Conceptual Site Plan for property placed in the M-X-T Zone by a Sectional Map Amendment, transportation facilities that are existing; that are under construction; or for which one hundred percent (100%) of construction funds are allocated within the adopted County Capital Improvement Program, or the current State Consolidated Transportation Program, will be provided by the applicant, or are incorporated in an approved public facilities financing and implementation program, will be adequate to carry anticipated traffic for the proposed development. The finding by the Council of adequate transportation facilities at the time of Conceptual Site Plan approval shall not prevent the Planning Board from later amending this finding during its review of subdivision plats.

COMMENT: The property was placed in the M-X-T zone by a Sectional Map Amendment. A traffic study has been prepared which indicates that transportation facilities will be adequate to carry anticipated traffic for the proposed development.

(10) On the Detailed Site Plan, if more than six (6) years have elapsed since a finding of adequacy was made at the time of rezoning through a Zoning Map Amendment, Conceptual Site Plan approval, or preliminary plat approval, whichever occurred last, the development will be adequately served within a reasonable period of time with existing or programmed public facilities shown in the adopted County Capital Improvement Program, within the current State Consolidated Transportation Program, or to be provided by the applicant.

COMMENT: More than six years has elapsed since the previous finding of adequacy was made. A new finding of adequacy will be required and will be made consistent with the applicable.

(11) On a property or parcel zoned E-I-A or M-X-T and containing a minimum of two hundred fifty (250) acres, a Mixed-Use Planned Community including a combination of residential, employment, commercial and institutional uses may be approved in accordance with the provisions set forth in this Section and Section 27-548.

COMMENT: This provision does not apply to the Subject Property as it does not exceed 250 acres.

5.0 CONCLUSION

In conclusion, the Applicants submit that the proposed amendment to Conceptual Site Plan CSP-87128 represents a most reasonable alternative for satisfying the site design guidelines without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use. In addition, the other findings required for Conceptual Site Plans in the M-X-T zone can also be made. For these reasons, the Applicants respectfully request approval of the CSP.

Respectfully submitted




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301-952-3972

February 6, 2023

MEMORANDUM

TO: Tom Burke, Planner IV, Urban Design Section, Development Review Division

VIA: David A. Green, MBA, Planner IV, Long Range Planning Section, Community Planning Division 

FROM: Andrew Bishop, Planner II, Placemaking Section, Community Planning Division 

SUBJECT: **CSP-87128-02, Penn Place**

FINDINGS

Pursuant to Part 3, Division 9, Subdivision 2 of the Prior Zoning Ordinance, Master Plan conformance is not required for this application.

BACKGROUND

Application Type: Conceptual Site Plan for property located outside of an overlay zone.

Location: On the south side of Penn Crossing approximately 800 feet west of its intersection with MD-4 (Pennsylvania Ave)

Size: 7.54 acres

Existing Use: Vacant

Proposal: Construction of a mixed-use development containing 168 multifamily dwelling units and 767 square feet of commercial space.

GENERAL PLAN, MASTER PLAN, AND SMA

General Plan: The 2014 *Plan Prince Georges 2035 Approved General Plan (Plan 2035)* places this application in the Established Communities. Established communities are most appropriate for context-sensitive infill and low- to medium-density development. Plan 2035 recommends maintaining and enhancing existing public services (police and fire/EMS), facilities (such as libraries, schools, parks, and open space), and infrastructure in these areas (such as sidewalks) to ensure that the needs of existing residents are met. (pg. 20)

Master Plan:

The 2009 *Approved Marlboro Pike Sector Map Amendment* (Marlboro Pike Sector Plan) recommends a mix of uses on the subject property. The Marlboro Pike Sector Plan envisions a safe, attractive, walkable, vibrant community where people live, work, shop, dine, and recreate.

Neighborhoods are livable and offer desirable and attractive housing choices which blend old and new communities (Page 13).

The Master Plan identifies the following recommendations, goals and objectives to achieve the vision of the Marlboro Pike Sector Plan.

GREEN BUILDING/ ENERGY EFFICIENCY

The Marlboro Pike Sector Plan recommends the use of green building techniques (Page 108). Staff recommend that green building techniques be used to the extent practical and encourage design practices to reduce environmental impacts and improve energy efficiency.

POLICY: Utilize the most innovative energy efficient and environmentally sensitive design technologies for all new development and redevelopment that occurs throughout the sector plan area.

STRATEGIES:

- Encourage new buildings to meet LEED certification standards. Attract developers who are accredited LEED professionals or follow environmentally sensitive design practices.
- Encourage infill development and adaptive reuse of existing buildings.
- Encourage mixed-use and pedestrian and transit oriented development.
- Encourage waste reduction strategies and use of recycled construction materials.
- Encourage community gardening for family food production in public parkland and open spaces.
- Encourage the use of alternative energy sources, such as solar and wind power.
- Require the use of green building techniques such as green roofs, rainwater capture and reuse, sustainable building materials, and alternative energy sources.

(Page 76)

SAFETY

The Marlboro Pike Sector Plan recommends the use of crime prevention through environmental design (CPTED) principles to create attractive and safe community and if incorporated early in the design process, these strategies can be made compatible and seamless with the overall site development pattern. Staff encourage the use of CPTED design principles to create a safe and attractive development at the time of Detailed Site Plan.

POLICY: Reduce opportunities for criminal activity along Marlboro Pike.

STRATEGIES:

- Create an inviting atmosphere that allows the community to feel safe and secure.

CSP-87128-02, Penn Place

- Target blight that attracts criminal activity for aggressive code enforcement.
- Attract pedestrian activity and outdoor gatherings so that people can see and be seen.
- Increase visibility with better lighting and sight lines.
- Initiate intensive code enforcement for areas that attract criminal activity.
- Focus police attention and resources on quality-of-life crime. Target community police efforts in areas with ongoing public safety issues to develop, implement, and maintain solutions to improve safety.
- Facilitate a partnership with the Police Department, Department of Environmental Resources, and the community to develop legislation to deter criminal activity.
- Discourage the use of advertising signs on store windows to maintain visibility from the street.
- Develop a crime watch group to report suspicious activity.

(Page 47)

Pedestrian Amenities and Streetscape Enhancements

The Marlboro Pike Sector Plan recommends enhancing streetscape conditions to encourage pedestrian activity that will help Marlboro Pike evolve into a thriving, community-oriented main street. (Page 56).

The Master Plan includes streetscape recommendations for specific main street areas and activity nodes. This site is partially within one of the activity nodes and is between two of these main street areas. The property has a street frontage on Penn Crossing which intersects with Marlboro Pike. Staff recommends that the applicant work with the Transportation Planning Section during this and future approvals to make sure the frontage of the property is designed to create an attractive, walkable pedestrian streetscape, and promotes access the main streets and adjacent activity nodes.

POLICY 2:

Implement a boulevard streetscape between main street areas throughout the corridor to improve the overall look and function of the corridor.

STRATEGIES:

- Develop boulevard typical sections that include two lanes in each direction, bike lanes, a landscaped median, and sidewalks. These areas will include thematic and attractive streetscaping to tie together the main street areas corridor-wide. Figures IV-6 and IV-7, on pages 59 and 60, illustrate the typical section for boulevard areas with and without a median.
- Include sidewalks at least five feet in width and that are set back at least five feet to allow for roadside landscaping to serve as a buffer between the pedestrian environment and the roadway.

(Page 56)

POLICY 3: Enhance and provide pedestrian-friendly amenities throughout the corridor that assist in transforming the corridor into a safe and comfortable environment.

CSP-87128-02, Penn Place

STRATEGIES:

Include continuous sidewalks along the entire length of Marlboro Pike and Pennsylvania Avenue. Priority sidewalk installation sites include: the front of District Heights commercial center and Parkland Shopping Center, along the parcel of land to the west of Regency Parkway, and throughout the Forestville Community.

- Improve pedestrian crossings by using reflective paint to ensure better visibility.
- Buffer pedestrians from vehicular areas where possible with on-street parking or landscaped verges.
- Minimize driveway entrances by channeling multiple entrances together into one shared driveway entrance, or move them to side streets, where feasible, to improve safety and levels of service.
- Enhance or provide pedestrian streetscape treatments to be included with property development and redevelopment projects through a private and public partnership or development proffer.
- Incorporate pedestrian accessibility from the school into the Marlboro Pike roadway improvements.

POLICY 4: Establish a theme that creates an identifiable look that visually unifies the corridor.

STRATEGIES:

Install a 10-foot median, where feasible, based on available right-of-way. Areas include:

- Southern Avenue to Benning Road
- Walker Mill to Penn Crossing Drive
- Viceroy Avenue to Forestville Road
- Install decorative community banners and gateway signs along the corridor identifying it as a community main street.
- Include continuous sidewalks, bike paths, and landscaping.
- Incorporate thematic landscaping along sidewalks and the median.
- Install attractive bus shelters or waiting areas at all corridor bus stops.

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Planning Area: 75A

Community: Suitland-District Heights and Vicinity

CSP-87128-02, Penn Place

Aviation/MIOZ: This application is not located within the Aviation Policy Area or the Military Installation Overlay Zone.

SMA/Zoning: The Marlboro Pike sector plan retained the subject property in the Mixed-Use Transportation Oriented (M-X-T) zone.

On November 29, 2021, the District Council approved CR-136-2021, the Countywide Sectional Map Amendment (“CMA”) which reclassified the subject property from M-X-T to Residential, Multifamily-48 (RMF-48) effective April 1, 2022.

ADDITIONAL INFORMATION: None.

Cc: Long-Range Agenda Notebook

Adam Dodgshon, Supervisor, Placemaking Section, Community Planning Division



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

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Countywide Planning Division
Environmental Planning Section

301-952-3650

February 13, 2023

MEMORANDUM

TO: Tom Burke, Planner IV, Urban Design Section, DRD

VIA: Maria Martin, Acting Supervisor, Environmental Planning Section, CWPD *MM*

FROM: Alex Kirchhof, Planner I, Environmental Planning Section, CWPD *ANK*

SUBJECT: **Penn Place 1; CSP-87128-02 and TCP1-017-2022**

The Environmental Planning Section (EPS) has reviewed the above referenced Conceptual Site Plan (CSP-87128-02) and Type 1 Tree Conservation Plan (TCP1-017-2022) received on January 5, 2023. Comments were provided in a Subdivision and Development Review Committee (SDRC) meeting on January 20, 2023. Revised materials were received on February 6, 2023. The EPS recommends approval of CSP-87128-02 and TCP1-017-2022, subject to findings and conditions recommended at the end of this memorandum.

BACKGROUND

The EPS previously reviewed the following applications and associated plans for the subject site:

Development Review Case #	Associated Tree Conservation Plan #	Authority	Status	Action Date	Resolution Number
CSP-87128	N/A	Planning Board	Approved	3/24/1988	PGCPB No. 88-126
CSP-87128-01	N/A	Planning Board	Approved	12/12/1991	PGCPB No. 91-451
NRI-132-2021	N/A	Staff	Approved	10/6/2021	N/A
CSP-87128-02	TCP1-017-2022	Planning Board	Pending	Pending	Pending

PROPOSED ACTIVITY

This CSP application proposes the development of a multifamily community, on a 7.54-acre site, identified as Parcels 21 and 100. The current zoning for the site is Residential Multifamily – 48 (RMF-48); however, the applicant has opted to apply the zoning standards to this application that were in effect prior to April 1, 2022, for the M-X-T (Mixed Use Transportation Oriented) Zone.

PRIOR CASES

The site was subject to several prior approvals, which proposed to develop an institutional and then a residential subdivision. This institutional use will not be implemented with the development

with CSP-87128-02. Previous conditions of approval are not applicable to this application because the type of residential use and site design have changed. These prior CSP cases were reviewed prior to the implementation of the woodland conservation ordinance.

GRANDFATHERING

The project is subject to the environmental regulations contained in Subtitle 25 and prior Subtitles 24 and 27 that came into effect on September 1, 2010, because the application will require a new preliminary plan of subdivision (PPS).

SITE DESCRIPTION

This 7.54-acre site is located just west of the MD-4 (Pennsylvania Avenue) and Penn Crossing Drive intersection. The site is bounded to the north by Penn Crossing Drive and to the south by an existing shopping center, which has frontage on MD-4. A review of the available information indicates that no streams or wetlands occur on the property. Steep slopes occur on the property along the site edges. There is no potential forest interior dwelling species (FIDS) habitat mapped on-site. According to information obtained from the Maryland Department of Natural Resources Natural Heritage Program, there are no rare, threatened, or endangered species on or in the vicinity of this property. The site has frontage on MD-4, which is identified as a master plan freeway, and is a traffic noise generator. The property is not adjacent to any roadways indicated as scenic or historic. The site is located within the Environmental Strategy Area 1 (formerly the Developed Tier) of the Regulated Environmental Protection Areas Map and in the Established Communities of the General Plan Growth Policy (2035) map, as designated by *Plan Prince George's 2035 Approved General Plan* (2014). The CSP is shown on the General Plan Generalized Future Land Use (2035) map as Mixed-Use. According to the *Approved Countywide Green Infrastructure Plan* (2017), the site contains evaluation areas.

ENVIRONMENTAL REVIEW

Natural Resource Inventory/Environmental Features

An approved Natural Resource Inventory (NRI-132-2021) was submitted with the application. The site contains no regulated environmental features (REF), such as steep slopes, streams, or wetlands. The site also contains no specimen trees. No additional information regarding the NRI is required at this time.

Woodland Conservation

The site is subject to the provisions of the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance (WCO) because the property is greater than 40,000 square feet in size and it contains more than 10,000 square feet of existing woodland. This project is subject to the WCO and the Environmental Technical Manual (ETM). Type 1 Tree Conservation Plan TCP1-017-2022 has been submitted with the subject application and requires minor revisions to be found in conformance with the WCO.

The site contains a total of 5.82 acres of woodlands and no wooded floodplain. The site has a woodland conservation threshold of 15 percent, or 1.13 acres. The TCP1 proposes to clear 5.08 acres of woodland resulting in a total woodland conservation requirement of 2.70 acres. The woodland conservation requirement is proposed to be met with 0.19 acres on-site preservation, 0.41 acres reforestation, and 2.49 acres of off-site credits.

Although this site has been part of several entitlement reviews, future applications should continue to look for opportunities to provide additional areas of preservation and reforestation. This area of the County is generally underserved in terms of green spaces, and providing additional green space on-site promotes the goals of the Green Infrastructure Plan.

When calculated by staff, the woodland conservation requirement is 2.69 acres. Based on the proposed conservation and afforestation, the applicant requires 2.09 acres of off-site credits. The worksheet shall be verified prior to signature approval of the TCP1.

Technical revisions are required to the TCP1, prior to certification of the CSP, in conformance with the conditions provided at the end of this memorandum.

Specimen Trees

The approved natural resources inventory (NRI-132-2021) identifies no specimen trees on site; thus, no specimen trees are requested for removal.

Preservation of Regulated Environmental Features/Primary Management Area

The approved Natural Resources Inventory (NRI-132-2021) identifies no primary management area (PMA) or REF, such as streams or wetlands on-site. No impacts are requested with this application.

Soils

The predominant soils found to occur according to the U.S. Department of Agriculture (USDA) Natural Resource Conservation Service (NRCS) Web Soil Survey (WSS) include the Beltsville-Urban land complexes and Udorthents-Urban land complex. According to available mapping information, unsafe soils containing Marlboro clay or Christiana clay do not occur on this property. This information is provided for the applicant's benefit.

Stormwater Management

An unapproved Site Development Concept Plan was submitted with the current application. The submittal of an approved Stormwater Concept Letter and plan will be required for subsequent development review applications. No further information pertaining to stormwater management is required at this time.

SUMMARY OF RECOMMENDED FINDINGS AND CONDITIONS OF APPROVAL

The EPS recommends approval of Conceptual Site Plan CSP-87128-02 and Type 1 Tree Conservation Plan TCP1-017-2022, subject to the following findings and conditions:

Recommended Findings:

1. No regulated environmental features are recorded on-site, and no impacts are requested.
2. No specimen trees are recorded on-site, and no removals are requested.

Recommended Conditions:

1. Prior to certification of the CSP, the TCP1 shall be revised as follows:

- a. The type 1 tree conservation plan (TCP1) woodland conservation worksheet shall be revised to indicate 2.09 acres of off-site credits.
 - b. The soils table on the TCP1 shall be reflective of the approved natural resources inventory and indicate Beltsville-Urban land complexes and Udothents-Urban land complex as the soils on-site.
 - c. General TCP1 note 8 shall be revised to state "This site is located adjacent to Pennsylvania Avenue, a designated Star-Spangled Banner scenic byway".
 - d. General TCP1 note 9 shall be revised to state "This property is adjacent to Pennsylvania Avenue which is classified as a freeway".
 - e. General TCP1 note 10 shall be revised to state "This site is not grandfathered by CB-27-2010, Section 25-119(g)".
3. Prior to the issuance of any permits, the stormwater concept plan shall be approved and submitted with the TCP2.



Division of Environmental Health/Disease Control

Date: January 23, 2023

To: Thomas Burke, Urban Design, M-NCPPC

From: Adebola Adepoju, Environmental Health Specialist, Environmental Engineering/ Policy Program

Re: CSP-87128-02 (Penn Place 1)

The Environmental Engineering / Policy Program of the Prince George's County Health Department has completed a desktop health impact assessment review of the conceptual site plan submission for Penn Place 1 and has the following comments / recommendations:

1. Health Department permit records indicate there are approximately 10 existing carry-out/convenience store food facilities and two market/grocery store within a ½ mile radius of this location. Research has found that people who live near an abundance of fast-food restaurants and convenience stores compared to grocery stores and fresh produce vendors, have a significantly higher prevalence of obesity and diabetes.
2. During the construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.
3. During the construction phases of this project, noise should not be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.

If you have any questions or need additional information, please contact me at 301-883-7677 or aoadepoju@co.pg.md.us.



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February 7, 2023

MEMORANDUM

TO: Thomas Burke, Urban Design Section, Development Review Division

VIA: Thomas Gross, Planning Supervisor, Historic Preservation Section, Countywide Planning Division **TWG**

FROM: Jennifer Stabler, Historic Preservation Section, Countywide Planning Division **JAS**
Tyler Smith, Historic Preservation Section, Countywide Planning Division **TAS**
Amelia Chisholm, Historic Preservation Section, Countywide Planning Division **Age**

SUBJECT: CSP-87128-02, Penn Place 1

The subject property comprises 7.54 acres and is located on the south side of Penn Crossing Drive in District Heights, Maryland. The subject property consists of two parcels, Parcels 21 and 100, that are not platted. The applicant proposed to construct a mixed-use development containing 168 multifamily dwelling units and 767 square feet of commercial space. The multifamily dwelling units will be in four buildings, each containing 42 units. The subject property is zoned RMF-48, but was previously zoned, and the previous conceptual site plan was approved in the M-X-T Zone.

The subject property is within the 2009 *Approved Marlboro Pike Sector Plan* area. The Sector Plan contains goals and policies related to Historic Preservation (pages 17-18, 55, 192). However, these are not specific to the subject site or applicable to the proposed development.

The subject property does not contain, and is not adjacent to, any designated Prince George's County Historic Sites or resources. The site was previously graded. A Phase I archeology survey will not be recommended. The Historic Preservation Section staff recommends approval of CSP-87128-02, Penn Place 1, without conditions.



MEMORANDUM

DATE: February 13, 2023

TO: Thomas Burke, Planning Supervisor
Urban Design Section
Development Review Division
Planning Department

VIA: Sonja Ewing, Assistant Division Chief SME
Dominic Quattrocchi, Planning Supervisor DQ
Park Planning and Development Division
Department of Parks and Recreation

FROM: Ivy R. Thompson, Planner III IRT
Land Acquisition/Management & Development Review Section
Park Planning and Development Division
Department of Parks and Recreation

SUBJECT: **CSP-87128-02 Penn Place 1**

The Department of Parks and Recreation (DPR) has reviewed and evaluated this conceptual site plan amendment for conformance with the requirements as they pertain to public parks and recreational facilities.

PROPOSAL

This application is for the development of 168 Multifamily Units and 767 square feet of commercial space.

BACKGROUND:

The subject property is approximately 7.54-acres located on the south side of Penn Crossing, approximately 800 feet west of its intersection with Pennsylvania Avenue (MD4). This proposal is subject to the 2009 Approved *Marlboro Pike Sector Plan*, the 2017 *Land Preservation, Parks and Recreation Plan for Prince George's County*, and *Formula 2040, Functional Master Plan for Parks, Recreation and Open Space*. This property is currently unimproved.

DISCUSSION:

The applicant is proposing to provide on-site recreation amenities to meet the public spaces requirements of 27-274(a)(9). The conceptual site plan identifies a 9,000 square-foot open space amenity area along the eastern edge of the property, four multifamily pods and the existing stormwater management facility to the west of the amenity area, the parking area, and a new stormwater management facility along the southern border of the property. All of which will be complemented by the onsite tree conservation, landscaping, and pedestrian connections throughout the development. Nearby park facilities include the Oakcrest Community Center, which includes a gymnasium, fitness center, dance center,

and multi-purpose rooms, located 1.3 miles northeast of the subject site and the John E. Howard Community Center, which includes a gymnasium, game room, multi-purpose room, and a kitchen along with outdoor amenities, such as play equipment, tennis courts, two athletic fields, and a picnic area, is located 2.2 miles northwest of the subject property. The undeveloped Oakland Neighborhood Park is within a half mile of the development site. The 2009 Approved *Marlboro Pike Sector Plan* indicates there are adequate recreation opportunities in the area, yet more bicycle trails and green community spaces are desired. The identification of an amenity area increases the recreation opportunities in the subject area.

RECOMMENDATION:

The Park Planning & Development Division of DPR recommends to the Planning Board approval of Conceptual Site Plan amendment CSP-87128-02 Penn Place 1 with the recommendation that at the time of preliminary plan of subdivision review, the applicant proposes the provision of on-site recreation facilities, including outdoor active and passive amenities to fulfill the dedication of parkland requirement.

cc: Alvin McNeal
Bridget Stesney

February 13, 2023

MEMORANDUM

TO: Thomas Burke, Planner IV, Urban Design Section
VIA: Sherri Conner, Supervisor, Subdivision Section *SC*
FROM: Antoine Heath, Planner II, Subdivision Section *AH*
SUBJECT: CSP-87128-02; Penn Place I

The subject property is located in Tax Map 81, in Grid A2. The property consists of two parcels totaling 7.54 acres, known as Parcel 100 and Parcel 21, both recorded by deed in the Prince George's County Land Records as Liber 48143 at Folio 1. The property is located within the Residential, Multifamily-48 (RMF-48) Zone. However, this conceptual site plan (CSP) application was submitted for review under the prior Zoning Ordinance. Therefore, the property is reviewed pursuant to the prior Mixed-Use Transportation Oriented (M-X-T) zoning of the property.

The applicant proposes development consisting of 168 multifamily dwelling units and 767 square feet of commercial development. There are no prior preliminary plans of subdivision (PPS) approved for the subject property. The proposed development will require a PPS and a certificate of adequacy in accordance with the Subdivision Regulations.

There are no prior final plats of subdivision recorded for this property. Final plats of subdivision will be required subsequent to approval of this CSP amendment and following the approval of the new PPS and DSP before any building permits may be approved for the development of this site.

Plan Comments

1. The property is located adjacent to Pennsylvania Avenue (MD 4), which is designated as a master plan expressway. A Phase I noise study was submitted with this CSP. This Phase I noise study will be required with the PPS as well, to demonstrate that any planned outdoor recreation areas and the residential dwelling units are not impacted by noise. While the CSP depicts the concept layout of multifamily buildings and location of on-site recreational facilities, these will be finalized at the time of DSP, at which time Phase II noise studies will be required. Mitigation will be required for all exterior noise-sensitive areas exposed to traffic noise levels above 65 dBA Ldn, to ensure traffic noise is reduced to not higher than that level for the affected area. All dwelling units exposed to noise levels above 65 dBA Ldn must achieve an interior noise level no higher than 45 dBA Ldn.

2. The Subdivision Regulations require that all residential lots and parcels adjacent to existing or planned roadways of expressway or freeway, or higher classification shall be platted with a minimum depth of 300 feet, which is shown to be met by the subject proposal.
3. The CSP identifies locations for the proposed on-site recreational facilities. The adequacy of any on-site recreational facilities to satisfy mandatory dedication of parkland requirement in accordance with the Subdivision Regulations will be evaluated at the time of PPS. Recreational facilities should include a mix of active and passive recreation, indoor and outdoor, for all seasons and age groups.
5. The CSP proposes two parcels for 168 multifamily dwelling units and 767 square feet of commercial development. The development is proposed to be served by two driveways. The lotting and circulation pattern, and any required right-of-way dedication, will be reviewed further with the PPS application.

Recommended Conditions

None.

This referral is provided for the purposes of determining conformance with any underlying subdivision approvals for the subject property and Subtitle 24. A preliminary plan of subdivision and final plat will be required for the proposed development. There are no other subdivision issues at this time.



Countywide Planning Division
Transportation Planning Section


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February 22, 2023

MEMORANDUM

TO: Thomas Burke, Urban Design Section, Development Review Division

FROM: Benjamin Ryan, Transportation Planning Section, Countywide Planning Division


VIA:  William Capers III, PTP, Transportation Planning Section, Countywide Planning Division

SUBJECT: **CSP-87128-02 – Penn Place**

Proposal:

The subject Conceptual Site Plan (CSP) application proposes 168 multifamily dwelling units and 767 square feet of commercial/retail use. The site is located along the south side of Penn Crossing Drive, directly east of its intersection with Pennsylvania Avenue (MD-4), and is located within the Mixed-Use Transportation Oriented (M-X-T) zoning district. The Transportation Planning Section's review of the CSP application was evaluated using the standards of Section 27 of the prior Zoning Ordinance.

Prior Conditions of Approval:

The subject site is not governed by any previously approved development applications that contain conditions related to transportation adequacy, access, or circulation.

Master Plan Compliance

Master Plan Roads

The subject property fronts Penn Crossing Drive along the north side of the proposed development. The *2009 Countywide Master Plan of Transportation (MPOT)* and the *2009 Approved Marlboro Pike Sector Plan and Sectional Map Amendment* do not contain any right-of-way recommendations for Penn Crossing Drive.

The subject property has a small portion of its frontage along Pennsylvania Avenue (E-3). The *2009 Countywide Master Plan of Transportation (MPOT)* and the *2009 Approved Marlboro Pike Sector Plan and Sectional Map Amendment* recommend this portion of Pennsylvania Avenue as a 4-6 lane expressway with an ultimate right-of-way of 200 feet.

Comment: The extent and limits of the recommended master plan right-of-way along the property's Pennsylvania Avenue frontage are not shown on the latest CSP submission. Though master plan compliance is evaluated at the time of the Preliminary Plan of Subdivision, staff requests that the CSP show the limits of any recommended right-of-way that impacts the site. As a condition of approval, staff requests the applicant update the CSP to show the extent and limits of the ultimate right-of-way along the subject property's frontage of Pennsylvania Avenue.

Master Plan Pedestrian and Bike Facilities

This development case is subject to the 2009 *Approved Countywide Master Plan of Transportation* (MPOT), which recommends the following facilities:

Planned Side Path: Pennsylvania Avenue

The MPOT provides policy guidance regarding multimodal transportation and the Complete Streets element of the MPOT recommends how to accommodate infrastructure for people walking and bicycling.

Policy 1: Provide standard sidewalks along both sides of all new road construction within the Developed and Developing Tiers.

Policy 2: All road frontage improvements and road capital improvement projects within the Developed and Developing Tiers shall be designed to accommodate all modes of transportation. Continuous sidewalks and on-road bicycle facilities should be included to the extent feasible and practical.

Policy 4: Develop bicycle-friendly roadways in conformance with the latest standards and guidelines, including the 1999 AASHTO Guide for the Development of Bicycle Facilities.

Policy 5: Evaluate new development proposals in the Developed and Developing Tiers for conformance with the complete streets principles.

This portion of Marlboro Pike that fronts the subject property also falls within the 2013 *Approved Subregion 6 Master Plan and Sectional Map Amendment* which makes similar recommendations.

Comment: Bicycle facilities along Pennsylvania Avenue and Penn Crossing Drive will be addressed at the Preliminary Plan of Subdivision (PPS) and Detailed Site Plan (DSP) stage of development. In addition, to facilitate the MPOT policies regarding site-connected active transportation networks and complete streets, staff recommends that the site is served by an interconnected network of on-site bicycle and pedestrian facilities. The exact location and details of all pedestrian and bicycle facilities shall be provided at the time of DSP.

Transportation Planning Review

Analysis of Traffic Impacts

The applicant has submitted a full traffic impact analysis (TIS) at the request of staff. This study is used as the basis for a determination of transportation adequacy for developments located in the M-X-T zoning district in conformance to Section 27-546(d)(9) as stated below:

Section 27-546(d)(9) discusses anticipated transportation adequacy for a CSP for property placed in the M-X-T Zone by a Sectional Map Amendment and is copied below:

(d) In addition to the findings required for the Planning Board to approve either the Conceptual or Detailed Site Plan (Part 3, Division 9), the Planning Board shall also find that:

(7) On a Conceptual Site Plan for property placed in the M-X-T Zone by a Sectional Map Amendment, existing transportation facilities; that are under construction; or for which one hundred percent (100%) of construction funds are allocated within the adopted County Capital Improvement Program or the current State Consolidated Transportation Program, will be provided by the applicant (either wholly or, where authorized pursuant to Section 24-124(a)(8) of the County Subdivision Regulations, through participation in a road club), or are incorporated in an approved public facilities financing and implementation program will be adequate to carry anticipated traffic for the proposed development. The finding by the Council of adequate transportation facilities at the time of Conceptual Site Plan approval shall not prevent the Planning Board from later amending this finding during its review of subdivision plans.

The subject property is located within Transportation Service Area (TSA) 1, as defined in the *Plan Prince George's 2035 Approved General Plan*. As such, the subject property is evaluated according to the following standards:

Links and Signalized Intersections: Level-of-Service (LOS) E, with signalized intersections operating at a critical lane volume (CLV) of 1,600 or better.

Unsignalized Intersections:

For two-way stop-controlled intersections, a three-part process is employed: (a) vehicle delay is computed in all movements using the *Highway Capacity Manual* (Transportation Research Board) procedure; (b) the maximum approach volume on the minor streets is computed if the delay exceeds 50 seconds, (c) if the delay exceeds 50 seconds and at least one approach volume exceeds 100, the CLV is computed.

For all-way stop-controlled intersections, a two-part process is employed: (a) vehicle delay is computed in all movements using the *Highway Capacity Manual* (Transportation Research Board) procedure; (b) if the delay exceeds 50 seconds, the CLV is computed.

Trip Generation

The applicant's trip generation summary considers 168 apartment units and 767 square feet of commercial development. The table below summarizes trip generation in each peak hour that will be used in reviewing traffic and developing a trip cap for the site:

Land Use	Use Quantity	Metric	AM Peak Hour			PM Peak Hour		
			In	Out	Tot	In	Out	Tot
Apartments (Garden and Mid-Rise)	168	Units	17	70	87	66	35	101
Retail	767	Square-Foot	1	1	2	3	2	5
Total Proposed Trips			18	71	89	69	37	106

The traffic generated by the proposed CSP would impact the following intersections, and links in the transportation system:

- MD 4 & Brooks Drive (signalized)
- Marlboro Pike & Brooks Drive (signalized)
- MD 4 & Penn Crossing Drive (unsignalized)
- Penn Crossing Drive & Site Access (unsignalized)
- Marlboro Pike & Penn Crossing Drive (unsignalized)
- MD 4 & MD 458 (signalized)
- MD 458 & Marlboro Pike (signalized)

Existing Traffic

The following critical intersections, interchanges, and links identified above, when analyzed with existing traffic and existing lane configurations, operate as follows:

EXISTING TRAFFIC CONDITIONS				
Intersection	Critical Lane Volume (AM & PM)		Level of Service (LOS, AM & PM)	
MD 4 & Brooks Drive (Signalized)	828	1,023	A	B
Marlboro Pike & Brooks Drive (Signalized)	594	959	A	A
MD 4 & Penn Crossing Drive (Unsignalized)				
Step 1 - HCM Delay Test (Sec.)	12.4 Sec.	12.1 Sec.	-	-
Penn Crossing Drive & Site Access (Unsignalized)				
Step 1 - HCM Delay Test (Sec.)	-	-		
Marlboro Pike & Penn Crossing Drive (Unsignalized)				
Step 1 - HCM Delay Test (Sec.)	10.6 Sec.	16.5 Sec.	-	-
MD 4 & MD 458	767	1,067	A	B
MD 458 & Marlboro Pike	477	817	A	A

*In analyzing unsignalized intersections, average vehicle delay for various movements through the intersection is measured in seconds of vehicle delay. The numbers shown indicate the greatest average delay for any movement within the intersection. According to the Guidelines, a delay exceeding 50.0 seconds indicates inadequate traffic operations. Values shown as "+999" suggest that the parameters are beyond the normal range of the procedure and should be interpreted as severe inadequacy.

Background Traffic

A 0.5% growth rate was applied to the existing peak hour volumes for six years per M-NCPPC guidelines.

BACKGROUND TRAFFIC CONDITIONS				
<u>Intersection</u>	<u>Critical Lane Volume</u>		<u>Level of Service</u>	
	<u>(AM & PM)</u>		<u>(LOS, AM & PM)</u>	
<u>MD 4 & Brooks Drive (Signalized)</u>	983	1,143	A	B
<u>Marlboro Pike & Brooks Drive (Signalized)</u>	734	1,223	A	C
<u>MD 4 & Penn Crossing Drive (Unsignalized)</u>				
<u>Step 1 – HCM Delay Test (Sec.)</u>	13.0 Sec.	12.6 Sec.	-	-
<u>Penn Crossing Drive & Site Access (Unsignalized)</u>				
<u>Step 1 – HCM Delay Test (Sec.)</u>	-	-		
<u>Marlboro Pike & Penn Crossing Drive (Unsignalized)</u>				
<u>Step 1 – HCM Delay Test (Sec.)</u>	11.9 Sec.	23.4 Sec.	-	-
<u>MD 4 & MD 458</u>	876	1,307	A	D
<u>MD 458 & Marlboro Pike</u>	698	980	A	A
*In analyzing unsignalized intersections, average vehicle delay for various movements through the intersection is measured in seconds of vehicle delay. The numbers shown indicate the greatest average delay for any movement within the intersection. According to the Guidelines, a delay exceeding 50.0 seconds indicates inadequate traffic operations. Values shown as "+999" suggest that the parameters are beyond the normal range of the procedure and should be interpreted as severe inadequacy.				

Total Traffic

The following critical intersections identified above, when analyzed with total future traffic as developed using the "Transportation Review Guidelines, 2022 Supplement" (Guidelines) including the site trip generation as described above, operate as follows:

TOTAL TRAFFIC CONDITIONS				
<u>Intersection</u>	<u>Critical Lane Volume</u>		<u>Level of Service</u>	
	<u>(AM & PM)</u>		<u>(LOS, AM & PM)</u>	
<u>MD 4 & Brooks Drive (Signalized)</u>	990	1,157	A	C
<u>Marlboro Pike & Brooks Drive (Signalized)</u>	742	1,227	A	C
<u>MD 4 & Penn Crossing Drive (Unsignalized)</u>				
<u>Step 1 – HCM Delay Test (Sec.)</u>	13.4 Sec.	12.7 Sec.	Pass	Pass

Penn Crossing Drive & Site Access (Unsignalized) Step 1 – HCM Delay Test (Sec.)	9.0 Sec.	9.3 Sec.	Pass	Pass
Marlboro Pike & Penn Crossing Drive (Unsignalized) Step 1 – HCM Delay Test (Sec.)	12.3 Sec.	26.7 Sec.	Pass	Pass
MD 4 & MD 458	887	1,317	A	D
MD 458 & Marlboro Pike	713	993	A	A
*In analyzing unsignalized intersections, average vehicle delay for various movements through the intersection is measured in seconds of vehicle delay. The numbers shown indicate the greatest average delay for any movement within the intersection. According to the Guidelines, a delay exceeding 50.0 seconds indicates inadequate traffic operations. Values shown as "+999" suggest that the parameters are beyond the normal range of the procedure and should be interpreted as severe inadequacy.				

The TIS indicates that the site will be served by a single access connection along Penn Crossing Drive which is not consistent with the latest submission of the CSP. The CSP shows an additional point of access along Penn Crossing Drive on the site’s western boundary which will create a shared access connection with the existing Penn Station Shopping Center. The shared access connection will be required to be evaluated at the time of the PPS application as part of the site’s determination of adequacy and will be further assessed to determine if any additional improvements are needed to offset the site’s incremental impacts with existing traffic conditions. Staff believes that the site design layout was refined to include the shared access after the TIS was scoped and accepted by staff in July 2021, but believes that the exclusion of the site access does not significantly change the conclusion of the TIS submitted with the CSP application. However, as a condition of approval, staff recommends that a new scoping agreement is included with the TIS as part of the PPS submission and that an operational analysis that assesses the shared site access connection is provided to determine if any additional improvements are needed along Penn Crossing Drive to support the development.

Based on the findings and conclusions of the TIS, the Transportation Planning Section concludes that the existing road network will not be adversely impacted by traffic generated by the subject site and that the analyses provided by the applicant, are sufficient to support the proposed development and meets the requirements of Section 27-546 (d) (9).

Zoning Ordinance Compliance

Section 27-546(b)(7) and Section 27-546(d)(6-7) discuss transportation and circulation requirements in the M-X-T Zone and are copied below:

(b) In addition to the information required by Part 3, Division 9, for Conceptual Site Plans, the following information shall be included in Plans in the M-X-T Zone:

(7) The physical and functional relationship of the project uses and components.

(d) In addition to the findings required for the Planning Board to approve either the Conceptual or Detailed Site Plan (Part 3, Division 9), the Planning Board shall also find that:

(7) The pedestrian system is convenient and is comprehensively designed to encourage pedestrian activity within the development.

Comment: The applicant proposes two points of vehicle access to the site, both of which originate along Penn Crossing Drive. The proposed western point of access is along an existing roadway that falls between an existing SWM facility and the shopping center to the south. This point of access leads to a parking area situated at the rear of the multifamily buildings. The proposed eastern point of access also originates along Penn Crossing Drive and is proposed between the second and third multifamily buildings, also leading to the parking area to the rear.

Conceptual pedestrian access has been provided from Penn Crossing drive to each multifamily building. An additional conceptual point of pedestrian access has also been provided from the western point of vehicle access to the shopping center to the south. The applicant's submission displays a pedestrian connection to all buildings on site and an existing sidewalk along Penn Crossing Drive. At the time of subsequent development applications, staff will evaluate the need for on-site and off-site bicycle and pedestrian facilities. However, in reviewing the subject application, staff finds that CSP is sufficient for facilitating bicycle and pedestrian access and meets the required findings per section 27-546 of the Ordinance which examines the "physical and functional relationship of the project uses and components" within the M-X-T Zone.

Section 27-574(a) and Section 27-574(b)(1-3) discuss parking requirements in the M-X-T Zone and are copied below:

- (a) The number of parking spaces required in the M-X-T Zone and in a Metro Planned Community is to be calculated by the applicant and submitted for Planning Board approval at the time of Detailed Site Plan approval.
- (b) The number of off-street parking spaces required for development in the M-X-T Zone and a Metro Planned Community shall be calculated using the following procedures:
 - (1) Determine the number of parking spaces required for each use proposed, based on the requirements of Section 27-568. These parking spaces are to be considered as the greatest number of spaces which are occupied in any one (1) hour and are to know as the peak parking demand for each use. At less than this peak, the number of spaces being occupied is assumed to be directly proportionate to the number occupied during the peak (i.e., at eighty percent (80%) of the peak demand, eighty percent (80%) of the peak parking demand spaces are being occupied).
 - (2) For each hour of the day, the number of parking spaces to be occupied by each user shall be calculated. These numbers are known as the hourly fluctuation pattern. For each use, at least one (1) hour shall represent the peak parking demand, and the remaining hours will represent a percentage of the peak. There may be more than one (1) hour at the peak level.
 - (3) The total number of parking spaces required for all uses proposed in the M-X-T Zone and a Metro Planned Community shall be the greatest number of spaces in any one (1) hour for the combined total of all uses proposed, based on the calculations in

paragraphs (1) and (2), above. This total is known as the base requirement. The maximum parking allowable for non-residential uses is 115% of the base requirement for M-X-T properties. Parking spaces within a parking structure shall not be counted in the calculation of the maximum number of parking spaces.

Comment: Section 27-574 of the Ordinance allows the Planning Board to develop a criterion for parking standards, specific to the proposed development (subject to the evaluation on an analysis), for developing parking standards in the M-X-T zoning district. Staff recommends as a condition of approval that prior to acceptance of a DSP, the applicant shall submit a parking analysis to determine the parking rate for the proposed development, which examines both the residential and non-residential uses in accordance with Section 27-574 of the prior Ordinance.

Conclusion:

Overall, from the standpoint of The Transportation Planning Section it is determined that this plan is acceptable if the following conditions are met:

1. Prior to certification of the conceptual site plan, the applicant, and the applicant's heirs, successors, and/or assigns shall provide the following:
 - a. Show the limits of the ultimate right-of-way along the subject property's frontage of Pennsylvania Avenue
2. Prior to the acceptance of a preliminary plan of subdivision, the applicant, and the applicant's heirs, successors, and/or assigns shall:
 - a. Submit a new and approved Traffic Impact Study Scoping Agreement as part of a new Traffic Impact Study to evaluate transportation adequacy as part of the Preliminary Plan of Subdivision application.
 - i. The Traffic Impact Study Scoping Agreement shall include all site access connections consistent with the approved conceptual site plan application, CSP-87128-02.
 - b. Submit an operational and queuing analysis using the Highway Capacity Manual (HCM) methodology for the proposed site access driveways along Penn Crossing Drive.
3. Prior to the acceptance of a detailed site plan, the applicant, and the applicant's heirs, successors, and/or assigns shall:
 - a. Submit a parking analysis to determine the parking rate for the proposed development, which examines both the residential and commercial/retail uses in accordance with Section 27-574 of the prior Ordinance.
 - b. Submit a bicycle and pedestrian plan which displays the details, location, and extent of an interconnected network of onsite bicycle and pedestrian facilities.