



The Maryland-National Capital Park and Planning Commission

**PRINCE GEORGE'S COUNTY**  
**Planning Department**

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## Detailed Site Plan Ourisman of Bowie

## DSP-23026

REQUEST	STAFF RECOMMENDATION
Development of a 22,111-square-foot automotive sales and service dealership.	With the conditions recommended herein: <ul style="list-style-type: none"> <li>• APPROVAL of Detailed Site Plan DSP-23026</li> <li>• APPROVAL of Type 2 Tree Conservation Plan TCP2-089-05-01</li> <li>• APPROVAL of a Variance to Section 25-122(b)(1)(G)</li> </ul>

<b>Location:</b> On the west side of US 301 (Robert Crain Highway), approximately 410 feet south of its intersection with Mount Oak Road.	
Gross Acreage:	6.294
Zone:	CS
Prior Zone:	C-M
Reviewed per prior Zoning Ordinance:	Section 27-1903(b)
Gross Floor Area:	22,111 sq. ft.
Planning Area:	71B
Council District:	04
Municipality:	City of Bowie
<b>Applicant/Address:</b> Rowhit Bowie LLC 807 Rockville Pike Rockville, MD 20852	
<b>Staff Reviewer:</b> David Myerholtz <b>Phone Number:</b> 301-780-3411 <b>Email:</b> David.Myerholtz@ppd.mncppc.org	



Planning Board Date:	11/20/2025
Planning Board Action Limit:	Indefinite*
Staff Report Date:	11/06/2025
Date Accepted:	03/17/2025
Informational Mailing:	01/17/2024
Acceptance Mailing:	03/17/2025
Sign Posting Deadline:	10/21/2025

\*See page 3 of the technical staff report for Planning Board action, in accordance with Code requirements.

The Planning Board encourages all interested persons to request to become a person of record for this application. Requests to become a person of record may be made online at [http://www.mncppcapps.org/planning/Person\\_of\\_Record/](http://www.mncppcapps.org/planning/Person_of_Record/). Please call 301-952-3530 for additional information.

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THE MARYLAND-NATIONAL CAPITAL  
PARK AND PLANNING COMMISSION

PRINCE GEORGE'S COUNTY PLANNING BOARD

STAFF REPORT

SUBJECT: Detailed Site Plan DSP-23026  
Type 2 Tree Conservation Plan TCP2-089-05-01  
Variance to Section 25-122(b)(1)(G)  
Ourisman of Bowie

The Urban Design Section has reviewed the detailed site plan for the subject property and recommends APPROVAL, with conditions, as described in the Recommendation section of this technical staff report. In accordance with Section 27-285(c)(2) of the prior Prince George's County Zoning Ordinance, under which this application is being reviewed, the applicant has provided a waiver specifying an indefinite time period for action to be taken on this detailed site plan.

**EVALUATION CRITERIA**

The subject property is located within the Commercial, Service (CS) Zone. However, this application is being reviewed and evaluated in accordance with the Prince George's County Zoning Ordinance effective prior to April 1, 2022 (prior Zoning Ordinance). Pursuant to former Section 27-1903(b) of the Zoning Ordinance, development applications of any type for properties in the CS Zone may utilize the prior Zoning Ordinance and the property's prior zoning, provided such application was accepted for review on or before April 1, 2025, for development of the subject property.

Under the prior Zoning Ordinance, the property is subject to the standards of the Miscellaneous Commercial (C-M) Zone, which applied to this property prior to April 1, 2022. Staff considered the following in reviewing this detailed site plan:

- a. The requirements of the prior Prince George's County Zoning Ordinance for the Miscellaneous Commercial (C-M) Zone, and site design guidelines;
- b. The requirements of Preliminary Plan of Subdivision 4-04047;
- c. The requirements of the 2010 *Prince George's County Landscape Manual*;
- d. The requirements of the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance;
- e. The requirements of the Prince George's County Tree Canopy Coverage Ordinance;

- f. Referral comments; and
- g. Community feedback.

**FINDINGS**

Based upon the analysis of the subject application, the Urban Design staff recommend the following findings:

1. **Request:** This detailed site plan (DSP) is for development of a 22,111-square-foot vehicle sales and service building on 6.294 acres.
2. **Development Data Summary:**

	<b>EXISTING</b>	<b>EVALUATED</b>
Zone(s)	CS	C-M
Use(s)	Vacant	Automotive Sales and Service Dealership
Gross tract acreage	6.294	6.294
Lots	1*	1*
Gross floor area (sq. ft.)	-	22,111 sq. ft.

**Note:** \*The proposed vehicle sales and service building and associated parking will be on a single lot (Lot 1). The proposed access point is located beyond the subject property, on adjacent Lot 3, which was included in Preliminary Plan of Subdivision (PPS) 4-91054, approved by the Prince George’s County Planning Board on October 31, 1991, for five lots and one outparcel (PGCPB Resolution No. 91-393(C)). This access point is further discussed herein.

**Zoning Regulations** (Per Section 27-462(a) of the prior Prince George’s County Zoning Ordinance)

	<b>REQUIRED (min.)</b>	<b>PROVIDED</b>
Street setback – US 301	10 feet	89.3 feet
Side setback – North	12 feet	258.9 feet
Side setback - South	N/A	33.7 feet
Rear Setback	25 feet	93.1 feet
Building height	N/A	25 feet

**Parking Requirements** (Per Section 27-568(b) of the prior Zoning Ordinance)

<b>Requirement</b>		<b>Provided</b>
Vehicle Indoor Sales (1.0 sp/ 500 sq. ft. of GFA (15,464 sq. ft.))	min. 31	32
Vehicle Outdoor Sales (1 sp/ 1,000 sq. ft. of gross outdoor display area (1,032 sq. ft.))	2	2
Vehicle Service (3 sp/service bay (15 bays))	45	45
<ul style="list-style-type: none"> <li>Regular parking space (9.5 feet X 19 feet)</li> </ul>	-	58
<ul style="list-style-type: none"> <li>Compact parking space (8 feet X 16.5 feet)</li> </ul>	max. 26	17
<ul style="list-style-type: none"> <li>Handicap accessible space (8 feet x 19 feet)</li> </ul>	min. 4	4
<ul style="list-style-type: none"> <li><b>Total</b></li> </ul>	78*	<b>79</b>

**Note:** \*Of which at least four shall be handicap-accessible, in accordance with Section 27-566(b) of the prior Zoning Ordinance. In addition, up to 26 (one third of the requirement) may be compact, in accordance with Section 27-559(a) of the prior Zoning Ordinance.

**Loading Spaces** (Per Section 27-582(a) of the prior Zoning Ordinance)

	<b>Required (min.)</b>	<b>Provided</b>
Loading spaces (33 feet x 12 feet)	2	2

**Bicycle Spaces**

This DSP includes five U-shaped bicycle racks (10 spaces) located near the entrance of the front of the building, to support a multimodal system of service.

- 3. Location:** The subject site is in Planning Area 71B and Council District 4 within the limits of the City of Bowie. Geographically, it is located on the west side of US 301 (Robert Crain Highway), approximately 410 feet south of its intersection with Mount Oak Road.
- 4. Surrounding Uses:** The site is bounded to the north by a vacant lot in the Commercial, Service (CS) Zone (previously zoned Miscellaneous Commercial (C-M)); to the south by automotive sales and service facilities in the CS Zone (previously zoned C-M); to the east is US 301, and beyond is a residential property in the Agricultural-Residential (AR) Zone, (previously zoned Residential-Agricultural (R-A)); and to the west are residential properties in the Residential, Rural (RR) Zone (previously zoned Rural Residential (R-R)).
- 5. Previous Approvals:** The 2006 *Approved Master Plan and Sectional Map Amendment for Bowie and Vicinity* reclassified the subject property from the R-A Zone to the prior C-M Zone. The 2022 *Approved Bowie-Mitchellville and Vicinity Master Plan* retained the property in the prior C-M Zone.

PPS 4-04047 was approved by the Planning Board on September 9, 2004 (PGCPB Resolution No. 04-177), for one lot in support of a 29,800-square-foot automotive dealership in the C-M Zone, with direct access to US 301. This lot, known as Lot 1, was platted under Final Plat of Subdivision 5-06093 and recorded in the Prince George's County Land Records in Plat Book REP 212 Plat No. 11, titled Route 301 Realty Corp.

DSP-05013 was approved by the Planning Board on July 28, 2005 (PGCPB Resolution No. 05-173), for a 29,700-square-foot automotive dealership. This DSP was then amended by the Prince George's County Planning Director on August 26, 2006, to add basement parking in lieu of the previously approved rooftop parking. No physical development has occurred on this site, and the approval of DSP-05013 has lapsed.

6. **Design Features:** The proposal includes a single 22,111-square-foot building, with parking and circulation areas around it. The site will be accessed from a single access point on US 301 to the southeast. The proposed access point is located on adjacent Lot 3, which was approved under PPS 4-91054. The location of this access point consolidates access to the subject property with that of the adjacent vehicle sales and service use. Staff support this consolidation, however, this location for access was not considered at the time of PPS for the subject site, and therefore, no access easements have been established between the subject property and Lot 3. This is further discussed in Finding 7a below.

A large section of the northern portion of the site is within a 100-year floodplain easement (Plat Book 212, page 11) and will be retained as woodlands.

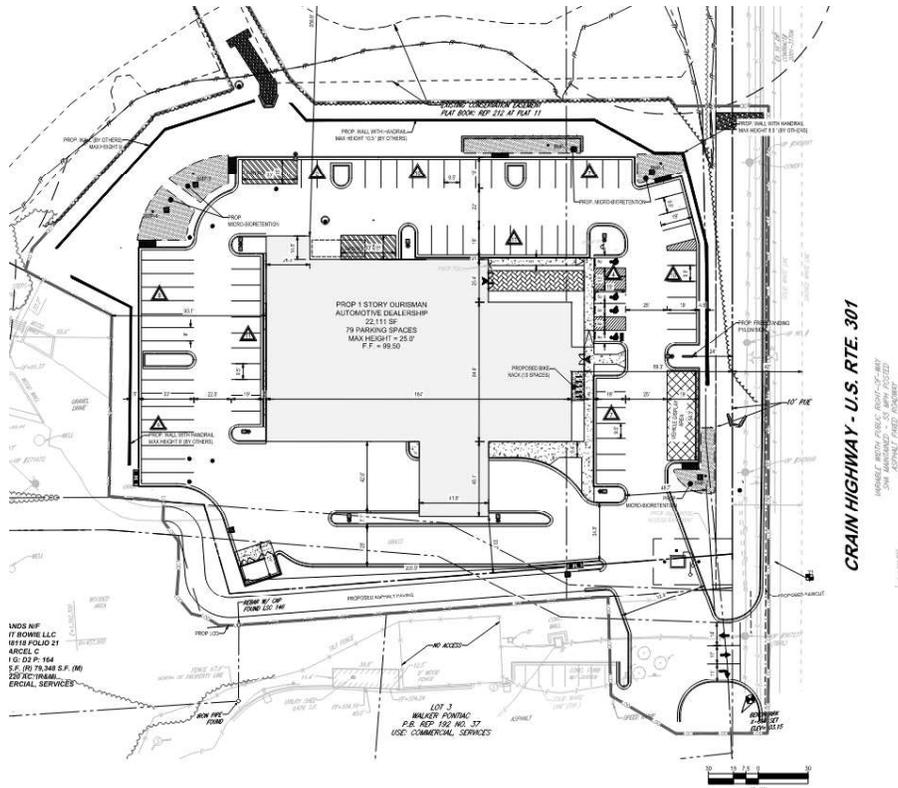
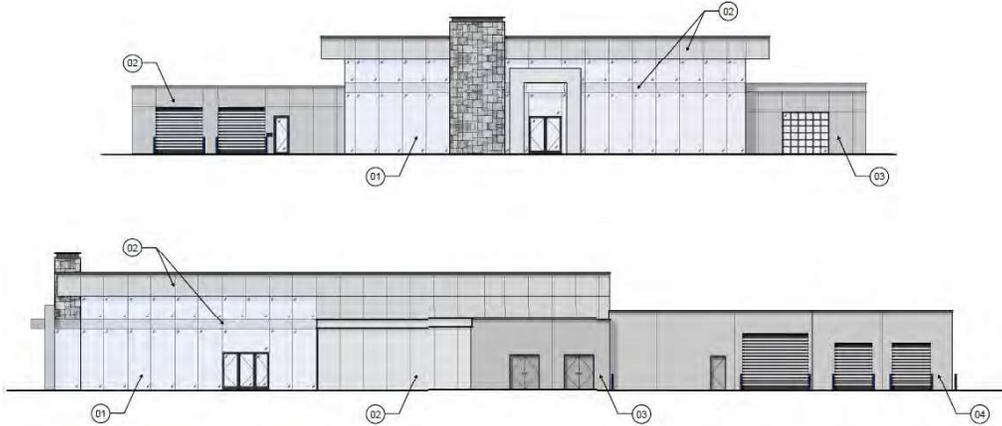


Figure 2: Detailed Site Plan

### Architecture

The architectural design of the proposed building is contemporary, with a flat roof. The building façade is finished with a mix of materials, including glazing, masonry, metal coping, aluminum wall panels, and exterior insulation and finish system. The roof features a variety of parapet heights, which breaks up the massing of the building.



**Figure 3: Building Elevations**

### Signage

The building will have four wall signs, and one pylon sign. According to Section 27-613 of the prior Zoning Ordinance, the total area of all wall signs for the proposed one-story building must not exceed 2 square feet for each linear foot of width along the building's principal entrance. The maximum allowable wall signage area is 310 square feet. The proposed signage locations and sizes shown on the east architectural elevation total 169 square feet, which complies with the allowable signage area. While the proposed signs do meet all prior Zoning Ordinance regulations in Section 27-613 relative to location, height, and area, the submitted plans do not include a detail for the "Ourisman" sign identified on Sheet 12 of the DSP, and the applicant did not submit a complete signage schedule on the plans. Therefore, a condition is included herein requiring the applicant to submit this detail, along with a complete signage schedule, prior to certification of the DSP. The 90-square-foot pylon sign will be located in the middle of the site's eastern frontage, set back 24 feet from the front property line, and is in conformance with Section 27-614 of the prior Zoning Ordinance.

### Lighting

The subject DSP includes both building-mounted and pole-mounted lighting throughout the site, with details. Staff find that the submitted photometric plan shows adequate lighting for users on-site and is sufficient for illuminating drive aisles, building entryways, and walking paths.

### Loading and Trash Facilities

The subject DSP includes two loading spaces located on the north side of the subject property. The trash enclosure is located near the southwest corner of the site. All trash and loading facilities will be screened from adjacent properties through landscaping and woodland preservation. Landscaping is shown along US 301, consistent with Section 4.2, Requirements for Landscape Strips Along Streets, of the 2010 *Prince George's County Landscape Manual* (Landscape Manual), which screens the loading space from the

right-of-way. The loading spaces shown on the DSP are 15 feet wide and 33 feet long, which conform to the requirements of Section 27-587 of the prior Zoning Ordinance.

Per Section 4.4 of the Landscape Manual, all dumpsters, trash pads, and trash collection or storage areas, including recycling facilities, are required to be screened from all outdoor recreation areas, retail parking areas, and entrance drives. The submitted plans also show the location of the proposed dumpster, with the details and dimensions of the dumpster enclosure that will wholly screen the dumpster from view.

## COMPLIANCE WITH EVALUATION CRITERIA

7. **Prince George’s County Zoning Ordinance:** The DSP application has been reviewed for compliance with the requirements of the C-M Zone, and the site design guidelines of the prior Zoning Ordinance:

a. This application is subject to the requirements of Section 27-448.01, Frontage, of the prior Zoning Ordinance, as follows:

**“Each lot shall have frontage on, and direct vehicular access to, a public street, except lots for which private streets or other access rights-of-way have been authorized pursuant to Subtitle 24 of this Code.”**

The use of a private easement must be authorized by the Planning Board, pursuant to the prior Prince George’s County Subdivision Regulations and a subdivision application. The applicant proposes joint access between the subject property and the adjacent dealership to the south of the subject site, in an effort to consolidate access points along US 301. Staff are supportive of this access consolidation. Further, the applicant provided correspondence with the Maryland State Highway Administration, which further supports consolidating the access points along US 301, and is included in the backup for this case. A final plat should be approved by the Planning Board that will authorize the use of a shared vehicular access easement pursuant to Section 24-128(b)(9) of the prior Subdivision Regulations, and the plat will reflect the shared vehicular access easement with the liber/folio reference and denial of access, as conditioned herein.

b. This application is subject to the requirements of Section 27-459, C-M Zone, of the prior Zoning Ordinance as follows:

**(b) Landscaping and screening.**

**(1) Landscaping and screening shall be provided in accordance with Section 27-450.**

In accordance with Section 27-450 of the prior Zoning Ordinance, “Landscaping, screening, and buffering of all development in the Commercial Zones shall be in accordance with the provisions of the Landscape Manual.” Evaluation of the Landscape Manual has been addressed in Finding 9 below.

**(c) Uses**

**(1) The uses allowed in the C-M Zone are as provided for in Table of Uses I (Division 3 of this Part).**

The subject DSP proposes to develop a vehicle sales and services establishment. Per Section 27-461(b) of the prior Zoning Ordinance, a vehicle sales lot, which may include dealer servicing and outdoor storage of vehicles awaiting sale, is a permitted use in the C-M Zone, subject to Footnote 72, which is relevant to properties located within an applicable Development District Overlay (D-D-O) Zone. This subject property is not within a D-D-O Zone, and as such, Footnote 72 is not relevant.

**(d) Regulations.**

**(1) Additional regulations concerning the location, size, and other provisions for all buildings and structures in the C-M Zone are as provided for in Divisions 1 and 5 of this Part, the Regulations Table (Division 4 of this Part), General (Part 2), Off-Street Parking and Loading (Part 11), Signs (Part 12), and the Landscape Manual.**

The subject DSP is in conformance with these regulations. Specific details have been discussed in Findings 2 and 6 above, and Finding 9 below.

c. Section 27-274(a) of the prior Zoning Ordinance provides site design guidelines for a DSP. The applicable design guidelines are described as follows:

**(2) Parking, loading, and circulation.**

**(A) Surface parking lots should be located and designed to provide safe and efficient vehicular and pedestrian circulation within the site, while minimizing the visual impact of cars. Parking spaces should be located to provide convenient access to major destination points on the site.**

**(i) Parking lots should generally be provided to the rear or sides of structures;**

**(ii) Parking spaces should be located as near as possible to the uses they serve;**

**(iii) Parking aisles should be oriented to minimize the number of parking lanes crossed by pedestrians;**

**(iv) Large, uninterrupted expanses of pavement should be avoided or substantially mitigated by the location of**

**green space and plant materials within the parking lot, in accordance with the Landscape Manual, particularly in parking areas serving townhouses; and**

- (v) Special areas for van pool, car pool, and visitor parking should be located with convenient pedestrian access to buildings.**

Access to the site is provided by one driveway along US 301. The development requires 78 parking spaces of which 79 are provided to include 4 ADA, 17 compact, 58 standard parking spaces, and 5 bicycle racks to accommodate 10 bicycles. The submitted plans show sidewalk connections within the subject site from the parking areas to the primary entrance into the building. Parking spaces are arranged along the drive aisles on the east, north, and west sides of the building for easy access, and to avoid conflicts with pedestrian connectivity. The majority of the provided parking is located to the rear and sides of the primary structure on the site, however there are 17 spaces (approximately 21 percent of the provided parking) between the building and US 301. Four of these spaces are handicap-accessible and are appropriately located close to the primary entrance. Vehicle sales uses also typically locate vehicles that are for sale near the front of the site. Staff find that the parking lot for this development is generally located to the sides and rear of the primary structure, while being located as near as possible to the use they serve. Handicap-accessible parking spaces are provided and easily visible for patron use. The perimeter parking lot landscaping minimizes the visual impact of cars from US 301.

- (B) Loading areas should be visually unobtrusive and located to minimize conflicts with vehicles or pedestrians. To fulfill this goal, the following guidelines should be observed:**

- (i) Loading docks should be oriented toward service roads and away from major streets or public view; and**
- (ii) Loading areas should be clearly marked and should be separated from parking areas to the extent possible.**

Two loading spaces are included in this DSP, which has been discussed in Finding 6 above. The loading spaces are located internal to the subject site, and to the north of the proposed building, set back a minimum of 200 feet from US 301 and public view, and are further screened in accordance with Section 4.3 of the Landscape Manual. The loading spaces are located away from the on-site pedestrian circulation and outside of the drive aisles. Accordingly, they are visually unobtrusive and have minimal conflicts with vehicles and pedestrians.

- (C) Vehicular and pedestrian circulation on a site should be safe, efficient, and convenient for both pedestrians and drivers.**
- (i) The location, number and design of driveway entrances to the site should minimize conflict with off-site traffic, should provide a safe transition into the parking lot, and should provide adequate acceleration and deceleration lanes, if necessary;**
  - (ii) Entrance drives should provide adequate space for queuing;**
  - (iii) Circulation patterns should be designed so that vehicular traffic may flow freely through the parking lot without encouraging higher speeds than can be safely accommodated;**
  - (iv) Parking areas should be designed to discourage their use as through-access drives;**
  - (v) Internal signs such as directional arrows, lane markings, and other roadway commands should be used to facilitate safe driving through the parking lot;**
  - (vi) Drive-through establishments should be designed with adequate space for queuing lanes that do not conflict with circulation traffic patterns or pedestrian access;**
  - (vii) Parcel pick-up areas should be coordinated with other on-site traffic flows;**
  - (viii) Pedestrian access should be provided into the site and through parking lots to the major destinations on the site;**
  - (ix) Pedestrian and vehicular circulation routes should generally be separated and clearly marked;**
  - (x) Crosswalks for pedestrians that span vehicular lanes should be identified by the use of signs, stripes on the pavement, change of paving material, or similar techniques; and**
  - (xi) Barrier-free pathways to accommodate the handicapped should be provided.**

The location of the driveway entrance from US 301 is to be collocated with the adjacent property to the south, which is also developed with a vehicle sales and service dealership. Consolidating these entrance drives provides a more efficient and safe accessway

to these two dealerships than would multiple driveway entrances fronting an arterial roadway. The proposed entrance drive provides adequate space for queuing, such that no cars will back up onto public right-of-way. Circulation through the site allows for free flowing traffic without encouraging higher speeds, and discouraging any through-traffic. On-site directional signage is provided. Given the site's frontage on US 301, which is classified as an arterial roadway adjacent to the property, this site and the surrounding areas are not conducive to pedestrian activity along the right-of-way. However, pedestrian access is provided internally from the customer parking areas to the primary entrance for the facility and is safely separated from vehicular circulation lanes. Barrier-free pathways to accommodate the handicapped are provided. The submitted circulation plans show both vehicular and pedestrian circulation to be safe, efficient, and convenient.

**(3) Lighting.**

**(A) For uses permitting nighttime activities, adequate illumination should be provided. Light fixtures should enhance the site design's character. To fulfill this goal, the following guidelines should be observed:**

- (i) If the development is used at night, the luminosity, orientation, and location of exterior light fixtures should enhance user safety and minimize vehicular/pedestrian conflicts;**
- (ii) Lighting should be used to illuminate important on-site elements such as entrances, pedestrian pathways, public spaces, and property addresses. Significant natural or built features may also be illuminated if appropriate to the site;**
- (iii) The pattern of light pooling should be directed on-site;**
- (iv) Light fixtures fulfilling similar functions should provide a consistent quality of light;**
- (v) Light fixtures should be durable and compatible with the scale, architecture, and use of the site; and**
- (vi) If a variety of lighting fixtures is needed to serve different purposes on a site, related fixtures should be selected. The design and layout of the fixtures should provide visual continuity throughout the site.**

Lighting for this DSP has been discussed in Finding 6 above, demonstrating adequate illumination for users and for the site in the evening. The lighting adequately illuminates on-site entryways and

will enhance user safety. Light pooling is directed on-site. The fixtures used throughout are similar in function, made from durable materials that are compatible with the scale and architecture of the site. These fixtures will provide a consistent quality of light for the development.

**(4) Views.**

**(A) Site design techniques should be used to preserve, create, or emphasize scenic views from public areas.**

The varied landscaping proposed along the property's border, facing US 301, creates a scenic view for both drivers passing by and for pedestrians within the site.

**(5) Green Area.**

**(A) On-site green area should be designed to complement other site activity areas and should be appropriate in size, shape, location, and design to fulfill its intended use. To fulfill this goal, the following guidelines should be observed:**

- (i) Green area should be easily accessible in order to maximize its utility and to simplify its maintenance;**
- (ii) Green area should link major site destinations such as buildings and parking areas;**
- (iii) Green area should be well-defined and appropriately scaled to meet its intended use;**
- (iv) Green area designed for the use and enjoyment of pedestrians should be visible and accessible, and the location of seating should be protected from excessive sun, shade, wind, and noise;**
- (v) Green area should be designed to define space, provide screening and privacy, and serve as a focal point;**
- (vi) Green area should incorporate significant on-site natural features and woodland conservation requirements that enhance the physical and visual character of the site; and**
- (vii) Green area should generally be accented by elements such as landscaping, pools, fountains, street furniture, and decorative paving.**

Approximately 61.21 percent of the site is provided as green area, and these areas are well-defined and appropriately scaled to support the subject development. Specifically, the required green area is provided along the perimeter of the property and acts as a significant buffer on the north and west sides of the property. This works to effectively screen the proposed use from the surrounding properties and the street. Accordingly, staff find that the proposed on-site green area is designed to complement other site activity areas and is appropriate in size, shape, location, and design to fulfill its intended use.

**(B) The application shall demonstrate the preservation and/or restoration of the regulated environmental features in a natural state to the fullest extent possible in accordance with the requirement of Subtitle 24-130(b)(5).**

Regulated environmental features (REF) are required to be preserved and/or restored to the fullest extent possible under Section 24-130(b)(5) of the prior Subdivision Regulations. The on-site REF includes streams, stream buffers, wetlands, wetland buffers, 100-year floodplain, and steep slopes.

In conformance with Section 24-130(b)(5), the REF on the subject property have been preserved and/or restored to the fullest extent possible based on the limits of disturbance shown on the Type 2 tree conservation plan (TCP2). No additional primary management area (PMA) impacts were requested with this application.

**(6) Site and streetscape amenities.**

**(A) Site and streetscape amenities should contribute to an attractive, coordinated development and should enhance the use and enjoyment of the site. To fulfill this goal, the following guidelines should be observed:**

- (i) The design of light fixtures, benches, trash receptacles, bicycle racks and other street furniture should be coordinated in order to enhance the visual unity of the site;**
- (ii) The design of amenities should take into consideration the color, pattern, texture, and scale of structures on the site, and when known, structures on adjacent sites, and pedestrian areas;**
- (iii) Amenities should be clearly visible and accessible, and should not obstruct pedestrian circulation;**
- (iv) Amenities should be functional and should be constructed of durable, low maintenance materials;**

- (v) Amenities should be protected from vehicular intrusion with design elements that are integrated into the overall streetscape design, such as landscaping, curbs, and bollards;**
- (vi) Amenities such as kiosks, planters, fountains, and public art should be used as focal points on a site; and**
- (vii) Amenities should be included which accommodate the handicapped and should be appropriately scaled for user comfort.**

Proposed light fixtures and bike racks are adjacent to pedestrian and vehicular paths and finished to complement the establishment's color scheme and align with the overall building design. These amenities will be functional for the site's use and constructed of durable, low maintenance materials. Landscaping and curbs protect the site amenities from vehicular intrusion. No kiosks, planters, fountains, public art, or similar amenities are included in this DSP. The site is not intended as a public gathering space, nor will it function as a path between destinations, as such amenities such as kiosks, planters, fountains, and public art are not necessarily appropriate for this site. The proposed amenities are clearly visible and do not obstruct pedestrian circulation and will accommodate all users, as they are appropriately scaled for user comfort.

**(7) Grading.**

- (A) Grading should be performed to minimize disruption to existing topography and other natural and cultural resources on the site and on adjacent sites. To the extent practicable, grading should minimize environmental impacts. To fulfill this goal, the following guidelines should be observed:**
  - (i) Slopes and berms visible from streets and other public areas should appear as naturalistic forms. Slope ratios and the length of slopes should be varied if necessary to increase visual interest and relate manmade landforms to the shape of the natural terrain;**
  - (ii) Excessive grading of hilltops and slopes should be avoided where there are reasonable alternatives that will preserve a site's natural landforms;**
  - (iii) Grading and other methods should be considered to buffer incompatible land uses from each other;**

- (iv) Where steep slopes cannot be avoided, plant materials of varying forms and densities should be arranged to soften the appearance of the slope; and**
- (v) Drainage devices should be located and designed so as to minimize the view from public areas.**

The subject property has a moderate slope with existing steep slopes along the site's US 301 frontage. Grading will be implemented along the north and west sides of the proposed parking area to assist with site drainage toward the existing floodplain areas. The subject DSP also includes retaining walls along three sides of the development, to enhance stability for providing a flat area within the site while preserving existing grades off-site. Accordingly, staff find that the proposed grading will minimize disruption to existing topography and other natural and cultural resources on the site and on adjacent sites.

**(8) Service Areas.**

- (A) Service areas should be accessible, but unobtrusive. To fulfill this goal, the following guidelines should be observed:**
  - (i) Service areas should be located away from primary roads, when possible;**
  - (ii) Service areas should be located conveniently to all buildings served;**
  - (iii) Service areas should be effectively screened or enclosed with materials compatible with the primary structure; and**
  - (iv) Multiple building developments should be designed to form service courtyards which are devoted to parking and loading uses and are not visible from public view.**

The submitted plans show that the location of the proposed dumpster and loading spaces are accessible, but unobtrusive due to their locations near the west portion of the development, away from US 301. These locations are also convenient to the building which they will serve. While both are screened with landscaping, the dumpster is also enclosed with materials compatible with the primary structure.

**(9) Public Spaces.**

**(A) A public space system should be provided to enhance a large-scale commercial, mixed-use, or multifamily development. To fulfill this goal, the following guidelines should be observed:**

- (i) Buildings should be organized and designed to create public spaces such as plazas, squares, courtyards, pedestrian malls, or other defined spaces;**
- (ii) The scale, size, shape, and circulation patterns of the public spaces should be designed to accommodate various activities;**
- (iii) Public spaces should generally incorporate sitting areas, landscaping, access to the sun, and protection from the wind;**
- (iv) Public spaces should be readily accessible to potential users; and**
- (v) Pedestrian pathways should be provided to connect major uses and public spaces within the development and should be scaled for anticipated circulation.**

This DSP is for a single building and nonresidential use and is not considered a large scale or mixed-use development. As such, this section is not applicable.

**(10) Architecture.**

- (A) When architectural considerations are referenced for review, the Conceptual Site Plan should include a statement as to how the architecture of the buildings will provide a variety of building forms, with a unified, harmonious use of materials and styles.**
- (B) The guidelines shall only be used in keeping with the character and purpose of the proposed type of development and the specific zone in which it is to be located.**
- (C) These guidelines may be modified in accordance with Section 27-277.**

A detailed discussion regarding architecture has been addressed in Finding 6 above.

**(11) Townhouses and three-family dwellings.**

This requirement is not applicable to this DSP because it does not include any townhouses or three-story units.

- 8. Preliminary Plan of Subdivision 4-04047:** PPS 4-04047 was approved by the Planning Board on September 9, 2004 (PGCPB Resolution No. 04-177), for one lot in support of a 29,800-square-foot automotive dealership in the C-M Zone, subject to 13 conditions. The conditions relevant to the subject DSP are listed below, in **bold** text. Staff's analysis of the PPS conditions follows each one, in plain text:

- 1. Development of this subdivision shall be in compliance with an approved Type I Tree Conservation Plan (TCPI/32/04). The following note shall be placed on the Final Plat of Subdivision:**

**“This development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCPI/32/04), or modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy.”**

TCP2-089-05-01 was submitted with this application and is in compliance with Type I Tree Conservation Plan TCPI-32-04.

- 4. The applicant shall provide sufficient and uninterrupted access to Parcels 103 and 164, either along the existing ingress/egress easement or through some other means mutually agreeable to all parties.**

At the time of PPS 4-04047, Parcels 103 and 164 were under private ownership and used for residential purposes adjacent to the subject site. Both Parcels 103 and 164 are deed parcels and have since been purchased by the applicant, Rowhit Bowie LLC. The site plan shows asphalt paving that will provide sufficient and uninterrupted access to Parcels 103 and 164, within a 20-foot-wide ingress/egress easement (Liber 2296 Folio 329), in conjunction with the shared vehicular access easement, as conditioned herein.

- 8. Prior to the issuance of building permits, the applicant shall obtain detailed site plan approval by the Planning Board. The detailed site plan shall address buffering (including light and noise intrusion), landscaping areas, and techniques to further minimize PMA impacts and the visual relationship between the proposed development and adjoining residentially developed properties.**

This DSP application is submitted in accordance with the above condition. The submitted DSP set includes a landscape plan, lighting plan, and a revised TCP2 that fully addresses buffering (including light and noise intrusion), landscaping, access, techniques to further minimize PMA impacts, and the visual relationships between the proposed development and adjoining residentially developed properties. These

issues are addressed through consolidation of the development to the southeast portion of the site, situated far from the adjacent residential areas and PMA.

- 9. Development of this subdivision shall be in compliance with the approved stormwater management concept plan (8254-2004-01) or any approved revision thereto.**

Jurisdiction of stormwater management (SWM) regulations has transferred from the County to the City of Bowie, since the approval of the PPS. SWM Concept Plan No. 8245-2004-01 was replaced with the City of Bowie approved SWM Concept Plan No. 02-1223-204NE14. This application is in compliance with the City of Bowie's approved SWM concept plan.

- 10. The applicant shall provide for any necessary turn lanes and frontage improvements along US 301 as required by SHA. These may include turn lanes for deceleration and acceleration of vehicles and channelization (lane divider) at the site entrance to prevent weaving.**

The submitted DSP includes a transportation impact analysis which addresses the necessary turn lanes and frontage improvements along US 301. Per correspondence from the Maryland State Highway Administration (SHA), it is desired to retain the current 12-foot-wide shoulder, with the addition of curb and gutter and tapering, to accommodate the right turning movement into the site. A condition is included herein requiring the applicant to identify and provide details for the curb/gutter and tapering desired by SHA.

- 12. Total development within the subject property shall be limited to 29,800 square feet of automobile dealership facilities, or equivalent development that generates no more than 52 AM and 62 PM new peak-hour trips, taking into account estimated pass-by trips. Any development other than that identified herein above shall require a new preliminary plan of subdivision with a new determination of the adequacy of transportation facilities.**

A trip generation analysis submitted by the applicant indicates that the development will not exceed the established trip cap established by PPS 4-04047. Staff find this this submittal to be accurate and in conformance with the above condition.

- 9. 2010 Prince George's County Landscape Manual:** The application is subject to the requirements of Section 4.2, Landscape Strips Along Streets; Section 4.3, Parking Lot Requirements; Section 4.4, Screening Requirements; Section 4.7, Buffering Incompatible Uses; and Section 4.9, Sustainable Landscaping Requirements, of the Landscape Manual. The submitted landscape plan demonstrates conformance to these requirements, with the exceptions detailed below.

Section 4.3(c)(2)(D) provides regulations for interior parking lot landscaping planting space area. Specifically, a minimum of 160 square feet of contiguous, pervious land area shall be provided for each tree, and planting islands that are parallel to parking spaces on both sides shall be a minimum of 9 feet wide. The submitted landscape plans indicate that these regulations are not met. Staff find that minor adjustments to the plans will yield

conformance with these requirements, and a condition is included herein requiring those adjustments.

A technical correction is required for the schedule provided for Section 4.7, Bufferyard 1. The adjacent use is labeled as “service commercial,” but instead needs to be identified as single-family development. This correction is conditioned herein.

- 10. Prince George’s County Woodland and Wildlife Habitat Conservation Ordinance:** This property is subject to Subtitle 25, Division 2, the Woodland and Wildlife Habitat Conservation Ordinance (WCO), and the 2018 *Environmental Technical Manual* (ETM). Type II Tree Conservation Plan TCPII-089-05 was approved for the site on April 28, 2006. The worksheet on the prior TCPII showed that the site contained 2.94 acres of woodland in the net tract, and 1.86 acres of woodlands in the floodplain. Of this, 1.91 acres of net tract woodlands and 0.11 acre of woodlands in the floodplain were approved to be cleared. The woodland requirement for the TCPII was 1.17 acres, which was to be met by 1.03 acres of on-site preservation, and 0.14 acre to be met by fee-in-lieu. The site was cleared based on this prior TCPII, but without a permit from the County. To address the clearing that has occurred on-site, the current TCP2 worksheet calculations shall be based on the site woodlands as existing in June 2022.

TCP2-089-05-01 was submitted with this application showing that this 6.294-acre site contains 2.76 acres of floodplain for a net tract area of 3.53 acres. The TCP2 shows that the site contains 3.80 acres of woodlands in the net tract, and 2.75 acres in the floodplain, as of June 2022. The amount of woodlands shown is larger than the gross tract. Prior to signature approval, the worksheet will need to be corrected to reflect the amount of woodlands on-site in June 2022. Based on the acreage provided, the site woodland conservation threshold is 0.53 acre. The applicant plans to clear 1.82 acres of woodlands in the net tract and 0.14 acre in the floodplain. The TCP2 shows the woodland conservation requirement of 2.49 acres being met with 0.99 acre of preservation, 0.03 acre of on-site reforestation, and 1.47 acres of off-site woodland conservation credits. As proposed, the development meets the 15 percent woodland conservation threshold on-site.

- 11. Prince George’s County Tree Canopy Coverage Ordinance:** Subtitle 25, Division 3, the Tree Canopy Coverage Ordinance, requires a minimum percentage of the site to be covered by tree canopy for any development projects that propose more than 2,500 square feet of gross floor area, or disturbance, and requires a grading permit. The Tree Canopy Coverage Ordinance is not subject to the current Zoning Ordinance grandfathering provisions and does not contain any grandfathering provision for prior zoning, except for specified legacy zones or developments that had a previously approved and valid landscape plan demonstrating conformance to the Tree Canopy Coverage Ordinance. Therefore, this application was reviewed for conformance with the tree canopy coverage (TCC) requirement for the current property zone, which is Commercial, Service (CS) Zone.

Per Section 25-128(b) of the Tree Canopy Coverage Ordinance, the minimum TCC required in the CS Zone is 15 percent of the net tract area, or 23,065 square feet. The DSP meets this requirement by providing approximately 51,074 square feet of TCC within the net tract area. Technical corrections are needed to accurately reflect conformance to these requirements on the TCC schedule, as conditioned herein.

**12. Referral Comments:** The subject application was referred to the concerned agencies and divisions. The referral comments are summarized as follows, and incorporated herein by reference:

a. **Historic Preservation and Archeological Review**—In a memorandum dated April 15, 2025 (Stabler, Smith, and Chisholm to Myerholtz), the Historic Preservation Section offered the following comments:

A search of current and historic photographs, topographic and historic maps, and locations of currently known archeological sites indicates the probability of archeological sites within the subject property is high. The subject property had farm structures on it until the 1990s. A stream runs through the west side of the parcel that is a tributary of Mill Branch. Many indigenous archaeological sites have been identified within a mile of the subject property.

The 2010 *Approved Historic Sites and Districts Plan* contains goals and policies that are relevant to the subject property. A goal (page 59) and related policy in planning for archeology is to:

- **Incorporate archeological resource protection into the local land use and comprehensive planning processes through site identification and preservation.**
- **Policy 1: Ensure that archeological resources are considered and protected through all phases of the development process.**

To address the strategies above, staff recommend monitoring for archeological resources during any grading and ground disturbance on the property.

b. **Community Planning**—In a memorandum dated April 18, 2025 (Lester to Myerholtz), the Community Planning Division provided that the subject DSP does conform with the 2022 *Approved Bowie-Mitchellville and Vicinity Master Plan* recommended land use for the subject property.

c. **Transportation Planning**—In a memorandum dated October 17, 2025 (Wilson to Myerholtz), the Transportation Planning Section offered analysis of the DSP, with regards to relevant prior conditions of approval and Zoning Ordinance compliance, which have been incorporated into the findings and conditions of this technical staff report.

d. **Environmental Planning**—In a memorandum dated October 17, 2025 (Rea to Myerholtz), the Environmental Planning Section offered the following:

**Natural Resources Inventory/Environmental Features**

In conformance with Section 24-120(a)(22) of the prior Subdivision Regulations, an approved Natural Resources Inventory Plan (NRI-081-2024) was submitted with the application. The site contains streams, wetlands, 100-year floodplain, and severe slopes. No additional information is required for conformance to the NRI.

### **Riparian Stream Buffer**

The site contains a riparian stream buffer that is required to be fully wooded in accordance with Section 25-121(c)(1)(C) of the WCO; however, this site qualifies for exceptions to this requirement based on criteria outlined in the code. The TCP2 and statement of justification (SOJ) indicate that clearing is proposed within the stream and stream buffer for a sewer connection which was previously approved and unavoidable. This impact is discussed in the PMA impact section herein. Staff find compliance with the exceptions demonstrated with the application as submitted.

### **Specimen Trees**

Tree conservation plans are required to meet all the requirements of Subtitle 25, Division 2 of the Prince George's County Code, including the preservation of specimen trees in Section 25-122(b)(1)(G) of the WCO. Every effort should be made to preserve the trees in place, considering the different species' ability to withstand construction disturbance (refer to the Construction Tolerance Chart in the ETM for guidance on each species' ability to tolerate root zone disturbances).

If, after careful consideration has been given to the preservation of the specimen trees, there remains a need to remove any of the specimen trees, a variance from Section 25-122(b)(1)(G) is required. Applicants can request a variance from the provisions of Division 2 of Subtitle 25 (the WCO) provided all of the required findings in Section 25-119(d) of the WCO can be met. An application for a variance must be accompanied by an SOJ stating the reasons for the request and how the request meets each of the required findings.

### **Review of Subtitle 25 Variance Request**

The site contains 11 specimen trees. A Subtitle 25 variance application and an SOJ in support of the variance were received on September 15, 2025, and resubmitted on October 17, 2025. The request is for the removal of two specimen trees, identified on the TCP2 as ST-1377 and ST-1380. The trees are in poor and fair condition as indicated on the approved NRI. The TCP2 shows the location of the proposed trees for removal in the area of the proposed parking lot and access road.

It should be noted that specimen tree ST-1377 was previously approved for removal with TCPII-89-05. Also, the area where ST-1380 is located is shown as developed on TCPI-32-04; however, at the time of that application, the tree had likely not achieved specimen tree status and therefore, was not identified on the plan.

### **Analysis of Section 25-122(b)(1)(G) WCO Variance Request**

Section 25-119(d)(3) of the WCO contains six findings (text in **bold** below) to be made before a variance from the WCO can be granted. The submitted SOJ seeks to address the required findings for two specimen trees (ST-1377 and ST-1380) proposed for removal. Considerations for staff recommendation include construction tolerance, distance from development impacts to the trees, and condition of the trees. An evaluation of this variance request, with respect to the required findings, is provided below. Staff support the removal of the two specimen trees requested by the applicant, based on these findings.

- (A) Special conditions peculiar to the property have caused unwarranted hardship;**

This specimen tree removal variance request was evaluated using the Woodland and Wildlife Habitat Conservation Priorities as outlined in Section 25-121(b)(1) of the WCO. The specimen trees requested for removal will allow for the protection of the woodlands with the highest priorities, as listed in Section 25-121(b)(1), to the maximum extent practicable and allow for the development of this site to occur in the lower priority areas of the site. Requiring the applicant to retain these two specimen trees on the site by designing the development to avoid impacts to the critical root zone would further limit the area of the site available for the orderly development that is consistent with the zoning and the recommendations of the master plans, to the extent that it would cause the applicant an unwarranted hardship.

The applicant states in the variance request that special conditions peculiar to the property have caused unwarranted hardship. In relation to other properties in the area, special conditions peculiar to the property would cause an unwarranted hardship if the applicant were required to retain the two specimen trees identified as specimen trees ST-1377 and ST-1380. To retain a significant amount of PMA on the property, development has been consolidated to the southern portion of the site, toward the area where the subject specimen trees are located. Those special conditions relate to the specimen trees themselves, such as their size, condition, species, and on-site location.

This site sits on the west side of US 301. There is developed land to the south and the southwest of the site. Along the northern and northwest portion of the property, there is a stream and 100-year floodplain. The site has obtained prior PPS and DSP approvals for the same development area as this DSP. A conservation and floodplain easement have already been recorded protecting these regulated areas. The development requires SWM, grading, safe circulation, utilities, and landscaping on-site, in conformance with other sections of the County Code.

The applicant has designed the site in such a way that the proposal is in the developable areas outside of the PMA except for areas previously approved by the PPS for development.

The SOJ indicates that specimen trees ST-1377 and ST-1380 are proposed for removal because they are located within the footprint of the proposed buildings and access road to the adjoining property. The two specimen trees requested for removal are scattered throughout the site, are not located within the PMA, and are proposed to be removed to successfully construct the proposed development as envisioned with the PPS.

Requiring the applicant to retain the two specimen trees on the site would further limit the area of the site available for development, to the extent that it causes an applicant unwarranted hardship. Alternatives to save these trees would compromise other requirements of the zones, the sector plan, and the County Code given their location.

Based on the uniqueness of the property setting and the location of the trees, staff find that the two specimen trees are located on the developable portion of the site, and in areas necessary to meet the design and infrastructure requirements.

**(B) Enforcement of these rules will deprive the applicant of rights commonly enjoyed by others in similar areas;**

Enforcement of the requirement that all specimen trees be preserved, along with an appropriate percentage of their critical root zone, would deprive the applicant of rights commonly enjoyed by others in similar areas with comparable zoning. All variance applications for the removal of specimen trees are evaluated in accordance with the requirements of the WCO and the ETM for site-specific conditions.

Specimen trees grow to such a large size because they were left undisturbed on a site for sufficient time to grow. The development is required to provide SWM, grading, safe circulation, utilities, and landscaping on-site in conformance with other sections of the County Code. The applicant is seeking to develop this property in conformance with the applicable provisions of the Zoning Ordinance. The applicant has designed the site in a way which maximizes the buildable areas of the site, while limiting the impacts to the PMA to only those which are allowable, reasonable, and necessary. The removal of specimen trees for the installation of parking and an access road is expected with development. This application localizes the removal of specimen trees away from the REF. Other sites that contain constraints and conditions similar to these would be given the same considerations during the review of the variance application.

**(C) Granting the variance will not confer on the applicant a special privilege that would be denied to other applicants;**

All variance applications for the removal of specimen trees are evaluated in accordance with the requirements of Subtitle 25 of the County Code and the ETM for site-specific conditions. When similar trees were encountered on other sites for comparable developments, they have been evaluated under the same criteria.

The applicant states that given the evidence in Variance Findings (B) above, not granting the variance would prevent the project from being developed within the County standard design parameters for commercial use. The applicant has made considerable efforts to avoid additional PMA impacts. The two specimen trees proposed for removal are in poor and fair condition. The applicant is preserving 0.99 acre of woodland and retaining an additional 2.61 acres of woodland that is not being credited due to being in the floodplain. This is not a special privilege that would be denied to other applicants. If other properties encounter trees in similar locations on a site with REF and PMA, the same considerations would be provided during the review of the variance application.

**(D) The request is not based on conditions or circumstances which are the result of actions by the applicant;**

The request for removal of the two trees is a result of their location on the property and the limitations on site design which are not the result of actions by the applicant. SWM, road improvements, slope stability, and other requirements are established by the County. Any development on this site would be subject to meeting the current requirements of the County based on the scope of that proposed development. The removal of the two specimen trees is requested to achieve the development for the proposed car dealership with associated infrastructure, and woodland conservation.

**(E) The request does not arise from a condition relating to land or building use, either permitted or nonconforming, on a neighboring property; and,**

The variance SOJ states that this request is not from a condition on a neighboring property. The request to remove the specimen trees does not arise from a condition relating to land or building use, either permitted or nonconforming on a neighboring property. The trees have grown to this size because of favorable conditions and lack of disturbance.

**(F) Granting of the variance will not adversely affect water quality.**

The site is governed by the state and local SWM regulations, which require the post-development site to mimic pre-development conditions as “woods in good condition.”

Granting the variance for the removal of two specimen trees will not adversely affect water quality because the applicant is required to meet current SWM requirements on-site. Stormwater requirements will be evaluated by the City of Bowie and additional information regarding the proposed stormwater facilities can be located in the stormwater section of this memorandum. Sediment and erosion control measures for this site will be subject to the requirements of Prince George’s County Soil Conservation District. Both SWM and sediment and erosion control requirements are to be met in conformance with state and local laws to ensure that the quality of water leaving the site meets the state’s standards, which are set to ensure that no degradation occurs.

**Summary**

The required findings of Section 25-119(d) have been adequately addressed for the removal of two specimen trees identified as Specimen Trees ST-1377 and ST-1380.

Per the findings above, staff recommend that the Planning Board approve the requested variance to Section 25-122(b)(1)(G), for the removal of these two specimen trees for the construction of a car dealership in the prior C-M Zone. The replacement requirement for the specimen tree removal, in accordance with Section 25-119(d)(7) of the WCO will be evaluated at the time of signature approval.

- e. **Permit Review**—In a memorandum dated April 18, 2025 (Jacobs to Myerholtz), the Permit Review Section noted technical corrections needed to the plans, which have been incorporated into the conditions herein.
  - f. **Prince George’s County Department of Permitting, Inspections and Enforcement (DPIE)**—At the time of the writing of this technical staff report, DPIE had not offered comments on this application.
  - g. **Price George’s County Police Department**—At the time of the writing of this technical staff report, the Police Department had not offered comments on this application.
  - h. **Prince George’s County Fire/EMS Department**—At the time of the writing of this technical staff report, the Fire/EMS Department had not offered comments on this application.
  - i. **Prince George’s County Health Department**—In a memorandum dated March 24, 2025, the Health Department provided recommendations on mitigating air, noise, and dust pollution during the construction phase of this project.
  - j. **Washington Suburban Sanitary Commission (WSSC)**—At the time of the writing of this technical staff report, WSSC had not offered comments on the subject application.
  - k. **Public Utilities**—At the time of the writing of this technical staff report, the public utility companies did not offer comments on this application.
  - l. **The City of Bowie**—The Bowie City Council voted to recommend approval of DSP-23026 on May 5, 2025. The City’s findings and recommendations are included in the backup for this application.
13. **Community Feedback:** As of the writing of this technical staff report, staff did not receive any inquiries from the community regarding the subject DSP.
  14. Based on the foregoing analysis, and as required by Section 27-285(b)(1) of the prior Zoning Ordinance, the DSP, if approved with the proposed conditions below, represents a most reasonable alternative for satisfying the site design guidelines of prior Subtitle 27, Part 3, Division 9, of the County Code, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use.
  15. Section 27-285(b)(2) of the prior Zoning Ordinance is not applicable because there is no conceptual site plan.
  16. Section 27-285(b)(3) of the prior Zoning Ordinance does not apply to this DSP because it is not a DSP for infrastructure.
  17. As required by Section 27-285(b)(4) of the prior Zoning Ordinance, the Planning Board may approve a DSP if it finds that the REF have been preserved and/or restored in a natural state to the fullest extent possible, in accordance with the requirement of

Section 24-130(b)(5). The on-site REF include streams, stream buffers, wetlands, wetland buffers, 100-year floodplain, and steep slopes.

Impacts to the PMA were previously approved with PPS 4-04047 for a SWM outfall, a sewer connection, and for construction of a portion of the proposed parking lot. This DSP is reliant on that prior approval. No additional impacts were included with this application. In conformance with Section 24-130(b)(5), the REF on the subject property have been preserved and/or restored to the fullest extent possible based on the limits of disturbance shown on the TCP2.

## RECOMMENDATION

Based upon the foregoing evaluation and analysis, the Urban Design staff recommend that the Planning Board adopt the findings of this report and APPROVE Detailed Site Plan DSP-23026 and Type 2 Tree Conservation Plan TCP2-089-05-01, including a variance to Section 25-122(b)(1)(G), for Ourisman of Bowie, subject to the following conditions:

1. Prior to certificate approval of the detailed site plan, the following revisions shall be made, or information should be provided:
  - a. Revise General Note 6 to correct the proposed gross floor area.
  - b. Revise the landscape plans to demonstrate conformance with Section 4.3(c)(2)(D) of the 2010 *Prince George's County Landscape Manual*, regarding minimum tree planting area requirements.
  - c. Revise the tree canopy coverage schedule to accurately reflect the net tract area, per Section 25-128 of the Prince George's County Tree Canopy Coverage Ordinance, and demonstrate conformance to the requirement.
  - d. Identify right-of-way along the property frontage of US 301 (Robert Crain Highway).
  - e. Provide a site detail for the "Ourisman" sign identified on Sheet 12, and provide a complete signage schedule.
  - f. Provide site details for the curb and gutter and tapering along US 301 (Robert Crain Highway) and identify these details on the site plan.
  - g. Revise landscape Schedule 4.7 for Bufferyard 1 to correctly identify the adjacent use.
  - h. Show and label denial of access to US 301 (Robert Crain Highway) for Lot 1, except at the location of the shared vehicular access easement.
  - i. Add a note to the coversheet indicating that any off-site improvements to Lot 3 shall require proper permits and approvals per the Prince George's County Code.
2. Prior to approval of any building permits for Lot 1, a final plat shall be approved by the Prince George's County Planning Board, in accordance with Section 24-111 of the prior Prince George's County Subdivision Regulations, to authorize the use of a private easement

for vehicular access pursuant to Section 24-128(b)(9) of the prior Subdivision Regulations. The final plat shall reflect the location of the shared vehicular access easement, with liber/folio reference, and denial of access in conformance with the detailed site plan.

3. The applicant shall retain a consultant archeologist to perform monitoring while grading occurs. The archeologist shall have the authority to temporarily halt work if significant subsurface features are encountered and consult with Historic Preservation Section staff to determine appropriate mitigation measures before work resumes.
4. Prior to signature approval of the detailed site plan, the Type 2 tree conservation plan (TCP2) shall be revised as follows:
  - a. Add the following note under the specimen tree table:

“NOTE: This plan is in accordance with the following variance from the strict requirements of Subtitle 25 approved by the Planning Board on (ADD DATE) with DSP-23026 for the removal of two specimen trees (Section 25-122(b)(1)(G)) specifically Specimen Trees ST-1377 and ST-1380.”
  - b. Provide a 2-inch by 2-inch outlined blank square on each page for an approval block (to be inserted by staff at time of approval).
  - c. Provide a TCP2 approval history table, which can be found on The Maryland-National Capital Park and Planning Commission’s website.
  - d. Correct the woodland conservation worksheet to reflect the correct amount of woodlands on-site, as of June 2022.
  - e. Add the specimen tree removal worksheet to the plan.
  - f. Correct the Site Statistic Table to reflect the correct amount of existing woodlands, as of June 2022.
  - g. Label the specimen trees.
  - h. Show sewer connection.
  - i. Show all proposed grades.
  - j. Label top and bottom elevation of the retaining walls.
  - k. Add the signed owner’s awareness certificate.
5. Prior to issuance of any permits impacting 100-year floodplain, wetlands, wetland buffers, streams, or waters of the United States, the applicant shall submit copies of all federal and state wetland permits, evidence that approval conditions have been complied with, and associated mitigation plans.

# OURISMAN OF BOWIE

Detailed Site Plan

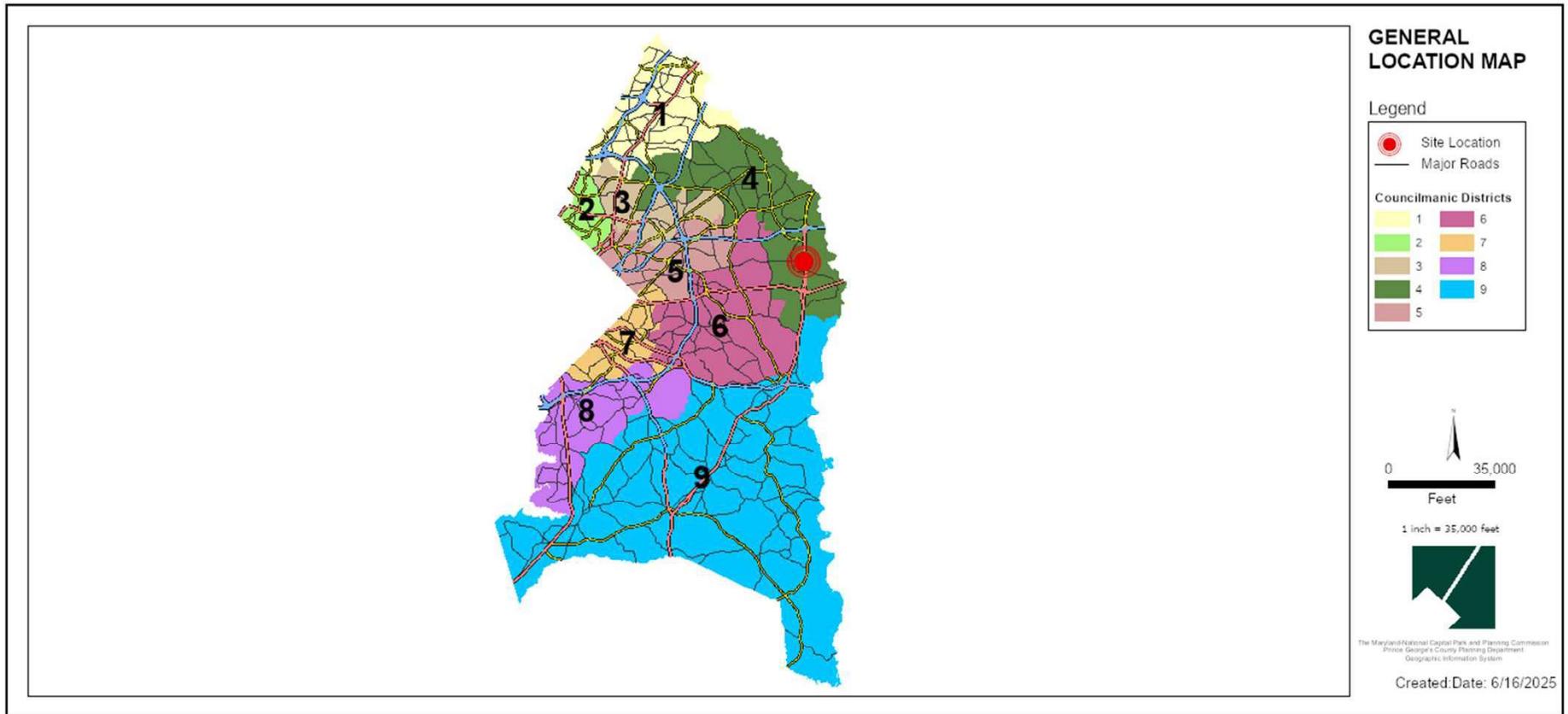
TCP2-089-05-01

Staff Recommendation: APPROVAL with conditions

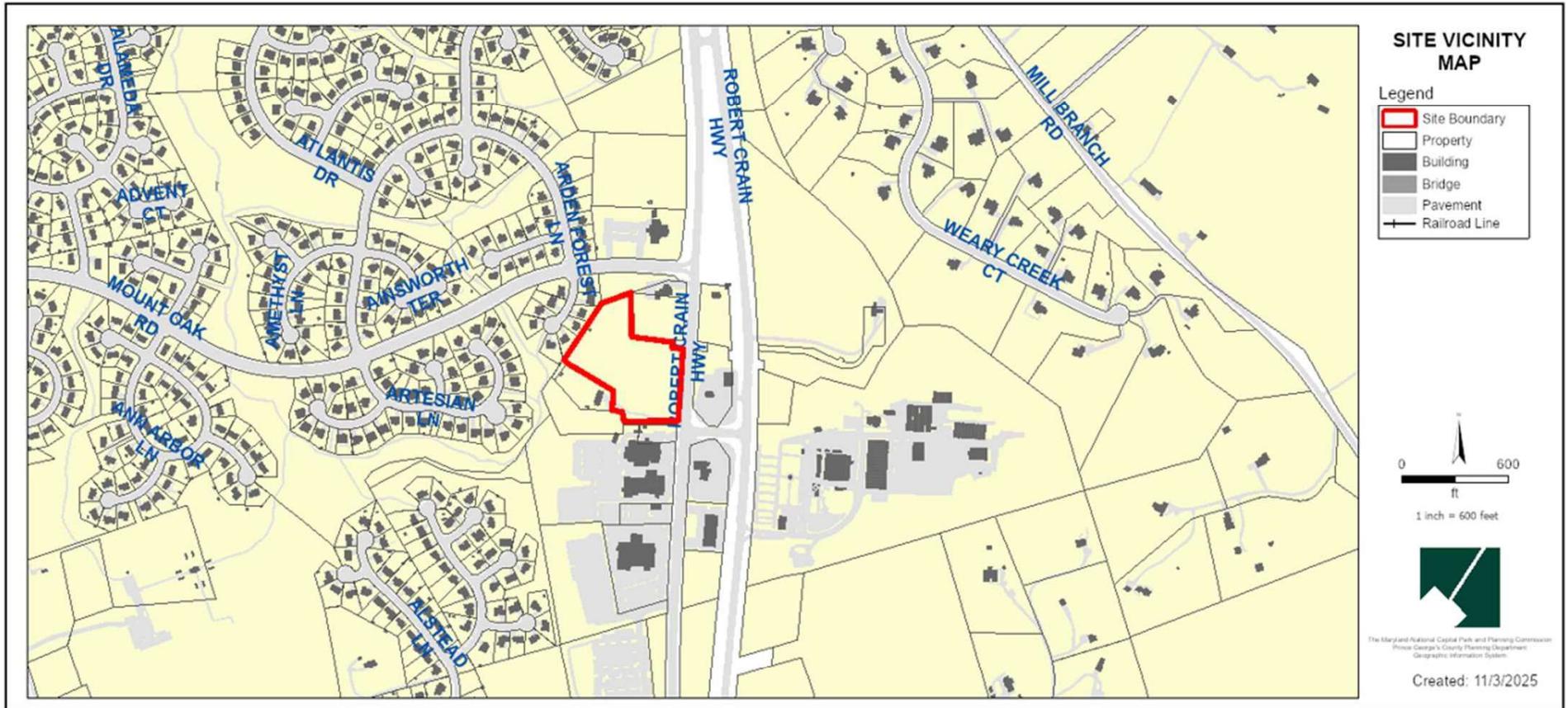


# GENERAL LOCATION MAP

Council District: 04  
Planning Area: 71B



# SITE VICINITY MAP

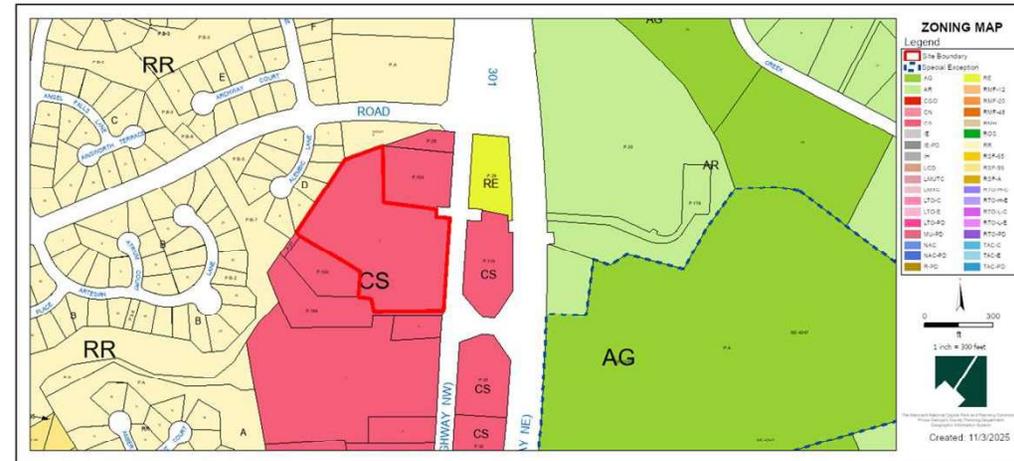


# ZONING MAP (PRIOR AND CURRENT)

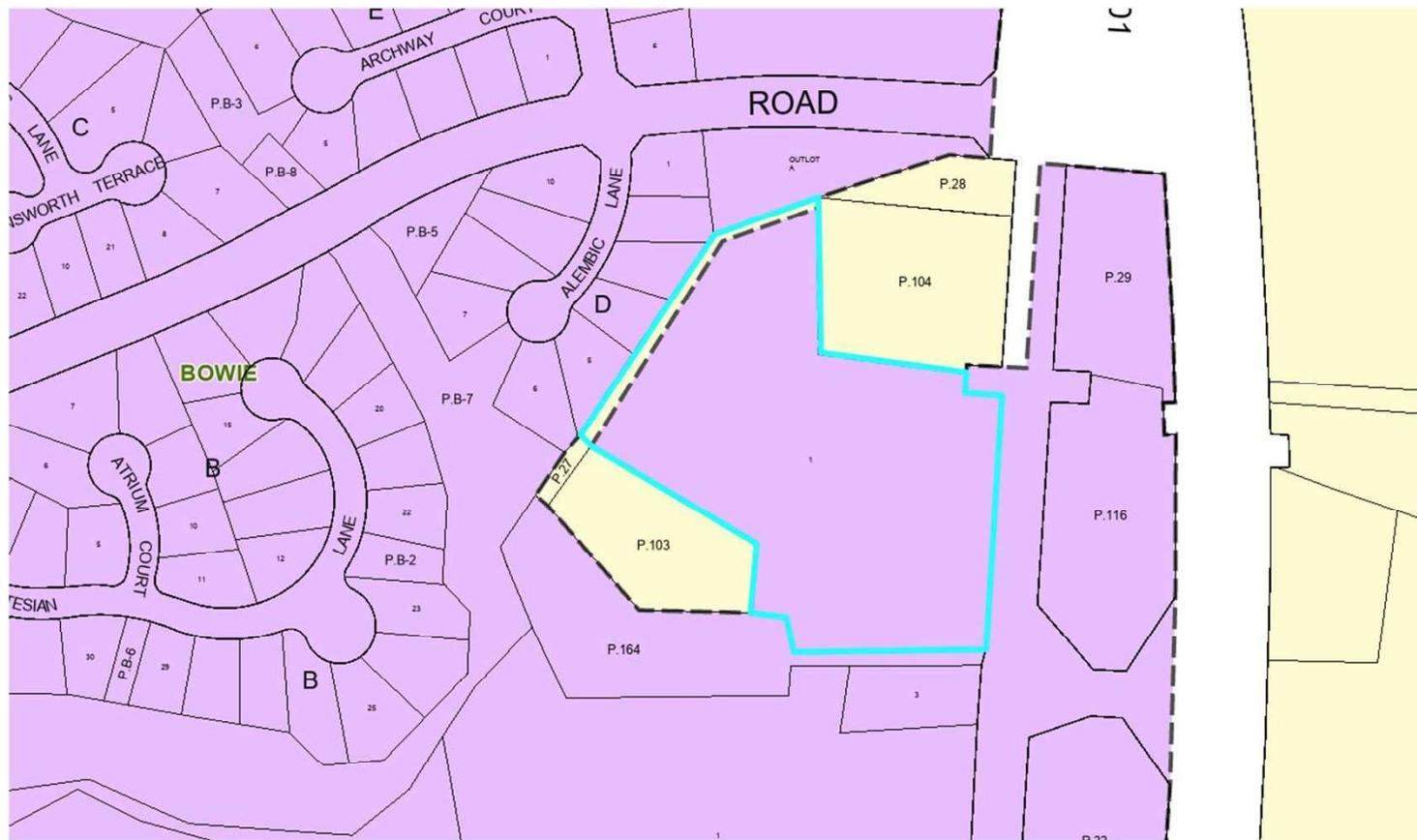
Prior Zoning Map: C-M



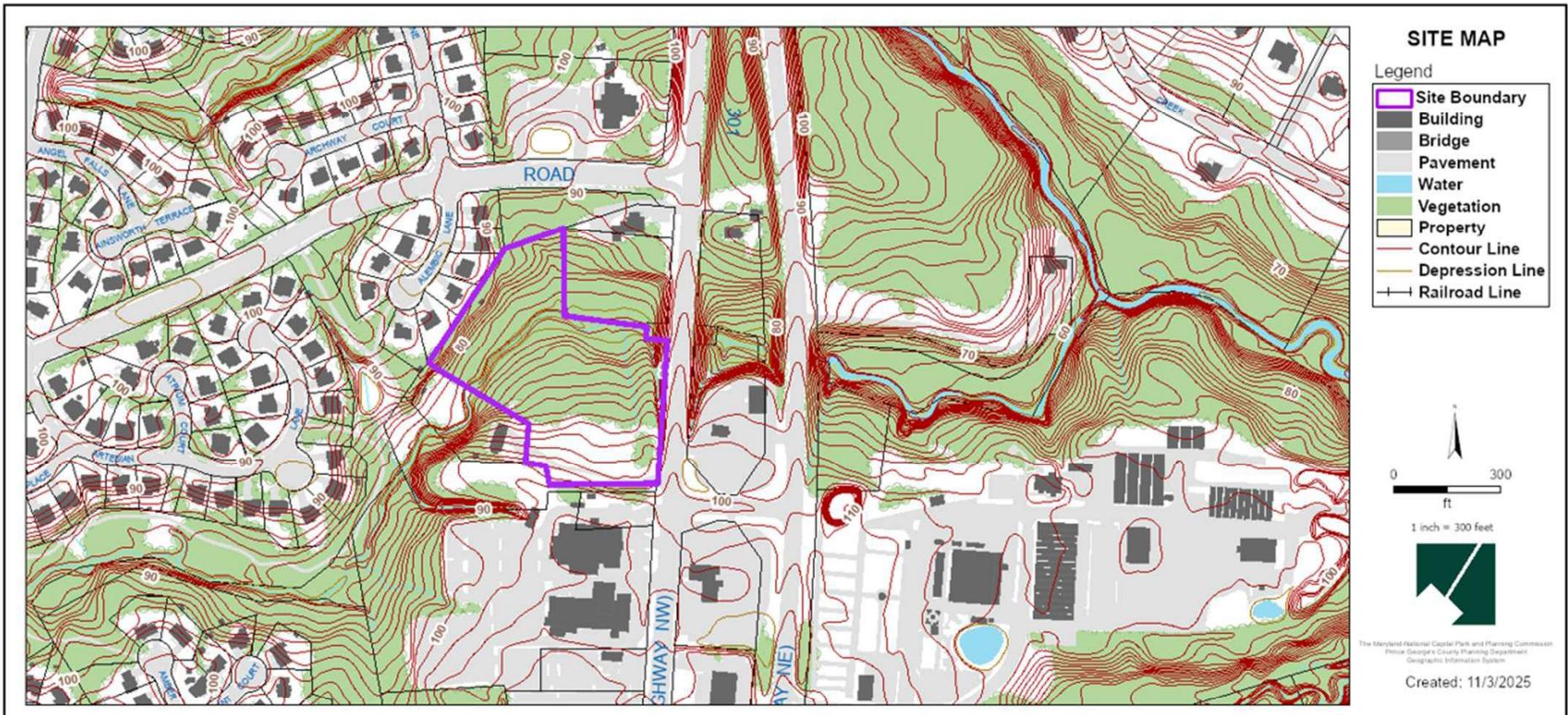
Current Zoning Map: CS



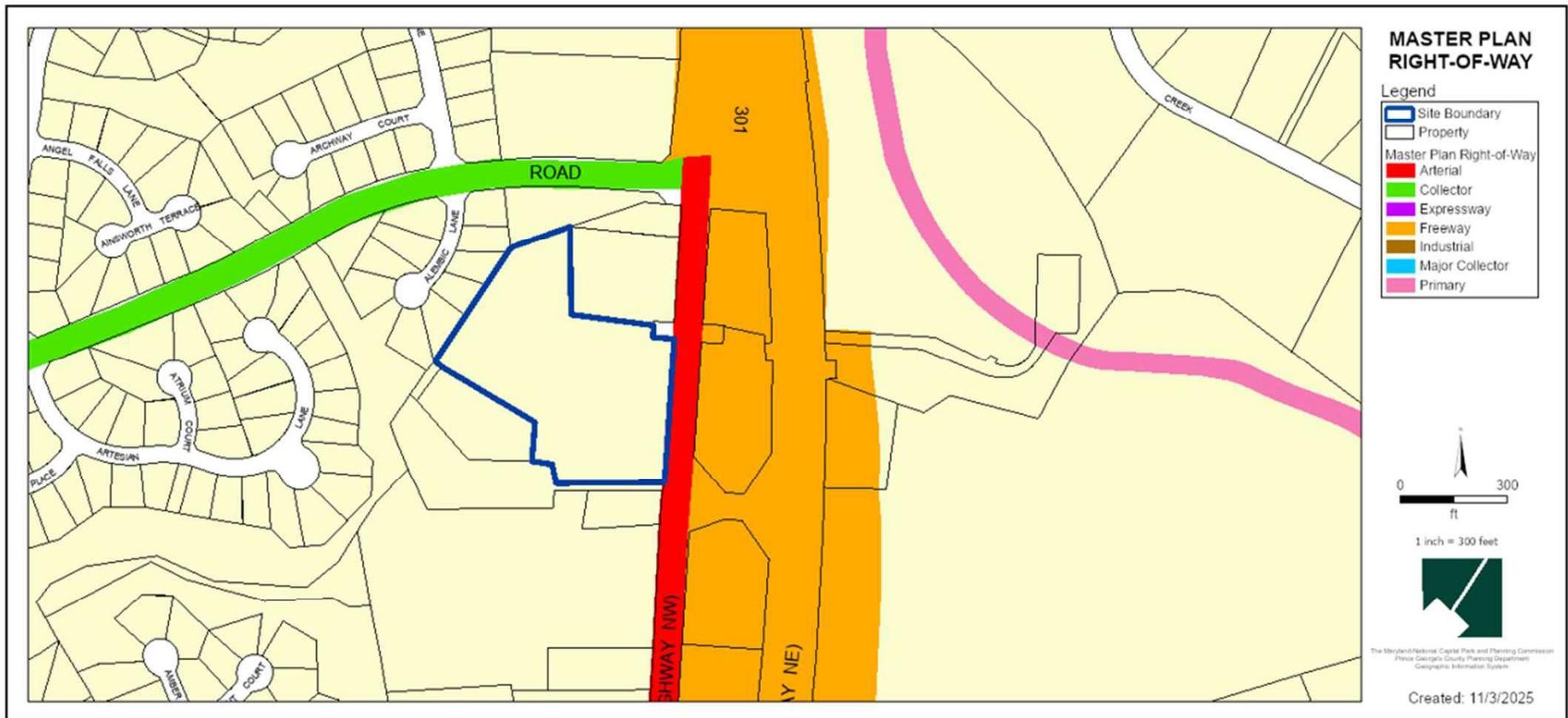
# MUNICIPAL BOUNDARY (BOWIE)



# SITE MAP



# MASTER PLAN RIGHT-OF-WAY MAP



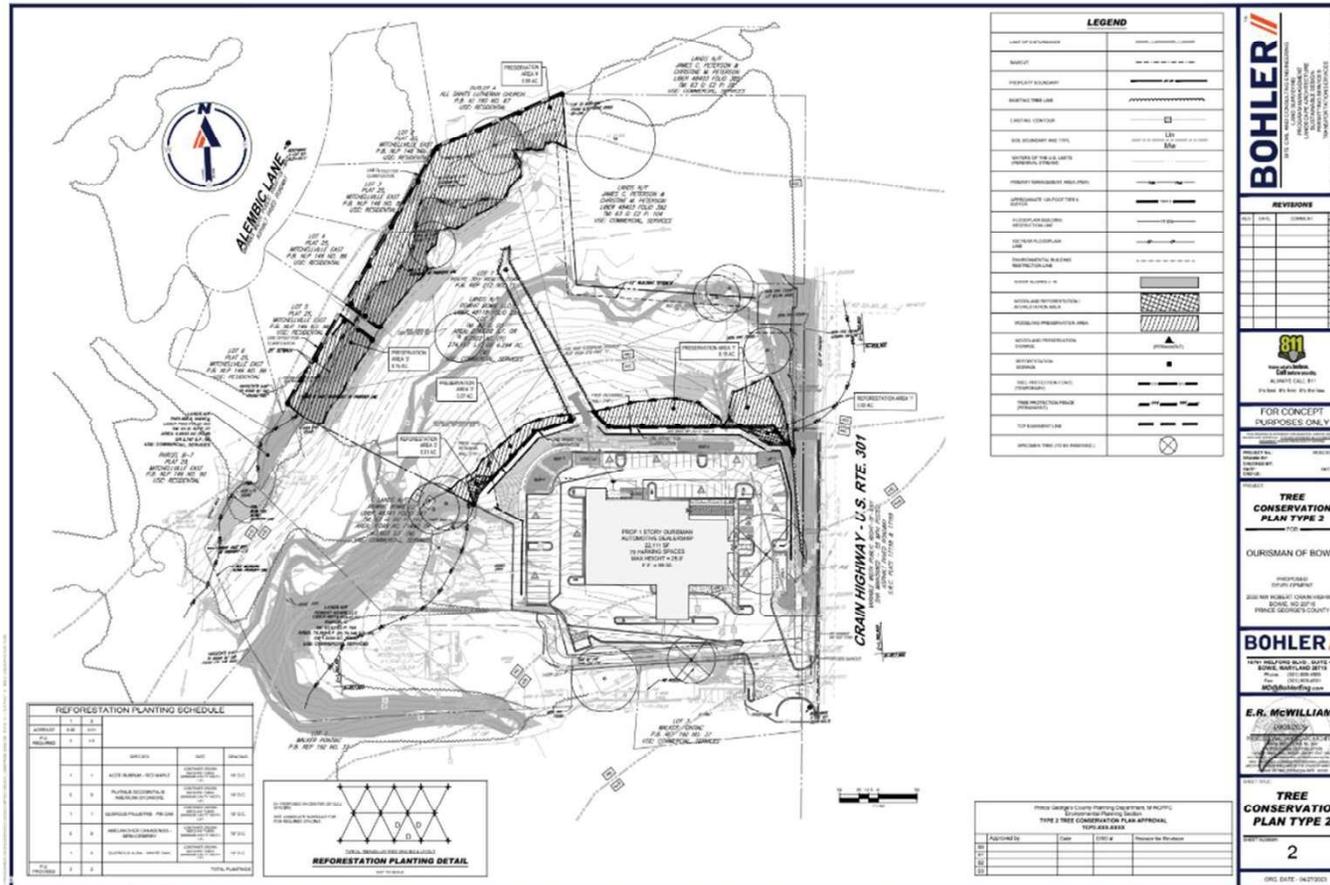
# BIRD'S-EYE VIEW WITH APPROXIMATE SITE BOUNDARY OUTLINED



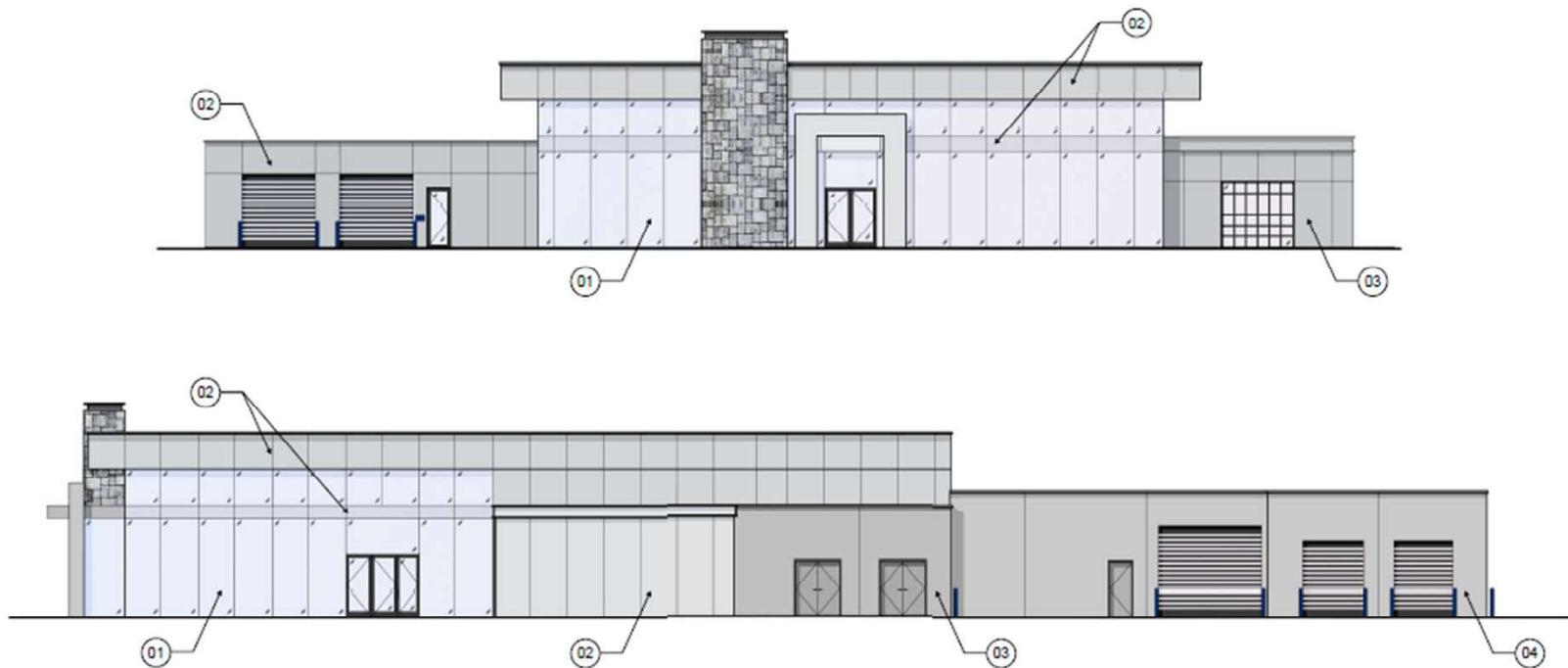




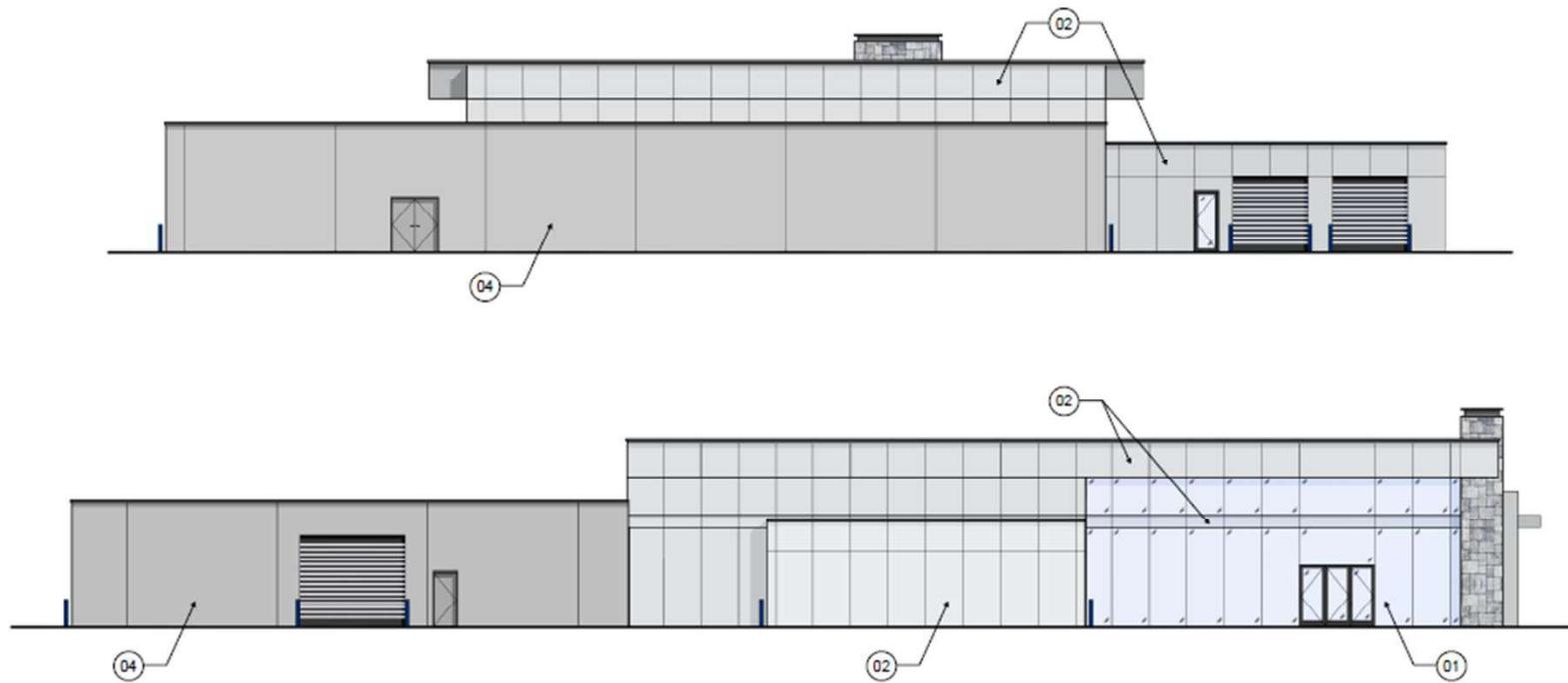
# TYPE II TREE CONSERVATION PLAN



# ELEVATIONS – EAST AND NORTH



# ELEVATIONS – WEST AND SOUTH



# RENDERED ELEVATION





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# STAFF RECOMMENDATION

## **APPROVAL** with conditions

- DSP-23026
- TCP2-089-05-01

## **Issues:**

- None

## **Applicant Required Mailings:**

- Informational Mailing: 01/17/2024
- Acceptance Mailing: 03/17/2025

LAW OFFICES  
**SHIPLEY & HORNE, P.A.**

Russell W. Shipley  
Arthur J. Horne, Jr.\*  
Dennis Whitley, III\*  
Robert J. Antonetti, Jr.

1101 Mercantile Lane, Suite 240  
Largo, Maryland 20774  
Telephone: (301) 925-1800  
Facsimile: (301) 925-1803  
www.shpa.com

Bradley S. Farrar  
L. Paul Jackson, II\*

\* Also admitted in the District of Columbia

October 11, 2024

**VIA ELECTRONIC MAIL**

Ms. Hyojung Garland, Acting Supervisor  
Urban Design Section  
Development Review Division  
Prince George's County Planning Department  
1616 McCormick Drive, Largo, MD 20774

**RE: Detailed Site Plan, DSP-23026  
Ourisman Dealership of Bowie  
STATEMENT OF JUSTIFICATION**

Dear Hyojung:

On behalf of our client, Rowhit Bowie LLC, (the "Applicant"), please accept this Statement of Justification in support of Detailed Site Plan application, DSP-23026. Specifically, the proposed application requests approval of a detailed site plan for a one-story, 22,111 square-foot vehicle sales and service dealership in the CS (Commercial, Service) Zone. The subject Property is known as Lot 1, consists of approximately 6.29 acres, and is located in the Route 301 Realty Corp. Subdivision, (The "Property"). The Route 301 Realty Corp. Subdivision was recorded in Land Records on April 26, 2006 as REP 212-11.

Preliminary Plan of Subdivision, 4-04047, and Type I Tree Conservation Plan, TCPI/32/04, were previously approved for the subject Property on September 9, 2004, for a 29,800 square-foot auto dealership in the C-M (Commercial Miscellaneous) Zone. (PGCPB No. 04-177). A prior Detailed Site Plan, DSP-05013, Type-II Tree Conservation Plan, TCPII-089-05, and Alternative Compliance Application, AC-05015, were then approved for the Property on July 28, 2005, for a 29,700 square-foot automotive dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 05-173). Finally, on August 24, 2006, DSP-05013-01, was approved by the Planning Director to add full basement parking to a 29,675 square-foot automobile dealership in lieu of previously approved rooftop parking, add a sidewalk to connect to the adjacent property, and to relocate a dumpster next to the building. As further outlined below, the Applicant elects to have DSP-23026 reviewed utilizing the applicable provisions of the prior Zoning Ordinance pursuant to Section 27-1904 of the current Zoning Ordinance. The Applicant's current proposal for a 22,111 square-foot vehicle sales and service dealership in the C-M (Commercial-Miscellaneous) Zone is in substantial harmony with these prior approvals.

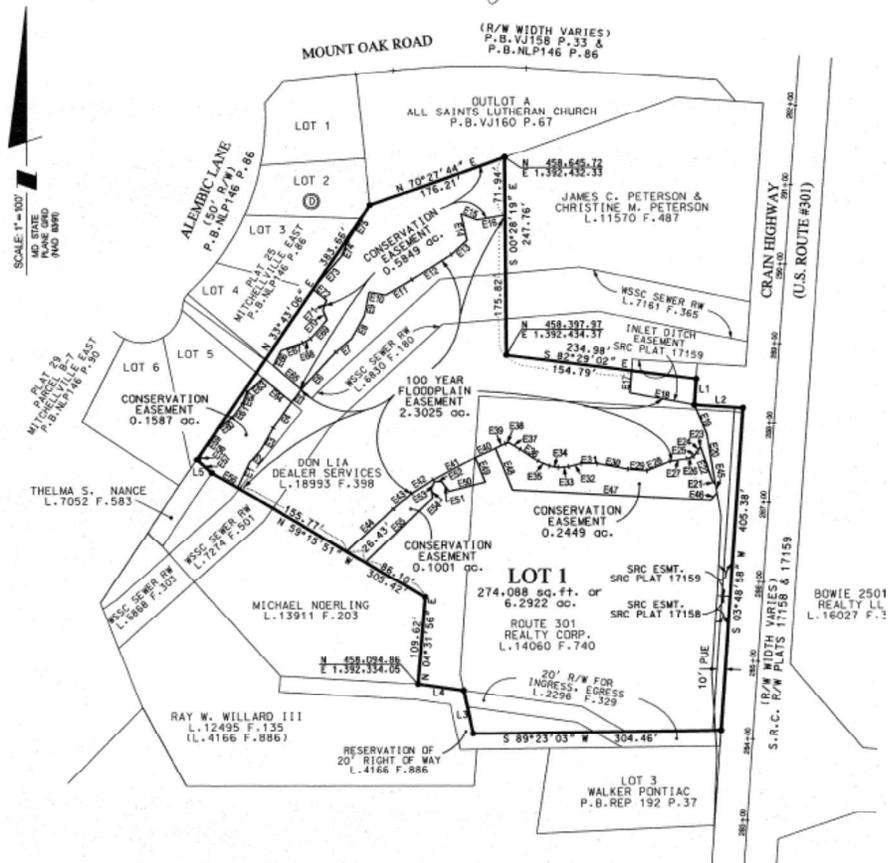
**Prior Zoning Map**



**Current Zoning Map**



**Final Plat of Subdivision, REP-212-11**



**ELECTION TO UTILIZE PRIOR C-M ZONING PROCEDURES (Section 27-1904)**

On April 1, 2022, the approved Countywide Sectional Map Amendment (“CMA”) and the updated Prince George’s County Zoning Ordinance (“New Zoning Ordinance”) became effective and rezoned the subject Property from the C-M (Commercial Miscellaneous) Zone, to the CS (Commercial, Service) Zone. Notwithstanding, the Applicant elects to have DSP-23026 reviewed utilizing the applicable provisions of the prior Zoning Ordinance pursuant to Section 27-1904 of the current Zoning Ordinance which states the following:

**27-1904. Procedures**

***In order to proceed with development under the prior Zoning Ordinance, the following procedures shall apply:***

- (a) If the development proposal will require an evidentiary hearing before the Planning Board, the applicant shall schedule and participate in a pre-application conference.***

**RESPONSE:** A Pre-Application Meeting with the Planning Staff was held for the subject application, DSP-23026, on October 9, 2023.

- (b) The applicant shall provide a statement of justification which shall explain why the Applicant has elected not to develop a specific property pursuant to the provisions of this Zoning Ordinance.***

**RESPONSE:** This submitted statement of justification is in compliance with the above requirement. Preliminary Plan of Subdivision, 4-04047, was previously approved for the subject Property on September 9, 2004, for a 29,800 square-foot auto dealership in the C-M (Commercial Miscellaneous) Zone, (PGCPB No. 04-177). A prior Detailed Site Plan, DSP-05013, and Alternative Compliance Application, AC-05015, were then approved for the Property on July 28, 2005, for a 29,700 square-foot automotive dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 05-173). Finally, on August 24, 2006, DSP-05013-01, was approved by the Planning Director to add full basement parking to a 29,675 square-foot automobile dealership in lieu of previously approved rooftop parking, add a sidewalk to connect to the adjacent property, and to relocate a dumpster next to the building.

The Applicant’s current proposal for a 22,111 square-foot vehicle sales and service dealership is in substantial harmony with all these prior approvals, and as a result, should continue to be reviewed utilizing the provisions of the Zoning Ordinance that existed prior to April 1, 2022.

- (c) The Planning Director shall submit quarterly reports to the District Council as to the development applications proceeding under the prior Ordinance.***

**RESPONSE:** The Planning Director will need to comply with the above requirement.

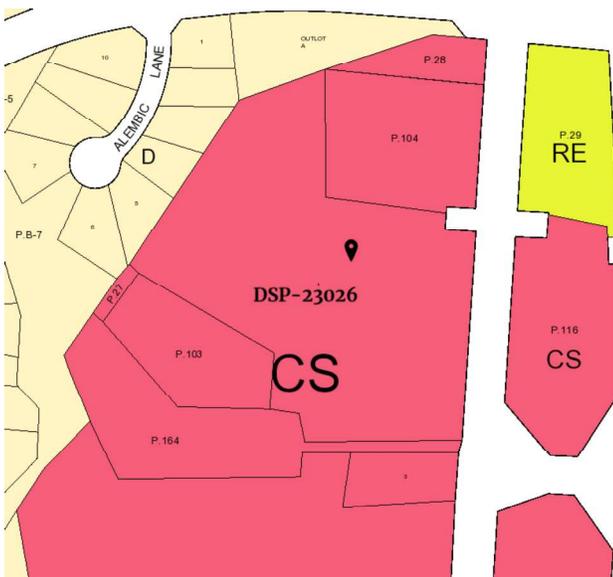
**I. DESCRIPTION OF SUBJECT PROPERTY**

The subject Property consists of approximately 6.29 acres, and has a premise address of 2550 Northwest Robert Crain Highway. The Property is further known as Lot 1 within the Route 301 Realty Corp. Subdivision. The Route 301 Realty Corp. Subdivision was recorded in Land Records on April 26, 2006 as REP 212-11. Although the property is currently located in the CS (Commercial, Service) Zone, the Applicant is requesting that the subject application be reviewed utilizing the C-M (Commercial, Miscellaneous) Zone provisions of the Zoning Ordinance that existed prior to April 1, 2022.

The northern portion of the property contains extensive environmentally-sensitive areas that are located within a recorded conservation easement, as well areas of 100-year floodplain that are located in a recorded floodplain easement. Consistent with the prior approved PPS and DSP applications for the Property, these sensitive areas will continue to be preserved with the subject application.

The Property is surrounded by the following uses:

- To the North** – Undeveloped wooded parcel in the CS Zone, (Parcel 104)
- To the South** – Parcels 103 & 164, Owned by the Applicant and used for automobile storage in the CS Zone
- To the East** – Robert S. Crain Highway (US Route 301), (Arterial Roadway, A-61)
- To the West** – Detached Single-Family Dwelling Units within the Mitchellville East Subdivision



## **II. ZONING AND DEVELOPMENT HISTORY**

**April 13, 2004** - Stormwater Management Concept Plan, 8254-2004-0, is approved by the Department of Environmental Resources (DER).

**September 9, 2004** - Preliminary Plan of Subdivision, PPS 4-04047, and Type-I Tree Conservation Plan, TCPI/32/04, are approved by the Planning Board for a 29,800 square-foot Auto Dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 04-177).

**July 28, 2005** - Detailed Site Plan, DSP-05013, Type-II Tree Conservation Plan, TCPII/89/05, and Alternative Compliance Application, AC-05015, are approved by the Planning Board for a 29,700 square-foot automotive dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 05-173).

**October 5, 2005** - The District Council waives their right to review Detailed Site Plan, DSP-05013, rendering the Planning Board's prior decision as final.

**April 26, 2006** - Final Plat of Subdivision, REP 212-11, for the Route 301 Realty Corp. Subdivision is recorded in Land Records.

**August 24, 2006** - Detailed Site Plan, DSP-05013-01 is approved by the Planning Director to add a full basement parking to a 29,675 square-foot automobile dealership in lieu of previously approved rooftop parking, add a sidewalk to connect to the adjacent property and to relocate a dumpster next to the building.

**October 9, 2023** - A Pre-Application Meeting with the Planning Staff is held for the subject application, DSP-23026.

**December 26, 2023** - Stormwater Management Concept Plan, 02-1223-204NE14, is approved by the City of Bowie.

## **III. OVERVIEW**

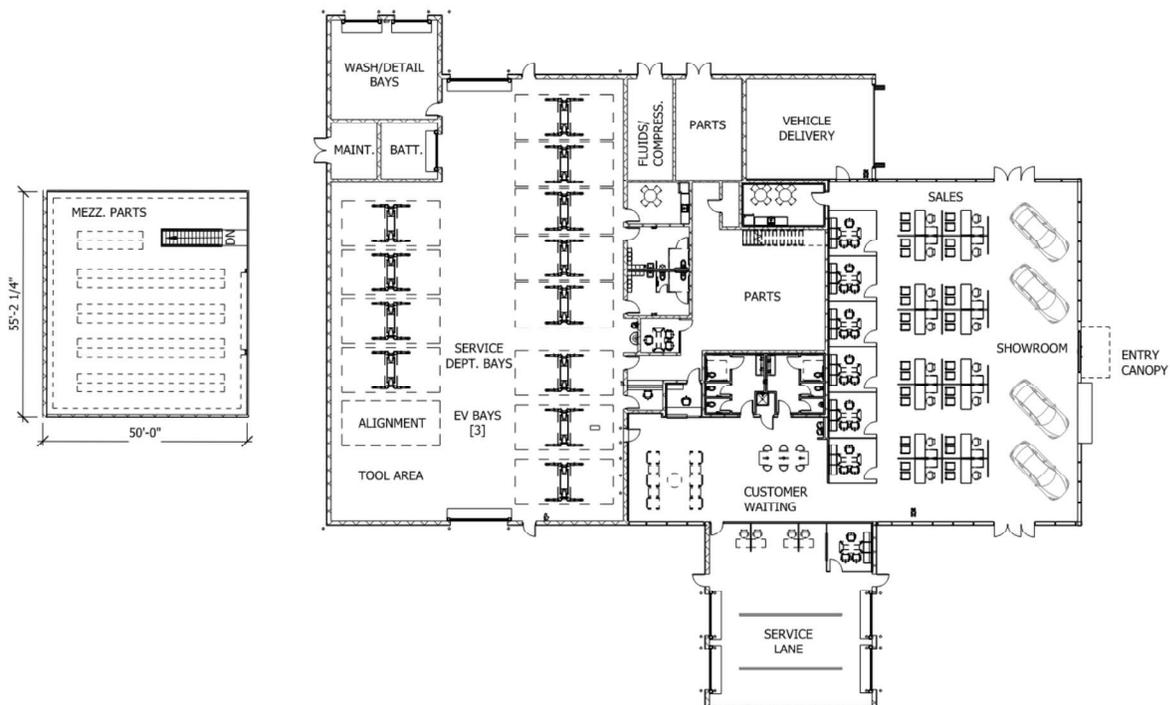
The subject Property consists The proposed application requests the approval of a detailed site plan application for a 22,111 square-foot vehicle sales and service dealership utilizing the C-M (Commercial, Miscellaneous) Zone provisions of the Zoning Ordinance that existed prior to April 1, 2022.

The front elevation for the proposed one-story building will primarily consist of clear insulated glass with a Tricorn Black aluminum composite metal panel along the service reception area. The proposed building will be 25-feet high at its highest level with a rooftop skylight system. The top of the building will be trimmed with Basil Gray aluminum composite metal banding, with building mounted signage along the left side of the building, and additional mounted signage along the right of the building. A "Service" sign will be located along the left side of the building to identify the

service department area. All of the building signage will be individual letters, figures/designs that are mounted directly on the architectural face of the building with no other decorative wall work. A front elevation of the proposed building (facing US Route 301) is provided below:



The proposed building will include a service department with a service reception area, customer lounge, sales and manager's offices, parts storage, parts receiving area, retail parts area, men's and woman's bathrooms (2), an indoor sales display area, two employee break rooms, various utility, equipment and storage rooms, and a vehicle delivery area. The service department will include 15 service bays that includes three electric vehicle (EV) lab areas, service lifts, an alignment bay, as well as wash and vehicle detail bays. All rooftop mechanical equipment will be completely screened. The Applicant is determined to move its new buildings into the future with sustainable and green design initiatives. As detailed further in this statement, the proposed building has been designed to implement many of the latest green design techniques and building materials.



The proposed building will meet all the required setbacks in the Zoning Ordinance and *Prince George’s County Landscape Manual*. The parking compound will include 79 parking spaces and two (2) loading spaces to serve the proposed dealership. An outdoor vehicle display area and one freestanding sign is also shown on the submitted DSP.

The stormwater management concept plan approved by the City of Bowie, (Approval No. 02-1223-204NE14), shows that surface runoff will be handled by various storm drain inlets, micro-bioretenion areas, and underground storage chambers with filtering and pre-treatment devices. These structures will ensure that all surface runoff will be properly captured, treated and released in accordance with Best Management Practices and Environmental Site Design (ESD) to ensure the protection of natural resources and prevent downstream flooding.

**IV. DEVELOPMENT DATA SUMMARY**

1. **Development Data Summary** - The following information relates to the subject DSP application:

<b>DEVELOPMENT DATA SUMMARY</b>		
	<b>Approved Applications (PPS 4-04047 &amp; DSP-05013-01)</b>	<b>Proposed (DSP-23026)</b>
<b>Zones:</b>	C-M	CS (Former C-M Zone regulations apply)
<b>Uses:</b>	Automobile Dealership  PPS 4-04047 - (29,800 sf) DSP-05013-01 - (29,675 sf)	Automobile Dealership  (22,111 sf)
<b>Total Gross Acreage:</b>	PPS 4-04047 - (6.29 acres) DSP-05013-01 - (6.44 acres)	6.29 acres
<b>100-Year Floodplain</b>	PPS 4-04047 - (2.49 acres) DSP-05013-01 - (2.46 acres)	2.49 acres

<b>Net Acreage:</b>	PPS 4-04047 - (3.8 acres) DSP-05013-01 - (3.98 acres)	3.8 acres
<b>Alternative Compliance:</b>	PPS 4-04047 - (No) DSP-05013-01 - (Yes - Landscape Buffer along Southern Property Line)	No
<b>Variance:</b>	PPS 4-04047 - (No) DSP-05013-01 - (No)	No

**PARKING SUMMARY**

<b>Parking Requirements</b>	<b><u>Required</u></b>	<b><u>Provided</u></b>
<b>Automobile Dealership:</b>	<p><b><u>Indoor Sales</u></b> 15,464 sf @ 1 space per 500 sf = <b>31</b> spaces</p> <p><b><u>Service Area</u></b> 15 service bays @ 3 spaces per bay = <b>45</b> spaces</p> <p><b><u>Outdoor Display Area</u></b> 1,032 sf @ 1 space per 1,000 sf of gross outdoor display area = <b>2</b> spaces</p> <p>-----  <b>Total Parking Spaces Required</b> = <b>78</b> spaces</p>	<p><b><u>Total Parking Spaces Provided</u></b> = <b>79</b> spaces</p> <p>(To include <b>58</b> standard spaces (at 9.5' x 19'), <b>17</b> compact spaces (at 8'x 16.5'), and <b>4</b> van accessible spaces for the physically handicapped (at 16' x 19'))</p>

**LOADING SUMMARY**

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<b>Loading Requirements</b>	<b><u>Required</u></b>	<b><u>Provided</u></b>
<b>Automobile Dealership:</b>	<p><b><u>Retail sales and service (per store)</u></b>                      Less than 2,000 sf - <b>None Required</b>                      2,000 to 10,000 sf of GFA - <b>1 space</b>                      10,000 to 100,000 sq ft of GFA -  <b>plus 1 additional space</b>                      Each additional 100,000 sq ft of GFA                      (or fraction)  <b>plus 1 additional space</b></p> <hr/> <p>Total GFA Proposed = 22,111 sf                      -----  <b>Total Loading Spaces Required</b>                      = <b>2 spaces</b> (at 12' x 33')</p>	<p><b>Total Loading Spaces                      Provided =</b>   <b>2 spaces</b> at 15' x 33'</p>

**V. MASTER PLAN & GENERAL PLAN CONFORMANCE:**

**2022 Approved Master Plan for Bowie- Mitchellville and Vicinity** The instant DSP is for an automobile dealership in the CS Zone, (being reviewed in accordance with the requirements of the C-M Zone). The 2022 Master Plan recommends a “Commercial” future land use for the subject Property.

Preliminary Plan of Subdivision, PPS 4-04047, was previously approved for the subject Property on September 9, 2004, for a 29,800 square-foot auto dealership in the C-M (Commercial Miscellaneous) Zone. (PGCPB No. 04-177). A prior Detailed Site Plan, DSP-05013, and Alternative Compliance Application, AC-05015, were then approved for the Property on July 28, 2005, for a 29,700 square-foot new automotive dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 05-173). Finally, on August 24, 2006, DSP-05013-01, was approved by the Planning Director to add full basement parking to a 29,675 square-foot automobile dealership in lieu of previously approved rooftop parking, add a sidewalk to connect to the adjacent property, and to relocate a dumpster next to the building.

The Applicant’s current proposal for a 22,111 square-foot vehicle sales and service dealership in the CS (Commercial, Service) Zone is in substantial harmony with these prior approvals, and consistent with the “Commercial” land use recommendations within the *2022 Approved Master Plan for Bowie- Mitchellville and Vicinity*.

**Plan Prince George’s 2035 Approved General Plan** – According to the approved 2035 General

Plan, the site is located within an Established Communities Growth Area, and is recommended for a “Commercial” Generalized Land Use consistent with the *2022 Approved Master Plan for Bowie-Mitchellville and Vicinity*.

The Applicant’s current proposal for a 22,111 square-foot vehicle sales and service dealership in the C-M (Commercial-Miscellaneous) Zone is in substantial harmony with all prior entitlement approvals, and is consistent with the “Commercial” land use recommendations within the *Plan Prince George’s 2035 Approved General Plan*.

**2009 Master Plan of Transportation (MPOT)** – There are no identified road improvements or right-of-way dedications required pursuant to recommendations contained in this plan or the *2006 Approved Master Plan for Bowie and Vicinity and Sectional Map Amendment for Planning Areas 71A, 71B, 74A and 74B* (Bowie and Vicinity Master Plan and SMA) that impact the proposed Detailed Site Plan.

**2010 Water Resources Functional Master Plan** – The instant DSP been reviewed against the recommendations in this plan and have been found to be consistent with the policies therein because sensitive environmental features are preserved to the greatest extend possible, and the Applicant intends to use environmental site design (ESD) concepts (as required by its approved stormwater management concept plan) in developing the Property as recommended by the 2010 Plan. The Property is identified as being within the Planned or Existing Community System Sewer Envelope, and having existing water and sewer category classifications of W-3 and S-3. As a result, adequate public water and sewer service currently exists to serve the subject Property.

**VI. RELATIONSHIP TO REQUIREMENTS IN THE ZONING ORDINANCE:**

**Section 27-281: - Purposes of Detailed Site Plans:**

***(b) General purposes:***

***(1) The general purposes of Detailed Site Plans are:***

***(A) To provide for development in accordance with the principles for the orderly, planned, efficient and economical development contained in the General Plan, Master Plan, or other approved plan;***

**RESPONSE:** The subject Property will be developed in accordance with the relevant land use policy recommendations contained in the 2035 General Plan and 2022 Master Plan as described in Section V above.

***(B) To help fulfill the purposes of the zone in which the land is located;***

**RESPONSE:** On April 1, 2022, the approved Countywide Sectional Map Amendment (“CMA”) and the updated Prince George’s County Zoning Ordinance (“New Zoning Ordinance”) became effective and rezoned the subject Property from the C-M (Commercial Miscellaneous) Zone, to the CS (Commercial, Service) Zone. Notwithstanding, the Applicant elects to have DSP-23026 reviewed utilizing the applicable provisions of the C-M Zone and the prior Zoning Ordinance pursuant to Section 27-1904 of the current Zoning Ordinance.

The purposes of the C-M Zone are found in Section 27-459(a) and include the following:

***Section 27-459. C-M Zone (Commercial Miscellaneous).***

***(a) Purposes.***

***(1) The purposes of the C-M Zone are:***

***(A) To provide locations for miscellaneous commercial uses which may be disruptive to the harmonious development, compactness, and homogeneity of retail shopping areas;***

***(B) To provide these locations, where possible, on nonresidential streets; and***

***(C) To provide concentrations of these uses which are relatively far apart.***

**RESPONSE:** The subject DSP application has frontage on Master Plan Arterial Roadway, (A-61, US Route 301), and is therefore, not located along a residential street. The proposed automobile dealership will abut property to the south that is owned by the Applicant and is (or will be) used for automotive purposes, and the property to the north is an undeveloped wooded parcel. The abutting properties to the north and south are located in same zoning category as the subject Property. The site is located in area that contains several, prior approved automotive dealerships along this section of southbound US Route 301. These abutting and nearby automotive dealerships were found to be in substantial harmony with the above requirements at that time of their entitlement applications.

***(b) Landscaping and screening.***

***(1) Landscaping and screening shall be provided in accordance with Section 27-450.***

**RESPONSE:** Section 27-450 of the Zoning Ordinance states the following:

***Section 27-450. - Landscaping, screening, and buffering.***

***Landscaping, screening, and buffering of all development in the Commercial Zones shall be in accordance with the provisions of the Landscape Manual.***

**RESPONSE:** The submitted DSP has been designed to be in full compliance with requirements of the Prince George’s County *Landscape Manual*, the Woodland and Wildlife Habitat Conservation requirements in Section 25-121, and the tree canopy coverage requirements in Section 25-128.

*(c) Uses.*

**(1) The uses allowed in the C-M Zone are as provided for in Table of Uses I (Division 3 of this Part).**

**RESPONSE:** The submitted DSP proposes an automotive dealership in the C-M Zone. The Use Tables in Section 27-461(b) state that the following concerning the proposed use:

USE	ZONE					
	C-O	C-A	C-S-C	C-W	C-M	C-R-C
Vehicle, mobile home, or camping trailer sales lot, which may include dealer servicing and outdoor storage of vehicles awaiting sale; but shall exclude the storage or sale of wrecked or inoperable vehicles, except as accessory to the dealership for vehicles which the dealership will repair <sup>37</sup>  (CB-95-1987; CB-87-2000; CB-29-2002; CB-25-2015; CB-48-2018)	p <sup>42</sup>	X	SE <sup>60</sup>	X	p <sup>72</sup>	X

**RESPONSE:** A vehicle, mobile home, or camping trailer sales lot, which may include dealer servicing and outdoor storage of vehicles awaiting sale; but shall exclude the storage or sale of wrecked or inoperable vehicles, except as accessory to the dealership for vehicles which the dealership will repair is a permitted use in the C-M Zone subject to the requirements of Footnotes 37 & 72.

Footnotes 37 & 72 state the following:

<sup>37</sup>	Except for new vehicle sales lots, the use shall be located on a tract of land containing a minimum of 25,000 square feet. All such uses on property less than 25,000 square feet in existence on September 1, 2000, may not be certified as nonconforming uses and must cease operations on or before August 31, 2003.  (CB-87-2000)
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**RESPONSE:** The subject Property includes 6.29 acres. The intention of the above footnote was to phase-out small used car dealerships that are located on property less than 25,000 square feet in size by August 31, 2003. The instant DSP application proposes a new car dealership that will be located on a 6.29 acre property (far in excess of 25,000 square feet). As such, the above footnote is not applicable to the review of the subject DSP application.

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For property located within an applicable D-D-O (Development District Overlay) Zone within an approved area Sector Plan and Sectional Map Amendment, the Development District requirements for development in the subject D-D-O Zone shall not apply. As such, development of use on the subject property shall be subject to the applicable requirements of the County Landscape Manual; all applicable parking and loading requirements within Part 11 of this Subtitle; and the applicable bulk regulations for building setbacks set forth in Part 6, Division 4 of this Subtitle.  
(CB-48-2018)

**RESPONSE:** The subject Property is not located in a D-D-O (Development District Overlay) Zone within an approved area Sector Plan and Sectional Map Amendment. As such, the above footnote is not applicable to the review of the subject DSP application. Notwithstanding, the submitted DSP has been designed in accordance with the requirements of the Prince George’s County *Landscape Manual*, the applicable parking and loading requirements in Part 11 of the Zoning Ordinance, and the applicable bulk regulations for building setbacks in Part 6, Division 4, (Section 27-462(b)) of the Zoning Ordinance. No variances, departures or requests for Alternative Compliance from these regulations are proposed with the subject DSP application.

***(d) Regulations.***

***(1) Additional regulations concerning the location, size, and other provisions for all buildings and structures in the C-M Zone are as provided for in Divisions 1 and 5 of this Part, the Regulations Table (Division 4 of this Part), General (Part 2), Off-Street Parking and Loading (Part 11), Signs (Part 12), and the Landscape Manual.***

**RESPONSE:** The submitted DSP has been designed in accordance with the above requirements. No variances, departures or requests for Alternative Compliance from these regulations are proposed with the subject DSP application.

**Section 27-281(b)(1) – General Purposes of Detailed Site Plans (Continued):**

***(C) To provide for development in accordance with the site design guidelines established in this Division; and***

**RESPONSE:** The proposed development is designed in accordance with site design guidelines in this Division. The “general” site design guidelines are found in Section 27-283 and require the following:

***Section 27-283. - Site design guidelines.***

***(a) The Detailed Site Plan shall be designed in accordance with the same guidelines as required for a Conceptual Site Plan (Section 27-274).***

*(b) The guidelines shall only be used in keeping with the character and purpose of the proposed type of development, and the specific zone in which it is to be located.*

*(c) These guidelines may be modified in accordance with Section 27-286.*

**RESPONSE:** The subject DSP has been developed in accordance with the Conceptual Site Plan design guidelines contained in Section 27-274 that pertain to the following relevant design elements:

***Section 27-274. - Design guidelines.***

*(a) The Conceptual Site Plan shall be designed in accordance with the following guidelines:*

*(1) General*

*(A) The Plan should promote the purposes of the Conceptual Site Plan.*

**RESPONSE:** The subject Property is located in the CS (Commercial, Service) Zone, and being reviewed in accordance with the requirements of the C-M Zone. As such, the approval of a comprehensive design zone is not required. Preliminary Plan of Subdivision, PPS 4-04047, was previously approved for the subject Property on September 9, 2004, for a 29,800 square-foot auto dealership in the C-M (Commercial Miscellaneous) Zone. (PGCPB No. 04-177). A prior Detailed Site Plan, DSP-05013, and Alternative Compliance Application, AC-05015, were then approved for the Property on July 28, 2005, for a 29,700 square-foot automotive dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 05-173). Finally, on August 24, 2006, DSP-05013-01, was approved by the Planning Director to add full basement parking to a 29,675 square-foot automobile dealership in lieu of previously approved rooftop parking, add a sidewalk to connect to the adjacent property, and to relocate a dumpster next to the building. The Applicant's current proposal for a 22,111 square-foot vehicle sales and service dealership in accordance with the requirements of the C-M Zone is in substantial harmony with these prior approvals.

*(2) Parking, loading, and circulation.*

*(A) Surface parking lots should be located and designed to provide safe and efficient vehicular and pedestrian circulation within the site, while minimizing the visual impact of cars. Parking spaces should be located to provide convenient access to major destination points on the site. As a means of achieving these objectives, the following guidelines should be observed:*

*(i) Parking lots should generally be provided to the rear or sides of structures;*

- (ii) Parking spaces should be located as near as possible to the uses they serve;*
- (iii) Parking aisles should be oriented to minimize the number of parking lanes crossed by pedestrians;*
- (iv) Large, uninterrupted expanses of pavement should be avoided or substantially mitigated by the location of green space and plant materials within the parking lot, in accordance with the Landscape Manual, particularly in parking areas serving townhouses; and*
- (v) Special areas for van pool, car pool, and visitor parking should be located with convenient pedestrian access to buildings.*

**RESPONSE:** The parking shown on the instant DSP has been designed to provide safe and efficient vehicular and pedestrian circulation throughout the site. Standard and van-accessible handicapped spaces have been strategically located as close to the front entrance as possible to provide easy and safe pedestrian access into the proposed building for the physically handicapped. Required driveway aisle widths a minimum of 22-feet wide will be maintained throughout the parking compound. The circulation pattern has been further studied to ensure safe and efficient truck turning movements can be accomplished along all sides of the proposed building. Interior green islands will be located throughout the parking compound in accordance with the requirements of the *Landscape Manual* which will provide shade and tree canopy coverage within the parking area and break up large expanses of surface pavement.

*(B) Loading areas should be visually unobtrusive and located to minimize conflicts with vehicles or pedestrians. To fulfill this goal, the following guidelines should be observed:*

- (i) Loading docks should be oriented toward service roads and away from major streets or public view; and*
- (ii) Loading areas should be clearly marked and should be separated from parking areas to the extent possible.*

**RESPONSE:** The two 12 x 33-foot exterior loading spaces shown on the DSP are in conformance with the above requirements. These spaces have been strategically-located along the rear of building to ensure they are completely screened from US Route 301.

*(C) Vehicular and pedestrian circulation on a site should be safe, efficient, and convenient for both pedestrians and drivers. To fulfill this goal, the following guidelines should be observed:*

- (i) The location, number and design of driveway entrances to the site should minimize conflict with off-site traffic, should provide a safe transition into the parking lot, and should provide adequate acceleration and deceleration lanes, if necessary;*
- (ii) Entrance drives should provide adequate space for queuing;*
- (iii) Circulation patterns should be designed so that vehicular traffic may flow freely through the parking lot without encouraging higher speeds than can be safely accommodated;*
- (iv) Parking areas should be designed to discourage their use as through-access drives;*
- (v) Internal signs such as directional arrows, lane markings, and other roadway commands should be used to facilitate safe driving through the parking lot;*
- (vi) Drive-through establishments should be designed with adequate space for queuing lanes that do not conflict with circulation traffic patterns or pedestrian access;*
- (vii) Parcel pick-up areas should be coordinated with other on-site traffic flows;*
- (viii) Pedestrian access should be provided into the site and through parking lots to the major destinations on the site;*
- (ix) Pedestrian and vehicular circulation routes should generally be separated and clearly marked;*
- (x) Crosswalks for pedestrians that span vehicular lanes should be identified by the use of signs, stripes on the pavement, change of paving material, or similar techniques; and*
- (xi) Barrier-free pathways to accommodate the handicapped should be provided.*

**RESPONSE:** The proposed DSP has been designed in accordance with the above standards to ensure vehicular and pedestrian circulation will be safe, efficient, and convenient throughout the site for both pedestrians and drivers.

**(3) Lighting.**

*(A) For uses permitting nighttime activities, adequate illumination should be provided. Light fixtures should enhance the site's design character. To fulfill this goal, the following guidelines should be observed:*

- (i) If the development is used at night, the luminosity, orientation, and location of exterior light fixtures should enhance user safety and minimize vehicular/pedestrian conflicts;*
- (ii) Lighting should be used to illuminate important on-site elements such as entrances, pedestrian pathways, public spaces, and*

- property addresses. Significant natural or built features may also be illuminated if appropriate to the site;*
- (iii) The pattern of light pooling should be directed on-site;*
  - (iv) Light fixtures fulfilling similar functions should provide a consistent quality of light;*
  - (v) Light fixtures should be durable and compatible with the scale, architecture, and use of the site; and*
  - (vi) If a variety of lighting fixtures is needed to serve different purposes on a site, related fixtures should be selected. The design and layout of the fixtures should provide visual continuity throughout the site.*

**RESPONSE:** The lighting proposed in this DSP meets all of the above requirements. All prominent on-site elements, such as the main entrance to the building and the parking compound, will be consistently lit throughout the appropriate portions of the day. The site will also incorporate full cut-off optics to limit light spill-over onto adjacent properties.

**(4) Views.**

- (A) Site design techniques should be used to preserve, create, or emphasize scenic views from public areas.*

**RESPONSE:** The proposed DSP is for an automotive dealership that will be located on one recorded parcel that has frontage on US Route 301. There are no scenic areas on the Property. The architecture for the proposed building has been carefully designed to provide pleasing views from US Route 301. All required loading and mechanical areas will be sufficiently screened from the public right-of-way.

**(5) Green area.**

- (A) On-site green area should be designed to complement other site activity areas and should be appropriate in size, shape, location, and design to fulfill its intended use. To fulfill this goal, the following guidelines should be observed:*
- (i) Green area should be easily accessible in order to maximize its utility and to simplify its maintenance;*
  - (ii) Green area should link major site destinations such as buildings and parking areas;*
  - (iii) Green area should be well-defined and appropriately scaled to meet its intended use;*
  - (iv) Green area designed for the use and enjoyment of pedestrians should be visible and accessible, and the location of seating should be protected from excessive sun, shade, wind, and noise;*

- (v) Green area should be designed to define space, provide screening and privacy, and serve as a focal point;*
- (vi) Green area should incorporate significant on-site natural features and woodland conservation requirements that enhance the physical and visual character of the site; and*
- (vii) Green area should generally be accented by elements such as landscaping, pools, fountains, street furniture, and decorative paving.*

**RESPONSE:** There is no specific green area requirement in the C-M Zone. However, the submitted DSP demonstrates that this 6.29-acre Property will include 4.53 acres of green area, (or 72.0 % of the total site). The proposed DSP has also been designed to be in full compliance with the interior green requirements in Section 4.3 of the Landscape Manual for the parking compound area, the tree canopy coverage requirements within Section 25-128 of the County Code, and the tree preservation requirements in the Woodland & Wildlife Habitat Conservation Ordinance.

**(6) Site and streetscape amenities.**

- (A) Site and streetscape amenities should contribute to an attractive, coordinated development and should enhance the use and enjoyment of the site. To fulfill this goal, the following guidelines should be observed:*
  - (i) The design of light fixtures, benches, trash receptacles, bicycle racks and other street furniture should be coordinated in order to enhance the visual unity of the site;*
  - (ii) The design of amenities should take into consideration the color, pattern, texture, and scale of structures on the site, and when known, structures on adjacent sites, and pedestrian areas;*
  - (iii) Amenities should be clearly visible and accessible, and should not obstruct pedestrian circulation;*
  - (iv) Amenities should be functional and should be constructed of durable, low maintenance materials;*
  - (v) Amenities should be protected from vehicular intrusion with design elements that are integrated into the overall streetscape design, such as landscaping, curbs, and bollards;*
  - (vi) Amenities such as kiosks, planters, fountains, and public art should be used as focal points on a site; and*
  - (vii) Amenities should be included which accommodate the handicapped and should be appropriately scaled for user comfort.*

**RESPONSE:** The subject DSP proposes an automotive dealership on one parcel in the C-M Zone. No internal streets are proposed with the subject application. The on-site pedestrian circulation has been designed to provide safe and convenient access into the proposed building. All required parking and access for the physical handicapped will be in full compliance with ADA requirements.

**(7) Grading.**

- (A) Grading should be performed to minimize disruption to existing topography and other natural and cultural resources on the site and on adjacent sites. To the extent practicable, grading should minimize environmental impacts. To fulfill this goal, the following guidelines should be observed:**
- (i) Slopes and berms visible from streets and other public areas should appear as naturalistic forms. Slope ratios and the length of slopes should be varied if necessary to increase visual interest and relate manmade landforms to the shape of the natural terrain;**
  - (ii) Excessive grading of hilltops and slopes should be avoided where there are reasonable alternatives that will preserve a site's natural landforms;**
  - (iii) Grading and other methods should be considered to buffer incompatible land uses from each other;**
  - (iv) Where steep slopes cannot be avoided, plant materials of varying forms and densities should be arranged to soften the appearance of the slope; and**
  - (v) Drainage devices should be located and designed so as to minimize the view from public areas.**

**RESPONSE:** All grading will conform to county code regulations and the approved Stormwater Management Concept Plan. The site contains sensitive environmental features including areas of steep slopes and 100-year floodplain. The submitted DSP has been designed to minimize disturbance to these sensitive areas to the maximum extent possible.

**(8) Service areas.**

- (A) Service areas should be accessible, but unobtrusive. To fulfill this goal, the following guidelines should be observed:**
- (i) Service areas should be located away from primary roads, when possible;**
  - (ii) Service areas should be located conveniently to all buildings served;**
  - (iii) Service areas should be effectively screened or enclosed with materials compatible with the primary structure; and**
  - (iv) Multiple building developments should be designed to form service courtyards which are devoted to parking and loading uses and are not visible from public view.**

**RESPONSE:** All required loading spaces will be located along the rear portion of the proposed building and will be fully screened from US Route 301. The service bay doors will primarily be located along the rear of the building and will not be visible from a public street.

***(9) Public spaces.***

***(A) A public space system should be provided to enhance a large-scale commercial, mixed-use, or multifamily development. To fulfill this goal, the following guidelines should be observed:***

- (i) Buildings should be organized and designed to create public spaces such as plazas, squares, courtyards, pedestrian malls, or other defined spaces;***
- (ii) The scale, size, shape, and circulation patterns of the public spaces should be designed to accommodate various activities;***
- (iii) Public spaces should generally incorporate sitting areas, landscaping, access to the sun, and protection from the wind;***
- (iv) Public spaces should be readily accessible to potential users; and***
- (v) Pedestrian pathways should be provided to connect major uses and public spaces within the development and should be scaled for anticipated circulation.***

**RESPONSE:** The subject DSP proposes an automotive dealership on one parcel in the C-M Zone. As a result, the above requirement is not applicable to the review of the subject application which only pertains to large-scale commercial, mixed-use, or multifamily development.

***(10) Architecture.***

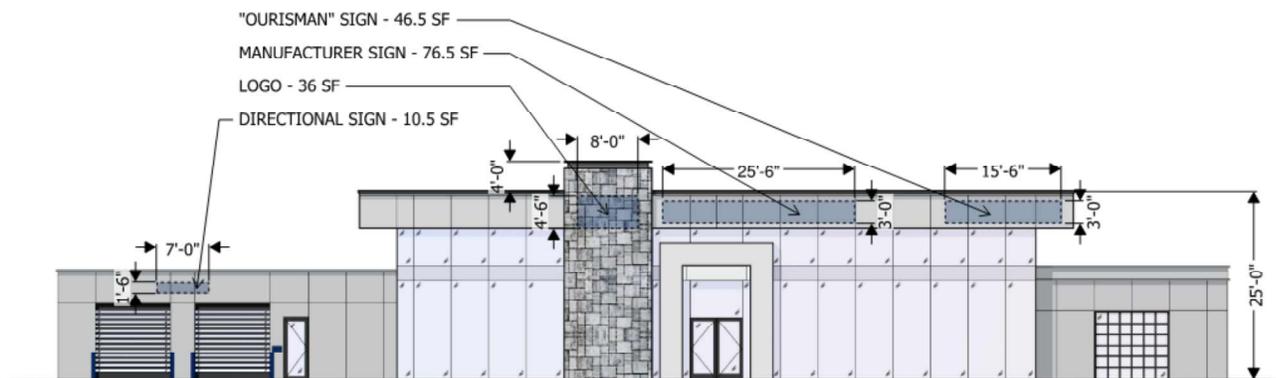
***(A) When architectural considerations are referenced for review, the Conceptual Site Plan should include a statement as to how the architecture of the buildings will provide a variety of building forms, with a unified, harmonious use of materials and styles.***

***(B) The guidelines shall only be used in keeping with the character and purpose of the proposed type of development and the specific zone in which it is to be located.***

***(B) These guidelines may be modified in accordance with Section 27-277.***

**RESPONSE:** The subject Property is located in the C-M Zone and did not require the approval of a CSP. As a result, there are no architectural guidelines that apply to the subject Property. The front elevation for the proposed one-story building will primarily consist of clear insulated glass with an aluminum composite metal panel along the service reception area. The proposed building will be approximately 25-feet high at its highest roof level, with a vertical element up to 29-feet high. The

top of the building will be trimmed with Basil Gray aluminum composite metal banding, with a future dealership brand signage and logo located near the center of the building, and building mounted signage along the right of the building. A “Service” sign will be located along the left side of the building to identify the service department area. All of the building signage will be individual letters, figures/designs that are mounted directly on the architectural face of the building with no other decorative wall work. A front elevation of the proposed building (facing US Route 301) is provided below:



The proposed building will include a service department with a service reception area, customer lounge, sales and manager’s offices, parts storage, parts receiving area, retail parts area, men’s and woman’s bathrooms (2), an indoor sales display area, two employee break rooms, various utility, equipment and storage rooms, and a vehicle delivery area. The service department will include 15 service bays that includes three electric vehicle (EV) lab areas, service lifts, an alignment bay, as well as wash and vehicle detail bays. All rooftop mechanical equipment will be completely screened. The Applicant is determined to move its new buildings into the future with sustainable and green design initiatives. As detailed further in this statement, the proposed building has been designed to implement many of the latest green design techniques and building materials.

**Section 27-281(c)(1) – Specific Purposes of Detailed Site Plans:**

**(c) Specific purposes:**

**(1) The specific purposes of Detailed Site Plans are:**

- (A) To show the specific location and delineation of buildings and structures, parking facilities, streets, green areas, and other physical features and land uses proposed for the site;**
- (B) To show specific grading, planting, sediment control, tree preservation, and storm water management features proposed for the site;**

- (C) To locate and describe the specific recreation facilities proposed, architectural form of buildings, and street furniture (such as lamps, signs, and benches) proposed for the site; and*
- (D) To describe any maintenance agreements, covenants, or construction contract documents that are necessary to assure that the Plan is implemented in accordance with the requirements of this Subtitle.*

**RESPONSE:** The DSP and related plans show all the above information proposed for the site.

**Section 27-285(b) - Required findings for Detailed Site Plans:**

- (1) The Planning Board may approve a Detailed Site Plan if it finds that the plan represents a reasonable alternative for satisfying the site design guidelines, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use. If it cannot make these findings, the Planning Board may disapprove the Plan.*
- (2) The Planning Board shall also find that the Detailed Site Plan is in general conformance with the approved Conceptual Site Plan (if one was required).*

**RESPONSE:** The subject Property is designed per the prior C-M Zone requirements, and did not require the approval of a CSP. As a result, there are no site design guidelines that apply to the subject Property. The submitted DSP has been designed to be in full compliance with the requirements of Subtitle 25, Subtitle 27, and the Prince George's County *Landscape Manual*. No variances, departures or Alternative Compliance from these regulations has been requested with the subject DSP.

- (3) The Planning Board may approve a Detailed Site Plan for Infrastructure if it finds that the plan satisfies the site design guidelines as contained in Section 27-274, prevents offsite property damage, and prevents environmental degradation to safeguard the public's health, safety, welfare, and economic well-being for grading, reforestation, woodland conservation, drainage, erosion, and pollution discharge.*

**RESPONSE:** The subject application is not an infrastructure only DSP. As demonstrated in the findings above, the development proposed in this DSP satisfies the site design guidelines contained in Section 27-274, prevents offsite property damage, and prevents environmental degradation to safeguard the public's health, safety, welfare, and economic well-being for grading, reforestation, woodland conservation, drainage, erosion, and pollution discharge.

- (4) The Planning Board may approve a Detailed Site Plan if it finds that the regulated environmental features have been preserved and/or restored in a natural state to the fullest extent possible in accordance with the requirement of Subtitle 24-130(b)(5).*

**RESPONSE:** The submitted DSP includes Stormwater Management Concept Plan, 02-1223-204NE14, approved by the City of Bowie on December 26, 2023. The subject application also includes a signed NRI and Type-2 Tree Conservation Plan which fully demonstrates that the regulated

environmental features have been preserved and/or restored in a natural state to the fullest extent possible in accordance with the requirement of Subtitle 24-130(b)(5).

**VII. CONFORMANCE WITH PPS & DSP CONDITIONS:**

**CONFORMANCE WITH PPS 4-04047**

The Planning Board approved Preliminary Plan of Subdivision, 4-04047, and Type I Tree Conservation Plan, TCPI/32/04, for the subject Property on September 9, 2004, for a 29,800 square-foot auto dealership in the C-M (Commercial Miscellaneous) Zone, (PGCPB No. 04-177). The following conditions relate to the review of the submitted DSP:

:

1. ***Development of this subdivision shall be in compliance with an approved Type I Tree Conservation Plan (TCPI/32/04). The following note shall be placed on the Final Plat of Subdivision:***

***"This development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCPI/32/04), or as modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy."***

**RESPONSE:** Final Plat of Subdivision, REP 212-11, was recorded for the subject Property on April 26, 2006. The above note is listed on the recorded plat as "Note 1". As a result, the above condition was fully met on April 26, 2006.

2. ***A conservation easement shall be described by bearings and distances at time of final plat. The conservation easement shall contain the delineated Patuxent River Primary Management Area, except for areas with approved impacts, and shall be reviewed by the Environmental Planning Section prior to certificate approval for accuracy. In addition, the following note shall be placed on the final plat:***

***"Conservation easements described on this plat are areas where the installation of structures and roads and the removal of vegetation are prohibited without prior written consent from the M-NCPPC Planning Director or designee. The removal of hazardous trees, limbs, branches, or trunks is permitted."***

**RESPONSE:** Final Plat of Subdivision, REP 212-11, was recorded for the subject Property on April 26, 2006. The above note is listed on the recorded plat as "Note 2". As a result, the above condition was fully met on April 26, 2006.

3. ***Prior to the issuance of any permits which impact wetlands, wetland buffers, streams, or waters of the U.S., copies of all federal and state wetland permits, evidence that approval conditions have been complied with, and associated mitigation plans shall be submitted to the M-NCPPC Planning Department.***

**RESPONSE:** The Applicant is agreement with the above condition and will submit the required evidence prior to the issuance of any permit that impacts these features.

4. ***The applicant shall provide sufficient and uninterrupted access to Parcels 103 and 164, either along the existing ingress/egress easement or through some other means mutually agreeable to all parties.***

**RESPONSE:** At the time of the prior PPS, these parcels were under private ownership and used for residential purposes. Both Parcel 103 and 164 have since been purchased by the Applicant, Rowhit Bowie LLC. As a result, sufficient and uninterrupted access to these parcels is no longer relevant. Permits are currently being prepared to utilize these CS-Zoned parcels for automotive storage and/or office purposes to further support the existing automotive dealership to the south, which was allowed by right use that did not require the approval of a DSP.

5. ***Any abandoned well or septic system shall be pumped, backfilled and/or sealed in accordance with COMAR 26.04.04 by a licensed well driller or witnessed by a representative of the Health Department prior to final plat approval.***

**RESPONSE:** Final Plat of Subdivision, REP 212-11, was recorded for the subject Property on April 26, 2006. The above condition was fully met prior to the approval of this plat.

6. ***All abandoned vehicles, domestic trash, and other debris found on the property shall be removed and properly stored or discarded prior to final plat approval.***

**RESPONSE:** Final Plat of Subdivision, REP 212-11, was recorded for the subject Property on April 26, 2006. The above condition was fully met prior to the approval of this plat.

7. ***Prior to the issuance of grading permits, the applicant, his heirs, successors and/or assignees shall have the scrap tires on the property hauled away by a licensed scrap tire hauler to a licensed scrap tire disposal/recycling facility. A receipt for the tire disposal shall be submitted to the Health Department prior to issuance of grading permits.***

**RESPONSE:** The Applicant is agreement with the above condition and will submit the required receipt for tire disposal prior to the issuance of a grading permit.

8. ***Prior to the issuance of building permits, the applicant shall obtain detailed site plan approval by the Planning Board. The detailed site plan shall address buffering (including light and noise intrusion), landscaping, access, techniques to further***

***minimize PMA impacts, and the visual relationships between the proposed development and adjoining residentially developed properties.***

**RESPONSE:** On July 28, 2005, the Planning Board approved Detailed Site Plan, DSP-05013, TCP2-089-05, and Alternative Compliance Application, AC-05015, for a 29,700 square-foot automotive dealership in the C-M (Commercial, Miscellaneous) Zone, (PGCPB No. 05-173). On October 5, 2005, the District Council waived their right to review Detailed Site Plan, DSP-05013. On August 24, 2006, DSP-05013-01, was approved by the Planning Director to add full basement parking to a 29,675 square-foot automobile dealership in lieu of the previously approved rooftop parking, add a sidewalk to connect to the adjacent property, and to relocate a dumpster next to the building.

The Applicant's current proposal for a 22,111 square-foot vehicle sales and service dealership in the C-M (Commercial-Miscellaneous) Zone is in substantial harmony with these prior approvals. The submitted DSP set includes a Landscape Plan, Lighting Plan, and revised Type-2 Tree Conservation Plan that fully addresses buffering (including light and noise intrusion), landscaping, access, techniques to further minimize PMA impacts, and the visual relationships between the proposed development and adjoining residentially developed properties.

**9. *Development of this subdivision shall be in compliance with the approved stormwater management concept plan (#8254-2004-01) or any approved revision thereto.***

**RESPONSE:** Stormwater Management Concept Plan, 02-1223-204NE14, was approved by the City of Bowie on December 26, 2023. This recent approval by the City of Bowie reflects the proposed development shown on the submitted DSP and supersedes the prior stormwater management concept approved by the County's Department of Environmental Resources (DER) on April 13, 2004.

**10. *The applicant shall provide for any necessary turn lanes and frontage improvements along US 301 as required by SHA. These may include turn lanes for deceleration and acceleration of vehicles and channelization (lane divider) at the site entrance to prevent weaving.***

**RESPONSE:** Within their memorandum dated April 17, 2005, SHA stated that they had no objection to the access locations that were reflected on the prior approved detailed site plan, DSP-05013. SHA will require an access permit for any work within the US Route 301 right-of-way, and it is anticipated that they will require a deceleration lane of at least 400 feet in length, (300 feet, plus a 100-foot painted taper). The submitted DSP includes a Transportation Impact Analysis that was prepared by Lenhart Traffic Consulting, Inc., which addresses the necessary turn lanes and frontage improvements along US Route 301. This study will be reviewed by both SHA and M-NCPPC's Transportation Section prior to the approval of the submitted DSP.

**11. *Prior to the issuance of any building permits within the subject property, the following road improvements shall (a) have full financial assurances, (b) have been permitted for construction, and (c) have an agreed-upon timetable for construction with SHA:***

**a. *Widen the westbound approach of Queen Anne Bridge Road at US 301 to provide an exclusive right turn lane and two through lanes. Widen the median to accommodate two through lanes from westbound Queen Anne Bridge Road, to create one left turn lane and one through lane. These improvements shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.***

**RESPONSE:** This has not been constructed nor has it been bonded or permitted by any other party. The median crossover going westbound between northbound and southbound US 301 has been improved to include two westbound through lanes in the median area, and along the receiving lane of Mitchellville Road on the west side of US 301. The only physical improvements required would be the widening of the north side of Queen Anne Bridge Road going westbound at US 301 to include a second through lane and a short right turn lane. The volumes for these turning movements are relatively light and can be accommodated with short lanes that can be developed within the existing right-of-way.

Some grading will be required in this area along with revisions to the traffic signal to accommodate the improvement. Coordination will be required with both SHA and DPIE to permit and build this improvement. The submitted DSP includes a Transportation Impact Analysis that was prepared by Lenhart Traffic Consulting, Inc., which further addresses the necessary improvements. This study will be reviewed by both SHA and M-NCPPC's Transportation Section prior to the approval of the submitted DSP.

**b. *Construct a free right turn lane on eastbound Mitchellville Road at US 301 and change from signal controlled movement to free-flow right turn movement. This improvement shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.***

**RESPONSE:** This improvement has already been constructed and is open to traffic.

**c. *The applicant will be responsible for any traffic signal modifications as a result of the proposed intersection improvements.***

**RESPONSE:** The signal modifications associated with Condition 11(b) above have already been constructed and are fully completed.

**d. *The type of material, length and construction of the channelized lane (lane divider), and how the merge of traffic will be handled beyond the channelized lane shall be determined.***

**RESPONSE:** This relates to the potential lane divider referenced in Condition 10 above. While it would not be confirmed until the access permit process is completed, it is not anticipated that SHA will require a lane divider, as this was not referenced by the operating agency during the approval of the subsequent DSP application, DSP-05013.

**12. *Total development within the subject property shall be limited to 29,800 square feet of automobile dealership facilities, or equivalent development that generates no more than 52 AM and 62 PM new peak-hour trips, taking into account estimated pass-by trips. Any development other than that identified herein above shall require a new preliminary plan of subdivision with a new determination of the adequacy of transportation facilities.***

**RESPONSE:** A trip generation analysis for the proposed DSP is included in the Transportation Impact Analysis prepared by Lenhart Traffic Consulting, Inc. and submitted with the subject application. The Applicant's current proposal for 22,111 square feet of automobile dealership facilities will remain squarely within the approved trip cap specified above.

**13. *The applicant, his successors and/or assignees shall conduct a traffic signal warrant study at the US 301/Walker Pontiac median crossover (for both northbound and southbound US 301). If signals are warranted and desired by the State Highway Administration, the applicant shall install the signals prior to the issuance of a use and occupancy permit.***

**RESPONSE:** New traffic counts were conducted and a signal warrant analysis was completed. The results of this analysis are provided in the Transportation Impact Analysis submitted with the subject DSP application and demonstrate that signalization is not required at this location.

### **CONFORMANCE WITH DSP-05013**

On July 21, 2005, the Planning Board approved Detailed Site Plan, DSP-05013, Type II Tree Conservation Plan, TCPII-089-05, and Alternative Compliance Application, AC-05015, with two (2) conditions for a 29,700 square-foot automotive dealership in the C-M (Commercial, Miscellaneous) Zone.

The resolution of approval, PGCPB Resolution No. 05-173, was adopted by the Planning Board on July 28, 2005. On October 5, 2005, the District Council waived their right to review Detailed Site Plan, DSP-05013. It should be noted that DSP-05013 has expired and the conditions of approval associated therein no longer are binding on the project. Nonetheless, the following is an analysis of how the development proposed in this DSP adhere to the prior conditions in DSP-05013:

**1. *Prior to certificate approval of this detailed site plan, the applicant shall***

- a. ***Complete certification of Preliminary Plan of Subdivision 4-04047.***
- b. ***Provide information on the proposed hours of operation, indicate if there will be a public address system (PA) and, if so, how it might be used and when. Provide a noise study with projected noise contours to reflect how the PA system might adversely impact the neighboring residential area, if one is proposed.***
- c. ***Submit a lighting plan with photometric diagram and details of all light fixtures showing full cut-off optics.***
- d. ***Revise Type II Tree Conservation Plan TCPII/89/05 as follows:***
  - (1) ***Prepare the TCPII and DSP at the same scale.***
  - (2) ***Identify each woodland conservation treatment area to the closest 1/100<sup>th</sup> of an acre.***
  - (3) ***Use more distinguishable shading or a pattern to better distinguish between the two woodland treatment areas (woodland cleared and woodland preserved) on the plan.***
  - (4) ***Adjust the worksheet to show 0.06-acre of off-site mitigation to be provided by fee-in-lieu payment or the following note should be placed on the plan under the worksheet: "off-site mitigation may be met by using the fee-in-lieu option."***
  - (5) ***Show the proposed grading.***
  - (6) ***Add details for the type of tree protection device and woodland conservation signage, including the general notes below each detail.***
  - (7) ***Provide the applicable edge management notes on the plan.***
  - (8) ***After these revisions have been made to the plans, have the qualified professional who prepared it sign and date it.***
- e. ***Replace the proposed white pine with either American holly or red cedar and replace the Pin Oak in the western bufferyard with fast-growing evergreen trees such as Leyland Cypress.***
- f. ***Add site plan notes as follows:***

*“All HVAC and other roof-mounted mechanical equipment shall be screened from view.”*

*“All rooftop parking shall be fully screened from view.”*

*“No flags or banners may be mounted, suspended or otherwise displayed for advertising anywhere on the building.”*

*“ All on-and-off loading shall take place on site.”*

- g. Provide the height information for the proposed retaining walls and fence details.*
- h. Provide a Section 4.2 schedule on the landscape plan*
- i. Show complete compliance to Section 4.3(a) along part of the site’s eastern frontage or seek alternative compliance from this requirement.*
- j. Provide a standard sidewalk connection to the adjacent commercial property to the south of the subject site.*
- k. Provide a Section 4.7 schedule for the northwest portion of the subject property on the landscape plan.*
- l. Provide the height information of the building and the proposed retaining wall on the site plan.*
- m. Provide retaining wall details on the detail sheet.*
- n. Relocate the seven display parking spaces away from the access easement to the Noerling property and stripe and paint the portion of the easement on the site plan. However, the spaces may be installed if the Noerling easement is relocated off-site.*
- o. Relocate the proposed dumpster to the north part of the site, to be reviewed and approved by the Urban Design Section as the designee of the Planning Board.*
- p. Show the proposed freestanding sign, including all dimensions and sign face area calculation, on the site plan.*

**RESPONSE:** The above revisions were met prior to the certification of DSP-05013 and TCPII-089-05.

- 2. Prior to issuance of any building permits, the applicant shall:**

- a. Provide a complete north elevation demonstrating enhanced visual and architectural interest to be reviewed and approved by the Urban Design Section as the designee of the Planning Board.*
- b. Submit certification by a professional engineer with competency in acoustical analysis that demonstrates that noise intrusion from the proposed use shall not exceed 65 dBA (Ldn) at the adjoining residential property line.*
- c. Relocate the existing access easement outside the boundary of the subject Detailed Site Plan or outside the dealership area, and to build the access easement to the Noerling Property at the applicant's expense.*

**RESPONSE:** The Applicant is in agreement with the above condition 2(a). Further, Condition 2(b) is no longer appropriate as the Applicant has acquired the former residentially used properties (which are all zoned for commercial use and are no longer used for residential purposes). Condition 2c would no longer apply as the prior Noerling Property is now owned by the Applicant.

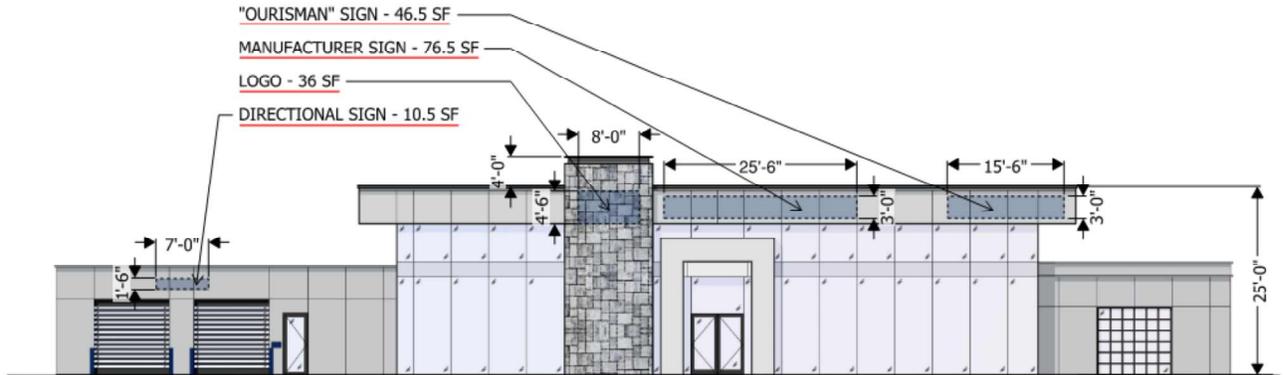
#### **CONFORMANCE WITH DSP-05013-01**

On August 24, 2006, DSP-05013-01, was approved by the Planning Director to add full basement parking to a 29,675 square-foot automobile dealership in lieu of the previously approved rooftop parking, add a sidewalk to connect to the adjacent property, and to relocate a dumpster next to the building.

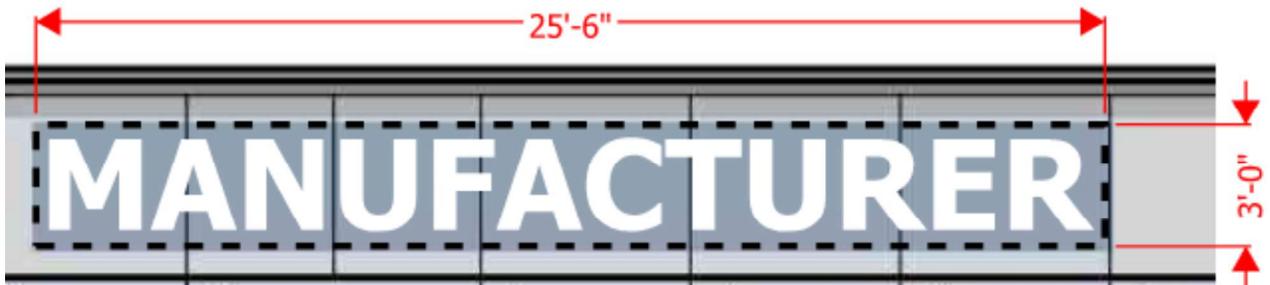
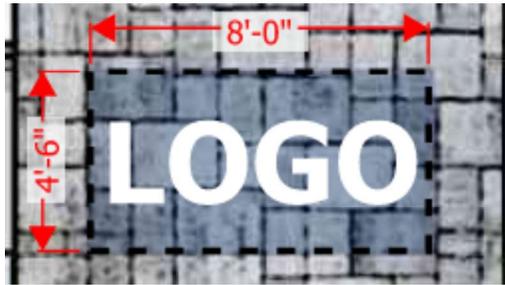
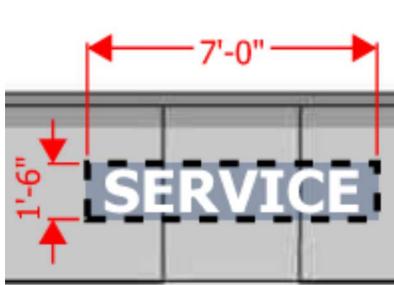
This Planning Director level revision did not include any new conditions of approval. The Certificate of Approval for DSP-05013-01 states that; "Conditions of the original approval shall remain in full force and effect."

#### **VIII. CONFORMANCE WITH SIGNAGE REGULATIONS IN PART 12**

##### **Proposed Building Signage**



**Building Signage Details.**



**Section 27-613 - Attached to a building or canopy.**

**(c) Area**

**(3) Commercial Zones (except the C-O Zone) and Industrial Zones (except the I-3 and U-L-I Zones).**

**(B) In all Commercial Zones (except the C-O Zone) and all Industrial Zones (except the I-3 and U-L-I Zones), if all of the permissible sign area is to be used on any building occupied by only one (1) use that is not located within an integrated shopping or industrial center or office building complex, the following applies:**

**(i) Each building shall be allowed a sign having an area of at least sixty (60) square feet.**

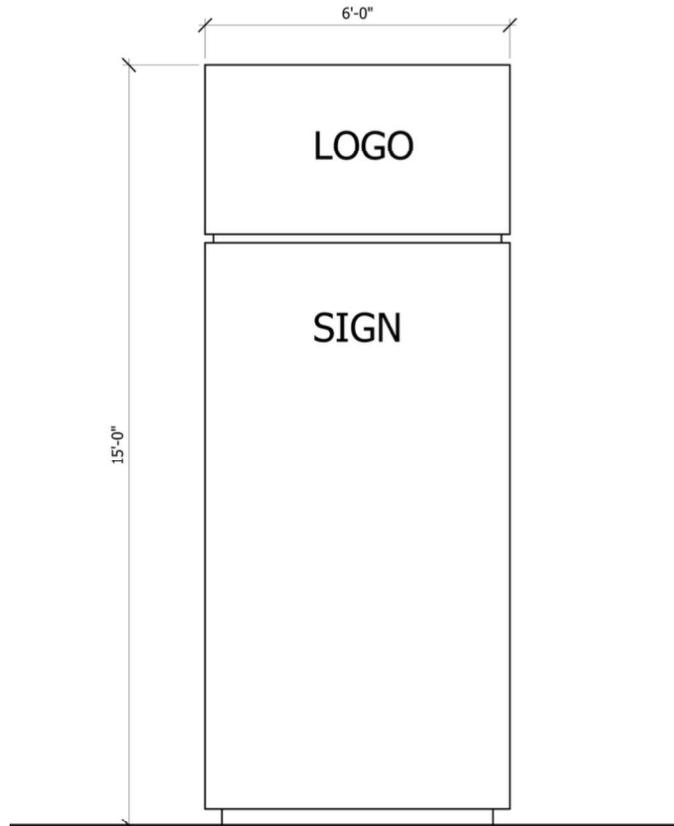
**(ii) Except as provided in (i), above, the area of all of the signs on a building shall be not more than two (2) square feet for each one (1) lineal foot of width along the front of the building (measured along the wall facing the front of the lot or the wall containing the principal entrance to the building, whichever is greater), to a maximum of four hundred (400) square feet.**

**RESPONSE:** The front of the proposed (1) story building (measured along the wall facing the front of the lot and containing the principal entrance to the building) is 85 lineal feet.

<b>BUILDING SIGNAGE TABLE</b>	
<b>Allowed Building Signage</b>	<b>Proposed Building Signage</b>
<p>Two (2) square feet for each one (1) lineal foot of width along the front of the building</p> <p>Lineal feet of width along front of building = 85 feet</p> <p>(85 lineal feet x 2) = <b>170 sf</b></p>	<p>*Service sign (7'-0" x 1'-6") = 10.50 sf            *Logo sign (8'-0" x 4'-6") = 36.00 sf            *Manufacturer sign (25'-6" x 3'-0") = 76.50 sf            *Ourisman sign (15'-6" x 3'-0") = 46.50 sf</p> <p style="text-align: center;">-</p> <p><i>*Note - Section 27-591(a) allows a 50% reduction for letters, figures, and designs that are mounted directly on the architectural face of a building with no other decorative wall work</i></p> <hr style="width: 20%; margin-left: auto; margin-right: auto;"/>

	Total Proposed Building Signage = (169.50 sf minus 50%) = <b>84.75 sf</b>
--	--

**Proposed Freestanding Signage**



**Section 27-614. - Freestanding signs.**

**(a) Location.**

- (1) In all Commercial and Industrial Zones (except the I-3 and U-L-I Zones), signs shall only be located on property where the main building associated with the sign is located at least forty (40) feet behind the front street line. This shall not apply to integrated shopping centers, other commercial centers with three (3) or more businesses served by common and immediate off-street parking and loading facilities, industrial centers, or office building complexes.***

**RESPONSE:** The proposed building is setback approximately 89.3 feet from the front street line. Therefore, a freestanding sign is permitted on the subject Property.

- (2) *In the C-O Zone, signs shall not be located within fifty (50) feet of any land in any Residential Zone, or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan.***

**RESPONSE:** The proposed freestanding sign is not located within fifty (50) feet of any land in any Residential Zone, or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan.

- (3) *In the I-3 Zone, signs may be located anywhere that the Planning Board deems appropriate, subject to the height limitations below.***

**RESPONSE:** The Property is located in the C-M Zone. Therefore, the above finding is not applicable to the review of the submitted DSP.

- (4) *Notwithstanding any other provisions of this Subtitle addressing setbacks and yards, in all Commercial and Industrial Zones (except the I-3 Zone), signs need only be located ten (10) feet behind the street line. Where the street line is situated behind the actual existing street right-of-way line, freestanding on-site signs may be temporarily located within the area between the street line and the existing street right-of-way line (the area of proposed future widening of an existing street), provided that:***

- (A) *The land area involved has not been, and is not in the process of being, acquired for street purposes;***
- (B) *The sign is located at least ten (10) feet behind the existing street right-of-way line; and***
- (C) *A written agreement between the owner and the Department of Permitting, Inspections, and Enforcement assures that the sign will be removed, at the owner's expense, at the time of acquisition of that area for street purposes.***

**RESPONSE:** The proposed freestanding sign will be located a minimum of ten (10) feet behind the front street line.

- (5) *In the R-10 and R-H Zones, where general business and professional offices are allowed by Special Exception, the location of signs shall be approved as a part of the Special Exception approval (Section 27-386).***
- (6) *Gas station price signs shall be affixed to the same freestanding structure that***

*supports the sign containing other advertising matter. A freestanding price sign erected in accordance with this Section shall not be permitted on the premises if other price signs erected in accordance with Section 27-623 are present on the premises.*

**RESPONSE:** The subject DSP application proposed automobile dealership in the C-M Zone. Therefore, the above findings are not applicable to the review of the submitted DSP.

**(b) Height.**

**(1) The maximum height of signs shall be as shown in the following table. Measurements are from the finished grade at the base of the sign to the top of the sign. (See Figure 65.)**

ZONE	MAXIMUM HEIGHT (IN FEET)
C-O	8
Commercial Zones (except C-O)	25
Industrial Zones (except I-3)	25
I-3 Zone	Not greater than the lowest point of the roof of any building in the employment park

**RESPONSE:** The proposed freestanding sign will be fifteen (15) feet in height, and will therefore, be in compliance with Section 27-614(b).

**(c) Area. (See Figure 66.)**

**(1) The area of freestanding on-site signs is in addition to that allowed for any other signs on a specific property.**

**RESPONSE:** The Applicant is in agreement with the above requirement.

**(2) In the C-O Zone, the area of a freestanding sign shall be not more than one (1) square foot for each eight (8) lineal feet of street frontage of the property occupied by the use associated with the sign. The area of the sign shall generally be not more than twenty-five (25) square feet. On a lot having frontage on a divided highway with an existing right-of-way of at least one hundred (100) feet, the maximum size of the sign shall be fifty (50) square feet.**

**RESPONSE:** The subject DSP application proposed automobile dealership in the C-M Zone. Therefore, the above finding is not applicable to the review of the submitted DSP.

**(3) In all Commercial Zones (except the C-O Zone) and all Industrial Zones (except the I-3 Zone), the area of the sign shall be not more than either:**

**(A) One (1) square foot for each two (2) lineal feet of street frontage, to a maximum of two hundred (200) square feet for each sign, if the building is located in an integrated shopping center, other commercial center with three (3) or more businesses served by common and immediate off-street parking and loading facilities, industrial center, or office building complex. The street frontage shall be measured on the property occupied by the center or complex associated with the sign; or**

**RESPONSE:** The subject Property is not located in an integrated shopping center. Therefore, the above finding is not applicable to the review of the submitted DSP.

**(B) One (1) square foot for each four (4) lineal feet of street frontage, to a maximum of two hundred (200) square feet for each sign, if the business is not located in an integrated shopping center, other commercial center with three (3) or more businesses served by common and immediate off-street parking and loading facilities, industrial center, or office building complex. The street frontage shall be measured on the property occupied by the use associated with the sign.**

**RESPONSE:** As shown in the table below, the proposed freestanding sign is in compliance with the above requirement.

**(d) Quantity.**

**(1) In the C-O Zone, only one (1) freestanding on-site sign shall generally be permitted for each office building complex or single office building. If the property has frontage on two (2) parallel (or approximately parallel) streets, one (1) freestanding sign shall be permitted on each street.**

**RESPONSE:** The subject Property is not located in the C-O Zone. Therefore, the above finding is not applicable to the review of the submitted DSP.

**(2) In all Commercial Zones (except the C-O Zone) and all Industrial Zones (except the I-3 Zone), there are two (2) options (subparagraphs (A) and (B), below) which may be used to calculate the number of signs allowed. Option One can be used for any property which has the specified minimum frontage on one (1) or more streets. Option Two may be used only for property which has the specified minimum frontage on each of two (2) parallel (or approximately parallel) streets. Both options can apply to either a business located within an integrated shopping center, other commercial center with three (3) or more businesses served by common and immediate off-street parking and loading facilities, industrial center, or office building complex, or a business not**

*located in a center or complex. In the case of a center or complex, the street frontage is that which the entire center or complex has, and the number of signs permitted are the total number for the entire center or complex.*

**RESPONSE:** The subject Property has the specified minimum street frontage, and is therefore, required to utilize Option one as provided below.

**(A) Option One:**

TOTAL STREET FRONTAGE ON ALL STREETS	NUMBER OF SIGNS PERMITTED
If in an integrated shopping center, other commercial center with three (3) or more businesses served by common and immediate off-street parking and loading facilities, industrial center, or office building complex:	
0 to under 100 feet	None
100 to 1,100 feet	1.0
Each additional 1,000 feet (or fraction)	1.0
If <b>not</b> in an integrated shopping center, other commercial center with three (3) or more businesses served by common and immediate off-street parking and loading facilities, industrial center, or office building complex:	
0 to under 40 feet	None
<del>40 to 1,100 feet</del>	<del>1.0</del>
Each additional 1,000 feet (or fraction)	1.0

**RESPONSE:** The subject Property has 431 feet of lineal street frontage along US Route 301 and is entitled to one (1) freestanding sign. In compliance with the above requirement, only one (1) freestanding sign is proposed on the Property.

FREESTANDING SIGNAGE TABLE	
Allowed Freestanding Signage	Proposed Freestanding Signage
	1- Freestanding Sign

<p>One (1) square foot for each four (4) lineal feet of street frontage, to a maximum of two hundred (200) square feet for each sign</p> <p>Lineal feet of street frontage = 431 feet</p> <p><math>(431 \text{ lineal feet} \div 4) = \mathbf{107 \text{ sf}}</math></p>	<p>(6' x 15')</p> <p>Total Proposed Freestanding Signage = <b>90 sf</b></p>
--	---

**RESPONSE:** The above information and signage table clearly demonstrate that the proposed freestanding sign will be in full compliance with the requirements of Part 12 of the Zoning Ordinance. All of the proposed signage information has been added to submitted DSP.

**IX. GREEN BUILDING AND SUSTAINABLE DEVELOPMENT TECHNIQUES**

The Applicant has designed the proposed building with green and sustainable design elements that include the following:

**Green/Sustainable Building Elements:**

Lighting:

- Daylight harvesting, LED fixtures, dimming systems, and lighting strategies associated with day and after hour time periods.
- Mecho shades will be provided around the showroom (not to hang lower than 7 feet above ground floor level) with solar fabric that provides maximum daylight management and protection against damaging UV rays and glare.

High Speed Overhead Doors to reduce opening times and minimize air loss in service bay areas.

Low-flow plumbing fixtures.

Energy efficient roof-top HVAC units.

Light color roofing materials to reduce solar heat absorption.

**Electric Vehicle Charging:**

Automotive dealerships are moving a large portion of its fleet to hybrids and electric vehicles. As a result, the following electric vehicle items will be included with this development:

- Customer Parking: Minimum (2) Level 3 Chargers.

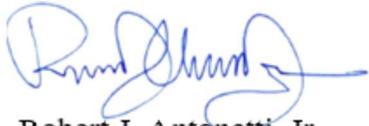
- Serviced EVs, Service Valet, Inventory, Test Drive, and Loaner Vehicles Parking: Minimum (1) Level 3 Fast Charger, conduit for additional chargers based on EV demand.
- Showroom: (1) Level 2 Charger for demonstration.
- New Vehicle Delivery: (1) Level 2 Charger per bay to demonstrate to new owners.
- Service: Minimum (3) Level 2 Chargers with future EV Charger capacity as volume increases.
  - ICE Bays convert to EV Lab with additional EV models.
  - Extra wide vehicle lifts to allow battery removal. Minimum of 13'-0" wide bays with inground lifts to allow circulation (additional space required if above ground lifts are installed)
  - 13'-6"x13'-6" battery storage room with roll-up door (8'-0" wide), storage space for battery lift equipment and replacement batteries.
  - Special EV tools and equipment.
- Trained/certified EV service technicians, sales, and service staff.

**Sustainable Site Development:**

The Applicant will institute the use of low impact development techniques and Environmental Site Design in the handling of storm water runoff, to the maximum extent practicable. Specifically, the project incorporates various storm drain inlets, micro-bioretenion areas, and underground storage chambers with filtering and pre-treatment devices. These facilities will improve water quality, recharge groundwater, cool runoff from impervious areas, and decrease downstream pollution and sedimentation.

**X. CONCLUSION:**

In consideration of the foregoing statement of justification, the Applicant respectfully requests approval of DSP-23026 for the Ourisman Dealership of Bowie. Thank you in advance for your consideration of this application. If you have any questions or comments, please do not hesitate to contact me.

Sincerely,  
  
Robert J. Antonetti, Jr.

RJA/jjf

cc: Rowhit Bowie LLC  
Bohler Engineering  
Ammon Heisler Sachs Architects, Inc.

**STATEMENT OF JUSTIFICATION IN SUPPORT OF**  
**MODIFICATION TO WOODLAND CONSERVATION PRIORITIES**  
**OURISMAN DEALERSHIP OF BOWIE**  
**2550 NW ROBERT CRAIN HIGHWAY**  
**DSP-23026-SDRC**  
**July 14, 2025**

## **1.0 INTRODUCTION/OVERVIEW/ORIENTATION**

On behalf of the Applicant, Ourisman Dealership of Bowie, please accept this Statement of Justification addressing compliance with the Woodland Conservation Priorities set forth in Section 25-121(c)(1)(c) and 25-121(c)(3) of the County Code for DSP-23026-SDRC. The property forming the subject matter of the Detailed Site plan consists of approximately 6.29 acres and is located at 2550 NW Robert Crain Highway in Bowie. The property's location is also shown on Prince George's County Tax Map 63 on Grid D-2, Parcels 00 (the "Subject Property").

The Subject Property is currently zoned CS, (Commercial, Service) and was previously zoned C-M (Commercial, Miscellaneous). The property is situated west of Robert S Crain Highway (I-301 North), south of Mount Oak Road, and north of the Ourisman Chrysler Dodge Jeep Ram of Bowie property. The Property is impacted by an existing floodplain easement, which extends approximately 2.76 acres onto the northern portion of the Property.

## **II. NATURE OF REQUEST**

As noted above, the Applicant proposes to construct a 1-story automotive dealership building (allowable uses per zoning code) and associated parking areas on the Subject Properties. Section 25-121(c)(1)(c) of the establishes priorities for how the WCO is satisfied. On-site preservation and/or afforestation is required pursuant to 25-121(c)(3) required to off-site preservation/afforestation. The Applicant proposes a combination of on-site preservation and off-site woodland conservation credits to satisfy the WCO requirements.

The TCPII proposes to clear 1.35 acres of woodland including 0.14 acres clearing within the flood plain, resulting in a woodland conservation requirement of 1.88 acres. This requirement is proposed to be met with 1.02 acres of on-site preservation/afforestation and 0.88 acres of off-site mitigation.

### **III. APPLICABLE STATUTORY PROVISIONS**

The Woodland and Wildlife Habitat Conservation Ordinance (WCO), Section 25, Division 2, Sec. 25-121 and 25-122 recommend methods for meeting the Woodland and Wildlife Conservation Requirements.

Per the approved Natural Resource Inventory (NRI-081-2024), the existing forest stand contains a total of 3.22 acres of Primary Management Area (“PMA”) and includes approximately 658 linear feet of regulated streams and 2.76 acres of 100-year floodplain. The Applicant is proposing 0.99 acres of on-site preservation outside of the floodplain at the northern perimeter of the site. This project seeks 0.88 acres of off-site woodland conservation, as other areas of existing

forest are encumbered by existing easements and will remain forest that will not be counted for credit. There are no other acceptable areas of the site that can be utilized for additional woodland conservation that would not deprive the owner of developing the site in accordance with the allowable uses per Code. We are proposing to preserve nine existing specimen trees on-site and the maximum additional woodland preservation practicable. With these efforts, we meet the required woodland conservation requirements from 25-121(c)(1)(c) and 25-121(c)(3).

#### IV. CONCLUSION

In view of all the above, the Applicant submits that the proposed methodology for satisfying the woodland priorities for the proposed development conforms to the requirements of Subtitle 25.

Prepared by:



Daniel Park, PLA  
Director, Landscape Architecture and Planning  
Bohler

**Ourisman Dealership of  
Bowie  
2550 NW Robert Crain  
Highway, Bowie, MD 20716  
Prince George's County  
P/O Parcel 00  
July 14, 2025**

**Letter of Justification re: Variance for Removal of Specimen Trees**

**INTRODUCTION**

The Ourisman Dealership of Bowie Property is a 6.29 ± acre site situated on undeveloped land located west of Robert S Crain Highway (I-301 North), south of Mount Oak Road, and north of the Ourisman Chrysler Dodge Jeep Ram of Bowie property. The proposed project includes the construction of a 1-story automotive dealership buildings (allowable uses per zoning code) and associated parking areas on the Subject Properties.

The property is largely forested. The property contains approximately 4.95 acres +/- defined as forest pursuant to Natural Resources Inventory Plan NRI-081-2024, approved by the Environmental Planning Section and identifies 4.95 acres of woodland including 11 specimen trees on or within 50' of the property.

**NATURE OF THE REQUEST**

Variance from Section 25-122(b)(1)(G) – (Specimen Trees)

The property contains a total of 4.95 acres of forested area. The approved Natural Resources Inventory Plan (NRI-081-2024) identifies 11 specimen trees located on the property. The Applicant requests a variance from Section 25-122(b)(1)(G) of the County Code to allow removal of 2 of the 11 specimen trees on the Property. The removal of these specimen trees is necessary to facilitate the proposed redirection of the access road to Rowhit Bowie LLC. property and for the proposed parking lot facing Crain Highway. The 2 trees in question are within the proposed LOD on the property and their removal is critical to the development of the proposed site.



The specimen trees requested to be approved for removal include the following: Tree 1377 and Tree 1380. We have provided the specimen tree table on the TCP2 to reflect these trees to be removed and there are no PMA impacts proposed for the removal of these trees. The trees are outside of the PMA and are in poor or fair condition.

In the cases of all specimen trees noted above, they are either directly located within the proposed site area, or along areas impacted significantly by proposed grading for the redirected access road construction and hence are requested for approval for removal. Specifically, the reasons for removal are as follows:

Tree 1377 and Tree 1380– These trees are all located directly within the proposed site area and/or proposed grades for the redirected access road construction. The trees that are proposed to be removed are one (1) Red Maple (Tree 1377), which has heavy vine coverage, broken branches and dead wood and one (1) Sweet Gum (Tree 1380),, which has hardware in the trunk.

## REQUIRED FINDINGS

Section 25-119(d) sets forth the following requirements for approvals of variances to requirements of Subtitle 25 – Trees and Vegetation.

### (d) Variances

(1) An applicant may request a variance from this Division as part of the review of a TCP where owing to special features of the site or other circumstances, implementation of this subtitle would result in unwarranted hardship to an applicant. To approve a variance, the approving authority shall find that:

(A) Special conditions peculiar to the property have caused the unwarranted hardship;

RESPONSE: The project is proposing a redirection to the access road to the Rewhit Bowie LLC property as well as parking along the frontage of the site toward Crain Highway and as such, the existing access road alignment and proposed road redirection proposed the impacts to the existing features of the site including the specimen trees. For the access road and parking to be constructed, the 2 specimen trees will be required to be removed. Given the location of the property line in relation to the access road and the need for parking and circulation around the building, there is no practical alternative layout option.

- (B) Enforcement of these rules will deprive the applicant of rights commonly enjoyed by others in similar areas;

RESPONSE: The applicant is seeking to develop this property as is allowed per the appropriate provisions of the Zoning Ordinance and the development is situated along Crain Highway. Enforcement of these rules, given the unique characteristics of the property and the need for site visibility along Crain Highway, would deprive the applicant of the right to develop the property in a similar fashion to other properties in the immediate area.

- (C) Granting the variance will not confer on the applicant a special privilege that would be denied to other applicants.

RESPONSE: Similar to the Finding (B) above, the variance confers no special privileges on the applicant that would be denied to other applicants. This Property serves an area planned for commercial use. The variance is necessary if the applicant is to be permitted to redirect the access road as well as provide adequate parking for customers and employees.

- (D) The request is not based on conditions or circumstances which are the result of actions by the applicant;

RESPONSE: The request is based on required site access, including a connection to the existing improved property as well as required parking. The request is necessary due to the property conditions of the site relative to the required improvements, and is not a result of actions by the Applicant.

- (E) The request does not arise from a condition relating to land or building use, either permitted or nonconforming, on a neighboring property; and

RESPONSE: This request did not arise from a land or building use either permitted or nonconforming on a neighboring property. The design of the Automotive Dealership considered the location of specimen trees and limited the impact to these trees as best as possible while maintaining the adjacent driveway access on property of common ownership.

- (F) Granting of the variance will not adversely affect water quality.

RESPONSE: Impact on water quality for the development of this project will be controlled by the stormwater management facilities proposed within the site. There is no evidence that the removal of the specimen trees in an area approved for development will have any impact on water quality.

## CONCLUSION

This specimen tree variance application meets all applicable requirements for approval set forth in the Prince George's County Code, as discussed herein. As such, the Applicant respectfully requests that the instant variance be approved.

Thank you in advance for your consideration of this Application. If you have any questions or comments, please do not hesitate to contact the undersigned.

Prepared by:

A handwritten signature in blue ink, appearing to read "D. Park.", is centered below the text "Prepared by:". The signature is stylized and cursive.

Daniel Park, PLA  
Director, Landscape Architecture and Planning  
Bohler

## Christopher Rizzi

---

**From:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Sent:** Friday, June 13, 2025 10:38 AM  
**To:** Robert Antonetti; Joe Heisler; AHSa Shared Calendar; Christopher Rizzi; John Lawall; John Ferrante; David McFadden  
**Cc:** Katherine Duerr; Hannah Haught; Jordan Sexton; Dylan McAndrew; Brian Hardham; mlenhart  
**Subject:** FW: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our discussion this morning

**EXTERNAL:** Use caution with attachments and links.

See SHA's response below confirming to use retain the existing shoulder for the right turns into the site.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

Office: [\(410\) 216-3333](tel:4102163333) (Ext. 1)  
Mobile: [\(410\) 980-2367](tel:4109802367)



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**From:** Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>  
**Sent:** Friday, June 13, 2025 9:53 AM  
**To:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Cc:** Dylan McAndrew <DMcAndrew@LENHARTTRAFFIC.COM>  
**Subject:** RE: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our discussion this morning

Mike,

Your recap of our conversation this morning is accurate. I am okay with using the existing shoulder as an unstriped right turn lane and adding curb and gutter up to the location of the guardrail. The shoulder will have to be verified that it is traffic bearing.

Thanks, Kwesi



[roads.maryland.gov](http://roads.maryland.gov)

**Kwesi Woodroffe (he/him)**  
*Regional Engineer*  
District 3 Access Management

301.513.7347 office  
[KWoodroffe@mdot.maryland.gov](mailto:KWoodroffe@mdot.maryland.gov)

**Maryland State Highway Administration**  
9300 Kenilworth Ave, Greenbelt, MD 20770

**From:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Sent:** Friday, June 13, 2025 9:20 AM  
**To:** Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>  
**Cc:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>; Dylan McAndrew <DMcAndrew@LENHARTTRAFFIC.COM>  
**Subject:** DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

**Caution:** Suspicious? Double-check! This email is from an external source. If something seems unusual, even from someone you know, verify directly. Report concerns to [MDOTServiceDesk@mdot.maryland.gov](mailto:MDOTServiceDesk@mdot.maryland.gov) or 410-768-7181.

Hi Kwesi,

This is a follow up to our meeting this morning. I've attached a pdf of DSP-23026. This is the site along the west side of southbound US 301 just north of the Ourisman Jeep dealership.

The attached DSP shows a potential right turn lane for the site, but after discussing this with you, it was agreed that you would prefer to leave the existing 12' shoulder unstriped for the right turn movement. The right turn volume is projected to be 20 in the AM and 16 in the PM which does not require a right turn lane, and based on Maryland law and the SHA Guidelines the 12' shoulder can be used for right turning vehicles.

Would you like a 12' curb and gutter installed back to the guardrail? I believe this would be reasonable for the frontage channelization.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

## Christopher Rizzi

---

**From:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Sent:** Friday, June 13, 2025 10:39 AM  
**To:** Robert Antonetti; Joe Heisler; AHSa Shared Calendar; Christopher Rizzi; John Lawall; John Ferrante; David McFadden  
**Cc:** Katherine Duerr; Hannah Haught; Jordan Sexton; Dylan McAndrew; Brian Hardham; mlenhart  
**Subject:** FW: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

**EXTERNAL:** Use caution with attachments and links.

SHA's email below confirms no sidewalk will be required.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

Office: [\(410\) 216-3333](tel:4102163333) (Ext. 1)  
Mobile: [\(410\) 980-2367](tel:4109802367)



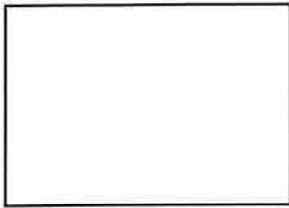
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**From:** Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>  
**Sent:** Friday, June 13, 2025 9:54 AM  
**To:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Cc:** Dylan McAndrew <DMcAndrew@LENHARTTRAFFIC.COM>  
**Subject:** RE: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

Mike,

Agreed. I don't think adding sidewalk here would be necessary because of grades and no continuity.

Thanks, Kwesi



[roads.maryland.gov](http://roads.maryland.gov)

**Kwesi Woodroffe (he/him)**  
*Regional Engineer*  
District 3 Access Management

301.513.7347 office  
[KWoodroffe@mdot.maryland.gov](mailto:KWoodroffe@mdot.maryland.gov)

**Maryland State Highway Administration**  
9300 Kenilworth Ave, Greenbelt, MD 20770

**From:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Sent:** Friday, June 13, 2025 9:24 AM  
**To:** Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>  
**Cc:** Dylan McAndrew <DMcAndrew@LENHARTTRAFFIC.COM>; mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Subject:** RE: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

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Hi Kwesi,

Would also be helpful if you could confirm that you won't require sidewalk along the frontage.

There is no sidewalk anywhere along this section, and as discussed and shown in the email below there is significant grading and environmental conditions that would prohibit the construction of sidewalk.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

Office: [\(410\) 216-3333](tel:4102163333) (Ext. 1)  
Mobile: [\(410\) 980-2367](tel:4109802367)



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**From:** mlenhart <[mlenhart@LENHARTTRAFFIC.COM](mailto:mlenhart@LENHARTTRAFFIC.COM)>  
**Sent:** Friday, June 13, 2025 9:20 AM  
**To:** Kwesi Woodroffe <[KWoodroffe@mdot.maryland.gov](mailto:KWoodroffe@mdot.maryland.gov)>  
**Cc:** mlenhart <[mlenhart@LENHARTTRAFFIC.COM](mailto:mlenhart@LENHARTTRAFFIC.COM)>; Dylan McAndrew <[DMcAndrew@LENHARTTRAFFIC.COM](mailto:DMcAndrew@LENHARTTRAFFIC.COM)>  
**Subject:** DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

Hi Kwesi,

This is a follow up to our meeting this morning. I've attached a pdf of DSP-23026. This is the site along the west side of southbound US 301 just north of the Ourisman Jeep dealership.

The attached DSP shows a potential right turn lane for the site, but after discussing this with you, it was agreed that you would prefer to leave the existing 12' shoulder unstriped for the right turn movement. The right turn volume is projected to be 20 in the AM and 16 in the PM which does not require a right turn lane, and based on Maryland law and the SHA Guidelines the 12' shoulder can be used for right turning vehicles.

Would you like a 12' curb and gutter installed back to the guardrail? I believe this would be reasonable for the frontage channelization.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

Office: [\(410\) 216-3333](tel:(410)216-3333) (Ext. 1)  
Mobile: [\(410\) 980-2367](tel:(410)980-2367)



*The information contained herein is confidential and intended for the exclusive use of the addressee(s). If you are not the intended recipient, you are hereby notified that any review, use, dissemination, distribution or copying of this message is strictly prohibited. If you received this e-mail in error, please notify the sender immediately and delete the message.*

## Christopher Rizzi

---

**From:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Sent:** Friday, June 13, 2025 10:40 AM  
**To:** Robert Antonetti; Joe Heisler; AHSa Shared Calendar; Christopher Rizzi; John Lawall; John Ferrante; David McFadden  
**Cc:** Katherine Duerr; Hannah Haught; Jordan Sexton; Dylan McAndrew; Brian Hardham; mlenhart  
**Subject:** FW: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

**EXTERNAL:** Use caution with attachments and links.

SHA's email below supporting the combination of the two driveways.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

Office: [\(410\) 216-3333](tel:4102163333) (Ext. 1)  
Mobile: [\(410\) 980-2367](tel:4109802367)



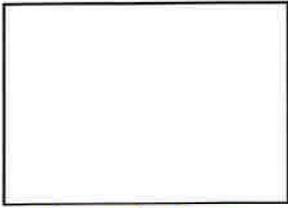
*The information contained herein is confidential and intended for the exclusive use of the addressee(s). If you are not the intended recipient, you are hereby notified that any review, use, dissemination, distribution or copying of this message is strictly prohibited. If you received this e-mail in error, please notify the sender immediately and delete the message.*

**From:** Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>  
**Sent:** Friday, June 13, 2025 10:01 AM  
**To:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Cc:** Dylan McAndrew <DMcAndrew@LENHARTTRAFFIC.COM>  
**Subject:** RE: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

Mike,

I think combining the two driveways would be a good idea.

Thanks, Kwesi



[roads.maryland.gov](http://roads.maryland.gov)

**Kwesi Woodroffe (he/him)**  
*Regional Engineer*  
District 3 Access Management

301.513.7347 office  
[KWoodroffe@mdot.maryland.gov](mailto:KWoodroffe@mdot.maryland.gov)

**Maryland State Highway Administration**  
9300 Kenilworth Ave, Greenbelt, MD 20770

**From:** mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Sent:** Friday, June 13, 2025 9:57 AM  
**To:** Kwesi Woodroffe <KWoodroffe@mdot.maryland.gov>  
**Cc:** Dylan McAndrew <DMcAndrew@LENHARTTRAFFIC.COM>; mlenhart <mlenhart@LENHARTTRAFFIC.COM>  
**Subject:** RE: DSP-23026 Ourisman of Bowie - Can you please reply to confirm our dicussion this morning

**Caution:** Suspicious? Double-check! This email is from an external source. If something seems unusual, even from someone you know, verify directly. Report concerns to [MDOTServiceDesk@mdot.maryland.gov](mailto:MDOTServiceDesk@mdot.maryland.gov) or 410-768-7181.

Thanks Kwesi!

Received both of your emails.

Also, I wanted to add that you were supportive of trying to combine the two driveways, if possible, to better align with the crossover. As stated, we have submitted a signal warrant study for the southbound lanes and combining these driveways would work much better if the signal is approved.

Thanks,  
Mike

**NOTE: We have moved as of May 1<sup>st</sup>. Please note the new address below.**

**Mike Lenhart, P.E., PTOE**  
President

Office: [\(410\) 216-3333](tel:4102163333) (Ext. 1)  
Mobile: [\(410\) 980-2367](tel:4109802367)



STANDARD DRAWING LEGEND FOR ENTIRE PLAN SET																				
LIMIT OF WORK ——— LOW ——— LOW ———																				
LIMIT OF DISTURBANCE ——— LOD ——— LOD ———																				
EXISTING NOTE	TYPICAL NOTE TEXT	PROPOSED NOTE																		
---	ON-SITE PROPERTY LINE / R.O.W. LINE	---																		
---	NEIGHBORING PROPERTY LINE / INTERIOR PARCEL LINE	---																		
---	EASEMENT LINE	---																		
---	SETBACK LINE	---																		
<table border="1"> <tr> <th colspan="2">CURB AND GUTTER</th> </tr> <tr> <td>=====</td> <td>CONCRETE CURB &amp; GUTTER</td> </tr> <tr> <td>=====</td> <td>SPILL TRANSITION</td> </tr> <tr> <td>=====</td> <td>DEPRESSED CURB AND GUTTER</td> </tr> </table>			CURB AND GUTTER		=====	CONCRETE CURB & GUTTER	=====	SPILL TRANSITION	=====	DEPRESSED CURB AND GUTTER										
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=====	CONCRETE CURB & GUTTER																			
=====	SPILL TRANSITION																			
=====	DEPRESSED CURB AND GUTTER																			
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---	POLE LIGHT	---																		
---	TRAFFIC LIGHT	---																		
---	UTILITY POLE	---																		
---	TYPICAL LIGHT	---																		
---	ACORN LIGHT	---																		
---	TYPICAL SIGN	---																		
---	PARKING COUNTS	---																		
<table border="1"> <tr> <th colspan="2">CONTOUR LINE</th> </tr> <tr> <td>---</td> <td>170</td> </tr> <tr> <td>---</td> <td>187</td> </tr> <tr> <td>---</td> <td>190</td> </tr> <tr> <th colspan="2">SPOT ELEVATIONS</th> </tr> <tr> <td>---</td> <td>TC 516.00</td> </tr> <tr> <td>---</td> <td>TC 516.00</td> </tr> <tr> <td>---</td> <td>BC 515.55</td> </tr> <tr> <td>---</td> <td>MATCHED 515.02 4)</td> </tr> </table>			CONTOUR LINE		---	170	---	187	---	190	SPOT ELEVATIONS		---	TC 516.00	---	TC 516.00	---	BC 515.55	---	MATCHED 515.02 4)
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---	MONITORING WELL	---																		
---	TEST PIT	---																		
---	BENCHMARK	---																		
---	BORING	---																		

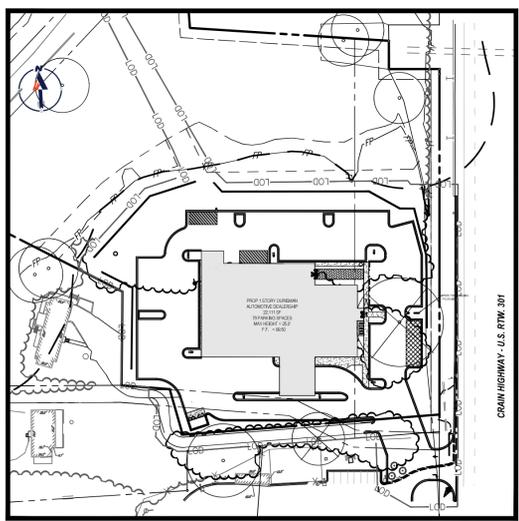
STANDARD ABBREVIATIONS FOR ENTIRE PLAN SET	
AC	ACRES
ADA	AMERICANS WITH DISABILITY ACT
ARCH	ARCHITECTURAL
BC	BOTTOM OF CURB
BF	BASEMENT FLOOR
BK	BLOCK
BL	BASELINE
BLDG	BUILDING
BM	BUILDING BENCHMARK
BRL	BUILDING RESTRICTION LINE
CF	CUBIC FEET
CL	CENTERLINE
CMP	CORRUGATED METAL PIPE
CONN	CONNECTION
CONC	CONCRETE
CPP	CORRUGATED PLASTIC PIPE
CY	CUBIC YARDS
DEC	DECORATIVE
DEP	DEPRESSED
DIP	DUCTILE IRON PIPE
DOM	DOMESTIC
ELEC	ELECTRIC
ELEV	ELEVATION
EP	EDGE OF PAVEMENT
ES	EDGE OF SHOULDER
EW	END WALL
EX	EXISTING
FES	FLARED END SECTION
FF	FINISHED FLOOR
FH	FIRE HYDRANT
FG	FINISHED GRADE
G	GRADE
GF	GARAGE FLOOR (AT DOOR)
GH	GRADE HIGHER SIDE OF WALL
GL	GRADE LOWER SIDE OF WALL
GRT	GRATE
GV	GATE VALVE
HDPE	HIGH DENSITY POLYETHYLENE PIPE
HP	HIGH POINT
HOR	HORIZONTAL
HW	HEADWALL
INT	INTERSECTION
INV	INVERT
LF	LINEAR FOOT
LOC	LIMITS OF CLEARING
LOD	LIMITS OF DISTURBANCE
LOS	LINE OF SIGHT
LP	LOW POINT
L/S	LANDSCAPE
MAX	MAXIMUM
MIN	MINIMUM
MH	MANHOLE
MJ	MECHANICAL JOINT
OC	ON CENTER
PA	POINT OF ANALYSIS
PC	POINT CURVATURE
PCCR	POINT OF COMPOUND CURVATURE, CURB RETURN
PI	POINT OF INTERSECTION
POG	POINT OF GRADE
PROP	PROPOSED
PT	POINT OF TANGENCY
PTCR	POINT OF TANGENCY, CURB RETURN
PVC	POLYVINYL CHLORIDE PIPE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
R	RADIUS
RCP	REINFORCED CONCRETE PIPE
RET WALL	RETAINING WALL
RAW	RIGHT OF WAY
S	SLOPE
SAN	SANITARY SEWER
SF	SQUARE FEET
STA	STATION
STM	STORM
SW	SIDEWALK
TBR	TO BE REMOVED
TBRL	TO BE RELOCATED
TC	TOP OF CURB
TELE	TELEPHONE
TPF	TREE PROTECTION FENCE
TW	TOP OF WALL
TYP	TYPICAL
UG	UNDERGROUND
UP	UTILITY POLE
W	WIDE
WL	WATER LINE
WM	WATER METER
±	PLUS OR MINUS
°	DEGREE
∅	DIAMETER
#	NUMBER

- GENERAL NOTES:**
- PROJECT NAME: OURISMAN DEALERSHIP OF BOWIE
  - TOTAL ACREAGE: 6.294 AC.
  - EXISTING ZONING: C-S (COMMERCIAL, SERVICE) PREVIOUS ZONING: C-M (COMMERCIAL, MISCELLANEOUS)
  - EXISTING USE: UNDEVELOPED PROPOSED USE: AUTOMOTIVE DEALERSHIP
  - NUMBER OF LOTS, PARCELS, OUTLOTS & OUTPARCELS: LOT 1
  - PROPOSED DWELLING UNITS: NONE
  - EXISTING GROSS FLOOR AREA: UNDEVELOPED PROPOSED GROSS FLOOR AREA: 15,464 GFA
  - WSSC GRID: 204NE14
  - TAX MAP & GRID: TM 63 GRID D2
  - AVIATION POLICY NUMBER AND GRID: NONE
  - EXISTING WATER/SEWER DESIGNATION: W-3 / S-3 PROPOSED WATER/SEWER DESIGNATION: W-3 / S-3
  - STORMWATER MANAGEMENT CONCEPT NUMBER: SWMC #02-1223-204NE14
  - PRIOR DEVELOPMENT APPROVAL: PPS-4-04047
  - 10-FOOT PUBLIC UTILITY EASEMENT ALONG CRAIN HWY.
  - MANDATORY PARK DEDICATION: NONE
  - CEMETERIES LOCATED IN VICINITY OF THE PROPERTY: NONE
  - HISTORIC SITES LOCATED IN VICINITY OF THE PROPERTY: NONE
  - STREAMS AND WETLANDS: YES
  - 100-YEAR FLOODPLAIN: YES
  - CHESAPEAKE CRITICAL BAY AREA: NO
  - THE SITE IS NOT LOCATED WITHIN A STRONGHOLD WATERSHED AS ESTABLISHED BY THE MARYLAND DEPARTMENT OF NATURAL RESOURCES.
  - THE SITE IS LOCATED IN THE PATUXENT RIVER UPPER WATERSHED.
  - THIS SITE IS WITHIN THE ENVIRONMENTAL STRATEGY AREA (ESA) 2.
  - SOURCE OF TOPOGRAPHY: BOHLER "BOUNDARY & TOPOGRAPHIC SURVEY OURISMAN GENESIS-BOWIE" FILE NO.: MDB230009.00 DATED: 04/10/23 FIELD DATE: 03/15/23

# DETAILED SITE PLAN DSP-23026

## FOR OURISMAN OF BOWIE

LOCATION OF SITE  
2550 NW ROBERT CRAIN HWY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY  
SUBDIVISION: ROUTE 301 REALTY CORP  
PLAT 212011; LOT 1  
LIBER 48118 FOLIO 021



LOCATION MAP  
SCALE: 1" = 100'

**OWNER**  
XAVIER CAMACHO  
ROWHIT BOWIE LLC  
807 ROCKVILLE PIKE  
ROCKVILLE MD 20852

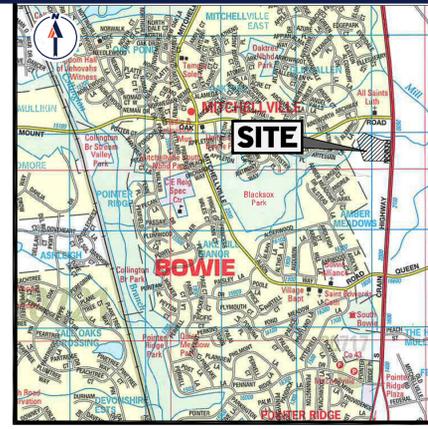
**APPLICANT**  
JOSEPH L. HEISLER III  
AMMON HEISLER SACHS ARCHITECTS, PC  
300 WEST PRATT ST., SUITE 275  
BALTIMORE, MD 21201

PREPARED BY



CONTACT: CHRISTOPHER RIZZI; CRIZZI@BOHLERENG.COM

WEB SOIL SURVEY SOILS TABLE PRINCE GEORGE'S COUNTY, MARYLAND			
MAP UNIT	MAP UNIT NAME	HYDROLOGIC GROUP	DRAINAGE CLASS
AB	Annapolis fine sandy loam, 2 to 5 percent slopes	C	WELL DRAINED
AC	Annapolis fine sandy loam, 5 to 10 percent slopes	C	WELL DRAINED
AD	Annapolis fine sandy loam, 10 to 15 percent slopes	C	WELL DRAINED
AD	Annapolis-Urban land complex, 0 to 5 percent slopes	C	WELL DRAINED



VICINITY MAP  
SCALE: 1" = 2000'

- REFERENCES**
- ALTA/ACSM LAND TITLE SURVEY: BOHLER "BOUNDARY & TOPOGRAPHIC SURVEY OURISMAN GENESIS BOWIE" FILE NO.: MDB230009.00 DATED: 04/10/23 FIELD DATE: 03/15/23
  - SOIL REPORT NRC'S ENTITLED: "CUSTOM SOIL RESOURCE REPORT FOR PRINCE GEORGE'S COUNTY, MARYLAND" DATED: JANUARY 26, 2023
  - STORM WATER MANAGEMENT CONCEPT BOHLER "GENESIS AT BOWIE 2550 NW ROBERT CRAIN HIGHWAY BOWIE, MD 20716 PRICE GEORGE'S COUNTY" DATED: 12/15/23 APPROVED: 12/21/23
  - NATURAL RESOURCE INVENTORY PLAN WSSI "NATURAL RESOURCE INVENTORY PLAN FOR GENESIS AT BOWIE" DATED: 2/12/24 APPROVED: TBD

\* THE ABOVE REFERENCED DOCUMENTS ARE INCORPORATED BY REFERENCE AS PART OF THESE PLANS. HOWEVER, BOHLER ENGINEERING DOES NOT CERTIFY THE ACCURACY OF THE WORK REFERENCED OR DERIVED FROM THESE DOCUMENTS, BY OTHERS.

**CONTACTS**

UTILITIES: THE FOLLOWING COMPANIES WERE NOTIFIED BY MARYLAND MISS UTILITY SYSTEM (1-800-257-7777) AND REQUESTED TO MARK OUT UNDERGROUND FACILITIES AFFECTING AND SERVICING THIS SITE. THE UNDERGROUND UTILITY INFORMATION SHOWN HEREON IS BASED UPON THE UTILITY COMPANIES RESPONSE TO THIS REQUEST. SERIAL NUMBER(S): 21251355

UTILITY COMPANY	LOCATE STATUS	PHONE NUMBER
BGE ELECTRIC	48 HR DELAY	(410) 536-0070
BGE GAS	48 HR DELAY	(410) 536-0070
CITY OF BOWIE	CLEAR/NO CONFLICT	(301) 672-8520
COMCAST	NOT COMPLETE	(301) 210-0355
PG & GVT	CLEAR/NO CONFLICT	(801) 364-1063
VERIZON	NOT COMPLETE	(301) 210-0355
WASHINGTON GAS	NOT COMPLETE	(301) 210-0355
WSSC	NOT COMPLETE	(301) 868-6803

\* THE ABOVE REFERENCED DOCUMENTS ARE INCORPORATED BY REFERENCE AS PART OF THESE PLANS. HOWEVER, BOHLER ENGINEERING DOES NOT CERTIFY THE ACCURACY OF THE WORK REFERENCED OR DERIVED FROM THESE DOCUMENTS, BY OTHERS.

**SHEET INDEX**

SHEET TITLE	SHEET NUMBER
COVER SHEET	DSP-1
APPROVALS SHEET	DSP-2
EXISTING CONDITIONS/DEMOLITION PLAN	DSP-3
SITE LAYOUT PLAN	DSP-4
GRADING/STORMDRAIN AND PAVING PLAN	DSP-5
SITE DETAILS	DSP-6
LANDSCAPE PLAN	DSP-7 - DSP-8
LANDSCAPE NOTES AND DETAILS	DSP-9
LIGHTING PLAN	DSP-10
CIRCULATION PLAN	DSP-11
BY OTHERS:	
ARCHITECTURAL ELEVATIONS	DSP-12
SIGN DETAILS	DSP-13

**BOHLER**  
SITE CIVIL AND CONSULTING ENGINEERING  
PROGRAM MANAGEMENT  
LANDSCAPE ARCHITECTURE  
SUSTAINABLE DESIGN  
PERMITTING SERVICES  
TRANSPORTATION SERVICES

**REVISIONS**

REV	DATE	COMMENT	DRAWN BY	CHECKED BY

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Know what's below.  
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ALWAYS CALL 811  
It's fast. It's free. It's the law.

NOT APPROVED FOR CONSTRUCTION

PROJECT No.: MDB230009.00  
DRAWN BY: HS  
CHECKED BY: JD  
DATE: 05/09/2025  
CAD LID: CNDS-0

**DETAILED SITE PLAN**  
FOR  
OURISMAN OF BOWIE

PROPOSED DEVELOPMENT

2550 NW ROBERT CRAIN HWY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY  
PLAT 212011, LOT 1  
LIBER 48118 FOLIO 021

**BOHLER**  
16701 MELFORD BLVD., SUITE 430  
BOWIE, MARYLAND 20715  
Phone: (301) 809-4500  
Fax: (301) 809-4501  
MD@BohlerEng.com

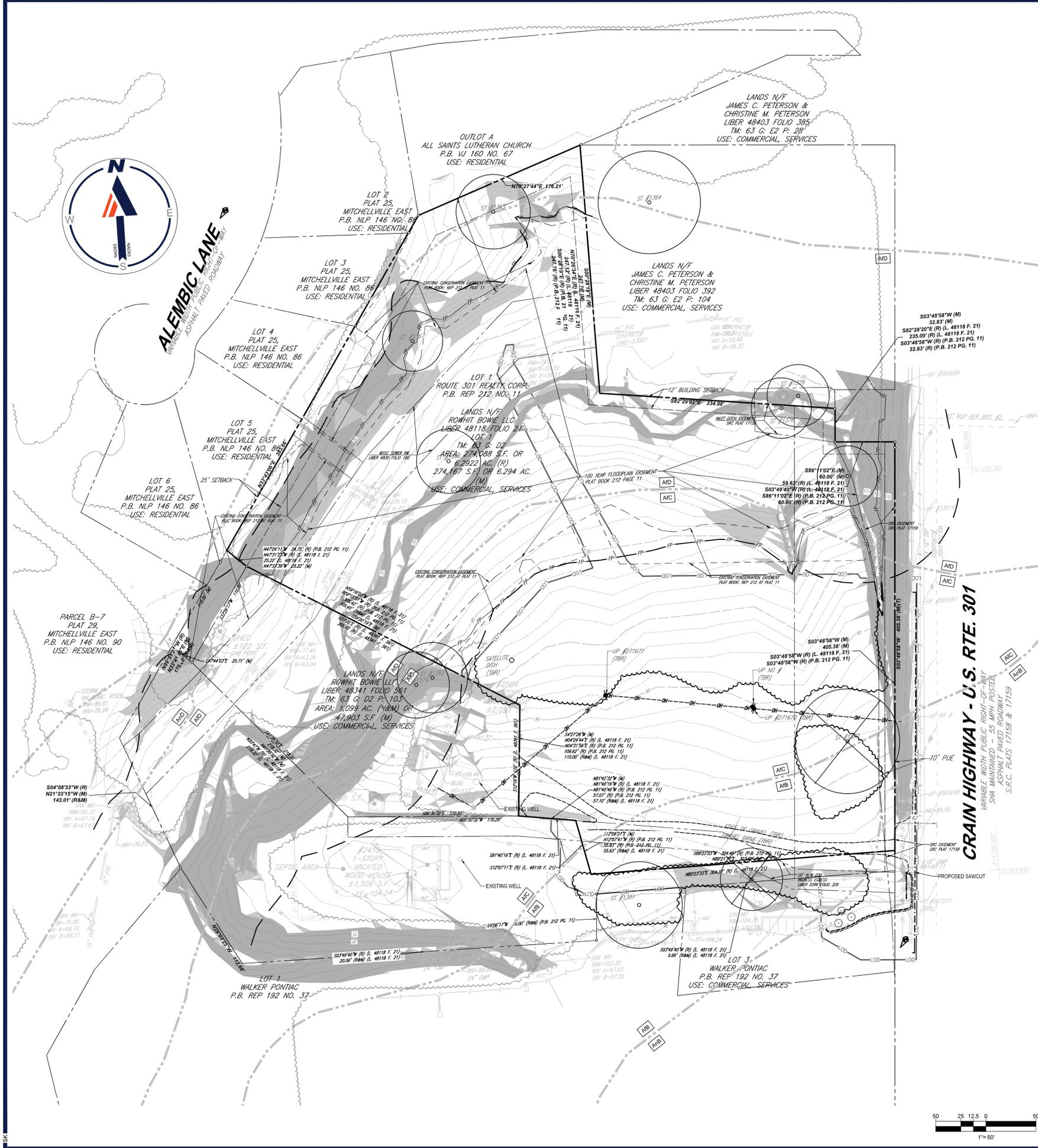
**J. DIMARCO**  
PROFESSIONAL ENGINEER  
PROFESSIONAL CERTIFICATION  
JOSEPH DIMARCO, HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. 34390, EXPIRATION DATE: 12/23/2025

SHEET TITLE:  
**COVER SHEET**

SHEET NUMBER:  
**DSP-1**

ORG. DATE - 09/13/2023





DEMOLITION/REMOVAL LEGEND	
DEMOLITION/REMOVAL NOTE	TYPICAL NOTE TEXT
	EASEMENT LINE
	CONCRETE CURB & GUTTER
	UTILITY POLE WITH LIGHT
	POLE LIGHT
	TRAFFIC LIGHT
	UTILITY POLE
	TYPICAL LIGHT
	ACORN LIGHT
	TYPICAL SIGN
	PARKING COUNTS
	SPOT ELEVATIONS
	SANITARY LABEL
	STORM LABEL
	SANITARY SEWER LATERAL

DEMOLITION/REMOVAL LEGEND	
DEMOLITION/REMOVAL NOTE	TYPICAL NOTE TEXT
	UNDERGROUND WATER LINE
	UNDERGROUND ELECTRIC LINE
	UNDERGROUND GAS LINE
	OVERHEAD WIRE
	UNDERGROUND TELEPHONE LINE
	UNDERGROUND CABLE LINE
	STORM SEWER
	SANITARY SEWER MAIN
	HYDRANT
	SANITARY MANHOLE
	STORM MANHOLE
	WATER METER
	WATER VALVE
	GAS VALVE
	GAS METER

LEGEND	
SAWCUT	
STEEP SLOPES 15 - 25%	
STEEP SLOPES > 25%	
SOIL DIVIDE	

**BOHLER**  
 SITE CIVIL AND CONSULTING ENGINEERING  
 PROGRAM MANAGEMENT  
 LANDSCAPE ARCHITECTURE  
 SUSTAINABLE DESIGN  
 PERMITTING SERVICES  
 TRANSPORTATION SERVICES

REVISIONS			
REV	DATE	COMMENT	DRAWN BY

**811**  
 Know what's below.  
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**NOT APPROVED FOR CONSTRUCTION**

THIS DRAWING IS INTENDED FOR MUNICIPAL AND/OR AGENCY REVIEW AND APPROVAL. IT IS NOT INTENDED AS A CONSTRUCTION DOCUMENT UNLESS INDICATED OTHERWISE.

PROJECT No.: MDE230009.00  
 DRAWN BY: HS  
 CHECKED BY: JD  
 DATE: 05/02/2025  
 CAD ID: DEMO-0

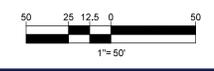
PROJECT:  
**DETAILED SITE PLAN**  
 FOR

OURISMAN OF BOWIE  
 PROPOSED DEVELOPMENT  
 2550 NW ROBERT CRAIN HWY  
 BOWIE, MD 20716  
 PRINCE GEORGE'S COUNTY  
 PLAT 212011, LOT 1  
 LIBER 48118 FOLIO 021

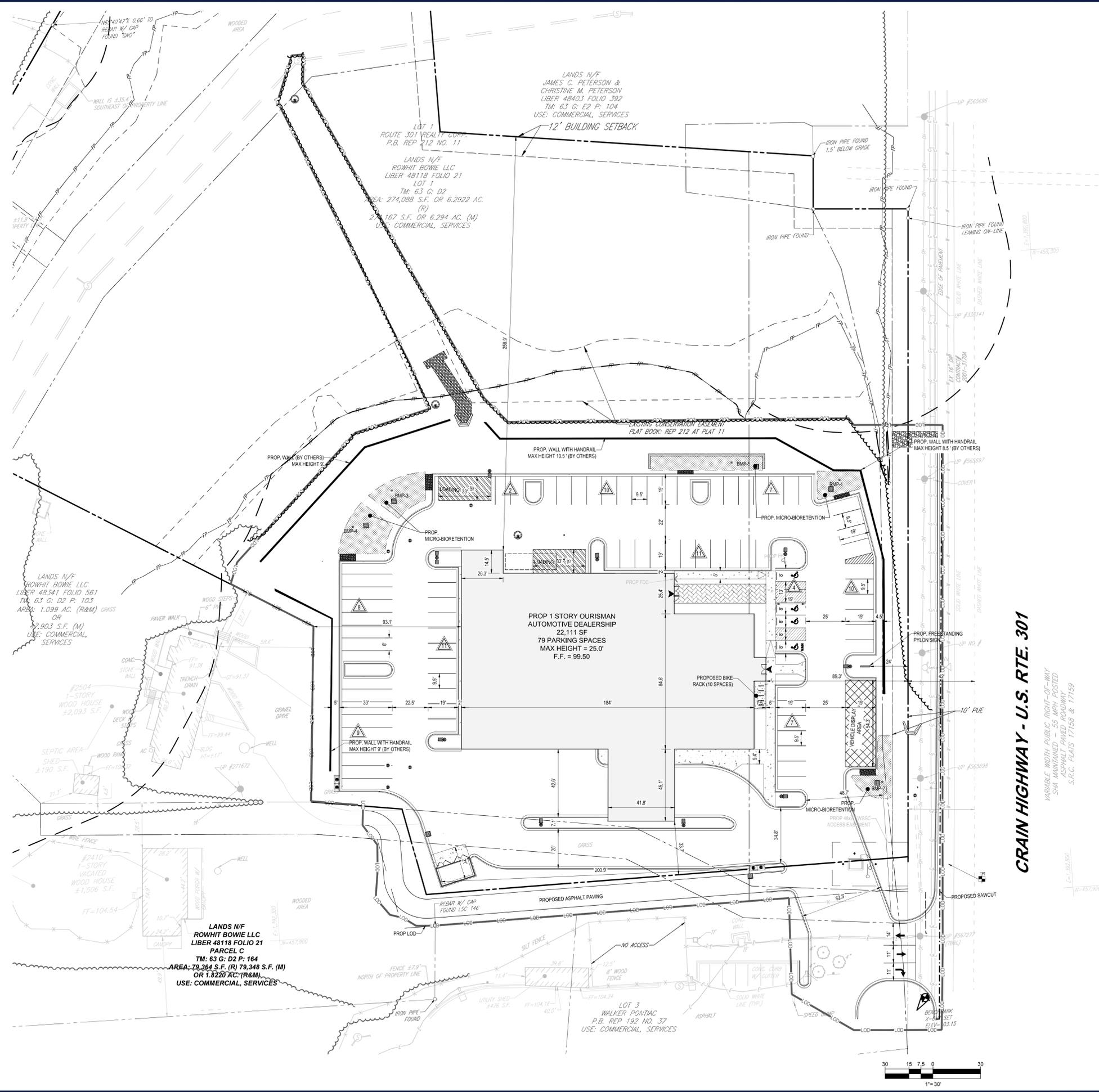
**BOHLER**  
 16701 MELFORD BLVD., SUITE 430  
 BOWIE, MARYLAND 20715  
 Phone: (301) 809-4500  
 Fax: (301) 809-4501  
 MD@BohlerEng.com

**J. DIMARCO**  
 PROFESSIONAL ENGINEER  
 PROFESSIONAL CERTIFICATION  
 JOSEPH DIMARCO, HEREBY CERTIFY THAT  
 THESE DOCUMENTS WERE PREPARED OR  
 APPROVED BY ME, AND THAT I AM A DAILY  
 LICENSED PROFESSIONAL ENGINEER UNDER THE  
 LAWS OF THE STATE OF MARYLAND.  
 LICENSE NO. 34390, EXPIRATION DATE: 12/23/2026

SHEET TITLE:  
**EXISTING CONDITIONS/ DEMOLITION PLAN**  
 SHEET NUMBER:  
**DSP-3**  
 ORG. DATE - 09/13/2023



8/6/12, 2025  
 H:\2025\MD\230009.00\CAD\DWG\SP\PLAN SETS\DETAILED SITE PLAN\MD230009-01-DEM-0-1-JAVOUT\_DSP-3-EXISTING & DEMO



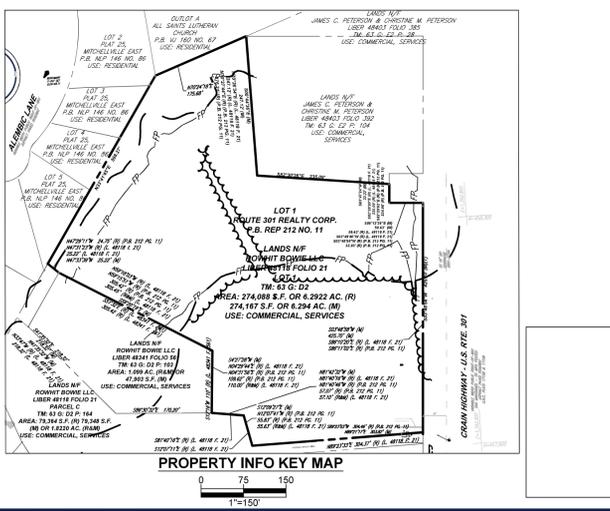
### LEGEND

LIMIT OF DISTURBANCE	
SAWCUT	
RETAINING WALL	
CONCRETE PAVEMENT	
MICRO-BIO RETENTION	

### STANDARD NOTES:

	ALLOWED/REQUIRED	PROVIDED
<b>A. BULK REQUIREMENTS:</b>		
A. MIN. LOT AREA	N/A	6.29 AC
<b>B. MIN. BUILDING SETBACK</b>		
FRONT SETBACK (CRAIN HIGHWAY)	10'	89.3'
REAR SETBACK	25'	93.1'
SIDE SETBACK (NORTH)	12'	258.9'
SIDE SETBACK (SOUTH)	NONE	33.7'
<b>C. MIN. LANDSCAPE SETBACK</b>		
FRONT SETBACK	10'	20.3'
REAR SETBACK	30'	16.1'
SIDE SETBACK (NORTH)	NONE	165.6'
SIDE SETBACK (SOUTH)	NONE	4.9'
<b>D. PARKING REQUIREMENTS</b>		
VEHICLE INDOOR SALES 1 SPACES PER 500 S.F. OF GROSS FLOOR AREA OF BUILDING = (15,464 S.F. / 500 S.F.) * 1 SPACE = 31 SPACES	31	32
VEHICLE OUTDOOR SALES 1 SPACES PER 1,000 S.F. OF GROSS OUTDOOR DISPLAY AREA = (1,032 S.F. / 1,000 S.F.) * 1 SPACE = 2 SPACES	2	2
VEHICLE SERVICE BAY 3 SPACES PER SERVICE BAY = (15) * 3 SPACES = 45 SPACES	45	45
<b>TOTAL</b>	<b>78</b>	<b>79</b>
<b>BICYCLE PARKING - 2 SPACES PER FIRST 10 PARKING SPACES + 1 SPACE PER EACH ADDITIONAL 10 SPACES</b>	9	10
<b>ADA - 4 SPACES PER 76 TO 100 SPACES = 4 SPACES (1/8 VAN ACCESSIBLE)</b>	4	4
<b>COMPACT SPACES 1/3 PER TOTAL SPACES = 83 X 1/3 = 27 SPACES</b>	27	17
<b>E. LOADING SPACE REQUIREMENTS:</b>		
RETAIL SALES AND SERVICE 1 SPACE UP TO 10,000 SF OF GFA = 1 +1 SPACE FOR EACH ADDITIONAL 10,000-100,000 SF OF GFA = 1	2 (15X33)	2 (15X33)
<b>F. MIN. PARKING SPACE DIMENSIONS</b>		
STANDARD PARKING SPACE	9.5'X19'	9.5'X19'
ADA ACCESSIBLE PARKING SPACE	8'X16.5'	8'X16.5'
COMPACT PARKING SPACE	8'X16.5'	8'X16.5'
<b>G. MAX. BUILDING HEIGHT</b>		
CAR DEALERSHIP	N/A	25'
<b>H. GREEN AREA CALCULATIONS</b>		
SITE AREA = 6.29 AC		
GREEN AREA = 4.53 AC (72.0%)		
<b>2. NEAREST INTERSECTING STREET - CRAIN HIGHWAY AND MITCHELLVILLE ROAD.</b>		
<b>3. PAVEMENT STRIPING TO BE 4" WHITE MDSHA STANDARD.</b>		
<b>4. PUBLIC WATER AND SEWER, STORMDRAIN, GAS, ELECTRIC AND TELEPHONE UTILITIES ARE AVAILABLE TO THE SITE.</b>		
<b>5. COMPANION APPLICATIONS:</b>		
-NRI	APPROVAL DATE: 08/18/24	APPROVAL NO. NRI-081-2024
-SVMC	APPROVAL DATE: 12/26/23	APPROVAL NO. 02-1223-204NE14
<b>6. THERE IS NO MANDATORY PARK DEDICATION REQUIRED FOR THIS SITE.</b>		
<b>7. SIGN REQUIREMENTS</b>		
<b>A. BUILDING SIGN AREAS</b>		
	SEE SHEET DSP-13	SEE SHEET DSP-13
<b>B. FREESTANDING SIGN</b>		
SIGNS SHALL ONLY BE LOCATED ON PROPERTY WHERE THE MAIN BUILDING ASSOCIATED WITH THE SIGN IS LOCATED AT LEAST TEN (10) FEET BEHIND THE FRONT STREET LINE	1	1

	ALLOWED/REQUIRED	PROVIDED
<b>A. BUILDING SIGN AREAS</b>		
	SEE SHEET DSP-13	SEE SHEET DSP-13
<b>B. FREESTANDING SIGN</b>		
SIGNS SHALL ONLY BE LOCATED ON PROPERTY WHERE THE MAIN BUILDING ASSOCIATED WITH THE SIGN IS LOCATED AT LEAST TEN (10) FEET BEHIND THE FRONT STREET LINE	1	1



**CRAIN HIGHWAY - U.S. RTE. 301**

VARIABLE WIDTH PUBLIC RIGHT-OF-WAY  
SHA MAINTAINED - 55 MPH POSTED  
ASPHALT PAVED ROADWAY  
S.R.C. PLANS 17158 & 17159

**BOHLER**  
SITE CIVIL AND CONSULTING ENGINEERING  
PROGRAM MANAGEMENT  
LANDSCAPE ARCHITECTURE  
SUSTAINABLE DESIGN  
PERMITTING SERVICES  
TRANSPORTATION SERVICES

### REVISIONS

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PROJECT No.:	MD8230009.00	HS
DRAWN BY:		JD
DATE:	05/30/2025	
CAD LID:		SITE-0

**DETAILED SITE PLAN**

**OURISMAN OF BOWIE**

PROPOSED DEVELOPMENT

2550 NW ROBERT CRAIN HWY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY  
PLAT 212011, LOT 1  
LIBER 48118 FOLIO 021

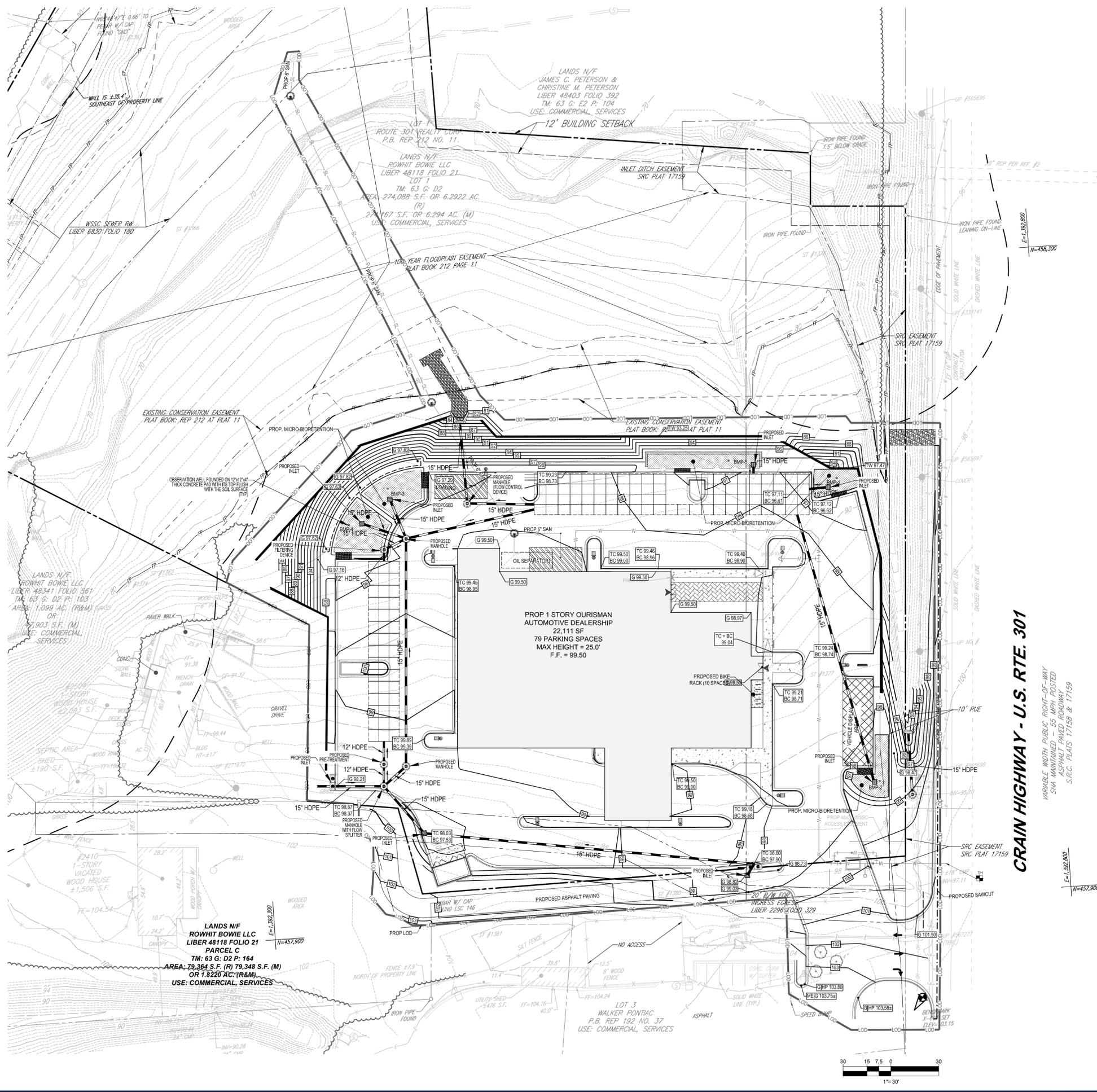
**BOHLER**  
16701 MELFORD BLVD., SUITE 430  
BOWIE, MARYLAND 20715  
Phone: (301) 809-4500  
Fax: (301) 809-4501  
MD@BohlerEng.com

**J. DIMARCO**  
PROFESSIONAL ENGINEER  
PROFESSIONAL CERTIFICATION  
JOSEPH DIMARCO, hereby certifies that  
THESE DOCUMENTS WERE PREPARED OR  
APPROVED BY ME, AND THAT I AM A DULY  
LICENSED PROFESSIONAL ENGINEER UNDER THE  
LAWS OF THE STATE OF MARYLAND.  
LICENSE NO. 34390, EXPIRATION DATE: 12/23/2026

**SITE LAYOUT PLAN**

SHEET NUMBER:  
**DSP-4**

ORG. DATE - 09/13/2023



**LEGEND**

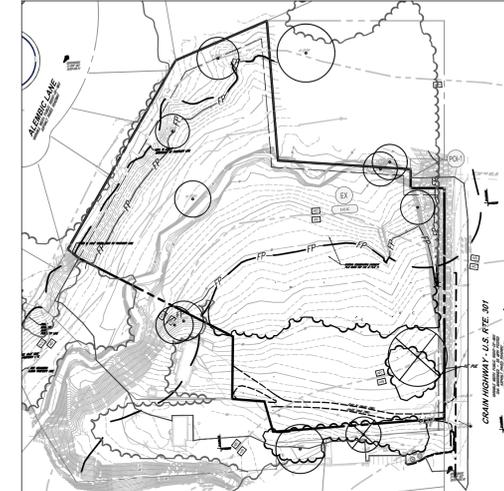
LIMIT OF DISTURBANCE	— LOD — LOD —
SAWCUT	---
CONCRETE PAVEMENT	[Pattern]
MICRO-BIO RETENTION	[Pattern]
PROPOSED DRAINAGE DIVIDE	—
DRAINAGE AREA LABEL	DA-X
POINT OF INTEREST LABEL	POI-X

**PRE-DEVELOPMENT DRAINAGE AREA TABLE**

DRAINAGE AREA	AREA (AC.)	IMPERVIOUS AREA (AC.)	C VALUE
EX-1	8.48	0.11	74

**10-YR PRE-DEVELOPMENT POI ANALYSIS**

LINE/POINT OF INTEREST	PRE DEVELOPMENT FLOW
POI-1	22.53



PRE-DEVELOPMENT DRAINAGE AREAS  
SCALE: 1"=150'

**POST-DEVELOPMENT DRAINAGE AREA TABLE**

DRAINAGE AREA	AREA (AC.)	IMPERVIOUS AREA (AC.)	C VALUE
ONSITE	2.42	1.92	94
BYPASS	6.06	0.37	75

**10-YR POST-DEVELOPMENT POI ANALYSIS**

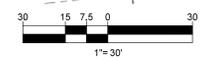
LINE/POINT OF INTEREST	POST DEVELOPMENT FLOW
POI-1	18.27 CFS



POST-DEVELOPMENT DRAINAGE AREAS  
SCALE: 1"=150'

CRAIN HIGHWAY - U.S. RTE. 301

VARIABLE WIDTH PUBLIC RIGHT-OF-WAY  
SHA MAINTAINED - 55 MPH POSTED  
ASPHALT PAVED ROADWAY  
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SITE CIVIL AND CONSULTING ENGINEERING  
PROGRAM MANAGEMENT  
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PROJECT No.: MD23009.00  
DRAWN BY: HS  
CHECKED BY: JD  
DATE: 05/02/2025  
CAD ID: GRAD-0

**DETAILED SITE PLAN**

**OURISMAN OF BOWIE**

PROPOSED DEVELOPMENT  
2550 NW ROBERT CRAIN HWY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY  
PLAT 212011, LOT 1  
LIBER 48118 FOLIO 021

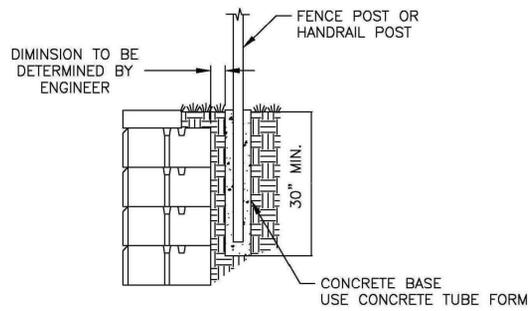
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Phone: (301) 809-4500  
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LICENSED PROFESSIONAL ENGINEER UNDER THE  
LAWS OF THE STATE OF MARYLAND.  
LICENSE NO. 34390, EXPIRATION DATE: 12/23/2025

SHEET TITLE  
**GRADING/  
STORMDRAIN  
AND PAVING  
PLAN**

SHEET NUMBER  
**DSP-5**

ORG. DATE - 09/13/2023



POST BEHIND WALL DETAIL

SCALE: NONE

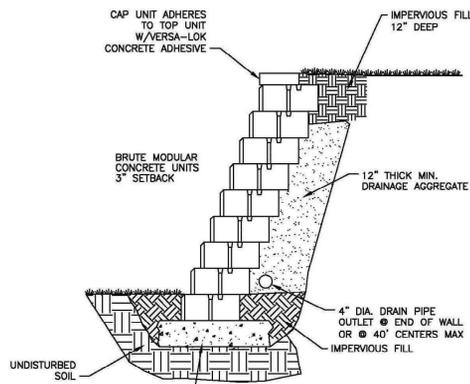
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BRUTE DETAILS

POST BEHIND WALL



TYPICAL SECTION—UNREINFORCED RETAINING WALL

SCALE: NONE

NOT FOR CONSTRUCTION

OR APPROVED EQUIVALENT

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BRUTE DETAILS

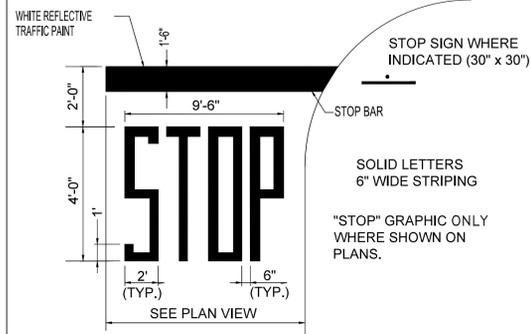
UNREINFORCED RETAINING WALL

NOTES:

REFER TO RETAINING WALL DESIGNER AND STRUCTURAL ENGINEER FOR FINAL DESIGN PLANS. THE CONSULTANT DESIGNING THE RETAINING WALL SHALL USE THIS DETAIL AS A GUIDELINE. BOHLER TO BE HELD HARMLESS IN THE EVENT OF ERRONEOUS INFORMATION CONTAINED WITHIN THESE DETAILS AND/OR STRUCTURE FAILURE.

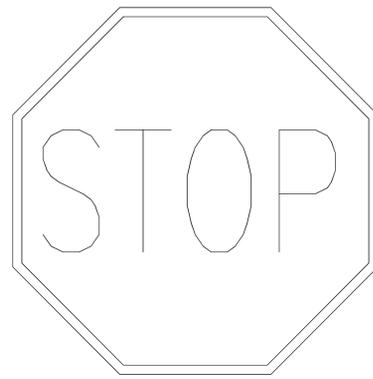
RETAINING WALL DETAIL

NOT TO SCALE



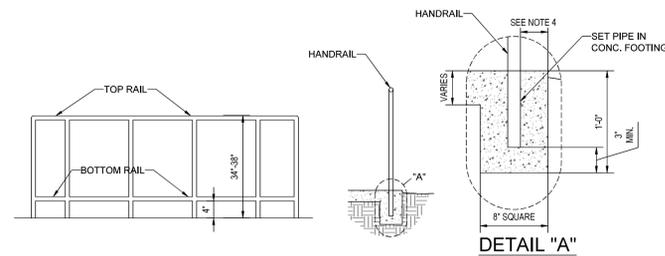
STANDARD STOP LINE GRAPHIC DETAIL

NOT TO SCALE



"STOP SIGN" DETAIL 'R1-1'

NOT TO SCALE



ELEVATION

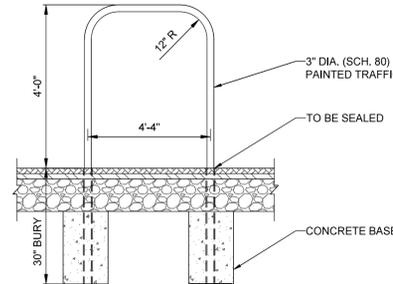
DETAIL "A"

NOTES:

- HANDRAILS WITH CIRCULAR CROSS SECTIONS SHALL BE 1.25" TO 2" OUTSIDE DIAMETER.
- NON-CIRCULAR CROSS SECTIONS SHALL HAVE A PERIMETER OF 4" TO 6.25" AND A MAXIMUM CROSS SECTION DIMENSION OF 2.25". (SEE ADA GUIDELINES)
- SIDEWALK TO EXTEND 4' BEYOND OUTSIDE OF RAILING WHEN A BOTTOM RAIL IS PROVIDED OR 12' BEYOND WHEN NO BOTTOM RAIL IS PROVIDED.
- FINAL DESIGN FOR ALL STRUCTURAL COMPONENTS TO BE PROVIDED BY STRUCTURAL ENGINEER.

TYPICAL HANDRAIL DETAIL

NOT TO SCALE

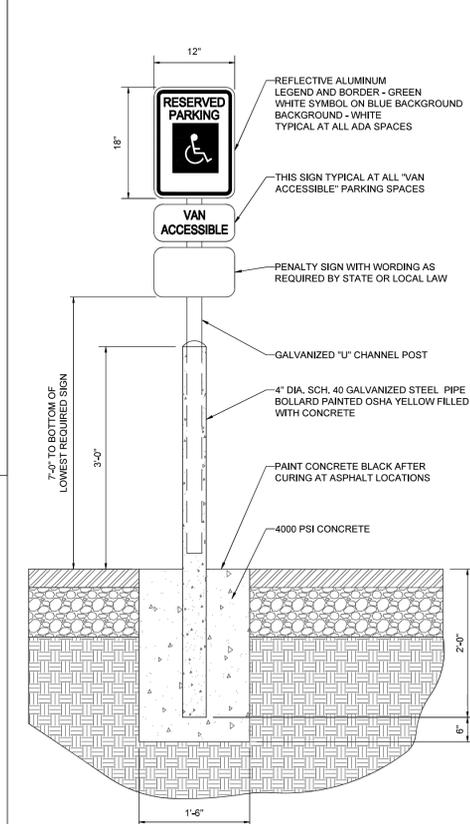


NOTE:

- U SHAPED BOLLARD TO BE USED AS BIKE RACK

'U' SHAPED BOLLARD DETAIL

NOT TO SCALE OR APPROVED EQUIVALENT

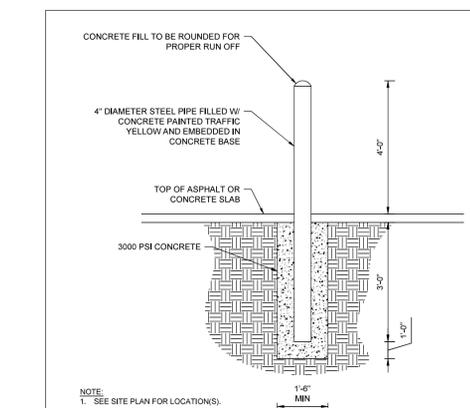


NOTE:

- ONE AT EACH ADA SPACE, WHERE ADA SPACES FACE EACH OTHER WITHOUT WALKWAY, THERE SHALL BE ONE POST WITH SIGNS MOUNTED BOTH SIDES

BOLLARD MOUNTED ADA PARKING SIGN DETAIL

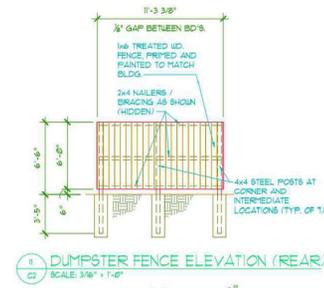
NOT TO SCALE



NOTE:  
1. SEE SITE PLAN FOR LOCATIONS.

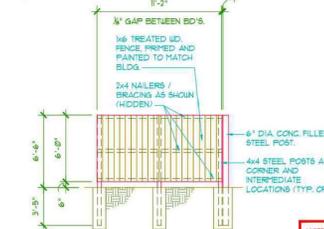
PIPE BOLLARD

NOT TO SCALE (BE-S07103 - 01/2025)



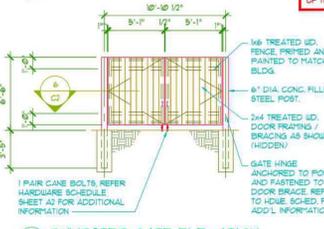
DUMPSTER FENCE ELEVATION (REAR)  
SCALE 3/8" = 1'-0"

NOTE TO DESIGNER:  
ONLY INSTALL DUMPSTER ENCLOSURE IF REQUIRED BY LOCAL MUNICIPALITY OR LANDLORD.



DUMPSTER FENCE ELEVATION  
SCALE 3/8" = 1'-0"

NOTE TO DESIGNER:  
COORDINATE HEIGHT OF DUMPSTER BEING USED WITH A.A.P. TO ENSURE FENCE ENTRIES SLOWS THE VIEW OF THE DUMPSTER.



DUMPSTER GATE ELEVATION  
SCALE 3/8" = 1'-0"

NOTE TO DESIGNER:  
PAIR CANE BOLTS REFER HARDWARE SCHEDULE SHEET A2 FOR ADDITIONAL INFORMATION.



TYPICAL PAD LAYOUT  
SCALE 3/8" = 1'-0"

NOTE TO DESIGNER:  
CONTRACTOR TO ALWAYS MAINTAIN MINIMUM INSIDE CLEAR DIMENSIONS FOR DUMPSTER REGARDLESS OF THE MATERIAL USED FOR ENCLOSURE.

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PROJECT No.: MD8230009.00  
DRAWN BY: HS  
DATE: 05/03/2025  
CAD ID: CND5-0

PROJECT:

DETAILED SITE PLAN

FOR

OURISMAN OF BOWIE

PROPOSED DEVELOPMENT

2550 NW ROBERT CRAIN HWY  
BOWIE, MD 20716

PRINCE GEORGE'S COUNTY  
PLAT 212011, LOT 1  
LIBER 48118 FOLIO 021

**BOHLER**

16701 MELFORD BLVD., SUITE 430  
BOWIE, MARYLAND 20715  
Phone: (301) 809-4500  
Fax: (301) 809-4501  
MD@BohlerEng.com

**J. DIMARCO**

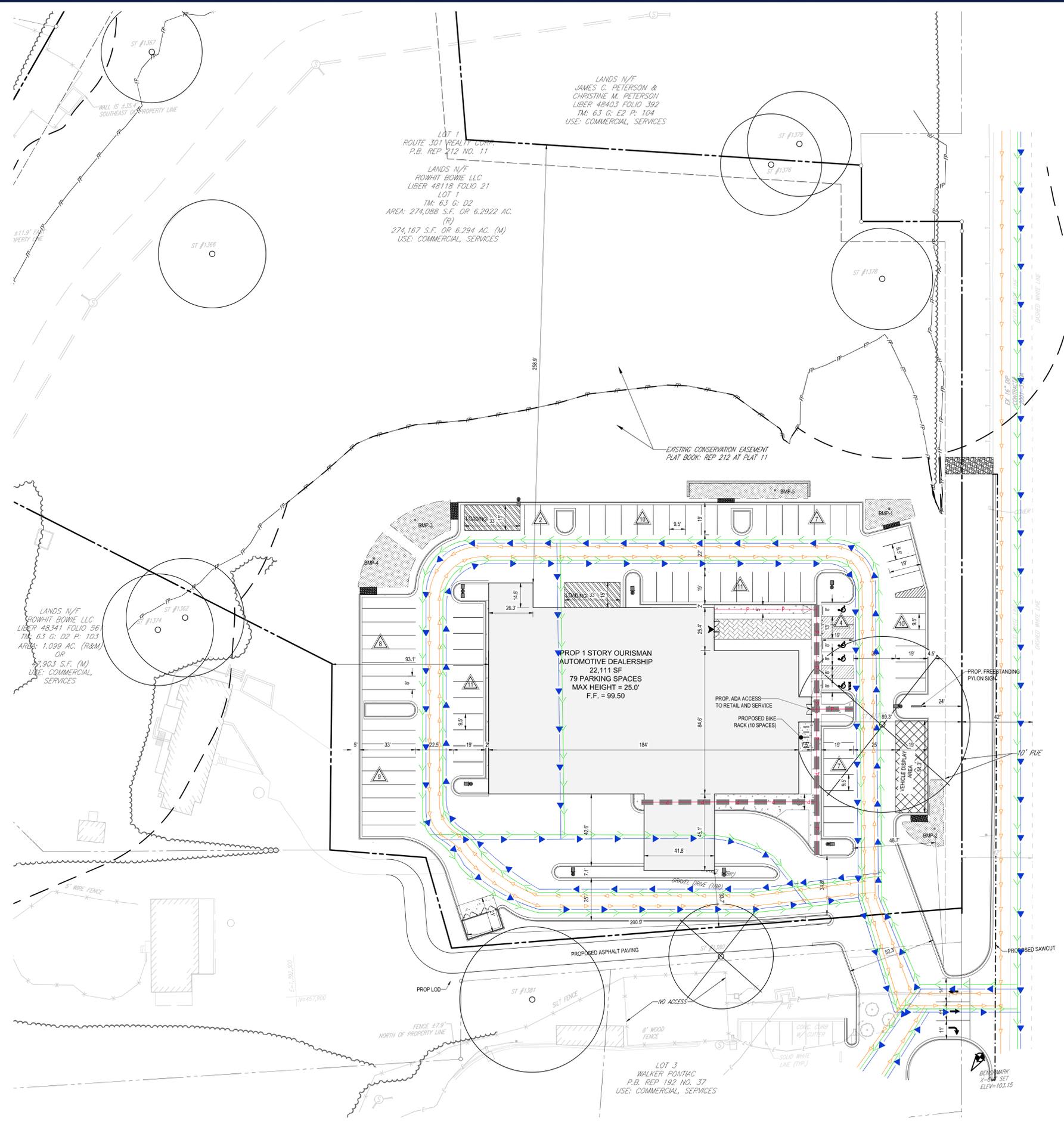
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PROFESSIONAL CERTIFICATION  
JOSEPH DIMARCO, HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DAILY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. 34390, EXPIRATION DATE: 12/23/2025

SHEET TITLE:

SITE DETAILS

SHEET NUMBER:  
DSP-6

ORG. DATE - 09/13/2023

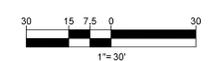


**LEGEND**

- VEHICULAR CIRCULATION
- LOADING CIRCULATION
- PEDESTRIAN CIRCULATION
- BICYCLE CIRCULATION
- ADA PATH

**CRAIN HIGHWAY - U.S. RTE. 301**

VARIABLE WIDTH PUBLIC RIGHT-OF-WAY  
 SHA MAINTAINED - 55 MPH POSTED  
 ASPHALT PAVED ROADWAY  
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PROJECT No.: MD8230009.00  
 DRAWN BY: HS  
 CHECKED BY: JD  
 DATE: 05/30/2025  
 CAD LD.: GRAD-D

PROJECT:  
**DETAILED SITE PLAN**  
 FOR  
**OURISMAN OF BOWIE**  
 PROPOSED DEVELOPMENT  
 2550 NW ROBERT CRAIN HWY  
 BOWIE, MD 20716  
 PRINCE GEORGE'S COUNTY  
 PLAT 212011, LOT 1  
 LIBER 48118 FOLIO 021

**BOHLER**  
 16701 MELFORD BLVD., SUITE 430  
 BOWIE, MARYLAND 20715  
 Phone: (301) 809-4500  
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 MD@BohlerEng.com

**J. DIMARCO**  
 PROFESSIONAL ENGINEER  
 LICENSE NO. 34390, EXPIRATION DATE: 12/23/2025  
 I, JOSEPH DIMARCO, HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DAILY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND.

SHEET TITLE:  
**CIRCULATION PLAN**

SHEET NUMBER:  
**DSP-11**

ORG. DATE - 09/13/2023

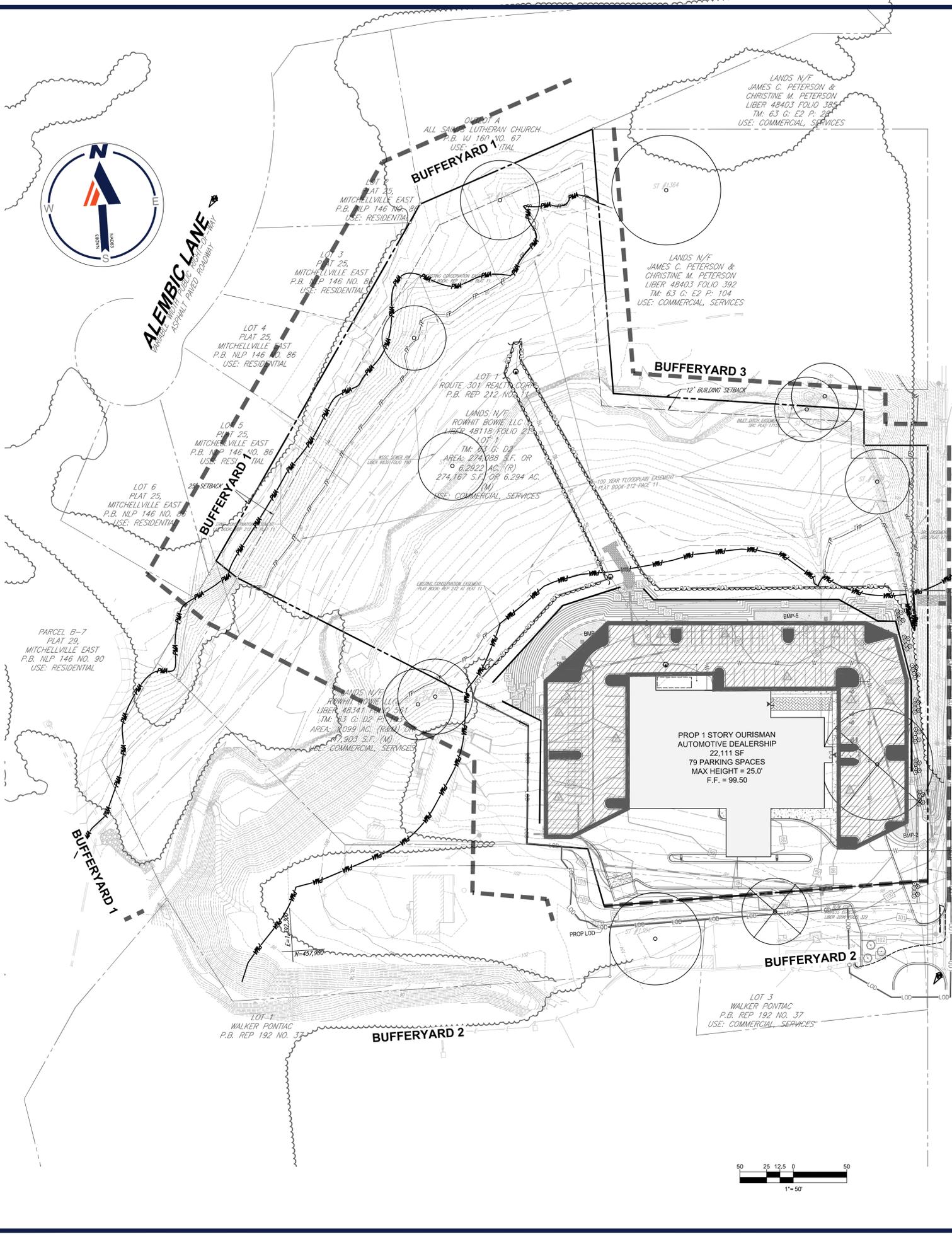
8/6/12, 2025  
 H:\2023\MD8230009.00\CADD\WMS\SP\PLAN SETS\DETAILED SITE PLAN\MD8230009 - CIRC - LAYOUT\_DSP-11 - CIRCULATION PLAN







**ALEMbic LANE**  
ASPHALT PAVED ROADWAY



**Tree Canopy Coverage Schedule for Sec. 25-128**

Project Name:	TCP2R:	DRD Case #:	Area (acres)
COURISMAN OF BOWIE MD230009			
Site Calculations:	Zone 1:	RS	9.28
	Zone 2:		
	Zone 3:	C-S	
	Zone 4:		
	Total Acres:		9.28
Total Acres (gross acres)	% of TCC required	TCC Required (Acres)	TCC Provided in (SF)
9.28	15.0%	1.39	60530
A. TOTAL ON-SITE TCC PROVIDED (acres) =		1.03	44866.8
B. TOTAL AREA EXISTING TREES (non-1/2 acre) =			114966.4
C. TOTAL SQUARE FOOTAGE IN LANDSCAPE TREES =		2.54	7950
D. TOTAL TREE CANOPY COVERAGE PROVIDED =			167815
E. TOTAL SQUARE FOOTAGE REQUIRED =			60636
			Requirement satisfied

**Credit Categories for Landscape Trees**

Credit Categories for Landscape Trees	TCC Credit per Tree Based on Size at Planting (SF)	Number of Trees	TCC Credit (SF)
Deciduous - columnar shade tree (50' or less height)	2-1/2" = 65		0
	3-3/4" = 75		0
Deciduous - ornamental tree (20' or less height with equal spread). Minimum planting size 7'-9" in height	1-1/2" = 34 = 75		0
	2-2 1/2" = 100		0
Deciduous - minor shade tree (25-50' height with equal spread or greater). Minimum planting size 8-10' in height	2-1/2" = 110		0
	2-1/2" = 160		0
Deciduous - major shade tree (50' or greater ht. with spread equal to or greater than ht) Minimum planting size 12 to 14' in height	3-3/4" = 175		0
	2-1/2" = 225	27	6075
Evergreen - columnar tree (less than 30' height with spread less than 15')	3-3/4" = 250		0
	6-8" = 40		0
	8-10" = 50		0
	10-12" = 75		0
	8-8" = 75		0
	8-10" = 100		0
Evergreen - small tree (30-40' height with spread of 15-20')	10-12" = 125		0
	6-8" = 125	15	1875
Evergreen - medium tree (40-50' height with spread of 20-30')	8-10" = 150		0
	10-12" = 175		0
	6-8" = 150		0
Evergreen - large tree (50' height or greater with spread of over 30')	8-10" = 200		0
	10-12" = 250		0
TOTAL NUMBER OF TREES TO CREDIT (SF)			7950

Dubler Engineering  
Prepared by

Seal: BOHLER ENGINEERING INC. 25-AUG-24  
Date: 09/13/2023  
Revised Line 2011

**SCHEDULE 4.7-1  
BUFFERING INCOMPATIBLE USES REQUIREMENTS: BUFFER YARD '1'**

GENERAL PLAN DESIGNATION	DEVELOPING TIER
1. GENERAL PLAN DESIGNATION	DEVELOPING TIER
2. USE OF PROPOSED DEVELOPMENT:	AUTO DEALERSHIP WITH REPAIR
3. IMPACT OF PROPOSED DEVELOPMENT	H
4. USE OF ADJOINING DEVELOPMENT	COMMERCIAL SERVICE
5. IMPACT OF ADJOINING DEVELOPMENT	M
6. MINIMUM REQUIRED BUFFERYARD (A, B, C, D OR E)	B
7. MINIMUM REQUIRED BUILDING SETBACK	30 FEET
8. BUILDING SETBACK PROVIDED	±50 FEET
9. MINIMUM REQUIRED WIDTH OF LANDSCAPE YARD	20 FEET
10. WIDTH OF LANDSCAPE YARD PROVIDED	±310 FEET
11. LINEAR FEET OF BUFFER STRIP REQUIRED ALONG PROPERTY LINE AND RIGHT-OF-WAY	1113 LF.
12. PERCENTAGE OF REQUIRED BUFFERYARD OCCUPIED BY EXISTING TREES	75%
13. IS A SIX FOOT HIGH FENCE OR WALL INCLUDED IN BUFFERYARD	NO
14. TOTAL NUMBER OF PLANT UNITS REQUIRED IN BUFFER STRIP	225.6 P.U.
15. TOTAL NUMBER OF PLANT UNITS PROVIDED	SHADE TREES 8 x 10 P.U. = 89 P.U. EVERGREEN TREES 13 x 5 P.U. = 65 P.U. ORNAMENTAL TREES 0 x 5 P.U. = 0 P.U. SHRUBS 108 X 1 P.U. = 108 P.U. TOTAL: 226 P.U.

**SCHEDULE 4.7-1  
BUFFERING INCOMPATIBLE USES REQUIREMENTS: BUFFER YARD '2'**

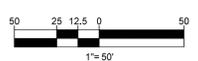
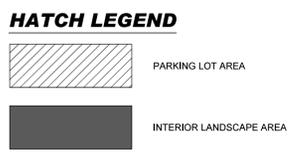
GENERAL PLAN DESIGNATION	DEVELOPING TIER
1. GENERAL PLAN DESIGNATION	DEVELOPING TIER
2. USE OF PROPOSED DEVELOPMENT:	AUTO DEALERSHIP WITH REPAIR
3. IMPACT OF PROPOSED DEVELOPMENT	H
4. USE OF ADJOINING DEVELOPMENT	AUTO DEALERSHIP WITH REPAIR
5. IMPACT OF ADJOINING DEVELOPMENT	H
6. MINIMUM REQUIRED BUFFERYARD (A, B, C, D OR E)	NONE
7. MINIMUM REQUIRED BUILDING SETBACK	0 FEET
8. BUILDING SETBACK PROVIDED	±0 FEET
9. MINIMUM REQUIRED WIDTH OF LANDSCAPE YARD	0 FEET
10. WIDTH OF LANDSCAPE YARD PROVIDED	±5 FEET
11. LINEAR FEET OF BUFFER STRIP REQUIRED ALONG PROPERTY LINE AND RIGHT-OF-WAY	812LF.
12. PERCENTAGE OF REQUIRED BUFFERYARD OCCUPIED BY EXISTING TREES	100%
13. IS A SIX FOOT HIGH FENCE OR WALL INCLUDED IN BUFFERYARD	NO
14. TOTAL NUMBER OF PLANT UNITS REQUIRED IN BUFFER STRIP	0 P.U.
15. TOTAL NUMBER OF PLANT UNITS PROVIDED	N/A

**SCHEDULE 4.7-1  
BUFFERING INCOMPATIBLE USES REQUIREMENTS: BUFFER YARD '3'**

GENERAL PLAN DESIGNATION	DEVELOPING TIER
1. GENERAL PLAN DESIGNATION	DEVELOPING TIER
2. USE OF PROPOSED DEVELOPMENT:	AUTO DEALERSHIP WITH REPAIR
3. IMPACT OF PROPOSED DEVELOPMENT	H
4. USE OF ADJOINING DEVELOPMENT	VETERINARY HOSPITAL
5. IMPACT OF ADJOINING DEVELOPMENT	H
6. MINIMUM REQUIRED BUFFERYARD (A, B, C, D OR E)	NONE
7. MINIMUM REQUIRED BUILDING SETBACK	0 FEET
8. BUILDING SETBACK PROVIDED	±0 FEET
9. MINIMUM REQUIRED WIDTH OF LANDSCAPE YARD	0 FEET
10. WIDTH OF LANDSCAPE YARD PROVIDED	±5 FEET
11. LINEAR FEET OF BUFFER STRIP REQUIRED ALONG PROPERTY LINE AND RIGHT-OF-WAY	812LF.
12. PERCENTAGE OF REQUIRED BUFFERYARD OCCUPIED BY EXISTING TREES	100%
13. IS A SIX FOOT HIGH FENCE OR WALL INCLUDED IN BUFFERYARD	NO
14. TOTAL NUMBER OF PLANT UNITS REQUIRED IN BUFFER STRIP	0 P.U.
15. TOTAL NUMBER OF PLANT UNITS PROVIDED	N/A

**SECTION 4.3-2  
INTERIOR PLANTING FOR PARKING LOTS 7,000 S.F. OR LARGER**

1. PARKING LOT AREA	36,181 S.F.
2. INTERIOR LANDSCAPE AREA REQUIRED	8%, 2,895 S.F.
3. INTERIOR LANDSCAPE AREA PROVIDED	8.14%, 3,336 S.F.
4. NUMBER OF SHADE TREES REQUIRED	
PARKING LOTS LESS THAN 50,000 S.F. (1 PER 300 S.F. OF INTERIOR PLANTING AREA PROVIDED)	10 SHADE TREES
PARKING LOTS GREATER THAN 50,000 S.F. (1 PER 200 S.F. OF INTERIOR PLANTING AREA PROVIDED)	N/A
5. NUMBER OF SHADE TREES PROVIDED	11 TREES
6. IS A MINIMUM OF 160 S.F. OF CONTIGUOUS PVIOUS LAND AREA PROVIDED PER SHADE TREE?	NO
7. IS THERE A PLANTING ISLAND ON AVERAGE EVERY 10 SPACES?	YES
8. IS A CURB OR WHEEL STOP PROVIDED FOR ALL PARKING SPACES ABUTTING A PLANTING OR PEDESTRIAN AREA?	YES
9. ARE PLANTING ISLANDS WHICH ARE EITHER PARALLEL OR PERPENDICULAR TO PARKING SPACES ON BOTH SIDES A MINIMUM OF 9 FEET WIDE?	NO
10. IS A PLANTING ISLAND THAT IS PERPENDICULAR TO PARKING SPACES ON ONE SIDE A MINIMUM OF 6 FEET WIDE?	NO
11. FOR PARKING LOTS 50,000 S.F. OR LARGER	
A) IS THERE A 9 FOOT WIDE PLANTING ISLAND PERPENDICULAR TO PARKING FOR EVERY 2 BAYS?	N/A
B) IS THE NUMBER OF SHADE TREES REQUIRED INCREASED (1 PER 200 S.F. OF INTERIOR PLANTING AREA PROVIDED)?	N/A



**BOHLER**  
SITE CIVIL AND CONSULTING ENGINEERING  
PROGRAM MANAGEMENT  
LANDSCAPE ARCHITECTURE  
SUSTAINABLE DESIGN  
PERMITTING SERVICES  
TRANSPORTATION SERVICES

**REVISIONS**

REV	DATE	COMMENT	DRAWN BY	CHECKED BY

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**FOR CONCEPT PURPOSES ONLY**

PROJECT No.: MD230009.00  
DRAWN BY: RF  
CHECKED BY: MG  
DATE: 09/13/2023  
CAD ID: LSCP-4

**DETAILED SITE PLAN**

FOR

**OURISMAN OF BOWIE**

PROPOSED DEVELOPMENT  
2550 NW ROBERT CRAIN HIGHWAY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY

**BOHLER**

16701 MELFORD BLVD., SUITE 430  
BOWIE, MARYLAND 20715  
Phone: (301) 809-4500  
Fax: (301) 809-4501  
MD@BohlerEng.com

**E.R. McWILLIAMS**  
09/09/2025  
PROFESSIONAL LANDSCAPE ARCHITECT  
LICENSE NO. 9687, EXPIRATION DATE: 9/2028

**CONTEXTUAL LANDSCAPE PLAN**

SHEET NUMBER:  
**DSP-7**

ORG. DATE - 09/13/2023







DSP-23026

OURISMAN OF BOWIE  
22111

04/22/2025

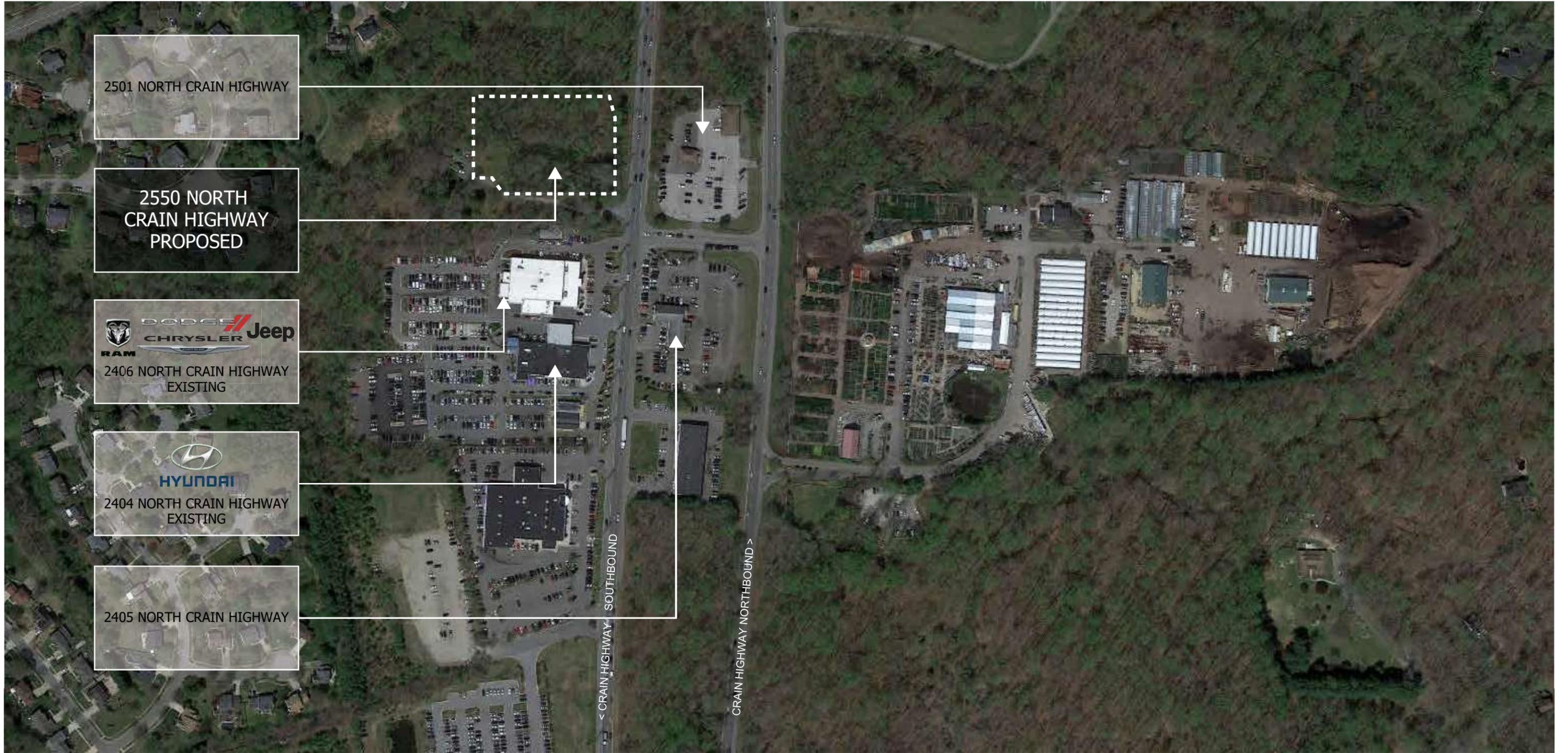
**OURISMAN**  
AUTOMOTIVE GROUP

A H AMMON HEISLER SACHS  
S a architects

300 West Pratt Street - Suite 209  
Baltimore, Maryland 21201  
DSP-23026\_Backup\_66 of 134  
F: 410.752.8358

# 2550 NORTH CRAIN HIGHWAY

- Year 1 SALES being in the new dealership \$39,170,872
- Employing 38 STAFF
- Construction cost \$7,788,550 plus site work
- Estimated property tax generated \$125,000



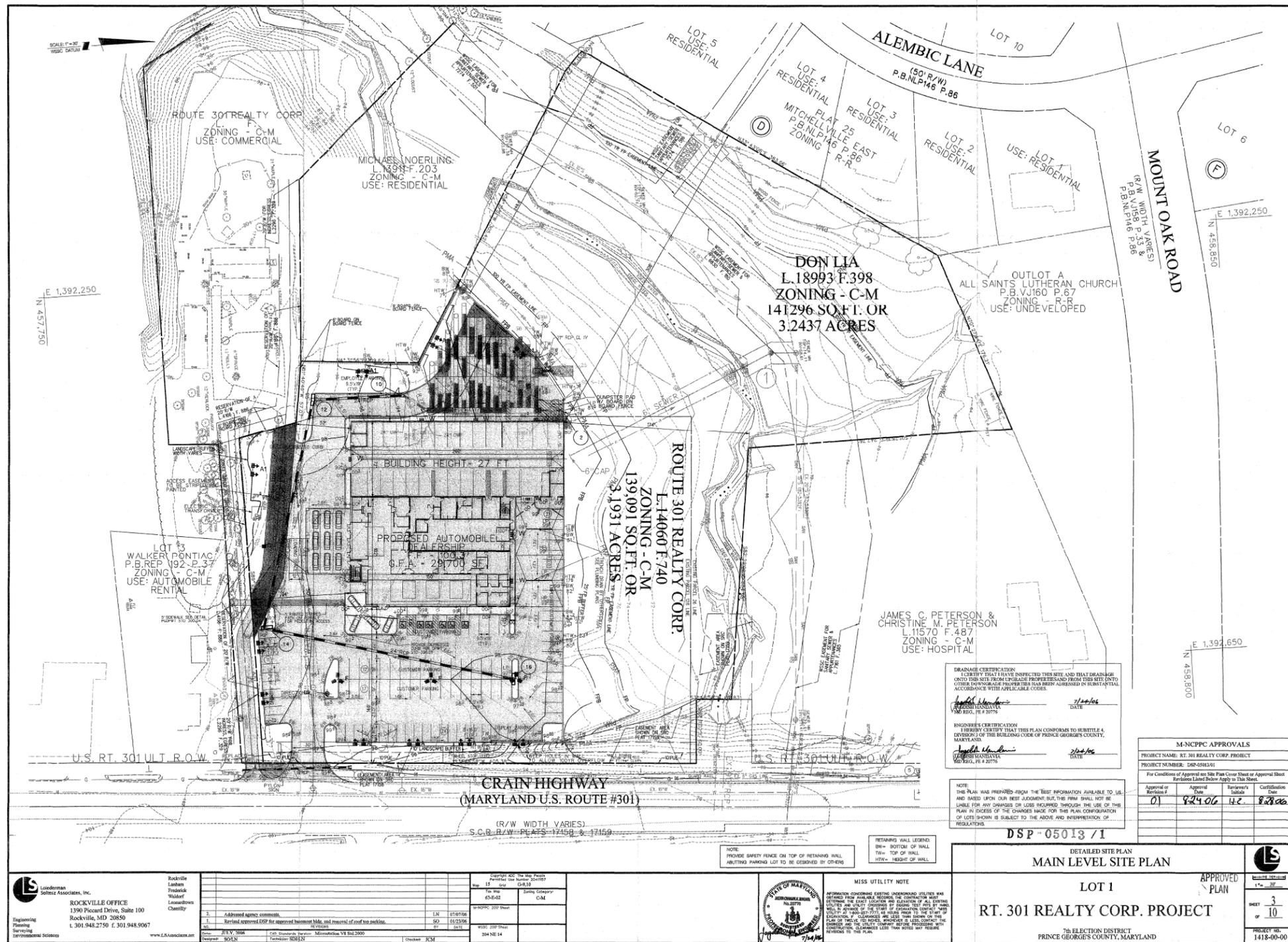
OVERALL AERIAL PLAN

OURISMAN OF BOWIE  
22111

04/22/2025



300 West Pratt Street - Suite 275  
Baltimore, Maryland 21201  
DSP-23026\_Backup\_68 of 134  
F: 410.752.8358



**DRAINAGE CERTIFICATION**  
 I CERTIFY THAT I HAVE INSPECTED THIS SITE AND THAT DRAINAGE  
 ON THE SITE IS IN ACCORDANCE WITH THE PROVISIONS OF THE  
 SUBDIVISIONS ACT AND THE SUBDIVISIONS REGULATIONS.  
 DATE: 1/24/2025  
 SIGNATURE: [Signature]

**ENGINEER'S CERTIFICATION**  
 I HEREBY CERTIFY THAT THIS PLAN CONFORMS TO SUBTITLE 4  
 DIVISION 3 OF THE BUILDING CODE OF PRINCE GEORGE'S COUNTY,  
 MARYLAND.  
 DATE: 1/24/2025  
 SIGNATURE: [Signature]

M-NCPPC APPROVALS			
PROJECT NAME: RT. 301 REALTY CORP. PROJECT			
PROJECT NUMBER: DSP-05013/1			
For Conditions of Approval on this Plan Cover Sheet or Approval Sheet Referenced Below Apply to this Sheet.			
Approval or Revised	Approval Date	Reviewer's Initials	Certification Date
01	8/24/06	U.C.	8/28/06

**NOTES:**  
 THIS PLAN WAS PREPARED FROM THE BEST INFORMATION AVAILABLE TO US  
 AND BASED UPON OUR BEST JUDGMENT BUT THE FIRM SHALL NOT BE  
 LIABLE FOR ANY CHANGES OR LOSS INCURRED THROUGH THE USE OF THIS  
 PLAN IN EXCESS OF THE CHARGES MADE FOR THIS PLAN CONSTRUCTION  
 OF LOTS SHOWN IS SUBJECT TO THE ABOVE AND INTERPRETATION OF  
 REGULATIONS.

**RETAINING WALL LEGEND:**  
 RW= BOTTOM OF WALL  
 TW= TOP OF WALL  
 HW= HEIGHT OF WALL

**MISS UTILITY NOTE**

**APPROVED PLAN**

**LOT 1**  
**RT. 301 REALTY CORP. PROJECT**

7th ELECTION DISTRICT  
 PRINCE GEORGE'S COUNTY, MARYLAND

**DSP - 05013 / 1**

**1" = 30'**

SHEET 3 OF 10

PROJECT NO.: 1418-00-00

<p><b>Legendman</b>          Sotheby's Associates, Inc.</p> <p>Rockville Office          1390 Piccard Drive, Suite 100          Rockville, MD 20850          L: 301.948.2750 F: 301.948.9067</p>	<p>Rockville          LaBaron          Prudek          Walker          Lawandrows          Chantilly</p>	<p>Submitted On: 1/24/2025          Project No: 1418-00-00          Title: RT. 301 REALTY CORP. PROJECT          Date: 1/24/2025          Drawing: 001-2</p>	<p>Scale: 1" = 30'          Date: 1/24/2025          Author: [Name]          Checker: [Name]</p>
--	--	--	--

PREVIOUSLY APPROVED SITE PLAN

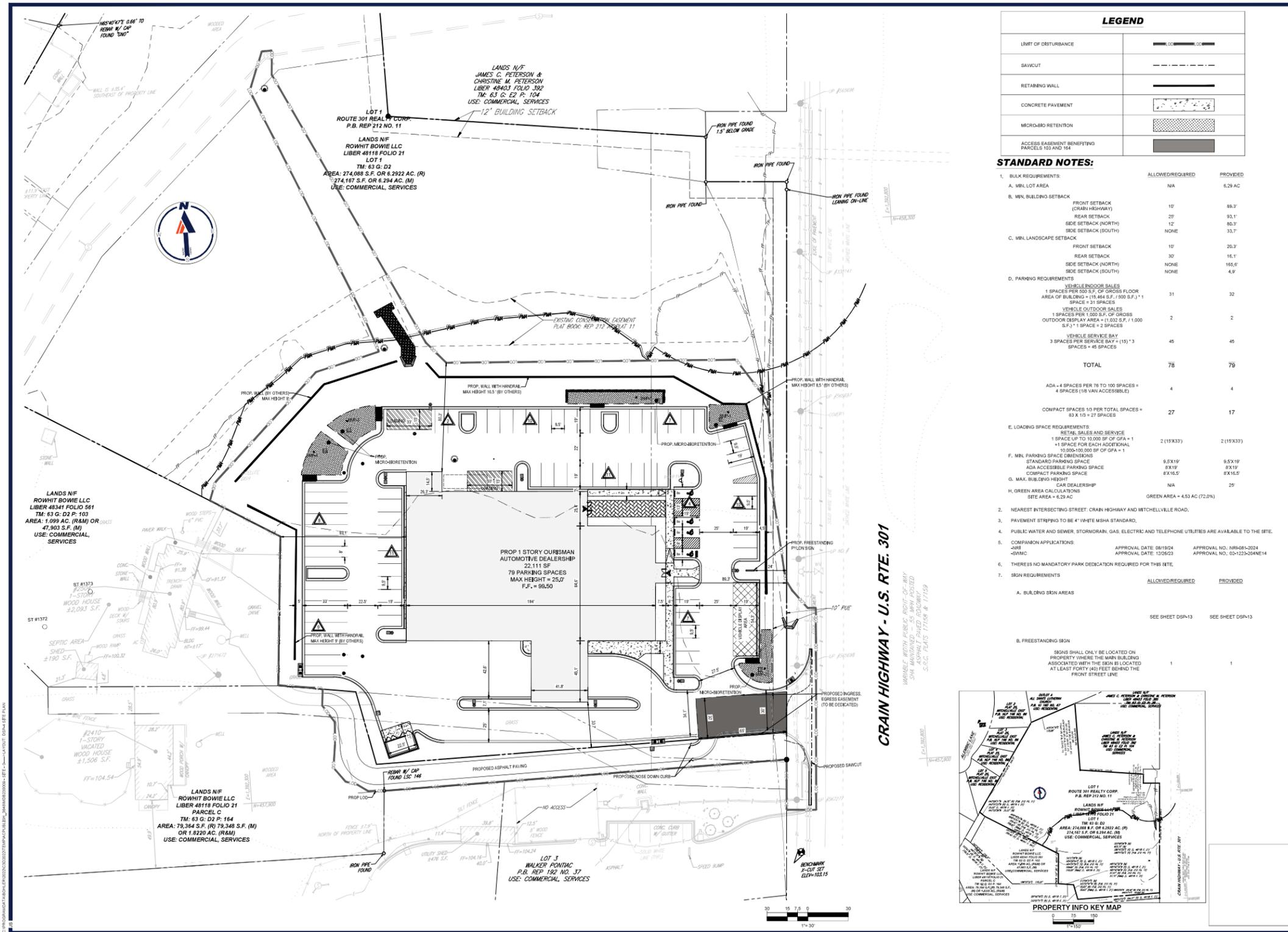
OURISMAN OF BOWIE 22111 04/22/2025



**A H AMMON HEISLER SACHS**  
**S a architects**

300 West Pratt Street - Suite 275  
 Baltimore, Maryland 21201

DSP-23026\_Backup\_69 of 134  
 410.752.8316  
 F: 410.752.8358



**LEGEND**

LIMIT OF DISTURBANCE	---
SAVCUT	---
RETAINING WALL	---
CONCRETE PAVEMENT	---
MICRO-SLO RETENTION	---
ACCESS EASEMENT BENEFTING PARCELS 103 AND 104	---

**STANDARD NOTES:**

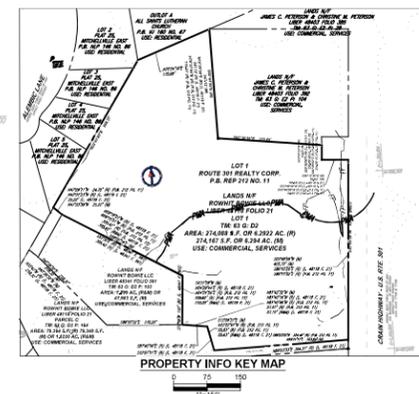
1. BULK REQUIREMENTS	ALLOWED/REQUIRED	PROVIDED
A. MIN. LOT AREA	N/A	6.29 AC
B. MIN. BUILDING SETBACK		
FRONT SETBACK (CRAIN HIGHWAY)	10'	88.7'
REAR SETBACK	20'	93.1'
SIDE SETBACK (NORTH)	12'	80.3'
SIDE SETBACK (SOUTH)	NONE	33.7'
C. MIN. LANDSCAPE SETBACK		
FRONT SETBACK	10'	20.3'
REAR SETBACK	30'	16.1'
SIDE SETBACK (NORTH)	NONE	165.6'
SIDE SETBACK (SOUTH)	NONE	4.9'
D. PARKING REQUIREMENTS		
VEHICLE INDOOR SALES		
1 SPACES PER 500 S.F. OF GROSS FLOOR AREA OF BUILDING = (15,464 S.F. / 500 S.F.) * 1 SPACES = 31 SPACES	31	32
VEHICLE OUTDOOR SALES		
1 SPACES PER 1,000 S.F. OF GROSS OUTDOOR DISPLAY AREA = (1,032 S.F. / 1,000 S.F.) * 1 SPACE = 2 SPACES	2	2
VEHICLE SERVICE BAY		
3 SPACES PER SERVICE BAY = (15) * 3 SPACES = 45 SPACES	45	45
<b>TOTAL</b>	<b>78</b>	<b>78</b>
ADA 4 SPACES PER 75 TO 100 SPACES = 4 SPACES (FOR VAN ACCESSIBLE)	4	4
COMPACT SPACES 10 PER TOTAL SPACES = 83 X 10 = 27 SPACES	27	17
E. LOADING SPACE REQUIREMENTS		
RETAIL SALES AND SERVICE		
1 SPACE UP TO 10,000 SF OF GFA + 1 SPACE FOR EACH ADDITIONAL 10,000-10,000 SF OF GFA + 1	2 (15X33)	2 (15X33)
F. MIN. PARKING SPACE DIMENSIONS		
STANDARD PARKING SPACE	9.5X19'	9.5X19'
ADA ACCESSIBLE PARKING SPACE	8X19'	8X19'
COMPACT PARKING SPACE	8X16.5'	8X16.5'
G. MAX. BUILDING HEIGHT	N/A	25'
H. GREEN AREA CALCULATIONS		
CAR DEALERSHIP		
SITE AREA = 6.29 AC		
GREEN AREA = 4.53 AC (72.0%)		

- NEAREST INTERSECTING STREET: CRAIN HIGHWAY AND MITCHELLVILLE ROAD.
- PAVEMENT STRIPPING TO BE 4" WHITE MSHA STANDARD.
- PUBLIC WATER AND SEWER, STORMDRAIN, GAS, ELECTRIC AND TELEPHONE UTILITIES ARE AVAILABLE TO THE SITE.
- COMPANION APPLICATIONS:
  - USE APPROVAL DATE: 06/19/24 APPROVAL NO.: NR061-2024
  - SVMC APPROVAL DATE: 12/26/23 APPROVAL NO.: 02-1223-204NE14
- THERE IS NO MANDATORY PARK DEDICATION REQUIRED FOR THIS SITE.
- SGN REQUIREMENTS

A. BUILDING SIGN AREAS	ALLOWED/REQUIRED	PROVIDED
	SEE SHEET DSP-13	SEE SHEET DSP-13

- FREESTANDING SIGN

SGNS SHALL ONLY BE LOCATED ON PROPERTY WHERE THE MAIN BUILDING ASSOCIATED WITH THE SIGN IS LOCATED AT LEAST FORTY (40) FEET BEHIND THE FRONT STREET LINE.



**BOHLER**  
 SITE CIVIL AND CONSULTING ENGINEERING  
 PROGRAM MANAGEMENT  
 LANDSCAPE ARCHITECTURE  
 PERMITTING SERVICES  
 TRANSPORTATION SERVICES

**REVISIONS**

REV	DATE	COMMENT	DRAWN BY	CHECKED BY

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**NOT APPROVED FOR CONSTRUCTION**

PROJECT No.: MD23009.00  
 DRAWN BY: JO  
 CHECKED BY: JO  
 DATE: 01/17/2024  
 CAD ID: [REDACTED]

**DETAILED SITE PLAN**  
 FOR

**OURISMAN OF BOWIE**  
 PROPOSED DEVELOPMENT  
 2550 NW ROBERT CRAIN HWY  
 BOWIE, MD 20716  
 PRINCE GEORGES COUNTY  
 PLAT 212211, LOT 1  
 LIBER 48118 FOLIO 021

**BOHLER**  
 16701 MELFORD BLVD., SUITE 430  
 BOWIE, MARYLAND 20715  
 Phone: (301) 808-4500  
 Fax: (301) 808-4501  
 MD@BohlerEng.com

**J. DIMARCO**  
 PROFESSIONAL ENGINEER  
 LICENSE NO. 12123215  
 PROFESSIONAL CERTIFICATION  
 LICENSE: DIMARCO, JEREMY C. THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. 1406, EXPIRES ON DATE 12/23/2024

**SITE LAYOUT PLAN**

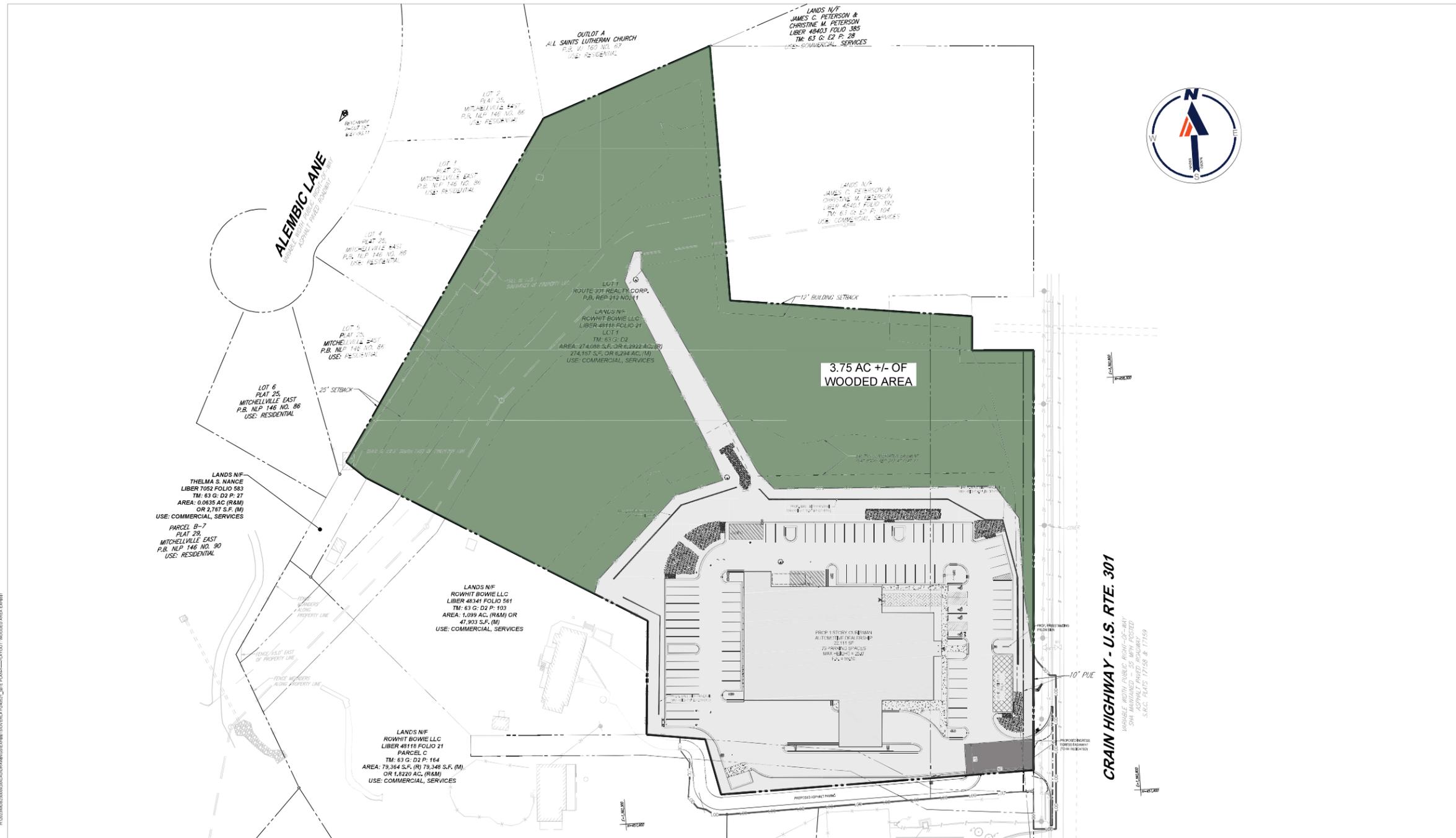
SHEET NUMBER:  
**DSP-4**  
 ORG. DATE - 09/13/2023

**2550 SITE PLAN**

**OURISMAN OF BOWIE**  
 22111  
 04/22/2025

**A H AMMON HEISLER SACHS**  
**s a architects**

300 West Pratt Street - Suite 275  
 Baltimore, Maryland 21201  
 DSP-23026\_Backup\_70 of 134  
 F: 410.752.8358



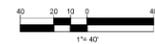
**BOHLER //**

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 BOWIE, MARYLAND 20715  
 Phone: (301) 809-4500  
 Fax: (301) 809-4501  
 MD@BohlerEng.com

**WOODED AREA EXHIBIT**

2550 NW ROBERT CRAIN HWY  
 BOWIE, MD 20716

5/21/2024 | KK | MD8230008.00 | Rev 1



**WOODED AREA EXHIBIT**

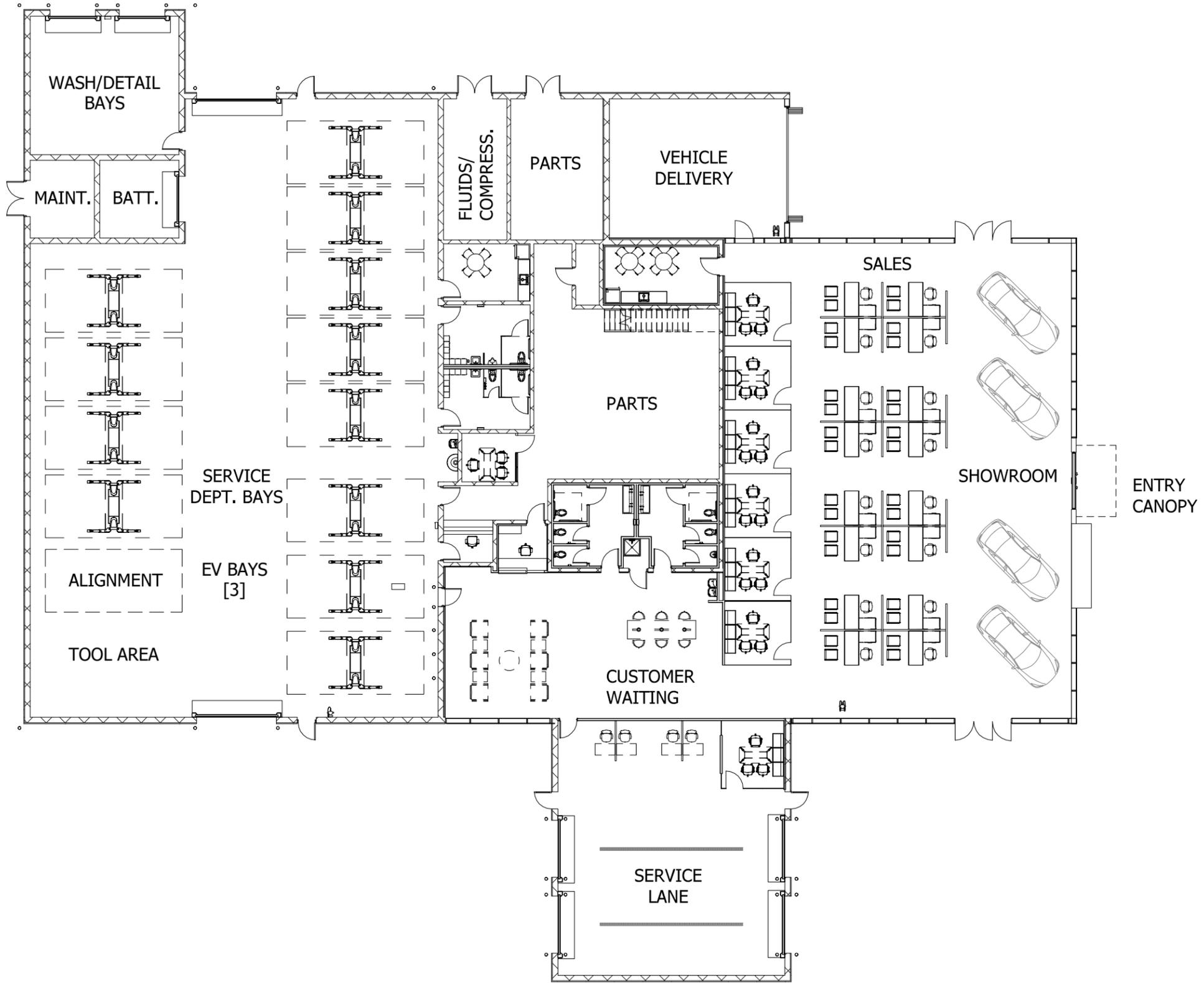
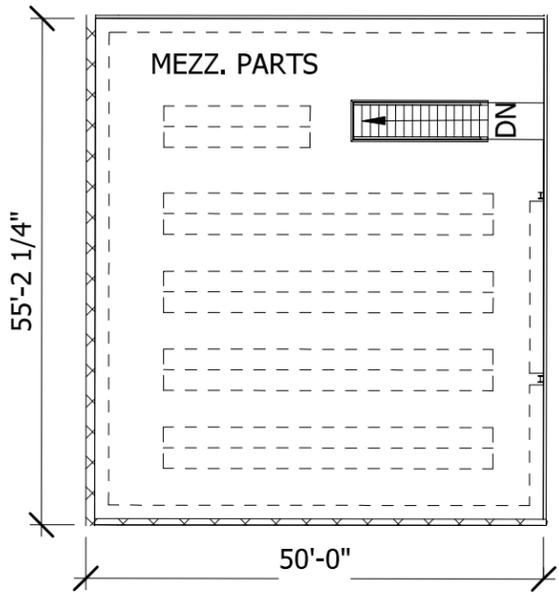
OURISMAN OF BOWIE  
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04/22/2025

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**S a architects**

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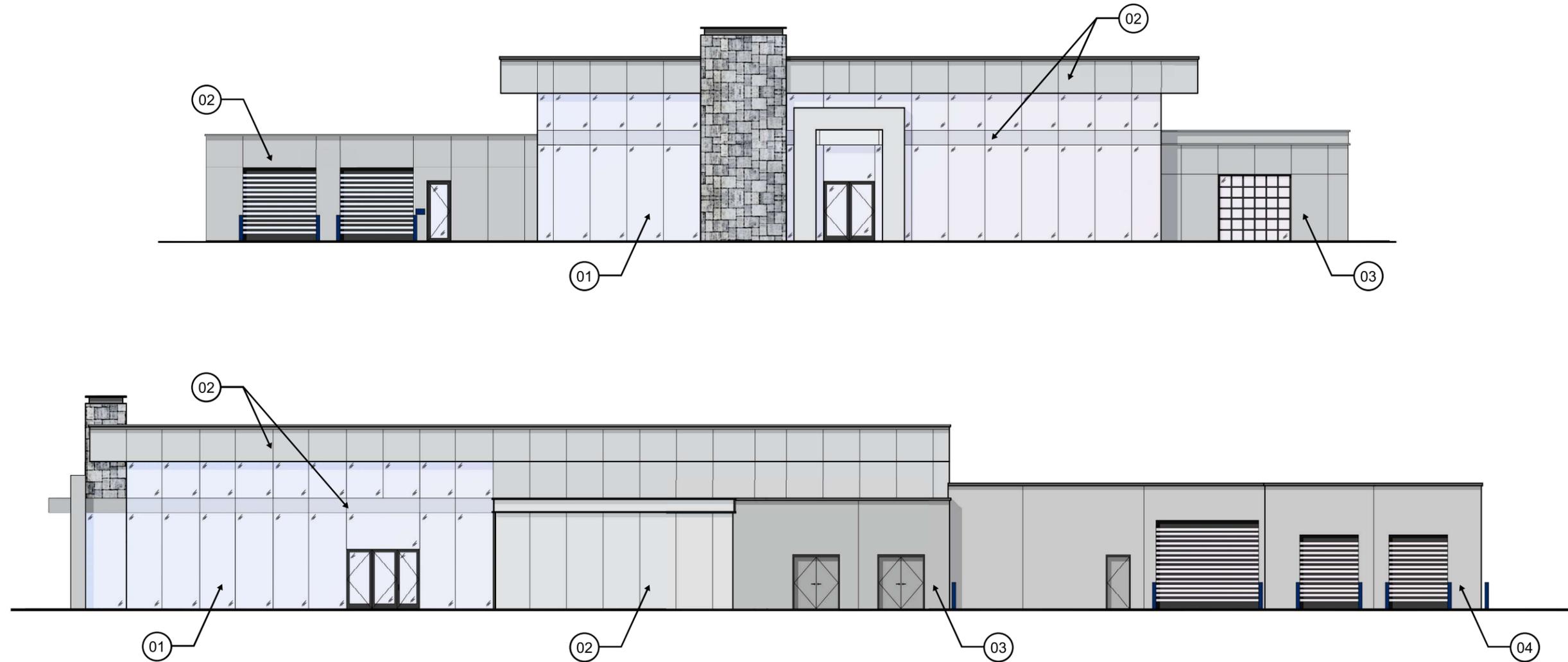
FLOOR PLAN

OURISMAN OF BOWIE  
22111

04/22/2025

SHEET NOTES:

- 01 GLASS.
- 02 ACM.
- 03 EIFS.
- 04 PAINTED CMU.



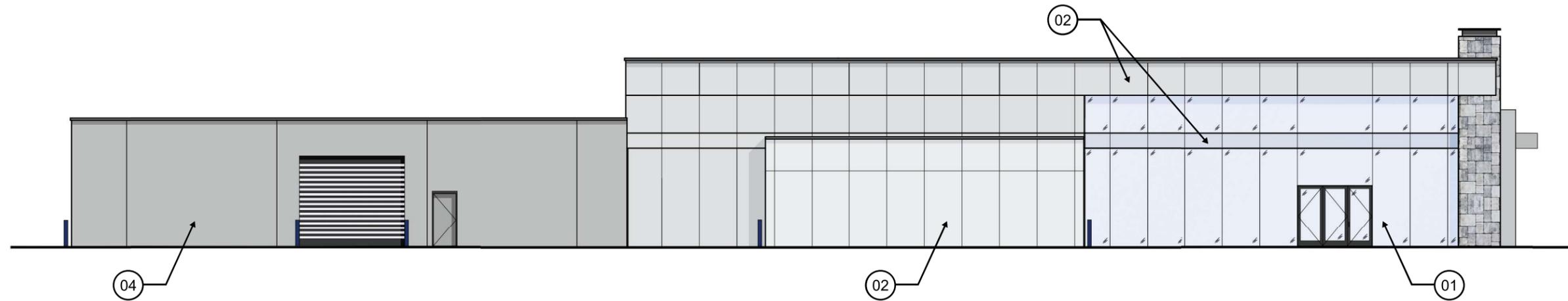
EAST & NORTH ELEVATIONS

OURISMAN OF BOWIE  
22111

04/22/2025

SHEET NOTES:

- 01 GLASS.
- 02 ACM.
- 03 EIFS.
- 04 PAINTED CMU.



WEST & SOUTH ELEVATIONS

OURISMAN OF BOWIE  
22111

04/22/2025



DSP-23026

OURISMAN OF BOWIE  
22111

04/22/2025

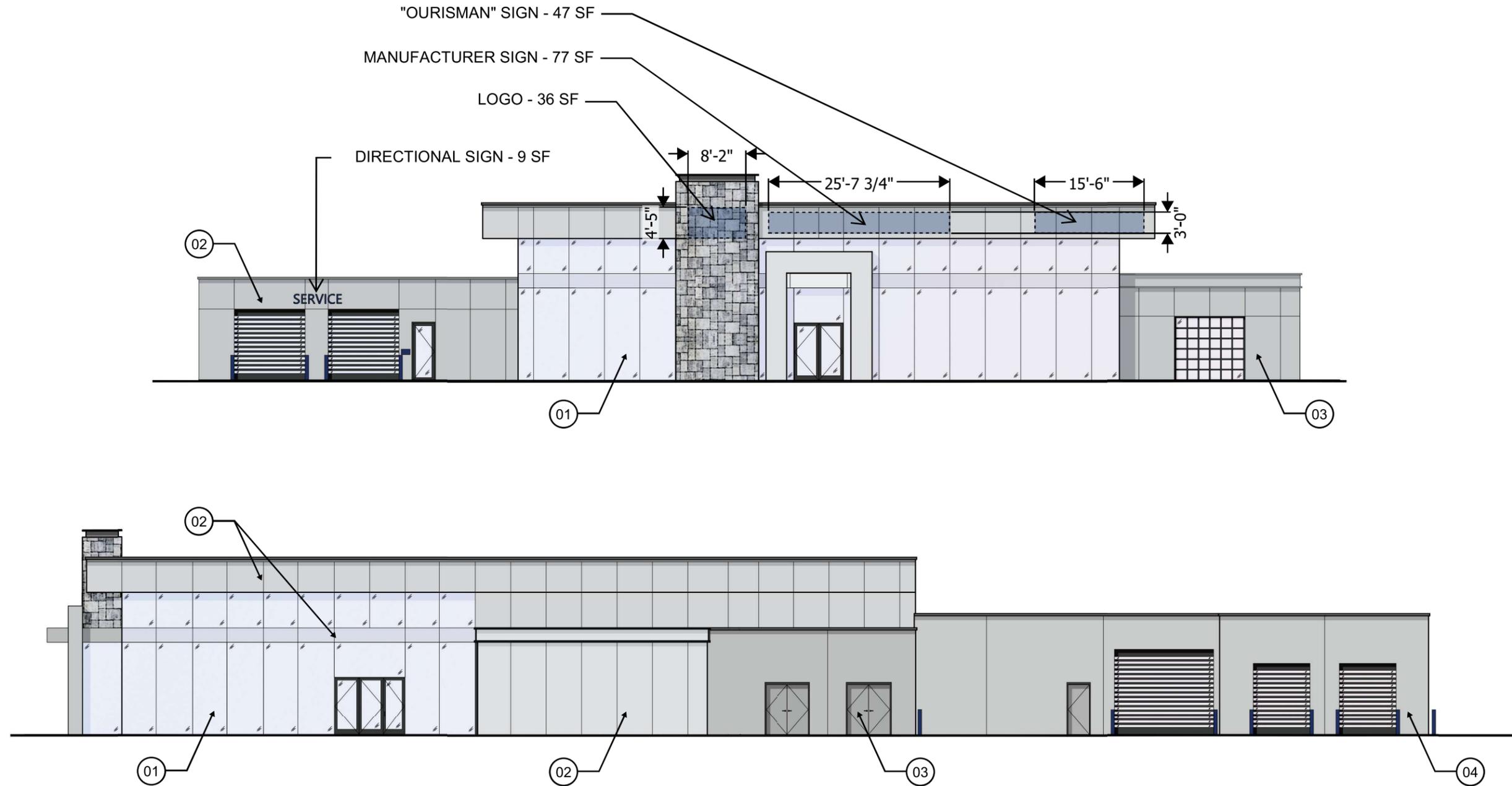
**OURISMAN**  
AUTOMOTIVE GROUP

A H AMMON HEISLER SACHS  
S a architects

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Baltimore, Maryland 21201  
DSP-23026\_Backup\_75 of 134  
F: 410.752.8358

SHEET NOTES:

- 01 GLASS.
- 02 ACM.
- 03 EIFS.
- 04 PAINTED CMU.



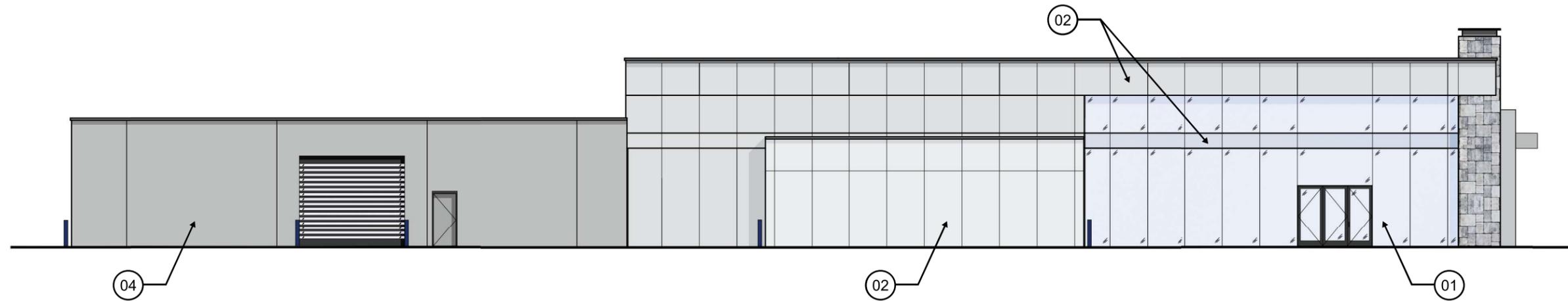
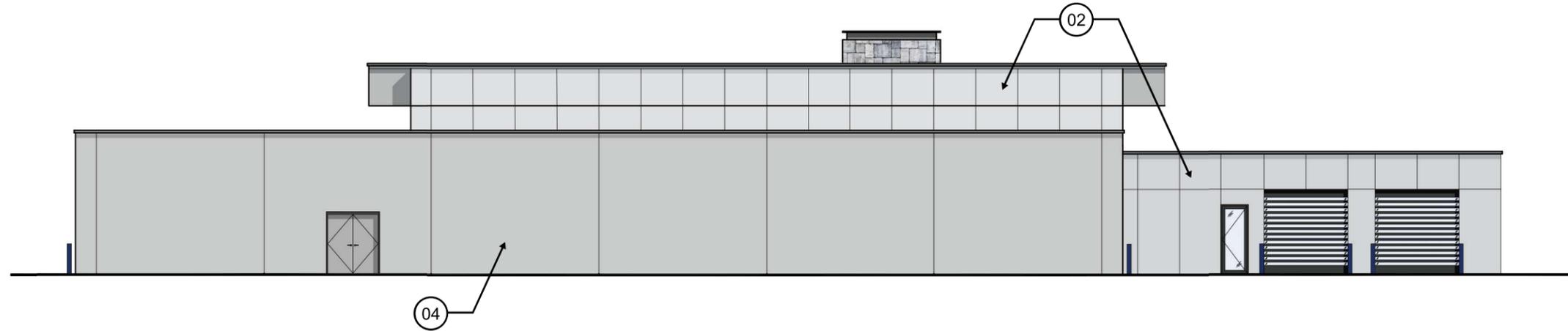
EAST & NORTH ELEVATIONS

SUBARU OF BOWIE  
22111

Scale: 1/16" = 1'  
08/27/2024

SHEET NOTES:

- 01 GLASS.
- 02 ACM.
- 03 EIFS.
- 04 PAINTED CMU.



WEST & SOUTH ELEVATIONS

SUBARU OF BOWIE  
22111

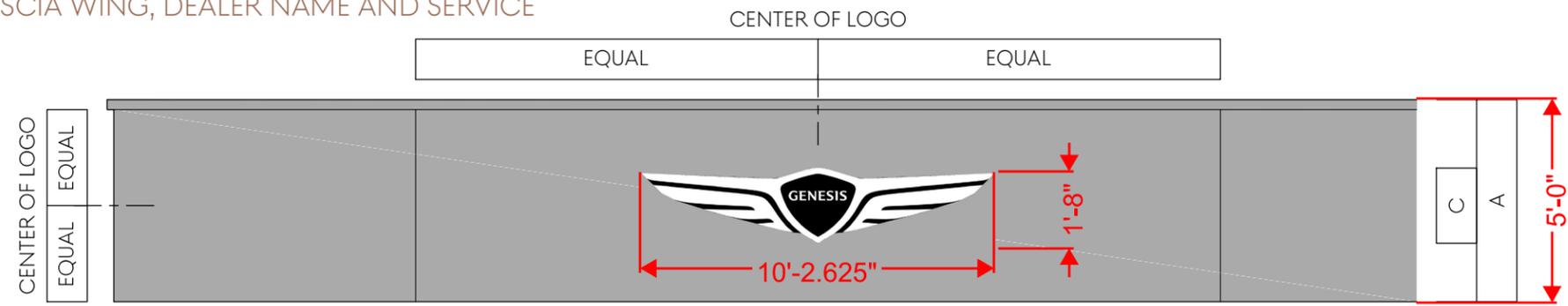
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08/27/2024

**A H** AMMON HEISLER SACHS  
**S a** architects

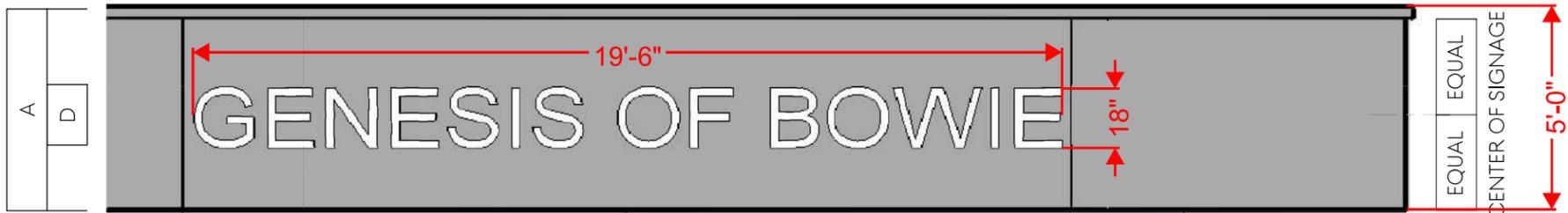
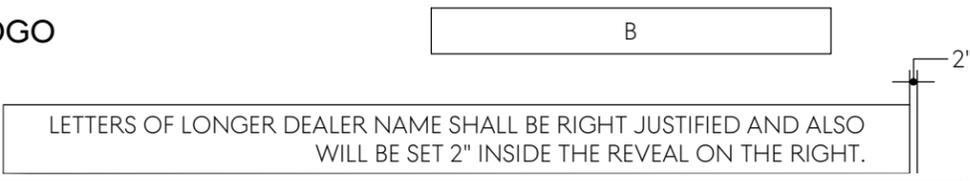
300 West Pratt Street - Suite 200  
Baltimore, Maryland 21201  
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F: 410.752.8358

# SIGNAGE AT EXTERIOR

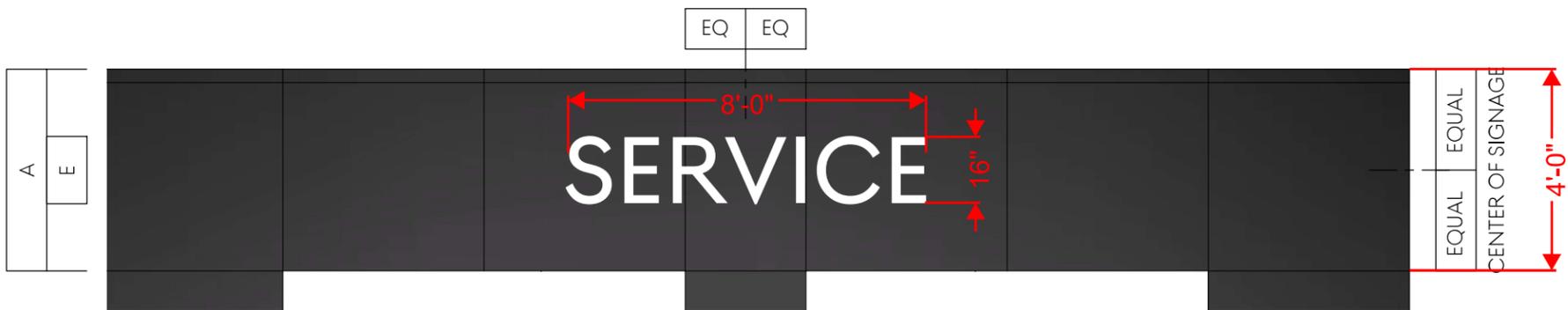
FASCIA WING, DEALER NAME AND SERVICE



GENESIS LOGO



DEALER NAME

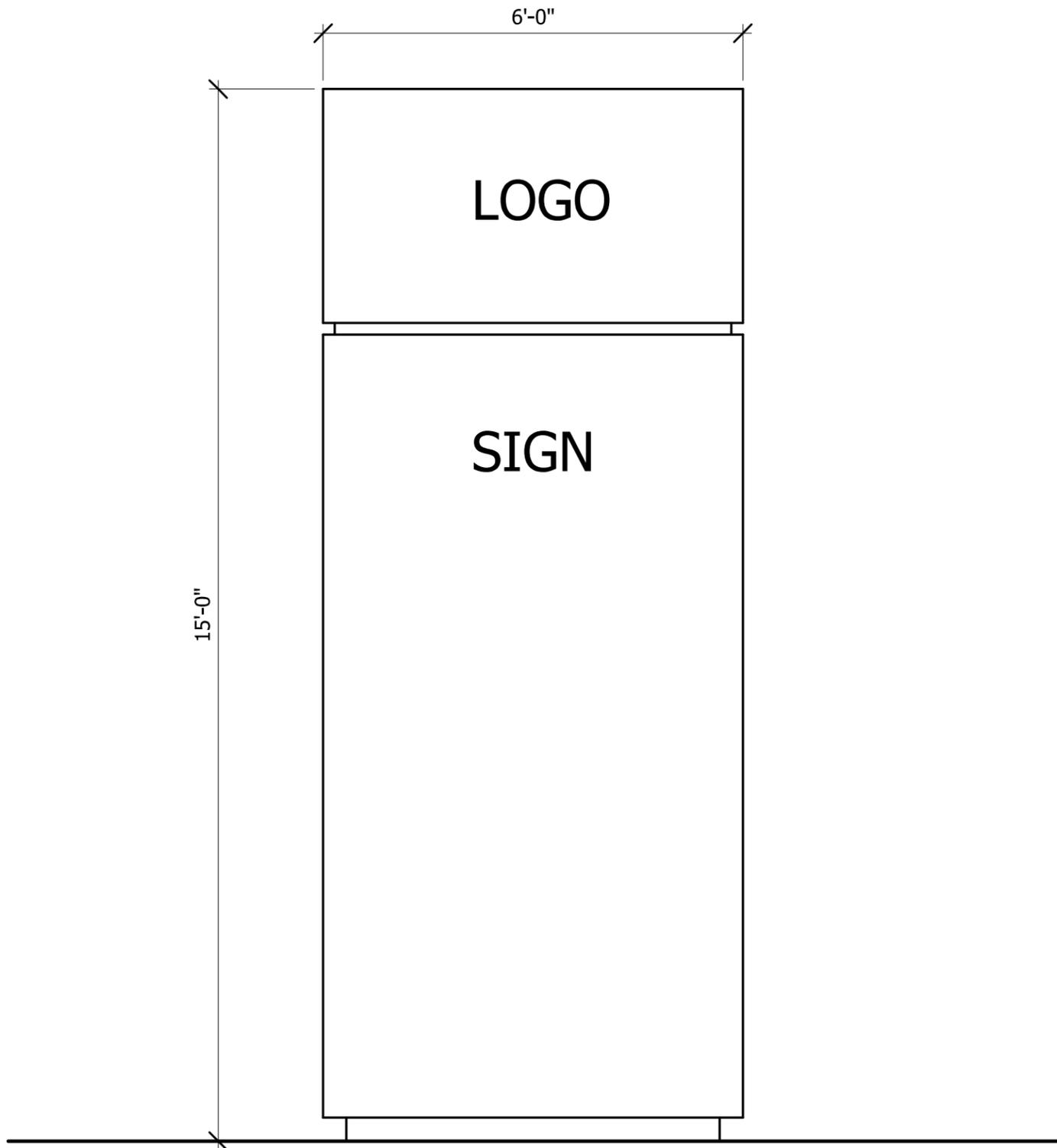


SERVICE

## WALL SIGN

A	B	C	D	E
FASCIA HEIGHT	LOGO WIDTH	LOGO HEIGHT	DEALER NAME HEIGHT	SERVICE HEIGHT
5'	10'-2.625"	1'-8"	18"	20"
4'	8'-2.125"	1'-4"	14"	16"

ALL EXTERIOR SIGNAGE WILL BE ILLUMINATED. PROVIDE ELECTRICAL POWER



**OURISMAN OF BOWIE - SITE PYLON**

OURISMAN OF BOWIE  
22111

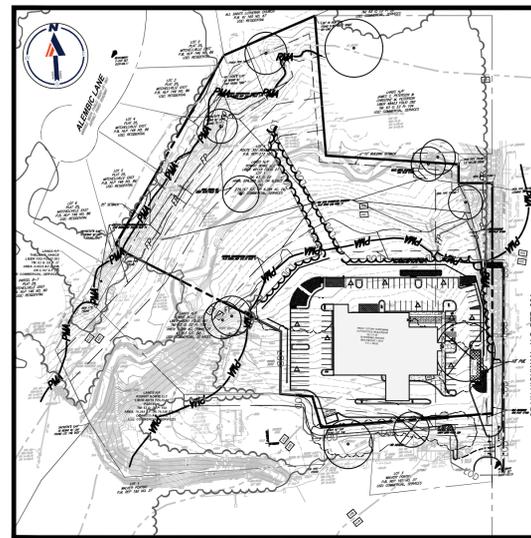
1/2" = 1'-0"  
08/28/24

# TREE CONSERVATION PLAN TYPE 2 TCP2-XXX-XXXX

FOR

## OURISMAN DEALERSHIP OF BOWIE

LOCATION OF SITE  
2550 NW ROBERT CRAIN HWY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY  
SUBDIVISION: ROUTE 301 REALTY CORP  
PLAT 212011; LOT 1  
LIBER 48118 FOLIO 021



LOCATION MAP  
SCALE: 1" = 150'

PREPARED BY

# BOHLER //

CONTACT: ERIC MCWILLIAMS, PLA

SHEET INDEX	
SHEET TITLE	SHEET NUMBER
COVER SHEET	1
TREE CONSERVATION PLAN	2
NOTES AND DETAILS	3

SITE STATISTICS	TOTAL
TOTAL ACREAGE	6.29 AC.
TOTAL ACREAGE 100-YEAR FLOODPLAIN	2.76 AC.
NET TRACT AREA	3.53 AC.
EXISTING WOODLAND IN THE FLOODPLAIN	2.75 AC.
EXISTING WOODLAND IN THE NET TRACT	2.20 AC.
EXISTING WOODLAND TOTAL	4.95 AC.
EXISTING PMA	3.22 AC.
REGULATED STREAM (LINEAR FEET OF CENTERLINE)	658 L.F.
RIPARIAN WOODED BUFFER 300 FT WIDE	3.61 AC.

for Prince George's County  
CB-20-2024 and CB-22-2024 (Effective July 1, 2024)

**SECTION I-Establishing Site Information-** (Enter acres for each zone)

1 Zone (s)	CS				
2 Gross Tract	6.29				
3 Floodplain:	2.76				
4 Previously Dedicated Land:	0.00				Total Net Tract Area:
5 Net Tract (NTA)	3.53	0.00	0.00		3.53

6 TCP Number \_\_\_\_\_ Revision # \_\_\_\_\_

7 Property Description or Subdivision Name: OURISMAN DEALERSHIP OF BOWIE

8 Location/Address of Project: 2550 NW ROBERT CRAIN HWY BOWIE, MD, 20716

9 Is this a Stream Restoration Project? (Y/N) N

10 Plan 2035 Transit oriented Center (T-O-C)? (Y/N) N

11 Priority Funding Area (PFA)? (Y/N) Y

12 Is any portion of the property in a WC Bank? (Y/N) N

13 Is the net tract area 5.00 acres or greater, and clearing 75% or more of existing woodlands is proposed? (Y/N) N

**SECTION II-Determining Requirements** (Enter acres for each corresponding column)

	Column A WCT/AFT %	Column B Net Tract	Column C Floodplain Impacts (1:1)	Column D Off-Site Impacts (1:1)	Column E Total
15 Existing Woodland		2.20	2.75	0.00	4.95
16 Woodland Conservation Threshold (WCT)=	15.00%	0.53			
17 Smaller of 15 and 16		0.53			
18 Woodland Cleared outside of T-O-C		1.21	0.14	0.00	1.35
19 Woodland Cleared within the T-O-C (if applicable)		0.00	0.00	0.00	0.00
19 Replacement Required (1:1 Outside T-O-C, 0.25:1 Inside T-O-C)		1.21	0.14	0.00	1.35
20 Afforestation Threshold (AFT) =	15.00%	0.00			
21 Off-site WCA (preservation) being provided on this property		0.00			
22 Off-site WCA (afforestation) being provided on this property		0.00			
23 Woodland Conservation Requirement		1.88			1.88

**SECTION III-Meeting the Requirements** (Enter acres for each corresponding column)

24 On-site Woodland Preservation	0.99				
25 Adjusted Stream Buffer Afforestation (calculated below)	0.00				
26 On-site Afforestation / Reforestation	0.03				Bond amount: \$392.04
27 On-site Natural Regeneration	0.00				
28 Specimen/Historic Tree Credit (Unimpacted CRZ area * 1.0)	0.00				
29 Off-site WCA (preservation) provided on this property	0.00				
30 Off-site WCA (afforestation) provided on this property	0.00				
31 Forest Enhancement Credit (Credit 50% of area provided)	0.00				
32 On-site Landscaping	0.00				
33 Street Tree Credit (Existing or 10-year canopy coverage)	0.00				
34 Off-site Woodland Conservation Credits Required	0.88				
35 Area provided in fee-in-lieu (One acre or less, unless in a T-O-C)	0.00				Fee amount: \$0.00
36 Woodland Conservation Provided		1.90			1.90

37 Area of woodland not cleared 0.99 acres

38 Net tract woodland retained not part of requirements: 0.00 acres

39 100-year floodplain woodland retained 2.61 acres

40 On-site woodland conservation provided 1.02 acres

41 On-site woodland conservation alternatives provided 0.00 acres

42 On-site woodland retained not credited 2.61 acres

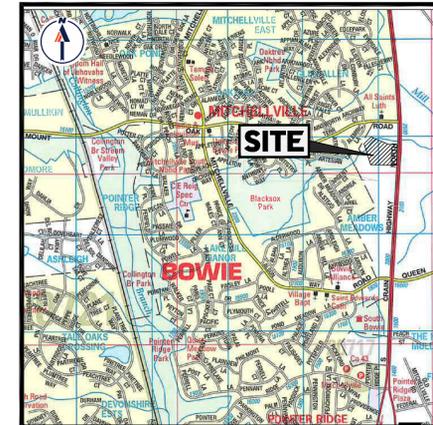
43 Off-site WC credits allowed in preservation 0.73 acres  
(Up to 60% of Replacement Required (Line 22))

**Regulated Stream Buffer (RSB) Afforestation Requirement**

44 On-site regulated stream buffer (RSB) area (See NRI Site Statistics Table)	0.00	acres
45 On-site unregulated RSB area (See NRI Site Statistics Table)	0.00	acres
46 Has the applicant demonstrated why full buffer afforestation cannot be provided? (See required SOJ)		(Y/N)
47 Adjustment granted to RSB afforestation Requirement based on SOJ?	0.00	acres
48 Adjusted Regulated Stream Buffer Afforestation Required	0.00	acres
49 Does on-site WC afforestation/reforestation satisfy the adjusted RSB requirement?		(Y/N)
50 Additional on-site afforestation/reforestation required (not credited as on-site WC)	0.00	acres

Prepared by: \_\_\_\_\_ Signed: \_\_\_\_\_ Date: 09/03/2025

Qualifications: \_\_\_\_\_



VICINITY MAP  
SCALE: 1" = 2000'

### GENERAL NOTES:

- PROJECT NAME: OURISMAN DEALERSHIP OF BOWIE
- TOTAL ACREAGE: 6.29 AC.
  - EXISTING ZONING: C-S (COMMERCIAL, SERVICE)  
PREVIOUS ZONING: C-M (COMMERCIAL, MISCELLANEOUS)
  - EXISTING USE: UNDEVELOPED  
PROPOSED USE: AUTOMOTIVE DEALERSHIP
  - NUMBER OF LOTS, PARCELS, OUTLOTS & OUTPARCELS: LOT 1
  - PROPOSED DWELLING UNITS: NONE
  - EXISTING GROSS FLOOR AREA: UNDEVELOPED  
PROPOSED GROSS FLOOR AREA: 22,111 S.F. GFA
  - WSSC GRID: 204NE14
  - TAX MAP & GRID: TM 63 GRID D2
  - AVIATION POLICY NUMBER AND GRID: NONE
  - EXISTING WATER/SEWER DESIGNATION: W-3 / S-3  
PROPOSED WATER/SEWER DESIGNATION: W-3 / S-3
  - STORMWATER MANAGEMENT CONCEPT NUMBER: SWMC #02-1223-204NE14
  - PRIOR DEVELOPMENT APPROVAL: PPS-4-04047
  - 10-FOOT PUBLIC UTILITY EASEMENT ALONG CRAIN HWY.
  - MANDATORY PARK DEDICATION: NONE
  - CEMETERIES LOCATED IN VICINITY OF THE PROPERTY: NONE
  - HISTORIC SITES LOCATED IN VICINITY OF THE PROPERTY: NONE
  - STREAMS AND WETLANDS: YES
  - 100-YEAR FLOODPLAIN: YES
  - CHESAPEAKE CRITICAL BAY AREA: NO
  - THE SITE IS NOT LOCATED WITHIN A STRONGHOLD WATERSHED AS ESTABLISHED BY THE MARYLAND DEPARTMENT OF NATURAL RESOURCES.
  - THE SITE IS LOCATED IN THE PATUXENT RIVER UPPER WATERSHED.
  - THIS SITE IS WITHIN THE ENVIRONMENTAL STRATEGY AREA (ESA) 2.
  - SOURCE OF TOPOGRAPHY:  
BOHLER  
"BOUNDARY & TOPOGRAPHIC SURVEY  
OURISMAN GENESIS-BOWIE"  
FILE NO.: MD8230009.00  
DATED: 04/10/23  
FIELD DATE: 03/15/23

**BOHLER //**

SITE CIVIL AND CONSULTING ENGINEERING  
PROGRAM MANAGEMENT  
LANDSCAPE ARCHITECTURE  
SUSTAINABLE DESIGN  
PERMITTING SERVICES  
TRANSPORTATION SERVICES

REVISIONS			
REV	DATE	COMMENT	DRAWN BY / CHECKED BY

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FOR CONCEPT  
PURPOSES ONLY

PROJECT No.:	MD8230009.00
DRAWN BY:	ER
CHECKED BY:	EM
DATE:	04/27/2023
CAD ID:	

**TREE  
CONSERVATION  
PLAN TYPE 2**

FOR

OURISMAN OF BOWIE

PROPOSED  
DEVELOPMENT

2550 NW ROBERT CRAIN HIGHWAY  
BOWIE, MD 20716  
PRINCE GEORGE'S COUNTY

**BOHLER //**

16701 MELFORD BLVD., SUITE 430  
BOWIE, MARYLAND 20715  
Phone: (301) 809-4500  
Fax: (301) 809-4501  
MD@BohlerEng.com

**E.R. McWILLIAMS**

09/03/2025

PROFESSIONAL LANDSCAPE ARCHITECT  
MARYLAND LICENSE # 14-3627

I, ERIC McWILLIAMS, HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR PROVIDED BY ME, AND THAT I AM A FULLY LICENSED PROFESSIONAL LANDSCAPE ARCHITECT UNDER THE JURISDICTION OF THE STATE OF MARYLAND. LICENSE # 14-3627 EXPIRATION DATE: 9/30/26

SHEET TITLE:  
**COVER SHEET**

SHEET NUMBER:  
**1**

ORG. DATE - 04/27/2023

Prince George's County Planning Department, M-NCPPC Environmental Planning Section TYPE 2 TREE CONSERVATION PLAN APPROVAL TCP2-XXX-XXXX			
Approved by	Date	DRD #	Reason for Revision
00			
01			
02			
03			







April 18, 2025

**MEMORANDUM**

**TO:** David Myerholtz, Planner III, Urban Design Section, Development Review Division

**VIA:** N. Andrew Bishop, Planner IV, Long-Range Planning Section, Community Planning Division *NAB*

**VIA:** Kierre McCune, Supervisor, Master Plans and Studies Section, Community Planning Division *SB for KM*

**FROM:** Thomas Lester, Planner IV, Long-Range Planning Section Community Planning Division *TL*

**SUBJECT:** DSP-23026 Ourisman of Bowie

**FINDINGS**

Pursuant to Part 3, Division 9, Subdivision 3 of the Prior Zoning Ordinance, Master Plan conformance is not required for this application.

**BACKGROUND**

**Application Type:** Detailed Site Plan for property located outside of an overlay zone.

**Planning Area:** 71B

**Community:** City of Bowie

**Location:** Western side of Robert Crain Highway, south of its intersection with Mount Oak Road at 2410, 2504, and 2550 NW Robert Crain Highway, Bowie, Maryland 20716

**Size:** 6.29 acres

**Existing Uses:** Vacant

**Future Land Use:** Commercial

**Proposal:** Construction of a 22,111-square-foot automobile dealership.

**Existing Zoning:** Commercial, Service (CS) Zone

**Prior Zoning:** Commercial, Miscellaneous (C-M) Zone

**Applicable Zoning Ordinance:** Prior Zoning Ordinance

## GENERAL PLAN, MASTER PLAN, AND SMA

**General Plan:** Plan Prince George's 2035 places this property is within the Established Communities Growth Policies Area (Map 1. Prince George's County Growth Policy Map, p. 18). The vision for Established Communities is sensitive infill and low- to medium-density development. Plan 2035 classifies existing residential neighborhoods and commercial areas served by public water and sewer outside of the Regional Transit Districts and Local Centers, as Established Communities

**Master Plan:** The 2022 *Approved Bowie-Mitchellville and Vicinity Master Plan* recommends **Commercial** land uses on the subject property (p. 50). Commercial Land Use is defined as Retail and business areas, including employment uses, such as office and services. A range of services are provided at the neighborhood to regional level. New commercial areas have access to multimodal transportation options. These areas are intended to remain predominantly or entirely commercial (p. 49).

The master plan recommends the following strategies that affect the subject property and should be considered:

### Transportation and Mobility

- **Policy TM 2** All streets in Bowie-Mitchellville and Vicinity should accommodate traffic at Plan 2035-recommended levels of service (LOS).
  - **TM 2.4** Reconstruct or construct streets as recommended in Appendix D. Recommended Master Plan Transportation Facilities (p. 113). And **TM 3.2** Construct the pedestrian and bicycle facilities identified in Appendix D. Recommended Master Plan Transportation Facilities (p. 113). Appendix D recommends:
    - A-61 Robert Crain Highway (Parallel Route) from MD 214 to Mount Oak Road, should include 120' ROW with 4 lanes, bicycle lanes in both directions, a landscaping buffer in both directions, and 8-foot-wide sidewalks (p. 247).
- **Policy TM 3** Enhance active transportation infrastructure to create greater quality of life and attract businesses and employees.
  - **TM 3.1** Ensure all streets in Bowie-Mitchellville and Vicinity's Centers and Established Communities have sidewalks (p. 113).
- **Policy TM 26** Ensure access to all properties in the US 301 corridor during and after its upgrade to a limited-access freeway (F-10).
  - **TM 26.1** Construct a four-lane arterial road (A-61) west of, and parallel to F-10 from Mount Oak Road south toward Upper Marlboro to replace Robert Crain Highway and to provide access to properties that currently access or front the west side or median of US 301 (Robert Crain Highway) (p. 136).
  - **TM 26.2** Provide shared-use paths along A-61 as well as US 301 (F-10). Where US 301 (F-10) and A-61 abut, a single shared-use path can be provided between the roadways (p. 136).
  - **TM 26.3** A-61 should be a north-south route parallel to US 301 (F-10) and provide access to adjacent communities and to US 301 (F-10) (p. 136).

- **Policy TM 27** Increase opportunities for safe walking and bicycling along the US 301/MD 3 corridor.
  - **TM 27.2** Construct bicycle lanes with a landscaping buffer and eight-foot sidewalks on both sides of A-61 (p. 136).
  - **TM 27.5** Construct shared-use paths along both sides of US 301 and A-61 (p. 136).
- **Policy TM 28** Support enhanced regional mobility and the movement of goods.
  - **TM 28.5** Do not approve any development project along the US 301 corridor without the dedication, reservation, or other acquisition of the rights-of-way necessary to construct A-61 and F-10. Any construction in the right-of-way of A-61 or F-10 will fail to preserve the integrity of this master plan and cannot be permitted, pursuant to Section 27-3617(c)(4) of the 2018 Zoning Ordinance (p. 137).

**Analysis:** *The Detailed Site Plan fails to implement several transportation recommendations outlined in the master plan. Particularly those concerning the reconstruction of streets to include bicycle/pedestrian facilities. First, the Detailed Site Plan does not incorporate the wide bicycle lanes or shared-use path, and 8-foot-wide sidewalk along Robert Crain Highway (US 301) (A-61). Therefore, the Detailed Site Plan does not fulfill the transportation infrastructure recommendations outlined in the Master Plan. The applicant should work with the Transportation Section and State Highway Administration (SHA) to determine the feasibility and implementation of these recommended transportation facilities.*

### **Natural Environment**

- **NE 1** Ensure that areas of connectivity and ecological functions are maintained, restored, or established during development or redevelopment.
  - **NE 1.1** Use the green infrastructure network as a guide to decision-making, and as an amenity in the site design and development review processes (p. 142).

**Analysis:** *The subject properties includes Evaluation and Regulated portions of the Green Infrastructure Network. The Detailed Site Plan shows the retention of portions of the green infrastructure network and meets this recommendation.*

- **Policy NE 4** Support street tree plantings along transportation corridors and streets, reforestation programs, and retention of large tracts of woodland to the fullest extent possible to create a pleasant environment for active transportation users including bicyclists and pedestrians.
  - **NE 4.2** Plant street trees to the maximum extent permitted along all roads and trail rights-of-way (p. 145).

**Analysis:** *The Detailed Site Plan shows trees planted along the frontage of Robert Crain Highway (US 301) (A-61) and meets this recommendation.*

- **Policy NE 5** Reduce urban heat island effect, thermal heat impacts on receiving streams, and reduce stormwater runoff by increasing the percentage shade and tree canopy over impervious surfaces.

- **NE 5.1** Retrofit all surface parking lots using ESD and best stormwater management practices when redevelopment occurs. Plant trees wherever possible to increase tree canopy coverage to shade impervious surfaces, to reduce urban heat island effect, limit thermal heat impacts on receiving streams, and slow stormwater runoff amenity in the site design and development review processes (p. 155).

**Analysis:** *The Detailed Site Plan includes stormwater management facilities and other Environmental Site Design features such as trees, and shaded impervious surfaces, meeting this recommendation.*

### **Housing and Neighborhoods**

- **Policy HN 3** Encourage exterior home improvements that enhance the appearance and perceived safety of neighborhoods.
  - **HN 3.3** Implement Crime Prevention Through Environmental Design (CPTED) strategies with new and redeveloped projects that include unobstructed pedestrian-friendly sidewalks, well-lit parking areas, building entrances and yards, and well-maintained landscaping and common areas (p. 155).

**Analysis:** *The Detailed Site Plan's proposed site design is generally consistent with CPTED Principles, more specifically the project exhibits unobstructed pedestrian-friendly sidewalks, well-lit parking areas, building entrances, and yards, and well-maintained landscaping and common areas and meets this recommendation.*

**Aviation/MIOZ:** This application is not located within an Aviation Policy Area or the Military Installation Overlay Zone.

**SMA/Zoning:** The 2024 *Approved Bowie-Mitchellville and Vicinity Sectional Map Amendment* retained the subject property in the **Commercial, Service (CS) Zone**.



Countywide Planning Division  
Environmental Planning Section

301-952-3650

October 17, 2025

**MEMORANDUM**

**TO:** David Myerholtz, Planner III, Urban Design Section, DRD  
**VIA:** Tom Burke, Supervisor, Environmental Planning Section, CWPD *TB*  
**FROM:** Mary Rea, Planner II, Environmental Planning Section, CWPD *MR*  
**SUBJECT:** **Ourisman of Bowie; DSP-23026 and TCP2-089-05-01**

The Environmental Planning Section (EPS) has reviewed the above-referenced Detailed Site Plan (DSP-23026) received on March 17, 2025. Comments were provided in a Subdivision and Development Review Committee (SDRC) meeting on March 28, 2025. EPS finds the application in conformance with Sections 27-285(b)(3), 27-285(b)(4), 27-282(e)(5), 27-282(e)(9), 27-282(e)(11) of the prior Zoning Ordinance, and 24-131 of the prior Subdivision Regulations, and recommends approval of DSP-23026 and Type 2 Tree Conservation Plan (TCP2-089-05-01) subject to the findings and conditions listed at the end of this memorandum.

**BACKGROUND**

EPS previously reviewed the following applications and associated plans for the subject site:

<b>Development Review Case #</b>	<b>Associated Tree Conservation Plan #</b>	<b>Authority</b>	<b>Status</b>	<b>Action Date</b>	<b>Resolution Number</b>
4-04047	TCPI-032-04	Planning Board	Approved	7/22/2004	04-177
DSP-05013	TCPII-089-05	Planning Board	Approved	7/21/2005	05-173
NRI-081-2024	N/A	Staff	Approved	8/19/2024	N/A
DSP-23026	TCP2-089-05-01	Planning Board	Pending	Pending	Pending

**PROPOSED ACTIVITY**

The applicant is requesting approval of a DSP and a TCP2 for the construction of car dealership. The current zoning for the site is Commercial, Service (CS); however, the applicant has opted to apply the zoning standards to this application that were in effect prior to April 1, 2022, for the Commercial Miscellaneous (C-M) Zone.

**APPLICABLE ENVIRONMENTAL REGULATIONS**

The site is subject to Division 2 of the Woodland and Wildlife Habitat Conservation Ordinance.

The development is subject to the environmental regulations contained in prior Subtitles 24 and 27 of County Code because the application was subject to prior approvals associated with Preliminary Plan of Subdivision (4-04047).

#### **SITE DESCRIPTION**

This 6.29-acre site is located on the west side of US 301 (Robert Crain Highway) and is approximately 400 feet south of Mount Oak Road. A review of the available information indicates that streams, wetlands, 100-year floodplain, and severe slopes are present on-site. According to the Sensitive Species Project Review Area map received from the Maryland Department of Natural Resources Natural Heritage Program and provided on PGAtlas, there are no rare, threatened, or endangered (RTE) species found to occur on or near this property. The site has frontage on Robert Crain Highway, which is identified as a Master Plan freeway.

#### **REVIEW OF PRIOR APPROVALS**

Preliminary Plan of Subdivision (4-04047) was approved by the Planning Board on July 22, 2004, and conditions of approval can be found in PGCPB No. 04-177. DSP-05013 was approved by the Planning Board on July 7, 2005; however, the conditions of approval for the previous DSP do not apply for this case, because it has expired and the conditions of approval associated therein no longer are binding on the project.

#### **Preliminary Plan of Subdivision 4-04047**

The following text addresses previously approved applicable environmental conditions that need to be addressed with this application. The text in **bold** is the actual text from the previous cases or plans. The plain text provides comments on the plan's conformance with the conditions.

#### **1. Development of this subdivision shall be in compliance with an approved Type I Tree Conservation Plan (TCPI/32/04). The following note shall be placed on the Final Plat of Subdivision:**

**"This development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCPI/32/04), or modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy."**

Type 2 tree conservation plan (TCP2-089-05-01) was submitted with this application and is in compliance with TCPI-32-04.

#### **2. A conservation easement shall be described by bearings and distances at time of final plat. The conservation easement shall contain the delineated Patuxent River Primary management Area, except for areas with approved impacts, and shall be reviewed by the Environmental Planning Section prior to certificate approval for accuracy. In addition, the following note shall be placed on the final plat:**

**"Conservation easements described on this plat are areas where the installation of structures and roads and the removal of vegetation are prohibited without prior written consent from the M-NCPPC Planning Director or designee. The removal of**

**hazardous trees, limbs, branches, or trunks is permitted.”**

This condition was met at the time of the final plat acceptance.

**3. Prior to the issuance of any permits which impact wetlands, wetland buffers, streams, or water of the U.S., copies of all federal and state wetland permits, evidence that approval conditions have been complied with, and associated mitigation plans shall be submitted to the M-NCPPC Planning Department.**

This condition will be met at the time of permitting.

**8. Prior to the issuance of building permits, the applicant shall obtain detailed site plan approval by the Planning Board. The detailed site plan shall address buffering (including light and noise intrusion), landscaping, access, techniques to further minimize PMA impacts, and the visual relationships between the proposed development and adjoining residentially developed properties.**

This condition will be met at the time of permitting.

**9. Development of this subdivision shall be in compliance with the approved stormwater management concept plan (#8245-2004-01) or any approved revision thereto.**

Jurisdiction of stormwater management regulations has transferred from the County to the City of Bowie since the approval of the PPS. Stormwater management concept plan #8245-2004-01 was replaced with the City of Bowie stormwater management approved concept #02-1223-204NE14. This application is in compliance with the City of Bowie’s approved stormwater management concept plan.

## **ENVIRONMENTAL REVIEW**

### **Natural Resource Inventory/Environmental Features**

In conformance with Section 24-120(a)(22) of Subdivision Regulations, an approved Natural Resource Inventory (NRI-081-2024) was submitted with the application. The site contains streams, wetlands, 100-year floodplain, and severe slopes. No additional information is required for conformance to the NRI.

### **Woodland Conservation**

This property is subject to Division 2 of the Woodland and Wildlife Habitat Conservation Ordinance (WCO) and the Environmental Technical Manual (ETM). Type 2 Tree Conservation Plan (TCP2-089-05) was approved for the site on April 28, 2006. The worksheet on the prior TCP2 showed that the site contained 2.94 acres of woodland in the net tract and 1.86 acres of woodlands in the floodplain. Of this, 1.91 acres of net tract woodlands and 0.11 acre of woodlands in the floodplain were approved to be cleared. The woodland requirement for the TCP2 was 1.17 acres, which was to be met by 1.03 acres of on-site preservation and 0.14 acre to be met by fee-in-lieu. The site was cleared based on this prior TCP2, but without a permit from the County. Since a permit was not obtained prior to the current WCO going into effect, this application shall meet the current WCO regulations. To address the clearing that has occurred on-site, the current TCP2 worksheet calculations shall be based on the site woodlands as existing in June 2022.

TCP2-089-05-01 was submitted with this application showing that this 6.29-acre site contains 2.76 acres of floodplain for a net tract area of 3.53 acres. The TCP2 shows that the site contains 3.80 acres of woodlands in the net tract and 2.75 acres in the floodplain, as of June 2022. The amount of woodlands shown is larger than the gross tract. Prior to signature approval the worksheet will need to be corrected to reflect the amount of woodlands on-site in June 2022. Based on the acreage provided, the site woodland conservation threshold is 0.53 acre. The applicant plans to clear 1.82 acres of woodlands in the net tract and 0.14 acre in the floodplain. The TCP2 shows the woodland conservation requirement of 2.49 acres being met with 0.99 acre of preservation, 0.03 acre of on-site reforestation, and 1.47 acres of off-site woodland conservation credits. As proposed, the development does meet the 15 percent woodland conservation threshold on-site.

### **Riparian Stream Buffer**

The site contains a riparian stream buffer that is required to be fully wooded in accordance with Section 25-121(c)(1)(C) of County Code; however, this site qualifies for exceptions to this requirement based on criteria outlined in the code. The TCP2 and SOJ indicate that clearing is proposed within the stream and stream buffer for a sewer connection which was previously approved and unavoidable. This impact is discussed in the PMA impact section herein. Staff find compliance with the exceptions demonstrated with the application as submitted.

### **Specimen Trees**

Tree conservation plans are required to meet all the requirements of Subtitle 25, Division 2 of County Code, including the preservation of specimen trees in Section 25-122(b)(1)(G). Every effort should be made to preserve the trees in place, considering the different species' ability to withstand construction disturbance (refer to the Construction Tolerance Chart in the ETM for guidance on each species' ability to tolerate root zone disturbances).

If, after careful consideration has been given to the preservation of the specimen trees, there remains a need to remove any of the specimen trees, a variance from Section 25-122(b)(1)(G) of County Code is required. Applicants can request a variance from the provisions of Division 2 of Subtitle 25 (the WCO) provided all of the required findings in Section 25-119(d) can be met. An application for a variance must be accompanied by an SOJ stating the reasons for the request and how the request meets each of the required findings.

### **Review of Subtitle 25 Variance Request**

The site contains 11 specimen trees. A Subtitle 25 variance application and an SOJ in support of the variance were received on September 15, 2025, and resubmitted on October 17, 2025. The request is for the removal of two specimen trees, identified on the TCP2 as ST-1377 and ST-1380. The trees are in poor and fair condition as indicated on the approved NRI. The TCP2 shows the location of the proposed trees for removal in the area of the proposed parking lot and access road.

It should be noted that specimen tree ST-1377 was previously approved for removal with TCPII-89-05. Also, the area where ST-1380 is located is shown as developed on TCPI-32-04; however, at the time of that application, the tree had likely not achieved specimen tree status and was therefore not identified on the plan.

Details specific to individual trees have also been provided in the following table:

**SPECIMEN TREE SCHEDULE SUMMARY FOR TREES PROPOSED FOR REMOVAL**

ST #	COMMON NAME	Diameter at Breast Height -DBH (In inches)	Condition	Retain/Remove	Proposed Impact
ST-1377	Red maple	52	Poor	Remove	Within location of the parking lot
ST-1380	Sweet gum	31	Fair	Remove	Within location of the access road

**Analysis of 25-122(b)(1)(G) WCO Variance Request**

Section 25-119(d)(3) of the WCO contains six findings (text in **bold** below) to be made before a variance from the WCO can be granted. The submitted SOJ seeks to address the required findings for two specimen trees (ST-1377 and ST-1380) proposed for removal. Considerations for staff recommendation include construction tolerance, distance from development impacts to the trees, and condition of the trees. An evaluation of this variance request, with respect to the required findings, is provided below. Staff support the removal of the two specimen trees requested by the applicant, based on these findings.

**(A) Special conditions peculiar to the property have caused the unwarranted hardship**

The applicant states in the variance request that special conditions peculiar to the property have caused unwarranted hardship. In relation to other properties in the area, special conditions peculiar to the property would cause an unwarranted hardship if the applicant were required to retain the two specimen trees identified as specimen trees ST-1377 and ST-1380. Those special conditions relate to the specimen trees themselves, such as their size, condition, species, and on-site location.

This specimen tree removal variance request was evaluated using the Woodland and Wildlife Habitat Conservation Priorities as outlined in Section 25-121(b)(1) of the WCO. The specimen trees requested for removal will allow for the protection of the woodlands with the highest priorities as listed in Section 25-121(b)(1) of the WCO to the maximum extent practicable and allow for the development of this site to occur in the lower priority areas of the site. Requiring the applicant to retain these two specimen trees on the site by designing the development to avoid impacts to the critical root zone (CRZ) would further limit the area of the site available for the orderly development that is consistent with the zoning and the recommendations of the master plans, to the extent that it would cause the applicant an unwarranted hardship.

This site sits on the west side of US 301 (Robert Crain Highway). There is developed land to the south and the southwest of the site. Along the northern and northwest portion of the property there is a stream and 100-year floodplain. The site has obtained prior PPS and DSP approvals for the same development area as this DSP. A conservation and floodplain easement have already been recorded protecting these regulated areas. The development requires stormwater management (SWM), grading, safe circulation, utilities, and landscaping on-site, in conformance with other sections of the County Code.

The applicant has designed the site in such a way that the proposal is in the developable areas outside of the PMA except for areas previously approved by the PPS for development.

The SOJ indicates that specimen trees ST-1377 and ST-1380 are proposed for removal because they are located within the footprint of the proposed buildings and access road to the adjoining property. The two specimen trees requested for removal are scattered throughout the site, are not located within the PMA, and are proposed to be removed to successfully construct the proposed development as envisioned with the PPS.

Requiring the applicant to retain the two specimen trees on the site would further limit the area of the site available for development, to the extent that it causes an applicant unwarranted hardship. Alternatives to save these trees would compromise other requirements of the zones, the sector plan, and the County Code given their location.

Based on the uniqueness of the property setting and the location of the trees, staff finds that the two specimen trees are located on the developable portion of the site, and in areas necessary to meet the design and infrastructure requirements.

**(B) Enforcement of these rules will deprive the applicant of rights commonly enjoyed by others in similar areas**

Enforcement of the requirement that all specimen trees be preserved, along with an appropriate percentage of their CRZ, would deprive the applicant of rights commonly enjoyed by others in similar areas with comparable zoning. All variance applications for the removal of specimen trees are evaluated in accordance with the requirements of the WCO and the ETM for site-specific conditions.

Specimen trees grow to such a large size because they were left undisturbed on a site for sufficient time to grow. The development is required to provide SWM, grading, safe circulation, utilities, and landscaping on-site in conformance with other sections of the County Code. The applicant is seeking to develop this property in conformance with the applicable provisions of the Zoning Ordinance. The applicant has designed the site in a way which maximizes the buildable areas of the site, while limiting the impacts to the PMA to only those which are allowable, reasonable, and necessary. The removal of specimen trees for the installation of parking and an access road is expected with development. This application localizes the removal of specimen trees away from the REF. Other sites that contain constraints and conditions similar to these would be given the same considerations during the review of the variance application.

**(C) Granting the variance will not confer on the applicant a special privilege that would be denied to other applicants**

All variance applications for the removal of specimen trees are evaluated in accordance with the requirements of Subtitle 25 of County Code and the ETM for site-specific conditions. When similar trees were encountered on other sites for comparable developments, they have been evaluated under the same criteria.

The applicant states that given the evidence in Variance Findings (B) above, not granting the variance would prevent the project from being developed within the County standard design parameters for commercial use. The applicant has made considerable efforts to avoid additional PMA impacts. The two specimen trees proposed for removal are in poor and fair condition. The applicant is preserving 0.99 acre of woodland and retaining an additional 2.61 acres of woodland that is not being credited due to being in the floodplain. This is not a special privilege that would be denied to other applicants. If other properties encounter trees in similar locations on a site with REF and PMA, the same considerations would be provided during the review of the variance application.

**(D) The request is not based on conditions or circumstances which are the result of actions by the applicant**

The request for removal of the two trees is a result of their location on the property and the limitations on site design which are not the result of actions by the applicant. SWM, road improvements, slope stability, and other requirements are established by the County. Any development on this site would be subject to meeting the current requirements of the County based on the scope of that proposed development. The removal of the two specimen trees is requested to achieve the development for the proposed car dealership with associated infrastructure, and woodland conservation.

**(E) The request does not arise from a condition relating to land or building use, either permitted or nonconforming, on a neighboring property**

The variance SOJ states that this request is not from a condition on a neighboring property. The request to remove the specimen trees does not arise from a condition relating to land or building use, either permitted or nonconforming on a neighboring property. The trees have grown to this size because of favorable conditions and lack of disturbance.

**(F) Granting of the variance will not adversely affect water quality**

The site is governed by the state and local stormwater management regulations, which require the post-development site to mimic pre-development conditions as “woods in good condition.”

Granting the variance for the removal of two specimen trees will not adversely affect water quality because the applicant is required to meet current stormwater management requirements on-site. Stormwater requirements will be evaluated by the City of Bowie and additional information regarding the proposed stormwater facilities can be located in the stormwater section of this memorandum. Sediment and erosion control measures for this site will be subject to the requirements of Prince George’s County Soil Conservation District (PGSCD). Both SWM and sediment and erosion control requirements are to be met in conformance with state and local laws to ensure that the quality of water leaving the site meets the state’s standards, which are set to ensure that no degradation occurs.

**Summary**

The required findings of Section 25-119(d) of the WCO have been adequately addressed for the removal of two specimen trees identified as Specimen Trees ST-1377 and ST-1380.

As noted above, specimen tree ST-1377 was previously approved for removal with TCPII-89-05, and the area where ST-1380 is located is shown as developed on TCPI-32-04.

Staff recommend that the Planning Board approve the requested variance to Section 25-122(b)(1)(G) of the WCO for the removal of these two specimen trees for the construction of a car dealership in C-M Zone. The replacement requirement for the specimen tree removal, in accordance with Section 25-119(d)(7) of the WCO will be evaluated at the time of signature approval.

**Preservation of Regulated Environmental Features (REF)/Primary Management Area (PMA)**

Regulated environmental features (REF) are required to be preserved and/or restored to the fullest extent possible under Section 24-130(b)(5) of the Subdivision Regulations. The on-site REF includes streams, stream buffers, wetlands, wetland buffers, 100-year floodplain, and steep slopes.

Impacts to the PMA were previously approved with PPS-4-04047 for a stormwater management outfall, a sewer connection, and for the construction of a portion of the proposed parking lot. This DSP is reliant on that prior approval. No additional impacts were requested with this application.

**Soils**

In accordance with Section 24-131, this application was reviewed for unsafe land restrictions. The predominant soils found to occur according to the U.S. Department of Agriculture Natural Resource Conservation Service Web Soil Survey include Annapolis-Urban land complex, and Widewater and Issue soils, frequently flooded. A geotechnical report and a global stability analysis report, both prepared by ECS Mid-Atlantic, LLC, were submitted for the evaluation of the site and the tall retaining walls accommodating the proposed development. No Marlboro clay or Christiana clay was present on-site according to the report. It appears that the analyses have met the minimum required factor of safety greater than 1.5. The geotechnical analyses shall be reviewed and approved by the DPIE's geotechnical engineer prior to the issuance of building and grading permits.

The application included a Phase I and II Environmental Site Assessment which is for the property across the street from this project. Also, the application included a letter from the MDE Oil Control Program for the property across the street, which stated that any petroleum contamination present on-site does not present a risk for the current non-residential exposure pathways.

**Stormwater Management**

In accordance with Section 24-120(8), an approved Stormwater Management Concept plan and approval letter (No. 02-1223-204NE14) approved by the City of Bowie were submitted with this application. Proposed stormwater management features include micro-bioretenion facilities, and underground storage. The project requires no fee-in-lieu of providing on-site attenuation/ quality control measures. No further information pertaining to stormwater management is required.

**SUMMARY OF RECOMMENDED FINDINGS AND CONDITIONS**

The Environmental Planning Section recommends approval of Detailed Site Plan DSP-23026 and TCP2-089-05-01 subject to the following recommended findings and conditions.

### **Recommended Findings**

1. In conformance with Section 25-122(b)(1)(G) of the County Code, the required findings of Section 25-119(d) of County Code have been adequately addressed for the removal of two specimen trees identified as Specimen Trees ST-1377 and ST-1380.
2. In conformance with 24-130(b)(5) of the Subdivision Regulations, the regulated environmental features (REF) on the subject property have been preserved and/or restored to the fullest extent possible based on the limits of disturbance shown on the Type 2 tree conservation plan. No additional PMA impacts were requested with this application.

### **Recommended Conditions:**

1. Prior to signature approval of the detailed site plan, the Type 2 tree conservation plan shall be revised as follows:
  - a. Add the following note under the specimen tree table:

“NOTE: This plan is in accordance with the following variance from the strict requirements of Subtitle 25 approved by the Planning Board on (ADD DATE) with DSP-23026 for the removal of two specimen trees (Section 25-122(b)(1)(G)) specifically Specimen Trees ST-1377 and ST-1380.”
  - b. Provide a 2 inch by 2 inch outlined blank square on each page for an approval block (to be inserted by staff at time of approval).
  - c. Provide a Type 2 tree conservation plan (TCP2) approval history table, which can be found on the Commission’s website.
  - d. Correct the woodland conservation worksheet to reflect the correct amount of woodlands on-site as of June 2022.
  - e. Add the specimen tree removal worksheet to the plan.
  - f. Correct the Site Statistic Table to reflect the correct amount of existing woodlands as of June 2022.
  - g. Label the specimen trees.
  - h. Show sewer connection.
  - i. Show all proposed grades.
  - j. Label top and bottom elevation of the retaining walls.
  - k. Add the signed owner’s awareness certificate.

2. Prior to the issuance of any permits impacting 100-year floodplain, wetlands, wetland buffers, streams, or Waters of the U.S., the applicant shall submit copies of all federal and state wetland permits, evidence that approval conditions have been complied with, and associated mitigation plans.



Countywide Planning Department  
Transportation Planning Section

301-952-3650

October 17, 2025

**MEMORANDUM**

TO: David Myerholtz, Urban Design Section, Development Review Division

FROM: *Jon*  
*Wilson* Jon Wilson, Transportation Planning Section, Countywide Planning Division

VIA: *NS* Noelle Smith, AICP, Transportation Planning Section, Countywide Planning Division  
Crystal Saunders Hancock, Transportation Planning Section, Countywide Planning Division

**SUBJECT: Ourisman of Bowie, DSP-23026**

**Prior Conditions of Approval**

The subject application is governed by Preliminary Plan of Subdivision (PPS) 4-04047, Detailed Site Plan (DSP-05013) and DSP-05013-01. The following conditions still apply:

**4-04047**

10. The applicant shall provide for any necessary turn lanes and frontage improvements along US 301 as required by SHA. These may include turn lanes for deceleration and acceleration of vehicles and channelization (lane divider) at the site entrance to prevent weaving.

**Comment:** Per correspondence from the State Highway Administration (SHA), it is desired to retain the current 12-foot-wide-shoulder with the addition of curb and gutter and tapering to accommodate the right turning movement into the site.

11. Prior to the issuance of any building permits within the subject property, the following road improvements shall (a) have full financial assurances, (b) have been permitted for construction, and (c) have an agreed-upon timetable for construction with SHA:
  - a. Widen the westbound approach of Queen Anne Bridge Road at US 301 to provide an exclusive right turn lane and two through lanes. Widen the median to accommodate two through lanes from westbound Queen Anne Bridge Road, to create one left turn lane and one through lane. These improvements shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.
  - b. Construct a free right turn lane on eastbound Mitchellville Road at US 301 and

change from signal controlled movement to free-flow right turn movement. This improvement shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.

- c. The applicant will be responsible for any traffic signal modifications as a result of the proposed intersection improvements.
- d. The type of material, length and construction of the channelized lane (lane divider), and how the merge of traffic will be handled beyond the channelized lane shall be determined.

**Comment:** This condition will be evaluated at the time of permitting.

- 12. Total development within the subject property shall be limited to 29,800 square feet of automobile dealership facilities, or equivalent development that generates no more than 52 AM and 62 PM new peak-hour trips, taking into account estimated pass-by trips. Any development other than that identified herein above shall require a new preliminary plan of subdivision with a new determination of the adequacy of transportation facilities.

**Comment:** The development will not exceed the established trip cap set by the PPS.

- 13. The applicant, his successors and/or assignees shall conduct a traffic signal warrant study at the US 301/Walker Pontiac median crossover (for both northbound and southbound US 301). If signals are warranted and desired by the State Highway Administration, the applicant shall install the signals prior to the issuance of a use and occupancy permit.

**Comment:** This condition will be evaluated at the time permitting.

### **Master Plan Compliance**

The site is subject to the 2009 *Approved Countywide Master Plan of Transportation* (MPOT) and the 2022 *Approved Bowie-Mitchellville and Vicinity Master Plan*.

#### Right of Way (ROW)

US 301 (Crain Highway) (A-61): Varied ROW

**Comment:** The MPOT recommends a varied right-of-way. Staff recommends delineation of right-of-way on Crain Highway to be identified on plan sheets.

#### Master Plan Pedestrian and Bike Facilities

US 301 (Crain Highway): Planned bicycle Lane and shared use path

**Comment:** The sector plan recommends an eight-foot-wide shared-use path and standard bicycle lane along Crain Highway. However, the implementation of a bicycle lane is beyond the scope of a DSP and can be implemented as part of a future State roadway improvement project. Per the SHA, sidewalk is not desired along the property frontage of Crain Highway due to grade and continuity.

#### Recommendations, Policies, and Goals

MPOT Complete Streets Policies:

**Policy 2: All road frontage improvements and road capital improvement projects within the Developed and Developing Tiers shall be designed to accommodate all modes of transportation. Continuous sidewalks and on-road bicycle facilities should be included to the extent feasible and practical.**

**Comment:** Per SHA, there are no frontage improvements required with this application.

*2022 Approved Bowie-Mitchellville and Vicinity Master Plan*

**TM 27.3**

**Provide shared -use path along A-61 as well as US 301 (F-10).**

**Comment:** Per SHA, there are no frontage improvements required with this application.

**TM 28.2**

**Construct bicycle lanes with a landscaping buffer and eight-foot sidewalks**

**Comment:** Per SHA, there are no frontage improvements required with this application.

**Transportation Planning Review**

**Zoning Ordinance Compliance**

Section 27-274 of the Prince George's County Zoning Ordinance (Ordinance) provides guidance for detailed site plans. The section references the following design guidelines described in Section 27-274(2):

**Parking, loading, and circulation.**

- A. Surface parking lots should be located and designed to provide safe and efficient vehicular and pedestrian circulation within the site, while minimizing the visual impact of cars. Parking spaces should be located to provide convenient access to major destination points on the site.**

**Comment:** Access to the site is provided by one driveway along Crain Highway. The development requires 78 parking spaces of which 79 are provided to include 4 ADA, 17 compact, 58 standard parking spaces and 5 bicycle racks to accommodate 10 bicycles.

- B. Loading areas should be visually unobtrusive and located to minimize conflicts with vehicles or pedestrians.**

**Comment:** The site plan demonstrates two 15 x 33 feet loading spaces located in the rear of the subject property. The location of the loading spaces does not interfere with internal vehicular circulation or pedestrian walkways. Staff find that the intent of this section has been met.

- C. Vehicular and pedestrian circulation on a site should be safe, efficient, and convenient for both pedestrians and drivers.**

**Comment:** A circulation plan was provided demonstrating the movements of bicyclist, pedestrians and vehicles; staff find it acceptable.

**Conclusion**

Based on the findings presented above, staff conclude that the vehicular, pedestrian, and bicycle access and circulation for this plan are acceptable, consistent with the site design guidelines pursuant to Section 27 of County Code and meet the findings for pedestrian and bicycle transportation purposes if the following condition is met:

1. Prior to the certification of the detailed site plan, the applicant and the applicant's heirs, successors, and/or assigns shall show these facilities as part of the site plan:
  - a. Identify right-of way along the property frontage of US 301 (Crain Highway).



Countywide Planning Division  
Historic Preservation Section

301-952-3680

September 16, 2025

**MEMORANDUM**

**TO:** David Myerholtz, Urban Design Section, Development Review Division

**VIA:** Thomas Gross, Planning Supervisor, Historic Preservation Section, Countywide Planning Division **TWG**

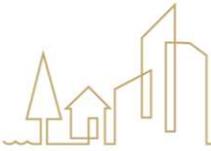
**FROM:** Jennifer Stabler, Historic Preservation Section, Countywide Planning Division **JAS**  
Tyler Smith, Historic Preservation Section, Countywide Planning Division **TAS**  
Amelia Chisholm, Historic Preservation Section, Countywide Planning Division **AGC**

**SUBJECT: DSP-25006 The Promise**

The subject property comprises 15.1 acres and is located on the southeast side of Southern Avenue, approximately 2,100 feet northeast of its intersection with Wheeler Road in Oxon Hill. The subject property was zoned Mixed Use Transportation Oriented (M-X-T), per the prior Zoning Ordinance, and is located within the Prior Development District Overlay Zone for the 2014 *Approved Southern Green Line Station Area Sector Plan and Sectional Map Amendment*. The subject application proposes a mixed-use development of seven buildings to include 985 dwelling units, an 18,946-square-foot grocery store, an approximately 150-student daycare center, and another 9,411 square feet of retail space.

The 2014 *Approved Southern Green Line Station Area Sector Plan and Sectional Map Amendment* contains limited information related to historic preservation (p. 84). There are no goals and policies specific to the subject site or applicable to the proposed development. A search of current and historic photographs, topographic and historic maps, and locations of currently known archeological sites indicates the probability of archeological sites within the subject property is low. A Phase I archeological survey was completed on the subject property in 1980 as part of the Metrorail Green Line extension. While archeological sites were identified during that survey, none were identified on the subject property. The subject property does not contain, and is not adjacent to, any designated Prince George's County Historic Sites or resources.

Historic Preservation Section staff recommends approval of DSP-25006, The Promise, with no conditions.



April 18, 2025

**MEMORANDUM**

TO: David Myerholtz, Planner III, Urban Design Section

FROM: Alice Jacobs, Planning Technician III, Permit Review Section 

SUBJECT: DSP-23026 – Ourisman of Bowie

1. The tree canopy coverage schedule on the plan is for a different project unrelated to this application.
2. A signage calculation table is suggested be added to the signage pages.
3. The Permit Review Section offers no further comments on this application at this time.

Date: August 4, 2025

To: David Myerholtz, Urban Design, M-NCPPC

From: Adebola Adepoyu, Environmental Health Specialist, Environmental Engineering/ Policy Program

Re: Ourisman Genesis of Bowie (DSP-23026)

The Environmental Engineering / Policy Program of the Prince George's County Health Department has completed a desktop health impact assessment review of the detailed site plan submission for the Ourisman Genesis of Bowie located at 2550 NW Robert Crain Highway, Bowie 20716 and has the following comments / recommendations:

1. The applicant should assure that all sources of air pollution have been registered with the Maryland Department of the Environment, Air and Radiation Management Administration. Such sources include gasoline underground storage tanks, degreasing tanks and paint spraying operations. Contact MDE – ARMA at 800-633-6101.
2. During the construction phases of this project, noise should not be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.
3. During the construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control

If you have any questions or need additional information, please contact me at 301-883-7677 or [aoadepoyu@co.pg.md.us](mailto:aoadepoyu@co.pg.md.us).



# City of Bowie

15901 Fred Robinson Way  
Bowie, Maryland 20716

May 7, 2025

The Honorable Peter A. Shapiro, Chairman  
Prince George's County Planning Board  
1616 McCormick Drive  
Largo, Maryland 20774

RE: Detailed Site Plan DSP-23026, Ourisman of Bowie

Dear Chairman Shapiro:

On May 5, 2025, the City Council reviewed DSP-23026 for a new 22,111 square foot vehicle sales and service dealership. The applicant, Rowhit Bowie, LLC, has filed an application for Detailed Site Plan approval under the prior Commercial Miscellaneous (C-M) zoning for the 6.29-acre subject property. The property is located on the west side of Crain Highway (US 301), approximately 400 feet south of its intersection with Mount Oak Road.

The City Council voted to recommend **APPROVAL** of DSP-23026, finding that the proposed Detailed Site Plan revision represents a reasonable alternative for satisfying the Zoning Ordinance's site design guidelines, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use. The City Council recommends the following two conditions, which are intended to improve the site design and address conformance with the County Zoning Ordinance and City Development Review Guidelines:

1. Update the site plan to include both master planned bicycle and pedestrian facilities along Crain Highway (A-61): 8-ft -wide sidewalks and bike lane unless modified by SHA with written correspondence.
2. Update the site plan to demonstrate the location and count of bicycle parking provided. The bicycle parking provided should be consistent with guidance from the 2012 AASHTO Guide for the Development of Bicycle Facilities.

Thank you for your consideration of the City's position regarding DSP-23026 for the proposed Ourisman of Bowie.

Sincerely,

Bowie City Council  
Timothy J. Adams  
Mayor

cc: Mr. Robert Antonetti

PGCPB No. 04-177

File No. 4-04047

RESOLUTION

WHEREAS, Route 301 Realty Corp. is the owner of a 6.29-acre parcel of land known as Parcels 26 and 128, being located on Tax Map 63, Grid D-2, said property being in the 7thth Election District of Prince George's County, Maryland, and being zoned C-M; and

WHEREAS, on March 31, 2004, Norman Rivera filed an application for approval of a Preliminary Subdivision Plan (Staff Exhibit #1) for one lot; and

WHEREAS, the application for approval of the aforesaid Preliminary Subdivision Plan, also known as Preliminary Plan 4-04047 for Route 301 Realty Corporation Project was presented to the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission by the staff of the Commission on July 22, 2004, for its review and action in accordance with Article 28, Section 7-116, Annotated Code of Maryland and the Regulations for the Subdivision of Land, Subtitle 24, Prince George's County Code; and

WHEREAS, the staff of The Maryland-National Capital Park and Planning Commission recommended APPROVAL of the application with conditions; and

WHEREAS, on July 22, 2004, the Prince George's County Planning Board heard testimony and received evidence submitted for the record on the aforesaid application.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to the provisions of Subtitle 24, Prince George's County Code, the Prince George's County Planning Board APPROVED the Type I Tree Conservation Plan (TCPI/32/04), and further APPROVED Preliminary Plan of Subdivision 4-04047, for Lot 1 with the following conditions:

1. Development of this subdivision shall be in compliance with an approved Type I Tree Conservation Plan (TCPI/32/04). The following note shall be placed on the Final Plat of Subdivision:

"This development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCPI/32/04), or as modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy."

2. A conservation easement shall be described by bearings and distances at time of final plat. The conservation easement shall contain the delineated Patuxent River Primary Management Area, except for areas with approved impacts, and shall be reviewed by the Environmental Planning

Section prior to certificate approval for accuracy. In addition, the following note shall be placed on the final plat:

✓ [ "Conservation easements described on this plat are areas where the installation of structures and roads and the removal of vegetation are prohibited without prior written consent from the M-NCPPC Planning Director or designee. The removal of hazardous trees, limbs, branches, or trunks is permitted." ]

✓ 3. Prior to the issuance of any permits which impact wetlands, wetland buffers, streams, or waters of the U.S., copies of all federal and state wetland permits, evidence that approval conditions have been complied with, and associated mitigation plans shall be submitted to the M-NCPPC Planning Department.

2 4. The applicant shall provide sufficient and uninterrupted access to Parcels 103 and 164, either along the existing ingress/egress easement or through some other means mutually agreeable to all parties.

5. Any abandoned well or septic system shall be pumped, backfilled and/or sealed in accordance with COMAR 26.04.04 by a licensed well driller or witnessed by a representative of the Health Department prior to final plat approval.

6 evidence All abandoned vehicles, domestic trash, and other debris found on the property shall be removed and properly stored or discarded prior to final plat approval.

7 Add note Prior to the issuance of grading permits, the applicant, his heirs, successors and/or assignees shall have the scrap tires on the property hauled away by a licensed scrap tire hauler to a licensed scrap tire disposal/recycling facility. A receipt for the tire disposal shall be submitted to the Health Department prior to issuance of grading permits.

✓ 8. Prior to the issuance of building permits, the applicant shall obtain detailed site plan approval by the Planning Board. The detailed site plan shall address buffering (including light and noise intrusion), landscaping, access, techniques to further minimize PMA impacts, and the visual relationships between the proposed development and adjoining residentially developed properties.

✓ 9. Development of this subdivision shall be in compliance with the approved stormwater management concept plan (#8254-2004-01) or any approved revision thereto.

10 OK note 7 The applicant shall provide for any necessary turn lanes and frontage improvements along US 301 as required by SHA. These may include turn lanes for deceleration and acceleration of vehicles and channelization (lane divider) at the site entrance to prevent weaving.

✓ 11. Prior to the issuance of any building permits within the subject property, the following road improvements shall (a) have full financial assurances, (b) have been permitted for construction, and (c) have an agreed-upon timetable for construction with SHA:

- a. Widen the westbound approach of Queen Anne Bridge Road at US 301 to provide an exclusive right turn lane and two through lanes. Widen the median to accommodate two through lanes from westbound Queen Anne Bridge Road, to create one left turn lane and one through lane. These improvements shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.
- b. Construct a free right turn lane on eastbound Mitchellville Road at US 301 and change from signal controlled movement to free-flow right turn movement. This improvement shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.
- c. The applicant will be responsible for any traffic signal modifications as a result of the proposed intersection improvements.
- d. The type of material, length and construction of the channelized lane (lane divider), and how the merge of traffic will be handled beyond the channelized lane shall be determined.

add  
note

12. Total development within the subject property shall be limited to 29,800 square feet of automobile dealership facilities, or equivalent development that generates no more than 52 AM and 62 PM new peak-hour trips, taking into account estimated pass-by trips. Any development other than that identified herein above shall require a new preliminary plan of subdivision with a new determination of the adequacy of transportation facilities.

7

13. The applicant, his successors and/or assignees shall conduct a traffic signal warrant study at the US 301/Walker Pontiac median crossover (for both northbound and southbound US 301). If signals are warranted and desired by the State Highway Administration, the applicant shall install the signals prior to the issuance of a use and occupancy permit.

BE IT FURTHER RESOLVED, that the findings and reasons for the decision of the Prince George's County Planning Board are as follows:

1. The subdivision, as modified, meets the legal requirements of Subtitles 24 and 27 of the Prince George's County Code and of Article 28, Annotated Code of Maryland.
2. The property is located on the west side of Crain Highway (US 301), 400± feet south of its intersection with Mount Oak Road.
3. **Development Data Summary**—The following information relates to the subject preliminary plan application and the proposed development.

	<b>EXISTING</b>	<b>PROPOSED</b>
Zone	C-M	C-M
Uses	Undeveloped	Auto Dealership
Acreage	6.29	6.29
Parcels	2	0
Lots	0	1
Square Footage	0	29,800

4. **Environmental**—A review of the available information indicates that streams, wetlands, 100-year floodplain, severe slopes, and areas of steep slopes with highly erodible soils are found to occur on the property. Although US 301 is a transportation-related noise generator, the proposed use of this site as a car dealership will not be adversely impacted by the noise. The primary soil found to occur according to the Prince George=s County Soil Survey is Monmouth fine sandy loam. Although this soil series has limitations with respect to slow permeability, the proposed development of this site will not be impacted. According to available information, Marlboro clay is not found to occur on this property. According to information obtained from the Maryland Department of Natural Resources Natural Heritage Program publication entitled “Ecologically Significant Areas in Anne Arundel and Prince George=s Counties,” December 1997, there are no rare, threatened, or endangered species found to occur in the vicinity of this property. There are no designated scenic and historic roads in close proximity to this property. This property is located in the Mill Branch subwatershed of the Patuxent River Basin and in the Developing Tier as reflected in the adopted General Plan.

**Woodland Conservation**

The Forest Stand Delineation (FSD) submitted with this application was found to address the requirements for a Forest Stand Delineation in accordance with the Prince George’s County Woodland Conservation and Tree Preservation Ordinance. This property is subject to the provisions of the Prince George=s County Woodland Conservation Ordinance because the gross tract area is greater than 40,000 square feet and there are more than 10,000 square feet of existing woodlands. This 6.29-acre property has a net tract area of 3.8 acres and a Woodland Conservation Threshold of 15 percent, or 0.57 acres. In addition, there are replacement requirements of 0.58 acre for the clearing of woodlands above the Woodland Conservation Threshold and for clearing woodland in the 100-year floodplain. The 1.15-acre requirement is proposed to be satisfied by 1.00 acres of on-site preservation in priority retention areas and 0.15 acre of off-site mitigation at a location to be determined. Type I Tree Conservation Plan TCPI/32/04 was found to address the requirements of the Prince George’s County Woodland Conservation Ordinance and is recommended for approval.

**Noise**

This property is located within the transportation-related noise impact zone associated with US 301. Based on the Environmental Planning Section’s Noise Model, the calculated 65 dBA Ldn noise contour is located approximately 565 feet from the centerline of US 301’s southbound

lane. That line encompasses approximately 85 percent of the land area included in this application. However, because of the proposed use of this site as a car dealership, the noise does not rise above the state noise standards for this use.

The subject property could be considered a noise generator, due to the possibility of having an outdoor speaker system. However, the noise generated by such a system would not exceed the state noise standard of 65 dBA Ldn at the property line.

#### **Patuxent River Primary Management Area**

Streams, wetlands, 100-year floodplain, steep slopes with highly erodible soils, and severe slopes are found on this property. These features along with their respective buffers compose the Patuxent River Primary Management Area, or PMA. The individual features composing the PMA have been correctly shown and the ultimate limits of the PMA have been correctly shown on the plans as required. The Wetland Delineation Report submitted was reviewed and was found to correctly reflect the existing conditions. An approved 100-year floodplain study was submitted for this site. No additional information is required with respect to the delineation of the Patuxent River Primary Management Area.

The Subdivision Ordinance, Section 24-130(b)(5), requires that the PMA be preserved in a natural state to the fullest extent possible. The TCPI proposes impacts to the PMA for the construction of a stormwater management outfall, a sewer connection, and for the construction of a portion of the proposed parking lot. The Letter of Justification submitted has addressed each of the proposed PMA impacts and demonstrated that each of the impacts has been minimized to the fullest extent possible.

The stormwater management and sewer outfalls are both required in order to develop this property, and the locations of the outfalls have been placed so that the area of PMA disturbance has indeed been minimized without eliminating the functionality of these structures. The impact for the parking lot is required because of the irregular shape of the PMA where the floodplain component of the PMA jogs into the developable portion of the site within a small ravine that is 20 by 50 feet in size. Approximately one-half of the small ravine would be impacted by the construction of the proposed parking lot. Avoidance of the small ravine would require the loss of several parking spaces and an irregular-shaped parking lot that would have some areas of restricted traffic flow. The Environmental Planning Section supports the proposed PMA impacts.

#### **Light Pollution**

The City of Bowie recommends the approval of a detailed site plan to address site layout issues and light pollution. Staff supports the inclusion of the condition recommended by the City of Bowie to require a detailed site plan for the subject application to address issues related to site layout and lighting.

### **Water and Sewer Categories**

The property is in water category W-4 and sewer category S-4, according to water and sewer maps dated June 2003 obtained from the Department of Environmental Resources. Category 3 must be obtained prior to the recordation of a final plat. It is served by public systems.

5. **Community Planning**—The property is in Planning Area 74B/Community I. The 2002 General Plan places the subject property in the Developing Tier. The vision for the Developing Tier is to maintain a pattern of low- to moderate-density suburban residential communities, distinct commercial Centers, and employment areas that are increasingly transit serviceable. This application is not inconsistent with the 2002 General Plan Development Pattern policies for the Developing Tier. The site is subject to the recommendation of the 1991 Bowie-Collington-Mitchellville and Vicinity Master Plan, which calls for a service-commercial use. This site is addressed specifically in the master plan as part of the “Walker Pontiac Area.” This application conforms to the master plan recommendation for service-commercial use.

### **Planning Issues**

While this preliminary plan application is in conformance with the master plan recommendation for service-commercial development, it fails to show several master plan elements, including:

- The recommended master plan alignment for A-61, an arterial highway that parallels US 301.

**Comment:** According to the Bowie-Collington-Mitchellville and Vicinity Master Plan, the southbound lanes of existing US 301 will be a part of arterial A-61, which will serve local traffic when US 301 is rebuilt as a freeway. In the vicinity of the subject property, A-61 is accommodated within the existing right-of-way and no additional dedication is necessary.

- Substantial landscape buffers along the north and west property lines, although the vast majority of this area is within the PMA and is shown as undeveloped.

**Comment:** If this application is approved, detailed site plan approval will be required to ensure adequate buffering is provided between the subject property and adjoining residentially developed properties.

6. **Parks and Recreation**—The site is exempt from mandatory dedication because it is a commercial development.
7. **Trails**—There are no master plan issues identified in the 1991 Bowie-Collington-Mitchellville and Vicinity Master Plan.

8. **Transportation**— The applicant submitted a traffic study dated November 11, 2003. The findings and recommendations outlined below are based upon a review of these materials and analyses conducted by staff of the Transportation Planning Section, consistent with the *Guidelines for the Analysis of the Traffic Impact of Development Proposals*.

#### **Growth Policy – Service Level Standards**

The subject property is located within the Developing Tier, as defined in the General Plan for Prince George's County. As such, the subject property is evaluated according to the following standards:

**Links and signalized intersections:** Level-of-service (LOS) D, with signalized intersections operating at a critical lane volume (CLV) of 1,450 or better. Mitigation, as defined by Section 24-124(a)(6) of the Subdivision Ordinance, is permitted at signalized intersections within any Tier subject to meeting the geographical criteria in the guidelines.

**Unsignalized intersections:** The Highway Capacity Manual procedure for unsignalized intersections is not a true test of adequacy but rather an indicator that further operational studies need to be conducted. Vehicle delay in any movement exceeding 50.0 seconds is deemed to be an unacceptable operating condition at unsignalized intersections. In response to such a finding, the Planning Board has generally recommended that the applicant provide a traffic signal warrant study and install the signal (or other less costly warranted traffic controls) if deemed warranted by the appropriate operating agency.

#### **Staff Analysis of Traffic Impacts**

The applicant has prepared a traffic impact study in support of the application using counts taken during October 2003. With the development of the subject property, the traffic consultant concluded that the proposed use would not adversely affect traffic conditions along US 301 if intersection improvements and traffic signal modifications were made at the intersection of US 301 and Mitchellville Road.

The traffic impact study that was prepared and submitted on behalf of the applicant analyzed the following intersections during weekday peak hours:

- US 301 SB/Mount Oak Road (unsignalized)
- US 301/Enterprise Access/Median Break NB (unsignalized)
- US 301/Enterprise Access/Median Break SB (unsignalized)
- US 301/Mitchellville Road/Queen Anne Bridge Road NB (signalized)
- US 301/Mitchellville Road/Queen Anne Bridge Road SB (signalized)

The following conditions exist at the critical intersections:

EXISTING TRAFFIC CONDITIONS				
Intersection	Critical Lane Volume (AM & PM)		Level of Service (LOS, AM & PM)	
	US 301 SB/Mount Oak Road	27.2*	21.7*	--
US 301/Enterprise Access/Median Break NB	38.9*	45.5*	--	--
US 301/Enterprise Access/Median Break SB	60.3*	113.7*	--	--
US 301/Queen Anne Bridge Road NB	1,378	1,376	D	D
US 301/Mitchellville Road SB	1,212	1,382	C	D

\*In analyzing unsignalized intersections, average vehicle delay for various movements through the intersection is measured in seconds of vehicle delay. The numbers shown indicate the greatest average delay for any movement within the intersection. According to the guidelines, an average vehicle delay exceeding 50.0 seconds indicates inadequate traffic operations. Values shown as "+999" suggest that the parameters are outside of the normal range of the procedure and should be interpreted as a severe inadequacy.

Staff visited the site and field checked the lane configurations and traffic operations at the intersections included in the traffic study. The intersection of Mount Oak Road provides right in, right out movements along the southbound lanes of US 301. This is an unsignalized, three-legged intersection with no median break on US 301. The Enterprise Access Road and median breaks on US 301 provide access to existing auto dealerships and allow left turning movements and U-turns on the southbound and northbound lanes of US 301.

The southbound and northbound lanes of US 301 at the Mitchellville Road/Queen Anne Bridge Road intersection are separated by a median strip approximately 100 feet wide. The two traffic signals were analyzed separately in the traffic study. The traffic consultant provided traffic counts and critical lane analysis for both the northbound and southbound legs of the intersection. Background and total traffic conditions for the intersection of US 301 and Mitchellville Road/Queen Anne Bridge Road were calculated the same way.

Background developments included the Amber Ridge Shopping Center (180,000 square feet of retail), Covington (89 single-family units), Glen Allen (36 single-family units), Mill Branch Service Station, Pin Oak (141 townhouses), and the Bowie Town Center (540,000 square feet of office). Background traffic or through traffic along US 301 was also increased by three percent annually up to the year 2006. This is the expected year of full build-out. There are no funded capital improvements in the area, so the resulting transportation network is the same as was

assumed under existing traffic. Given these assumptions, background conditions are summarized below:

BACKGROUND TRAFFIC CONDITIONS				
Intersection	Critical Lane Volume (AM & PM)		Level of Service (LOS, AM & PM)	
	US 301 SB/Mount Oak Road	34.2*	27.9*	--
US 301/Enterprise Access/Median Break NB	56.4*	65.5*	--	--
US 301/Enterprise Access/Median Break SB	84.8*	298.0*	--	--
US 301/Queen Anne Bridge Road NB	1,577	1,547	E	E
US 301/Mitchellville Road SB	1,350	1,638	D	F

\*In analyzing unsignalized intersections, average vehicle delay for various movements through the intersection is measured in seconds of vehicle delay. The numbers shown indicate the greatest average delay for any movement within the intersection. According to the guidelines, an average vehicle delay exceeding 50.0 seconds indicates inadequate traffic operations. Values shown as "+999" suggest that the parameters are outside of the normal range of the procedure and should be interpreted as a severe inadequacy.

Based on background traffic conditions, the US 301/Queen Anne Bridge Road intersection will operate at LOS E during the AM and PM peak hours. The US 301/Mitchellville Road intersection will operate at LOS F during the PM peak hour. The average vehicle delay exceeds 50.0 seconds at the US 301/Enterprise Access Road median break on both sides of US 301 during the AM and PM peak hours.

The site is proposed for development as an auto dealership of 29,800 square feet with associated improvements. Planned uses include a new car sales center and service facilities. The consultant assumed a 20 percent pass-by rate for these uses, which is reasonable. Trip rates were based on local trip rates that were found at three nearby automobile dealerships in the Bowie area. The resulting site trip generation would be 51 AM peak-hour trips (34 in, 17 out) and 62 PM peak-hour trips (27 in, 35 out). As mentioned, the traffic consultant assumed that 80 percent of these trips will be new trips destined for the auto dealership and that 20 percent of the trips would already be on US 301. With the 20 percent pass-by rate applied, the resulting trip generation would be 41 AM peak-hour trips (27 in, 14 out) and 50 PM peak-hour trips (22 in, 28 out).

The traffic study assumes an auto dealership of 37,500 square feet with associated improvements. A portion of this, 7,700 square feet, is proposed on another nearby parcel. Although the additional square footage is included in the traffic study, it is not part of the current proposal, the Route 301 Realty Corporation Project. Therefore, the traffic study represents 9 more new trips during the AM peak hour and 13 more new trips during the PM peak hour, accounting for the additional 7,700 square feet.

The resulting site trip generation is 64 AM peak-hour trips (42 in, 22 out) and 78 PM peak-hour trips (34 in, 44 out). As mentioned, the traffic consultant assumed that 80 percent of these trips will be new trips destined for the auto dealership and that 20 percent of the trips would already be on US 301. With the 20 percent pass-by rate applied, the resulting trip generation would be 52 AM peak-hour trips (34 in, 18 out) and 62 PM peak-hour trips (27 in, 35 out). With the site traffic cited in the traffic study the following operating conditions were determined:

TOTAL TRAFFIC CONDITIONS WITH/WITHOUT IMPROVEMENTS				
Intersection	Critical Lane Volume		Level of Service	
	(AM & PM)		(LOS, AM & PM)	
US 301 SB/Mount Oak Road	36.4*	28.8*	--	--
US 301/Enterprise Access/Median Break NB	64.6*	85.9*	--	--
US 301/Enterprise Access/Median Break SB	491.7*	386.8*	--	--
US 301/Queen Anne Bridge Road NB	1,586	1,554	E	E
US 301/Queen Anne Bridge Road NB**	1,559	1,526	E	E
US 301/Queen Anne Bridge Road NB***	1,153	1,132	C	B
US 301/Mitchellville Road SB	1,353	1,644	D	F
US 301/Mitchellville Road SB**	1,313	1,524	D	E
US 301/Mitchellville Road SB***	977	1,239	A	C
*In analyzing unsignalized intersections, average vehicle delay for various movements through the intersection is measured in seconds of vehicle delay. The numbers shown indicate the greatest average delay for any movement within the intersection. According to the guidelines, an average vehicle delay exceeding 50.0 seconds indicates inadequate traffic operations. Values shown as "+999" suggest that the parameters are outside of the normal range of the procedure and should be interpreted as a severe inadequacy.				

\*\*This includes intersection improvements.

\*\*\*This includes adding a third lane in each direction on US 301.

Based on total traffic conditions, and without any physical improvements to US 301, the US 301/Queen Anne Bridge Road intersection will operate at LOS E during the AM and PM peak hours. The US 301/Mitchellville Road intersection will operate at LOS F during the PM peak hour. The average vehicle delay exceeds 50.0 seconds at the US 301/Enterprise Access Road median break on both sides of US 301 during the AM and PM peak hours.

Section 24-124(a)(6) of the County Code authorizes the Planning Board to consider traffic mitigation procedures under certain circumstances. This includes major regional road facilities such as US 301 and/or MD 3. These roads experience a significant amount of external traffic. Under these circumstances the applicant needs to recommend improvements which will eliminate at least 150 percent of the development-generated critical lane volume at the critical intersection, which in this instance is the US 301/Mitchellville Road /Queen Anne Bridge Road intersection. This requirement was met by the applicant's proposed intersection improvements. These include providing an eastbound free right turn lane at US 301 and Mitchellville Road. At US 301 and Queen Anne Bridge Road, the applicant proposes to modify the westbound approach and improve the median between Mitchellville Road and Queen Anne Road.

These improvements, which will have to be approved by the State Highway Administration, will eliminate at least 150 percent of the auto dealership's generated critical lane volume at the intersections of US 301 SB/Mitchellville Road and US 301 NB/Queen Anne Bridge Road.

The applicant's consultant also included in the traffic study the benefits of adding one lane in each direction on US 301, providing three lanes in each direction. This adds significant improvements in levels of service (to LOS D) during the AM and PM peak hours at US 301/ Mitchellville Road and US 301/Queen Anne Bridge Road. These are noted in the table above. Preliminary cost estimates for the fifth and sixth lanes on this section of US 301 range from \$4 million to \$5 million dollars.

The State Highway Administration (SHA) has provided comments on the proposed development. SHA's comments are attached. They will require that the driveway access point to southbound US 301 be restricted to one right-in/right out entrance with a channelized lane divider to prevent weaving across US 301. Northbound traffic from the site will be diverted to the Mitchellville Road intersection. At present, the majority of left turns from southbound US 301 are U-turns.

#### **Site Plan Comments**

Site access will be from the west side of US 301, via a right-in/right-out entrance. This driveway access will have to be approved by the State Highway Administration and is addressed in their comments, which are attached. Staff has no other comments, and will defer until a detailed site plan is submitted. Staff is assuming that only one access point to US 301 will be approved.

### Master Plan Comments

The Bowie-Collington-Mitchellville & Vicinity Master Plan (1991) recommends that A-61, an arterial roadway, should be constructed parallel to US 301 (Crain Highway) along its western edge. The purpose of A-61 is to provide access to land use activities along US 301 between major interchanges. The right-of-way for A-61 will be within the right-of-way of F-10 or Crain Highway, which varies.

The *US 301 Access Control Study* (March 1999) provides a preliminary analysis of the right-of-way that may be required along US 301, between US 50 and MD 5. A-61 is shown contained within the existing southbound lanes of US 301. No further dedication is required.

### Transportation Staff Conclusions and Recommendations

Based on the preceding findings, adequate transportation facilities would exist to serve the proposed subdivision as required under Section 24-124 of the Prince George's County Code if the application is approved limiting the amount of development and requiring specific road improvements.

9. **Schools**—The Historic Preservation and Public Facilities Planning Section has reviewed this subdivision plan for adequacy of school facilities in accordance with Section 24-122.02 of the Subdivision Regulations and CB-30-2003 and CR-23-2003. The proposed subdivision is exempt from the adequacy test for schools because it is a commercial use.
10. **Fire and Rescue**—The Historic Preservation and Public Facilities Planning Section reviewed the subdivision plans for adequacy of fire and rescue facilities.
  - a. The existing fire engine service at Bowie Fire Station, Company 43, located at 16400 Pointer Ridge Drive, has a service travel time of 3.22 minutes, which is within the 3.25-minute travel time guideline.
  - b. The existing ambulance service at Bowie Fire Station, Company 43, has a service travel time of 3.22 minutes, which is within the 4.25-minute travel time guideline.
  - c. The existing paramedic service at Bowie Fire Station, Company 43, has a service travel time of 3.22 minutes, which is within the 7.25-minute travel time guideline.
  - d. The existing ladder truck service at Bowie Fire Station, Company 39, located at 15454 Annapolis Road, has a service travel time of 8.4 minutes, which is beyond the 4.25-minute travel time guideline.

To alleviate the negative impact on fire and rescue services due to the inadequate service discussed, an automatic fire suppression system shall be provided in all new buildings proposed in

this subdivision, unless the Prince George's County Fire/EMS Department determines that an alternative method of fire suppression is appropriate.

These findings are in conformance with the standards and guidelines contained in the 1990 *Approved Public Safety Master Plan* and the "Guidelines for the Analysis of Development Impact on Fire and Rescue Facilities."

11. **Police Facilities**—The proposed development is within the service area for Police District II-Bowie. The Planning Board's current test for police adequacy is based on a standard for square footage in police stations relative to the number of sworn duty staff assigned. The standard is 115 square feet per officer. As of January 2, 2004, the county had 823 sworn staff and a total of 101,303 square feet of station space. Based on available space, there is capacity for an additional 57 sworn personnel. Therefore, in accordance with Section 24-122.01(c) of the Subdivision Regulations, existing county police facilities will be adequate to serve the proposed development.
12. **Health Department**—The Health Department reviewed the application and offered the following comments:
  - "1. What appears to be an open shallow well or an abandoned septic tank was found near the specimen tree located at the southeast corner of the property. If the object is a well, it must be properly backfilled and sealed in accordance with COMAR 26.04.04 by a licensed well driller or witnessed by a representative from the Health Department as part of the grading permit. If the object is a septic tank, it must be pumped out by a licensed scavenger and either removed or backfilled in place as part of the grading permit. The location of the object should be shown on the preliminary plan.
  - "2. A significant amount of domestic trash and other debris was found on the property along the entire eastern boundary, in the PMA area, and on the northern side of the stream and should be removed and properly stored or discarded.
  - "3. Numerous tires were found on the property in the stream, in the PMA area and near the two abandoned vehicles located at the tree line in the southwest corner. The tires must be hauled away by a licensed scrap tire hauler to a licensed scrap tire disposal/recycling facility and a receipt for tire disposal must be submitted to this office prior to preliminary plan approval.
  - "4. Seven abandoned vehicles were found on the property near the PMA area and at the tree line in the southwest corner, and should be removed and properly stored or discarded."
13. **Stormwater Management**—The applicant received stormwater concept approval from the Prince George's County Department of Environmental Resources on April 13, 2004 (#8254-2004-01).
14. **Cemeteries**—There are no known cemeteries on the subject property.

15. **Public Utility Easement**—The plan shows a 10-foot-wide public utility easement adjacent to Crain Highway. It is accurately reflected on the proposed preliminary plan and will be included on the final plat.
16. **Easements**—There is an existing 20-foot-wide easement across the subject property to provide access to an otherwise landlocked parcel (Parcel 103). An adjoining property, Parcel 164, also accesses US 301 via this right-of-way, although it has its own frontage along US 301 via a narrow “pipestem.” A similar, but undeveloped, right-of-way is shown on the plan within the pipestem connecting Parcel 164 to US 301. The owners of these two properties presently gain access to US 301 via a 10-foot-wide driveway, partially within the easement and partially outside, on the subject property. The applicant provides for the easement in its “descriptive” location on the preliminary plan, but shows curbs and parking spaces along the “prescriptive” location as well as curbing where the easement crosses the western property line. Unless they can show that the existing easement across the subject property is no longer enforceable, the applicant should continue to provide sufficient and uninterrupted access to these two properties. Such access must be provided either along the existing ingress/egress easement or through some other means mutually agreeable to all parties. Given that Parcel 103 and the adjoining Parcel 164 are commercially zoned and may be commercially developed at some future date, it may be most advantageous to provide for a shared single point of access for all three properties with cross easements to minimize the number of driveway cuts along US 301.
17. **Detailed Site Plan Considerations**—As discussed previously, staff is recommending detailed site plan approval for any proposed development on this site. Site plan review is essential in order to ensure adequate buffering and screening from the adjoining residentially developed properties. Although this subdivision application does not approve the location of buildings or other improvements on the site, staff has several concerns after review of the applicant’s submitted site development plan that will need to be addressed at the time of detailed site plan:
  - a. **Buffering**—The *Landscape Manual* defines a car dealership as a medium impact use. When a medium impact use locates next to a single-family residence (such as are found on the adjoining parcels to the southwest), a Type C bufferyard (40-foot building setback, 30-foot wide landscaped strip) is required. The applicant’s proposed site development plan shows the very southwest corner of the dealership building within this setback and a five-foot-wide perimeter strip in this area, as if the adjoining properties were developed commercially. This would place the rear of the dealership approximately 120 feet from the residence on Parcel 103 with nothing more than a five-foot-wide landscaped strip as a buffer. The applicant should be advised that such a radical departure from the requirements of the *Landscape Manual* is not acceptable.
  - b. **Access** – Access to US 301 is a concern in this vicinity because of the numerous existing driveway cuts. The access point to this site must be carefully coordinated with the State Highway Administration to ensure that unsafe conditions are not created. Also, as discussed above, the applicant must provide for sufficient and uninterrupted access to the two residentially developed parcels to the southwest.

18. **City of Bowie**—The subject property adjoins the City of Bowie. The Bowie City Council recommends approval of this application, subject to the five conditions contained in their letter dated May 20, 2004, a copy of which is attached.

BE IT FURTHER RESOLVED, that an appeal of the Planning Board's action must be filed with Circuit Court for Prince George's County, Maryland within thirty (30) days following the adoption of this Resolution.

\* \* \* \* \*

This is to certify that the foregoing is a true and correct copy of the action taken by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the motion of Commissioner Harley, seconded by Commissioner Squire, with Commissioners Harley, Squire, Vaughns, Eley and Hewlett voting in favor of the motion, at its regular meeting held on Thursday, July 22, 2004, in Upper Marlboro, Maryland.

Adopted by the Prince George's County Planning Board this 9th day of September 2004.

Trudye Morgan Johnson  
Executive Director

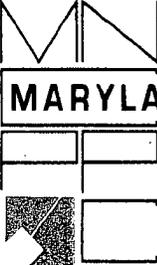
By *Frances J. Guertin*  
Frances J. Guertin  
Planning Board Administrator

TMJ:FJG:TEL:meg

APPROVED AS TO LEGAL SUFFICIENCY.

*Serge [Signature]*  
M-NCPR Legal Department

Date 8/16/04



**THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION**  
14741 Governor Oden Bowie Drive  
Upper Marlboro, Maryland 20772  
TTY: (301) 952-3796

August 2, 2005

Route 301 Realty Corporation  
1055 East Jericho Turnpike  
Huntington, NY 11743

Re: Notification of Planning Board Action on  
**Detailed Site Plan DSP-05013**  
**Bowie Nissan**

Dear Applicant:

This is to advise you that on **July 28, 2005** the above-referenced Detailed Site Plan was acted upon by the Prince George's County Planning Board in accordance with the attached Resolution.

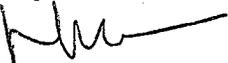
Pursuant to Section 27-290, the Planning Board's decision will become final 30 calendar days after the date of the final notice (**September 1, 2005**) of the Planning Board's decision unless:

1. Within the 30 days, a written appeal has been filed with the District Council by the applicant or any Person of Record in the case; or
2. Within the 30 days (or other period specified by Section 27-291), the District Council decides, on its own motion, to review the action of the Planning Board.

(You should be aware that you will have to reactivate any permits pending the outcome of this case. If the approved plans differ from the ones originally submitted with your permit, you are required to amend the permit by submitting copies of the approved plans. For information regarding reactivating permits, you should call the County's Permit Office at 301-883-5784.)

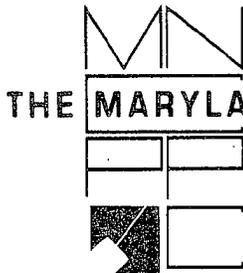
Please direct any future communication or inquiries regarding this matter to Ms. Redis C. Floyd, Clerk of the Council, at the above address.

Very truly yours,  
Faroll Hamer  
Development Review Division

By:   
\_\_\_\_\_  
Reviewer

c: Redis C. Floyd, Clerk to the County Council  
Persons of Record

PGCPB No. 05-173  
I:\forms\resol\dsp



**THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION**

14741 Governor Oden Bowie Drive  
 Upper Marlboro, Maryland 20772  
 TTY: (301) 952-3796

PGCPB No. 05-173

File No. DSP-05013

RESOLUTION

WHEREAS, the Prince George's County Planning Board is charged with the approval of Detailed Site Plans pursuant to Part 3, Division 9 of the Zoning Ordinance of the Prince George's County Code; and

WHEREAS, in consideration of evidence presented at a public hearing on July 21, 2005 regarding Detailed Site Plan DSP-05013 for Bowie Nissan (Formerly Route 301 Realty Corporation), the Planning Board finds:

1. **Request:** The subject application is for approval of a detailed site plan for a new automobile dealership in the C-M Zone.
2. **Development Data Summary:**

	<b>EXISTING</b>	<b>PROPOSED</b>
Zone(s)	C-M	C-M
Use(s)	Vacant/wooded	New automobile dealership
Acreage	6.29	6.29
Total square footage	-	29,700

**OTHER DEVELOPMENT DATA**

	<b>REQUIRED</b>	<b>PROPOSED</b>
Total parking spaces	118	370
Of which		
Office	24	91
Service bay	84	28
Indoor display	8	6
Outdoor display	2	42
Rooftop and basement storage		203
Spaces for the handicapped	5 (1 van accessible)	6 (2 van accessible)
Loading space	1	1

3. **Location:** The subject property is located on the west side of Crain Highway (US 301), approximately 400 feet south of its intersection with Mount Oak Road, in Planning Area 74B and Council District 4.
4. **Surroundings and Use:** The subject site is bounded to the east by the right-of-way of US 301 (Crain Highway) and bounded to the south by a 20-foot-wide private access easement. Across US 301 to the east is property in the C-M Zone and across the private easement to the south is the property of Walker Pontiac, an existing car dealership in the C-M Zone. To the north of the property are an outlot in the R-R Zone and a developed parcel in the C-M Zone with a hospital

use. To the west of the property are two existing single-family detached residences in the C-M Zone and an existing subdivision in the R-R Zone.

5. **Previous Approvals:** The subject site has an approved Preliminary Plan of Subdivision 4-04047, including a Type I Tree Conservation Plan (TCPI/32/04), which was approved by the Planning Board (PGCPB No. 04-177) on July 22, 2004, subject to 13 conditions. The site also has an approved stormwater management concept plan 8254-2004-01.
6. **Design Features:** The subject property consists of approximately 6.29 acres of land in the C-M Zone. There are streams, wetlands, 100-year floodplain, severe slopes, and areas of steep slopes with highly erodible soils are found to occur in the north and northwest of the property. The proposed dealership building, in a rectangular footprint, is located in the southeast part of the site and is surrounded with various types of parking lots. A continuous retaining wall has been shown along the northern boundary of the developed envelope of the site, but no height and wall details information have been provided on the site plan. A condition of approval has been proposed in the recommendation section of this report.

One access point off US 301 in the southeast corner of the site provides a vehicular right-in/right-out connection to US 301. Another access point off US 301, which is approximately 50 feet south of the first one, connects to the existing 20-foot-wide access easement leading to two existing single-family detached residences to the west of the subject property. The second private access easement branches into the subject detailed site plan site after traveling approximately 120 feet along the existing 20-foot-wide access easement, then cuts through the southwest corner of the site and merges back into the existing 20-foot-wide access easement.

The proposed one-story building is the new Nissan prototype dealership building with an underground storage facility. The building is oriented in an east-west direction with the main/eastern elevation facing US 301. The east elevation features a horizontal layout with a combination of storefront glass, solid concrete manufactured units in different textures, and aluminum composite material panels. Various internally illuminated building-mounted signs, such as the Nissan brand symbol, Nissan word mark, and dealer name signs are shown on the elevation. The main section of the east elevation is finished with silver-colored aluminum composite material panels. The main entrance is accented with red aluminum composite panel columns and the storefront is covered with fixed aluminum louvers. The service area canopy is finished with silver metallic horizontal ribbed panels. The south and north elevations use similar materials and treatments. The west elevation is simple and is finished with two types of concrete manufactured units. The elevations that can be viewed from US 301 in general are modern and attractive, except for the visible portion of the north elevation, which needs more elaborate treatments because US 301 is a divided freeway and the north elevation will be viewed by most passengers in the southbound traffic. A condition of approval has been proposed in the recommendation section to require the applicant to submit a full north elevation to be reviewed and approved by the Urban Design Section as the designee of the Planning Board.

A complete sign package including a freestanding sign and various building-mounted signs has been submitted and reviewed. The proposed signage package is in general compliance with the requirements of Section 27-613 and 614 for both building-mounted signs and freestanding signs.

No information regarding possible use of an outdoor speaker system/public address system (PA) has been provided with this application. A condition of approval has been proposed in the recommendation section of this report.

#### COMPLIANCE WITH EVALUATION CRITERIA

7. **The requirements of Zoning Ordinance in the C-M (Commercial Miscellaneous) Zone:** The subject site plan has been reviewed for compliance with the requirements of the C-M Zone and site design guidelines.
    - a. Pursuant to Section 27-461, which governs permitted uses in all commercial zones, the proposed new automobile dealership is a permitted use in the C-M Zone.
    - b. The site plan also complies with the requirements of Section 27-462, Regulations, regarding various setbacks and site design guidelines.
  
  8. **The requirements of Preliminary Plan of Subdivision 4-04047:** Preliminary Plan of Subdivision 4-04047 was approved by the Planning Board subject to 13 conditions.
    - a. The following conditions applicable to the review of this detailed site plan warrant discussion:
      1. **Development of this subdivision shall be in compliance with an approved Type I Tree Conservation Plan (TCPI/32/04). The following note shall be placed on the Final Plat of Subdivision:**

**“This development is subject to restrictions shown on the approved Type I Tree Conservation Plan (TCPI/32/04), or as modified by the Type II Tree Conservation Plan, and precludes any disturbance or installation of any structure within specific areas. Failure to comply will mean a violation of an approved Tree Conservation Plan and will make the owner subject to mitigation under the Woodland Conservation/Tree Preservation Policy.”**
- Comment:** A Type II Tree Conservation Plan TCPII/89/05, which is based on the approved Type I Tree Conservation Plan TCPI/32/04, has been submitted with this detailed site plan. Per the review by the Environmental Planning Section, TCPII/89/05 is consistent with TCPI/32/04 and is in conformance with the Prince George’s County

Woodland Conservation Ordinance. The Environmental Planning Section recommends approval of TCPII/89/05 subject to three conditions.

4. **The applicant shall provide sufficient and uninterrupted access to Parcels 103 and 164, either along the existing ingress/egress easement or through some other means mutually agreeable to all parties.**

**Comment:** The subject property carries a private access easement to an existing single-family detached residence, the Noerling property, to the west of the subject site. The Noerling property is also accessible from an existing 20-foot-wide access easement abutting the southern boundary of the subject site. The site plan shows two access points off US 301 that are approximately 50 feet apart. One is the access to the subject site; the other is the access point to the existing 20-foot-wide easement. From the transportation point of view, it would be preferable to have the two existing single-family detached residences and the subject site share a common access point off US 301 in order to minimize the impact on the heavily traveled freeway. In addition, the Bowie City Council, in its conditions of approval, also calls for the applicant to provide the Noerling property with alternative access that will not require traveling through the auto dealership parking area, if all adjacent easement owners agree. However, at time of the subject detailed site plan review, no agreement on a shared access easement has been reached by the three involved parties. The State Highway Administration has approved the two access points off US 301 as shown on the site plan.

The site plan shows a group of display parking spaces in the southwest corner of the subject property, which will attract traffic onto the private access easement to the Noerling property. The Urban Design Section recommends relocation of the seven display parking spaces to the north of the private access easement and print and stripe the portion of the easement in order to help ensure uninterrupted access to the Noerling property. A condition of approval has been proposed in the recommendation section of this report.

8. **Prior to the issuance of building permits, the applicant shall obtain detailed site plan approval by the Planning Board. The detailed site plan shall address buffering (including light and noise intrusion), landscaping, access, techniques to further minimize PMA impacts, and the visual relationships between the proposed development and adjoining residentially developed properties.**

**Comment:** The subject detailed site plan has been filed in order to fulfill this condition. Most parts of the subject site adjacent to the existing subdivision are wooded and will be preserved to fulfill on-site woodland conservation requirements. Only the two existing single-family detached residences will be impacted by the proposed development. A review by the Urban Design Section of this part of the site plan raises concerns regarding

screening from the existing residences and the location of dumpster. The main landscaping material (Pin Oak) proposed in the bufferyard for the screening purpose will not provide the required screening effect. The staff suggests fast-growing evergreen trees such as Leyland Cypress to be used to replace Pin Oak. The location of dumpster should be revisited in order to minimize impact on the existing single-family detached residence. The staff recommends relocating dumpster to the northern part of the site. Several conditions of approval have been proposed in the recommendation section of this report. The subject site plan, if revised pursuant to the recommended conditions of approval regarding light and noise intrusion, landscaping and access, complies with this condition.

9. **Development of this subdivision shall be in compliance with the approved stormwater management concept plan (#8245-2004-01) or any approved revision thereto.**

**Comment:** After review of the subject site plan, the Department of Environmental DSP-0513 is consistent with approved stormwater management concept plan 8245-2004-01.

12. **Total development within the subject property shall be limited to 29,800 square feet of automobile dealership facilities, or equivalent development that generates no more than 52 AM and 62 PM new peak-hour trips, taking into account estimated pass-by trips. Any development other than that identified herein above shall require a new preliminary plan of subdivision with a new determination of the adequacy of transportation facilities.**

**Comment:** The total development proposed with this detailed site plan is 29,700 square feet, which is 100 square feet less than the maximum allowable 29,800 square feet for the site. A review by the Transportation Planning Section (Masog to Zhang, July 11, 2005) indicates that the site plan is acceptable.

- b. Condition 11 of approval of 4-04047 as follows is not applicable to the review of this detailed site plan because the required transportation improvements will not be enforced until issuance of building permit. However, during the public hearing for this detailed site plan, the applicant proffered additional improvements as shown in applicant Exhibit # 2 to primarily fulfill sub-condition d, to prevent weaving movement from the site entrance to the median and unauthorized movements crossing over the median of US Route 301.

11. **Prior to the issuance of any building permits within the subject property, the following road improvements shall (a) have full financial assurances, (b) have been permitted for construction, and (c) have an agreed-upon timetable for construction with SHA:**

- a. **Widen the westbound approach of Queen Anne Bridge Road at US**

**301 to provide an exclusive right turn lane and two through lanes. Widen the median to accommodate two through lanes from westbound Queen Anne Bridge Road, to create one left turn lane and one through lane. These improvements shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.**

- b. Construct a free right turn lane on eastbound Mitchellville Road at US 301 and change from signal controlled movement to free-flow right turn movement. This improvement shall also include any signal, signage, and pavement marking modifications and additions to be determined by SHA.**
- c. The applicant will be responsible for any traffic signal modifications as a result of the proposed intersection improvements.**
- d. The type of material, length and construction of the channelized lane (lane divider), and how the merge of traffic will be handled beyond the channelized lane shall be determined.**

9. ***Landscape Manual:*** The subject application is subject to the requirements of Section 4.2 Commercial and Industrial Landscaped Strip Requirements; Section 4.3 Parking Lot Requirements; and Section 4.7 Buffering Incompatible Uses of the *Landscape Manual*.

- a. The proposed dealership is in the C-M Zone and is fronting US 301 to the east. Per Section 4.2, a landscape strip shall be provided on the property adjacent to all public rights-of-way. In any area where a parking lot is immediately adjacent to a public right-of-way, the provision of Section 4.3(a), Landscaped Strip Requirements, applies. The landscape plan does not identify the Section 4.2 landscape strip because only part of the frontage is occupied by the display parking lot. Even though the Section 4.2 requirement is virtually satisfied by the existing wooded area, a Section 4.2 schedule should be provided on the landscape plan. A condition of approval has been proposed in the recommendation section of this report.
- b. As discussed above, a Section 4.3(a) landscape strip is required along part of the site's eastern frontage onto US 301. The landscape plan provides a schedule, but does not show compliance with the requirement. A condition of approval has been proposed in the

} written per.

recommendation section to require the applicant either to show complete compliance to Section 4.3 (a) or to seek alternative compliance.

The landscape plan proposes 47,032 square feet of parking area on the site. Per Section 4.3(c), five percent of the total parking lot must be an interior planting area to be planted with one shade tree for each 300 square feet. The landscape plan provides 2,817 square feet of interior planting area, which is approximately six percent of the parking lot, with eight shade trees and, thus, complies with the requirements of Section 4.3(c). But the landscape plan shows two schedules that need to be consolidated into one.

- c. The proposed auto dealership is adjacent to existing residential use to the south and the west. Per Section 4.7, the proposed use is defined as a medium impact use; a Type C bufferyard is required. The Type C bufferyard requires a minimum 40-foot building setback and a minimum 30-foot-wide bufferyard to be planted with 120 plant units per 100 linear feet of property line.

The applicant is requesting alternative compliance along the southern and western property lines. The report of the Alternative Compliance Committee on this application is provided below:

**REQUIRED:** Section 4.7, Buffering Incompatible Uses, along the southern property line

Length of bufferyard	411 feet
Building setback	40 feet
Landscape yard	30 feet
Plant materials	246 plant units (with 50 percent opaque fence reduction)

**PROVIDED:**

Building setback	40-95 feet
Landscape yard	5-20 feet
Plant materials	289 plant units (with 50 percent opaque fence reduction)

**REQUIRED:** Section 4.7, Buffering Incompatible Uses, along the western property line

Length of bufferyard	190 feet
Building setback	40 feet
Landscape yard	30 feet
Plant materials	114 plant units (with 50 percent opaque fence reduction)

**PROVIDED:**

Building setback	56 feet
Landscape yard	8-28 feet

Plant materials                      132 plant units (with 30 percent existing woodlands)

**JUSTIFICATION:**

The site topography has presented limitations that make it impractical to build on a large portion of the site. In addition, space limitations are created on the site due to the unusual shape of the site, a large WSSC easement, a 20-foot access easement on the site for ingress and egress to the adjacent properties, and a 100-year floodplain that bisects the site creating additional limitations to the area in which the applicant can build. The applicant has provided additional plant units to compensate for the reduced bufferyard width, along with a site-tight fence. The required width of landscape yard has been provided in all areas except where the access easement is present. The committee is of the opinion that the alternative is equal to or better than normal compliance with the requirements of the *Landscape Manual*.

**RECOMMENDATION:**

The Alternative Compliance Committee recommends approval of alternative compliance pursuant to Section 4.7 of the *Landscape Manual*, along the southern and western property lines.

The northwest corner of the site, which is wooded, is adjacent to the existing residential uses. Since Section 4.7 treats the entire site as one entity, this part of the site is also subject to Section 4.7, which will be met 100 percent by the existing woodland. A landscape schedule should be provided on the landscape plan. A condition of approval has been proposed in the recommendation section of this report.

- d. During the public hearing on July 21, 2005, the Planning Board approved an Alternative Compliance from the Type "D" bufferyard (instead of the above-recommended Type "C" bufferyard) after discussion, due to the fact that the proposed car dealership has a service component. Pursuant to the *Landscape Manual*, the proposed car dealership is a high impact use. A Type "D" bufferyard is required when a high-impact use is adjacent to a single-family detached residence. The Planning Board determined that the revised alternative is equal to or better than normal compliance with the requirements of the *Landscape Manual*. The required and provided landscaping measures have been modified as follows:

**REQUIRED: Section 4.7, Buffering Incompatible Uses, along the southern property line**

Length of bufferyard	312 feet
Building setback	50 feet
Landscape yard	40 feet
Plant materials	250 plant units (with 50 percent opaque fence reduction)

**PROVIDED:**

Building setback	51- 92+ feet
Landscape yard	5-30 feet
Plant materials	250 plant units (with 50 percent opaque fence reduction)

**REQUIRED: Section 4.7, Buffering Incompatible Uses, along the western property line**

Length of bufferyard	226 feet
Building setback	50 feet
Landscape yard	40 feet
Plant materials	181 plant units (with 50 percent opaque fence reduction)

**PROVIDED:**

Building setback	54 feet
Landscape yard	20-30 feet
Plant materials	274 plant units (with 30 percent existing woodlands)

10. **Woodland Conservation Ordinance:** This property is subject to the provisions of the Prince George's County Woodland Conservation Ordinance because the gross tract area is in excess of 40,000 square feet, there are more than 10,000 square feet of existing woodland on site, and there is a Type I Tree Conservation Plan, TCPI/32/04.
  - a. A forest stand delineation (FSD) submitted with the preliminary plan of subdivision was found to satisfy the requirements for a FSD in accordance with the Prince George's County Woodland Conservation Ordinance and Tree Preservation Policy Document and Technical Manual. No further information regarding the FSD is required with this DSP application.
  - b. The Type II Tree Conservation Plan TCPII/89/05 submitted with this DSP is in general conformance with the requirements of the Woodland Conservation Ordinance except for several revisions. The plan proposes to fulfill the requirement primarily on site with only 0.06 acre of off-site mitigation. The Environmental Planning Section recommends approval of TCPII/89/05 subject to three conditions.
11. **Referral Comments:** The subject application was referred to the concerned agencies and divisions. The referral comments are summarized as follows:

- a. In a memorandum dated July 8, 2005, the Community Planning Division noted that the application is consistent with the 2002 General Plan Development Pattern policies for the Developed Tier and conforms to the land use policy of the 1991 Bowie-Collington-Mitchellville and Vicinity Master Plan.
- b. The Transportation Planning Section, in a memorandum dated July 11, 2005, noted that the site plan is acceptable.

In a separate memorandum from the Transportation Planning Section dated June 6, 2005, on detailed site plan review for master plan trail compliance, the trails planner stated that there are no master plan trails issues identified in the 1991 Bowie-Collington-Mitchellville and Vicinity Master Plan that impact the subject site.

- c. The Subdivision Section, in a memorandum dated May 27, 2005, provided a detailed discussion on the conditions of approval attached to Preliminary Plan of Subdivision 4-04047, which requires the subject detailed site plan approval prior to issuance of any building permit for the subject property. See the above Finding 8 for discussion on the applicable conditions. The Subdivision Section staff also indicates that the applicant has not yet completed the signature approval of the Preliminary Plan of Subdivision. A condition of approval has been proposed in the recommendation section to require the applicant to complete the signature approval process of the preliminary plan prior to certification of this detailed site plan.
- d. The subject application was also referred to the Department of Environmental Resources. In a memorandum dated May 27, 2005, the staff noted that the site plan for Bowie Nissan is consistent with approved stormwater management concept plan #8254-2004-01.
- e. The Environmental Planning Section, in a memorandum dated June 24, 2005, provided a comprehensive review on the Type II tree conservation plan and other environmental planning-related issues such as possible noise associated with the public address system and lighting intrusion. The Environmental Planning Section recommends approval of this detailed site plan subject to three conditions, which have been incorporated into the recommendation section of this report.
- f. The Permit Section, in a memorandum dated May 31, 2005, provided 11 comments on the subject site plan regarding compliance with the *Landscape Manual* and Zoning Ordinance. The comments have either been addressed by the applicant through the revised plan or will be addressed by conditions of approval.
- g. The State Highway Administration (SHA), in a memorandum dated April 17, 2005, noted that SHA has no objection to Detailed Site Plan DSP-05013 approval.
- h. The Bowie City Council, City of Bowie, in a letter dated April 19, 2005 (G. Frederic Robinson, Mayor, to the Honorable Elizabeth M. Hewlett, Chairman, Prince George's

County Planning Board) recommended approval of the subject detailed site plan subject to 14 conditions. The applicant has satisfied most of the conditions either by revisions to the plan or by compliance with conditions of approval for this detailed site plan.

- i. The Fire Department of Prince George's County had not responded to the referral request at the time the staff report was written.
12. As required by Section 27-285(b), the detailed site plan represents a reasonable alternative for satisfying the site design guidelines of Subtitle 27, Part 3, Division 9, of the Prince George's County Code without requiring unreasonable cost and without detracting substantially from the utility of the proposed development for its intended use.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to Subtitle 27 of the Prince George's County Code, the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission adopted the findings contained herein and APPROVED the Type II Tree Conservation Plan (TCPII/89/05) and APPROVED Alternative Compliance No. AC-05015, and further APPROVED Detailed Site Plan 05013 for the above-described land, subject to the following conditions:

1. Prior to certificate approval of this detailed site plan, the applicant shall
  - a. Complete certification of Preliminary Plan of Subdivision 4-04047.
  - b. Provide information on the proposed hours of operation, indicate if there will be a public address system (PA) and, if so, how it might be used and when. Provide a noise study with projected noise contours to reflect how the PA system might adversely impact the neighboring residential area, if one is proposed.
  - c. Submit a lighting plan with photometric diagram and details of all light fixtures showing full cut-off optics.
  - d. Revise Type II Tree Conservation Plan TCPII/89/05 as follows:
    - (1) Prepare the TCPII and DSP at the same scale.
    - (2) Identify each woodland conservation treatment area to the closest 1/100<sup>th</sup> of an acre.
    - (3) Use more distinguishable shading or a pattern to better distinguish between the two woodland treatment areas (woodland cleared and woodland preserved) on the plan.
    - (4) Adjust the worksheet to show 0.06-acre of off-site mitigation to be provided by fee-in-lieu payment or the following note should be placed on the plan under the worksheet: "off-site mitigation may be met by using the fee-in-lieu option."

- (5) Show the proposed grading.
  - (6) Add details for the type of tree protection device and woodland conservation signage, including the general notes below each detail.
  - (7) Provide the applicable edge management notes on the plan.
  - (8) After these revisions have been made to the plans, have the qualified professional who prepared it sign and date it.
- e. Replace the proposed white pine with either American holly or red cedar and replace the Pin Oak in the western bufferyard with fast-growing evergreen trees such as Leyland Cypress.
- f. Add site plan notes as follows:
- “All HVAC and other roof-mounted mechanical equipment shall be screened from view.”
- “All rooftop parking shall be fully screened from view.”
- “No flags or banners may be mounted, suspended or otherwise displayed for advertising anywhere on the building.”
- “ All on-and-off loading shall take place on site.”
- g. Provide the height information for the proposed retaining walls and fence details.
- h. Provide a Section 4.2 schedule on the landscape plan
- i. Show complete compliance to Section 4.3(a) along part of the site’s eastern frontage or seek alternative compliance from this requirement.
- j. Provide a standard sidewalk connection to the adjacent commercial property to the south of the subject site.
- k. Provide a Section 4.7 schedule for the northwest portion of the subject property on the landscape plan.
- l. Provide the height information of the building and the proposed retaining wall on the site plan.
- m. Provide retaining wall details on the detail sheet.

- n. Relocate the seven display parking spaces away from the access easement to the Noerling property and stripe and paint the portion of the easement on the site plan. However, the spaces may be installed if the Noerling easement is relocated off-site.
  - o. Relocate the proposed dumpster to the north part of the site, to be reviewed and approved by the Urban Design Section as the designee of the Planning Board.
  - p. Show the proposed freestanding sign, including all dimensions and sign face area calculation, on the site plan.
2. Prior to issuance of any building permits, the applicant shall:
- a. Provide a complete north elevation demonstrating enhanced visual and architectural interest to be reviewed and approved by the Urban Design Section as the designee of the Planning Board.
  - b. Submit certification by a professional engineer with competency in acoustical analysis that demonstrates that noise intrusion from the proposed use shall not exceed 65 dBA (Ldn) at the adjoining residential property line.
  - c. Relocate the existing access easement outside the boundary of the subject Detailed Site Plan or outside the dealership area, and to build the access easement to the Noerling Property at the applicant's expense.

BE IT FURTHER RESOLVED, that an appeal of the Planning Board's action must be filed with the District Council of Prince George's County within thirty (30) days following the final notice of the Planning Board's decision.

\* \* \* \* \*

This is to certify that the foregoing is a true and correct copy of the action taken by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the motion of Commissioner Eley, seconded by Commissioner Vaughns, with Commissioners Eley, Vaughns, and Hewlett voting in favor of the motion, and with Commissioner Squire absent at its regular meeting held on Thursday, July 21, 2005, in Upper Marlboro, Maryland.

Adopted by the Prince George's County Planning Board this 28<sup>th</sup> day of July 2005.

Trudye Morgan Johnson  
Executive Director



By Frances J. Guertin  
Planning Board Administrator

TMJ:FJG:HZ:rmk

APPROVED AS TO LEGAL SUFFICIENCY.

  
M-NCP&C Legal Department

Date 7/26/05

# **Additional Back-up**

**For**

**DSP-23026**  
**Ourisman of Bowie**



# Christopher Rizzi, RLA

## Senior Project Manager

Chris serves as Senior Project Manager in Bohler's Bowie, Maryland office. He is responsible for managing daily operations, and project management and design for a range of residential, mixed-use, commercial and industrial developments. Chris has over 30 years of academic and engineering experience in site feasibility analysis/due diligence, zoning/subdivision code interpretation, planning and site layout, vehicular circulation, landscaping design, site grading, road design and implementation.

Additional areas of land development experience include regulatory permitting and procurement, and the coordination and approvals for over 500 projects throughout the state of Maryland. His land use application and construction permit approval experience ranges from rezoning cases to Special Exception applications, Alternative Compliance, Site Plan and subdivision, as well as site and building permit and bond release stages.

[CRIZZI@BOHLERENG.COM](mailto:CRIZZI@BOHLERENG.COM)

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### EDUCATION

B.S. Landscape Architecture  
Pennsylvania State University

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### PROFESSIONAL LICENSES

Maryland RLA #3066

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### PROFESSIONAL AFFILIATIONS

ASLA  
International Council of Shopping Centers  
Urban Land Institute  
NAIOP  
Maryland Building Industry Association  
Bowie Chamber of Commerce

## Select Project Experience

Chris has made many presentations and has provided testimony before Planning Boards, Commissions, Councils, Board of Appeals and community meetings throughout multiple Maryland jurisdictions.

## Select Professional Experience

- + Melford Village, Bowie, MD
- + Locust Hill Residential Development, Bowie, MD
- + Willowbrook Residential Development, Bowie, MD
- + MetroPlace at Town Center, Camp Springs, MD
- + BeechTree, Upper Marlboro, MD
- + Thrive Senior Living, Bowie, MD
- + FutureCare Health and Management, Landover, MD
- + CMIT Public Charter School, Upper Marlboro
- + Bishop McNamara High School, Forestville, MD



**Joseph L. Heisler III, AIA, LEED AP**

**Principal**

**Joe leads each project under the guidance of quality to service, design, and construction.** With over 21 years of experience with a wide range of project types and clients, Joe's contribution to each project is essential to its success and is one of the reasons many of the firm's clients are repeat clients. Joe is **involved in all aspects of a project** from feasibility studies and programming through the production of construction documents and specifications to construction observation, and post construction services.

As a Principal he advises the staff, sets quality standards and reviews documents for compliance to these standards. His **attention to detail** and ability to visualize building systems has directed the firm's Quality Assurance Program.

Joe's experience across different project types brings value to each project he designs. Using best practices from different industries to influence concepts and design infuses each project with his unique perspective and creates a better design.

**EDUCATION**

University of Virginia  
 Bachelor of Science 1986  
 Master of Architecture, 1988

**REGISTRATIONS**

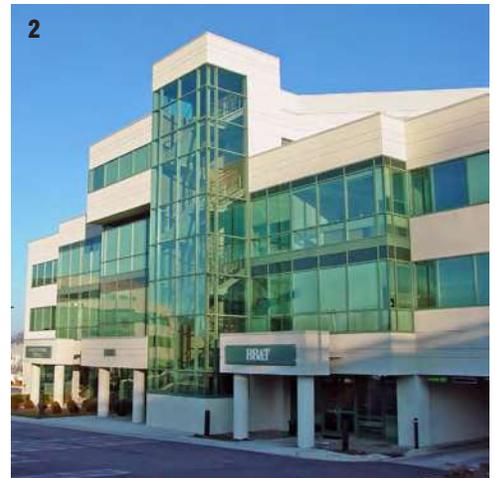
Maryland #9093  
 Pennsylvania #RA404939  
 West Virginia #4106

**AFFILIATIONS**

National Council of Architectural  
 Registration Boards  
 Certificate # 64193  
 American Institute of Architects  
 Chesapeake Area Society of Healthcare  
 Engineers, Inc.

**COMMUNITY**

Glen Rock, PA Borough Planning  
 Commission  
 Glen Rock, PA Library – Board of  
 Directors  
 BGSPS - Board of Directors



**Featured Projects**

- 1. Howard Business Park-Lot 5  
 Condominium Warehouse Buildings**
- 2. Columbia 100**
- 3. UMCES Horn Point AREL  
 Aquaculture and Restoration**

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# MICHAEL M. LENHART, P.E., P.T.O.E.

## PRESIDENT

Mike Lenhart is a professional traffic engineer with over 28 years of combined technical and academic experience. Responsibilities with the firm include, but are not limited to, proposal preparation, various traffic engineering and managerial tasks in the areas of traffic impact analysis, traffic safety studies, and transportation planning, as well as providing expert witness testimony at public hearings and community meetings.

Mr. Lenhart has worked as a transportation professional in the private sector since 1999 and has provided traffic engineering and transportation planning services for over one thousand projects in numerous jurisdictions across Maryland. Previously, Mr. Lenhart served as the Chief of the Engineering Access Permits Department for the Maryland State Highway Administration (SHA). During his tenure at the SHA, Mr. Lenhart also served as the Traffic Engineer overseeing Southern Maryland. During his career, he has performed various traffic engineering tasks, including traffic signal design, highway and intersection capacity analysis, maintenance and protection of traffic design, and transportation planning. He has also participated in engineering training programs and researched transportation related topics.

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### **Job History**

#### **2005 - Present**

*President* – Lenhart Traffic Consulting, Inc.

#### **2002 - 2005**

*Senior Project Manager* - The Traffic Group, Inc.

#### **2000 – 2002**

*Independent Consultant* - The Traffic Group, Inc.

#### **1999 - 2000**

*Senior Associate* - The Traffic Group, Inc.

#### **1998 – 1999**

*Division Chief – Engineering Access Permits*  
Maryland State Highway Administration

#### **1990 - 1998**

*Traffic Engineer*  
Maryland State Highway Administration

### **Educational Background**

- *Bachelors of Science in Civil Engineering*  
-1990 (U of MD @ College Park)
- *Masters of Science in Traffic Engineering & Transportation Planning*  
-1998 (U of MD @ College Park)

### **Affiliations**

- Registered Professional Engineer (P.E.) – MD, DE
- Professional Traffic Operations Engineer (PTOE) - ITE
- Member ITE

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### **Places where Mr. Lenhart has testified as an expert witness**

Allegany County – Board of Appeals, Planning Commission  
Annapolis - Planning Commission, Board of Appeals  
Anne Arundel County – Board of Appeals, Administrative Hearing Officer  
Baltimore County – Zoning Commissioner, Planning Board  
Calvert County – Planning Commission, Board of Appeals,  
County Commissioners  
Carroll County – Board of Zoning Appeals; Planning Board  
Charles County – County Commissioners, Circuit Court,  
Board of Appeals, Planning Commission,  
Town of LaPlata Planning Commission, Town Council, Board of Appeals  
City of Frederick – Planning Commission  
Frederick County – Planning Commission, County Commissioners  
Harford County – Circuit Court  
Prince George’s County – District Council, Planning Board, Zoning  
Examiner, Bowie City Council & Planning Commission, City of Laurel

Montgomery County – Planning Board, Zoning Examiner  
Queen Anne’s County – Planning Commission  
St. Mary’s County – Planning Commission; County  
Commissioners  
Sussex County, DE – Planning Commission, Board of  
County Commissioners  
Talbot County – Planning Commission  
Town of Leesburg, VA – Planning Commission  
Washington County – Board of County Commissioners  
Worcester County – Planning Commission

### **MARYLAND STATE HIGHWAY ADMINISTRATION- ENGINEERING SERVICES, DISTRICT 5**

High Accident Sections  
Traffic Safety Studies  
Traffic Signal Warrant Studies  
Highway Design Consultation  
Project Planning Consultation  
Traffic Impact Study Review

# RESUME

## **Eric R. McWilliams, R.L.A. Landscape Architect**

### **EDUCATION:**

Bachelor of Agricultural Business Management, Pennsylvania State University

### **PROJECT TESTIMONY:**

Testified on behalf of Bohler Engineering in numerous municipalities before Planning Commissions, City Councils, Board of Supervisors, Board of Appeals, Board of Adjustments and related municipal entities in Delaware, Maryland, and Pennsylvania. Admitted as an expert witness in Anne Arundel, Baltimore, Howard and Montgomery Counties for Landscape Architecture, Forest Conservation, Planning and Septic Design.

### **EXPERIENCE:**

Currently serves as a Senior Project Manager of Planning and Landscape Architect in Bohler Engineering's Towson, Maryland Office. Experience includes over twenty-five (25) years of design and project management. Primarily responsible for site planning, lighting design, landscape plans, environmental design and project management for various commercial and residential developments. Areas of experience include site feasibility analysis and budgeting, environmental permitting, forest conservation, site layout and planning, zoning and subdivision ordinance review/interpretation, site grading, earthwork balancing and analysis, soil erosion and sediment control measures and facilities, utility design, septic design, compliance/evaluations, lighting photometric studies/design, existing photometric studies, signage compliance, landscaping design, vehicular circulation design, oversight of expediting and application approvals, and related services. Expertise includes supermarkets, gasoline service stations, maintenance facilities, restaurants, shopping centers, retail centers, car washes, residential subdivision, industrial design and other related projects in municipalities in Maryland, Delaware and Pennsylvania.

### **PROFESSIONAL AFFILIATIONS:**

- International Council of Shopping Centers (ICSC) MD Government Relations Chairman
- Maryland Building Industry Association (MBIA) Environmental and Government Committees Member
- Urban Land Institute (ULI)
- Maryland Registered Landscape Architect #3697
- Delaware Licensed Landscape Architect #S1-0000502