



The Maryland-National Capital Park and Planning Commission
 Prince George's County Planning Department
 Development Review Division
 301-952-3530

Note: Staff reports can be accessed at <http://mncppc.iqm2.com/Citizens/Default.aspx>

Detailed Site Plan

DSP-95073-06

Kaiser Permanente Largo Medical Center

REQUEST	STAFF RECOMMENDATION
Construction of 11,050 square feet of additions to an existing medical office building and an 80,000-square-foot addition to an existing parking garage.	APPROVAL with conditions

Location: In the southwest quadrant of the intersection of MD 202 (Landover Road) and Technology Way.	
Gross Acreage:	14.71
Zone:	RTO-H-E
Dwelling Units:	N/A
Gross Floor Area:	247,250 sq. ft.
Planning Area:	73
Council District:	06
Election District:	13
Municipality:	N/A
200-Scale Base Map:	202NE09
Applicant/Address: A. Morton Thomas & Associates, Inc. 800 King Farm Blvd, 4th Floor Rockville MD, 20850	
Staff Reviewer: Andrew Bishop Phone Number: 301-952-4897 Email: Andrew.Bishop@ppd.mncppc.org	



Planning Board Date:	04/28/2022
Planning Board Action Limit:	05/03/2022
Staff Report Date:	04/14/2022
Date Accepted:	02/22/2022
Informational Mailing:	11/04/2021
Acceptance Mailing:	02/10/2022
Sign Posting Deadline:	03/29/2022

The Planning Board encourages all interested persons to request to become a person of record for this application. Requests to become a person of record may be made online at http://www.mncppcapps.org/planning/Person_of_Record/. Please call 301-952-3530 for additional information.

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THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION

PRINCE GEORGE'S COUNTY PLANNING BOARD

STAFF REPORT

SUBJECT: Detailed Site Plan DSP-95073-06
Kaiser Permanente Largo Medical Center

The Urban Design staff has completed its review of the subject application and appropriate referrals. The following evaluation and findings lead to a recommendation of APPROVAL, with conditions, as described in the Recommendation section of this report.

EVALUATION

This amendment to a detailed site plan was reviewed and evaluated for compliance with the following criteria:

- a. The requirements of the Development District Overlay (D-D-O) Zone standards of the 2013 *Approved Largo Town Center Sector Plan and Sectional Map Amendment*;
- b. The requirements of the prior Prince George's County Zoning Ordinance for the Mixed Use-Infill (M-U-I) and Development District Overlay (D-D-O) Zones, and site design guidelines of the Zoning Ordinance;
- c. The requirements of Conceptual Site Plan CSP-87168 and its amendment;
- d. The requirements of Preliminary Plans of Subdivision 4-79155 and 4-86107;
- e. The requirements of Detailed Site Plan DSP-95073 and its amendments;
- f. The requirements of the 2010 *Prince George's County Landscape Manual*;
- g. The requirements of the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance;
- h. The requirements of the Prince George's County Tree Canopy Coverage Ordinance; and
- i. Referrals.

FINDINGS

Based upon the evaluation and analysis of the subject application, the Urban Design Section recommends the following findings:

1. **Request:** The subject detailed site plan (DSP) is for the construction of 11,050 square feet of additions to an existing medical building and an 80,000-square-foot addition to the existing parking garage.
2. **Development Data Summary:**

	EXISTING	PROPOSED
Zone(s)	RTO-H-E (Prior M-U-I/D-D-O)	RTO-H-E (Prior M-U-I/D-D-O)
Use(s)	Medical Office Building	Medical Office Building
Lot 2 Acreage	8.51	8.51
Parcel 5 Acreage	6.20	6.20
Total Gross Acreage	14.71	14.71
Proposed Addition Phase 1 (Advanced Urgent Care)	-	4,850 sq. ft.
Proposed Addition Phase 2 (Imaging)	-	6,200 sq. ft.
Total Building Gross Floor Area	236,200 sq. ft.	247,250 sq. ft.
Proposed Addition Phase 1 (Parking Structure)	-	80,000 sq. ft.
Parking Structure	245,200 sq. ft.	325,200 sq. ft.

OTHER DEVELOPMENT DATA:

Parking	Number of Parking Spaces Required	Number of Parking Spaces Provided
247,250 sq. ft. Medical Office Building (4 to 5 spaces/1000 Gross Floor Area)	989-1,235*	1,221
Standard Spaces	-	1,157
Handicapped Spaces (Regular)	23	58
(1 of 4 handicapped spaces Van Accessible)	6	6
Loading**	3	3

Notes: *The Development District Overlay (D-D-O) Zone standard on page 166 of the 2013 *Approved Largo Town Center Sector Plan and Sectional Map Amendment* (Largo Town Center Sector Plan and SMA) sets a maximum rather than minimum parking space requirement to apply to this property. The minimum required on-site parking for the proposed use is 4.00 spaces/1,000 square feet, and the maximum is 5.00 spaces/1,000 square feet.

**The D-D-O Zone does not have a standard for required loading spaces. Therefore, the loading standards outlined in Section 27-582 of the prior Prince George's County Zoning Ordinance requires three spaces, which have been provided by the applicant.

3. **Location:** The property is located at 1221 Mercantile Lane and consists of one parcel known as Parcel 5, recorded in Plat Book NLP 116 on page 89, and one lot known as Lot 2, recorded in Plat Book NLP 151 on page 28. The 14.71-acre site is located on Tax Map 67 in Grid F-1, west of MD 202 (Landover Road), at the southwest corner of its intersection with Technology Way. The property is in the Largo Town Center Sector Plan and SMA, in Planning Area 73 and Council District 6.
4. **Surrounding Uses:** The subject property is bounded to the east by MD 202, to the west by Mercantile Lane, to the north by Technology Way, and to the south by commercial uses and a stormwater management (SWM) facility. The site and surrounding properties are in the edge area of the Regional Transit-Oriented, High-Intensity Zone.
5. **Previous Approvals:** The case history of this site precedes much of the recent development in the Largo area. The northern portion of the property, Parcel 5, was previously subdivided through Preliminary Plan of Subdivision (PPS) 4-79155, and Lot 2 is the subject of PPS 4-86107.

The site is the subject of multiple DSP applications, including DSP-95073, which was approved by the Prince George's County Planning Board on December 21, 1995 (PGCPB Resolution No. 95-417), for development of a 127,000-square-foot medical center and 635 surface parking spaces in the Planned Industrial/Employment Park (I-3) Zone of the prior Zoning Ordinance. The DSP has been amended five times, including DSP-95073-01, which was approved by the Planning Board on June 24, 2010 (PGCPB Resolution No. 10-74), for the development of a 106,700-square-foot addition to the existing medical office building and the construction of a 245,200-square-foot parking garage in the I-3 Zone.

The Planning Director approved the following four amendments: DSP-95073-02, approved on March 28, 2012, to reflect changes to parking space sizes and parking counts on-site; DSP-95073-03, approved on April 27, 2018, to add trailers within on-site surface parking and renovate the existing medical office to include two modular computerized tomography (CT) and magnetic resonance imaging (MRI) units; DSP-95073-04, approved on September 6, 2018, to relocate access doors for the modular CT scanner and MRI facility, and DSP-95073-05, approved on June 1, 2021, to install five natural gas energy servers on-site.

The Largo Town Center Sector Plan and SMA classified the property to the Mixed Use-Infill (M-U-I) Zone, with a D-D-O Zone that guides development of the property.

The site is the subject of an approved SWM Concept Plan, 44959-2021, which is valid until February 8, 2025.

6. **Design Features:** The subject application is being reviewed according to the applicable zoning standards for the subject property, and pursuant to the prior Zoning Ordinance. The application proposes to construct 11,050 square feet of additions to a 236,200-square-foot existing medical building, and an 80,000-square-foot addition to an existing 245,000-square-foot parking garage in two phases.

The site can be accessed from three existing points of ingress/egress: two along Mercantile Lane and the third along Technology Way. Phase 1 will include the proposed 4,850-square-foot expansion of the Advanced Urgent Care center, to modernize the existing facility with 15 new treatment rooms and supporting laboratory space. The on-site vehicular circulation will be revised by adding a 22-foot-wide two-way drive aisle and a covered drop-off area at the main entrance. Phase 2 includes the 6,200-square-foot expansion of the existing imaging facility and incorporation of new environmental site design features such as green roofs, native plantings, and a SWM facility. The DSP also proposes to construct an 80,000-square-foot addition to the existing parking garage in phase one, which will replace existing surface parking adjacent to Mercantile Lane.



Figure 1: Site Plan

Architecture

The proposed one-story building additions range in height and are approximately 14 to 18 feet in height. The Advanced Urgent Care addition is located on the west side of the building near the main entrance and the imaging addition is located at the rear of the building on the east side of the structure. The building additions are designed to blend seamlessly with the existing building with proposed materials, including a mix of brick, glass, concrete, fiber cement siding, and metal siding. Trim, coping, and other detail elements such as horizontal banding have been proposed to add architectural interest. The parking garage expansion is located on the west side of the existing parking garage. It is approximately 51 feet in height, 4 stories, and is constructed in brick to match the existing parking garage. The proposed western façade of the garage includes a decorative metal wall panel on the

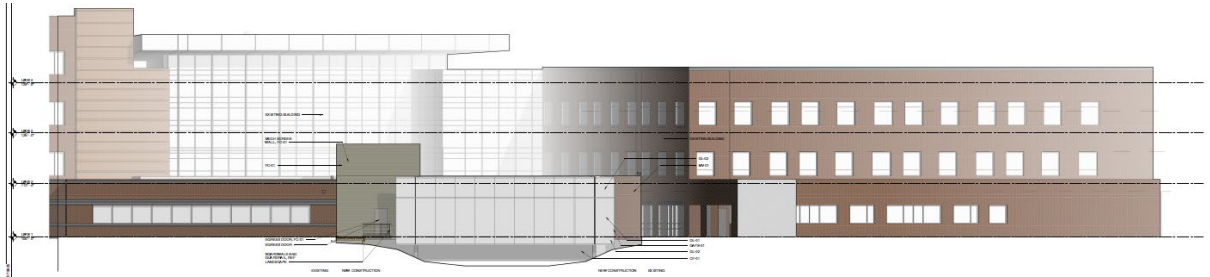


Figure 3: Advanced Urgent Care Addition Elevation

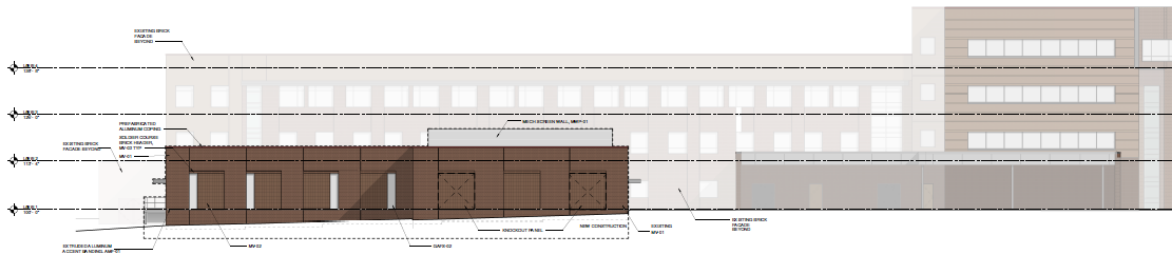


Figure 4: Imaging Addition Elevation

Lighting

A photometric plan was submitted with this application and proposes a combination of full cut-off, building-mounted, and pole-mounted lights and bollards to illuminate the site. The photometric plan demonstrates that there is adequate lighting for pedestrians and vehicles within the drive aisles and pedestrian pathways on site, with minimum spillover at property lines. Details of the lighting have been provided and are sufficient.

Signage

The application proposes one building-mounted sign on the parking garage that is not illuminated and includes the corporate logo and the name of the facility. The proposed sign is located on the west elevation of the parking garage and faces Mercantile Lane and exceeds the allowed square footage. An amendment has been filed by the applicant and staff recommends approval of this request, as discussed in Finding 7.

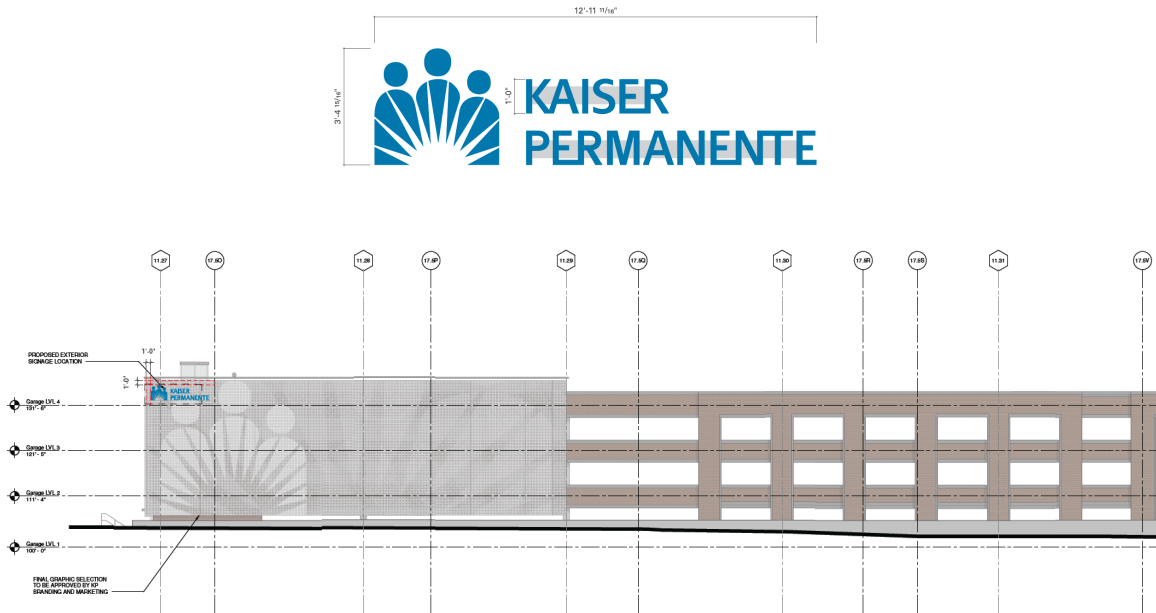


Figure 5: Proposed Signage

Loading, Trash Facilities, and Site Details

Loading is required for the medical office building and is provided on the east side of the building adjacent to the proposed imaging addition. The trash facilities are interior to the building and not proposed to change with this application. The loading spaces are set back from MD 202 and are adequately screened by landscaping and a brick wall. Labels showing the dimension and location of these spaces have not been included on the site plan and should be provided. Therefore, a condition has been included to clearly dimension these spaces and label them on the DSP for clarification.

Green Building Techniques

A green roof is proposed on the Advanced Urgent Care addition in combination with environmental site design features such as native plantings, and environmental site design stormwater techniques to mitigate the facility’s environmental impact and enhance on-site SWM on the property.

COMPLIANCE WITH EVALUATION CRITERIA

- 7. **2013 Approved Largo Town Center Sector Plan and Sectional Map Amendment and Development District Overlay Zone (D-D-O):** The subject site is located within the northeast quadrant of the Largo Town Center Sector Plan and SMA. The Largo Town Center Sector Plan envisions the transformation of the Largo Town Center Metro Station Area into one of Prince George’s County’s premiere mixed-used “downtowns” and 24-hour activity centers.

Section 27-548.25(b) of the Zoning Ordinance requires that in approving the DSP, the Planning Board shall find that the site plan meets applicable development district standards. This Kaiser Permanente medical facility was approved and developed under the I-3 Zone standards prior to the adoption of the Largo Town Center Sector Plan and SMA. This application does not propose all new construction, and it is challenging to fully comply with the standards relating to block length, build-to line/setback, building frontage, floor height,

fenestration, and garage placement requirements because it was originally developed under previous requirements. The submitted application and statement of justification indicate the need to deviate from several development district standards, in order to accomplish a uniform development on the subject property. In accordance with Section 27-548.25(c), Site Plan Approval, of the Zoning Ordinance, if the applicant so requests, the Planning Board may apply development standards which differ from the approved development district standards. These alternate standards may be approved if they can be found to benefit the development and the development district and will not substantially impair implementation of the master plan, master plan amendment, or sector plan. The application generally meets the standards of the development district; however, the applicant is requesting the following modifications from the development district standards in the Largo Town Center Sector Plan and SMA (all page numbers reference the sector plan):

- a. **Urban Design Criteria/Block Length (page 133): No block length shall be greater than 450 feet without a common access easement (CAE) or pedestrian passage providing through access to another street, CAE, or public open space.**

The applicant is requesting a modification to this Development District Standard to amend the block length to a maximum of 675 feet. The existing block along Mercantile Lane is 671 feet long and the applicant justifies this modification because the proposed DSP amendment will not impact the existing block length, street frontage, or site use. The property was developed prior to the implementation of the Largo Town Center Sector Plan and SMA and is legally existing. It would not be feasible to modify the existing block length to accommodate the maximum block length. For these reasons, staff recommends approval of this modification.

- b. **Urban Design Criteria/Build to Line (BTL) (page 135): BTLs shall be located within 15 feet back from the Pedestrian Zone, and the full width from face of curb to building front should not exceed 25 feet. (See the Street Design Criteria on page 152 for additional information.)**

The applicant is requesting a modification to this Development District Standard to amend the build-to line placement to 45 feet. This DSP proposes a garage addition to be constructed within 45 feet of the Pedestrian Zone, along Mercantile Lane. The addition brings the garage closer to the street, and promotes a more continuous street façade, as recommended by the Largo Town Center Sector Plan and SMA. In addition to bringing the building closer to the pedestrian zone, the applicant also proposes to activate the street through the addition of 3 seating areas, which further enhances and connects to the existing pedestrian loop. Within these seating areas, the applicant proposes benches, bollard lighting, and enhanced landscaping. These improvements advance the brand of New Urbanism envisioned by the Largo Town Center Sector Plan and SMA. For these reasons, staff recommends approval of this modification.

- c. **Urban Design Criteria/Frontage (page 138): The percent of building frontage shall be 70–100 percent of block length (or individual lot) in the Southeast, Northeast, and Northwest Quadrants.**

The applicant is requesting a modification to this Development District Standard to amend the minimum building frontage to 45 percent. This is the percentage of the block frontage that is occupied by the expanded parking garage. The applicant requests this modification to accommodate existing development on-site. The

existing block length, garage length, and frontage percentage are unchanged from the legally existing development. This DSP amendment is adding a bay to the existing garage, and it is proposed to be constructed over an existing surface parking lot bringing the garage closer to the street. In both the existing and proposed development, the block length is 671 feet, and the garage length is 329 feet for a 49 percent ratio. This ratio is appropriate to accommodate the existing uses on-site, which were approved pursuant to I-3 zoning requirements. For these reasons, staff recommends approval of this modification.

- d. **Urban Design Criteria/Building Height (page 141): Commercial uses at the ground level should have an interior clear height (floor to ceiling) of at least 14 feet contiguous to the BTL frontage, for a minimum depth of 20 feet. The maximum ground-level story height for commercial uses is 22 feet.**

The applicant states that the expansion of the parking garage is within view of the public realm and is a four-story structure. It is designed to align with the existing garage and will maintain existing clear heights, which were approved pursuant to the I-3 zoning on-site. The proposed improvements will allow for a contiguous connection and provide adequate vehicular circulation throughout the parking structure. For these reasons, staff recommends approval of this modification.

- e. **Architectural Design Criteria/Fenestration (page 159): The relationship between solid building walls and openings (fenestration) is critical; the ratios should vary according to use and shall be calculated per elevation and floor-to-floor. Ground and upper floor ratios for Commercial/Institutional uses shall be 40-90.**

The applicant indicates that the percentage of openings for the proposed structured garage will be consistent with the existing parking garage that was built according to the I-3 Zone standards. The fenestration size proposed in this DSP is consistent with the existing parking garage, and by maintaining this relationship it will retain the structural integrity. For these reasons, staff recommends approval of this modification.

- f. **Signage Design Criteria (page 170): The total sign area allowed per building shall be computed on the basis of two square feet of sign area for each one linear foot of building frontage. Where a building has multiple frontages, the allowed sign area should be distributed proportionally along each building frontage. Buildings with less than 60 linear feet of building frontage may be allowed up to 120 square feet of sign area.**

The applicant indicates the total allowed square footage for signs along Mercantile Lane is 654 square feet. The perforated graphic panel design includes portions of the Kaiser Permanente logo and is considered signage. The graphic design is a total of 928 square feet, which exceeds the allowed square footage by 274 square feet. The image is intended to be a mural, to screen the structured parking facility and add visual interest to the streetscape of Mercantile Lane. Its approval will enhance the proposed development and promote the goals and objectives of the development district by screening parking from the public realm, defining the vertical definition of the street, and activating the streetscape. For these reasons, staff recommends approval of this modification

8. **Prince George’s County Zoning Ordinance:** The subject DSP amendment has been reviewed for conformance with the applicable requirements of the M-U-I and D-D-O Zones and the site design guidelines. The following discussion is offered regarding these requirements:

a. Section 27-546.19(c), Site Plans for Mixed Uses, of the Zoning Ordinance, requires that:

(c) A Detailed Site Plan may not be approved unless the owner shows:

- (1) The site plan meets all approval requirements in Part 3, Division 9;**
- (2) All proposed uses meet applicable development standards approved with the Master Plan, Sector Plan, Transit District Development Plan, or other applicable plan;**

The site plan meets the site design guidelines and development district standards of the Largo Town Center Sector Plan and SMA, except those discussed in Finding 7 above.

- (3) Proposed uses on the property will be compatible with one another,**
- (4) Proposed uses will be compatible with existing or approved future development on adjacent properties and an applicable Transit or Development District; and**

The application proposes additions to a legally existing medical facility that was approved and developed under the I-3 Zone standards prior to the adoption of the D-D-O Zone standards. The existing use will not change with this application. The proposed additions are compatible with the existing structure and are similar in height and material.

(5) Compatibility standards and practices set forth below will be followed, or the owner shows why they should not be applied:

(A) Proposed buildings should be compatible in size, height, and massing to buildings on adjacent properties;

The property is bound by three public rights-of-way on the north, west and east with a micro-bioretenion area to the south. Adjacent properties to the north and west are developed with commercial and institutional uses. Specifically, the properties to the west include a multi-story commercial building and church and the building to the north is a multi-story bank. The height of the building additions proposed are compatible with the existing structure and in keeping with the vision and intent of the Largo Town Center Sector Plan and SMA.

- (B) Primary façades and entries should face adjacent streets or public walkways and be connected by on-site walkways, so pedestrians may avoid crossing parking lots and driveways;**

The primary façades of the buildings face MD 202, Mercantile Lane, and Technology Way. Proposed pedestrian connections with seating areas have been included on Mercantile Lane to improve connectivity and activate the street. The seating areas will enhance the streetscape and provide an additional connection to the existing pedestrian loop. Within these seating areas, the application proposes benches, bollard lighting, and enhanced landscaping.

- (C) Site design should minimize glare, light, and other visual intrusions into and impacts on yards, open areas, and building façades on adjacent properties;**

The photometric plan provided with the DSP indicates that the proposed lighting design will minimize glare, light, and visual intrusion onto nearby properties and buildings.

- (D) Building materials and color should be similar to materials and color on adjacent properties and in the surrounding neighborhoods, or building design should incorporate scaling, architectural detailing, or similar techniques to enhance compatibility;**

The materials and colors selected for the proposed building additions are compatible with the existing building and the surrounding buildings. The materials proposed include a mix of brick, concrete, fiber cement siding, and metal siding. Trim, coping, and other detail elements are provided in complimentary tones and materials and include a green roof and elevated outdoor walkway.

- (E) Outdoor storage areas and mechanical equipment should be located and screened to minimize visibility from adjacent properties and public streets;**

The DSP is consistent with prior approvals and adequately screens mechanical equipment and loading areas with brick walls and landscaping.

- (F) Signs should conform to applicable Development District Standards or to those in Part 12, unless the owner shows that its proposed signage program meets goals and objectives in applicable plans; and**

The signs conform to the applicable development district standards.

(G) The owner or operator should minimize adverse impacts on adjacent properties and the surrounding neighborhood by appropriate setting of:

(i) Hours of operation or deliveries;

The DSP is consistent with prior approvals and the proposed additions will not change the hours of operation and deliveries. The proposed development will have minimal impacts on adjacent properties, as trash is internal to the building and no additional loading spaces are proposed.

(ii) Location of activities with potential adverse impacts;

No new activities with adverse impacts are proposed by this application.

(iii) Location and use of trash receptacles;

The proposed trash receptacles are located internal to the building and are consistent with the prior approvals.

(iv) Location of loading and delivery spaces;

The applicant does not propose additional loading spaces on-site. The loading spaces are existing and will not be relocated with this DSP.

(v) Light intensity and hours of illumination; and

The photometric plan submitted with the application showing on-site lighting, confirms that there are minimal adverse impacts on adjacent properties and the surrounding neighborhood from the proposed development.

(vi) Location and use of outdoor vending machines.

The subject DSP does not propose any outdoor vending machines.

- b. The DSP is in general conformance with the applicable site design guidelines, as referenced in Section 27-283 and contained in Section 27-274 of the Zoning Ordinance. For instance, vehicular and pedestrian circulation are designed to be safe, efficient, and convenient for both pedestrians and drivers. Streetscape and site amenities contribute to an attractive, coordinated development that is appropriately scaled for user comfort. Additionally, the public spaces are designed to incorporate sitting areas and are readily accessible to potential users.

c. Section 27-548.25(a), (b), and (c), Site Plan Approval, of the Zoning Ordinance, are applicable to the review of this DSP, as follows:

(a) Prior to issuance of any grading permit for undeveloped property or any building permit in a Development District, a Detailed Site Plan for individual development shall be approved by the Planning Board in accordance with Part 3, Division 9. Site plan submittal requirements for the Development District shall be stated in the Development District Standards. The applicability section of the Development District Standards may exempt from site plan review or limit the review of specific types of development or areas of the Development District.

The subject application has been submitted in fulfillment of this request.

(b) In approving the Detailed Site Plan, the Planning Board shall find that the site plan meets applicable Development District Standards.

(c) If the applicant so requests, the Planning Board may apply development standards which differ from the Development District Standards, most recently approved or amended by the District Council, unless the Sectional Map Amendment text specifically provides otherwise. The Planning Board shall find that the alternate Development District Standards will benefit the development and the Development District and will not substantially impair implementation of the Master Plan, Amendment, or Sector Plan.

Due to the location of the existing building, site constraints, the construction of the proposed additions, five requested alternate development district standards as discussed in Finding 7 will benefit the development and will not substantially impair implementation of Largo Town Center Sector Plan and SMA. All other applicable development district standards are met.

9. Conceptual Site Plan CSP-87168-01: The property is subject to Conceptual Site Plan CSP-87168-01, approved for Largo Park, by the Planning Board on August 3, 1989. The subject amendment to the DSP satisfies the relevant CSP conditions, as explained below:

1. A minimum 30-foot landscaped yard shall be provided along the property line adjacent to MD 202 (as required by I-3 Zone regulations of the Zoning Ordinance, Subtitle 27).

A 30-foot landscaped yard was approved with DSP-95073 and is unchanged with this application.

10. Preliminary Plans of Subdivision 4-79155 and 4-86107: This property is subject to PPS 4-79155 and PPS 4-86107. Parcel 5 is subject to PPS 4-79155, which was approved by the Planning Board on December 3, 1979, for 17 lots and uses in the I-3 Zone, and Lot 2 is subject to PPS 4-86107, which was approved by the Planning Board on July 24, 1986 (PGCPB Resolution No. 86-297), for 11 lots and uses in the I-3 Zone.

The available records for the two PPSs do not include a trip cap or establish specific development quantities, therefore the determination of development entitlement for Parcel 5 and Lot 2 was analyzed by the Transportation Planning Section to determine capacity of the site. It is determined that while PPS 4-79155 and PPS 4-86107 included a finding of transportation adequacy, Parcel 5 and Lot 2 were recorded without a trip cap or other similar restriction on the quantity of development.

The methodology for analyzing the trip cap used in the previously approved DSPs is acceptable and suitable for the current development proposal. A new PPS or final plat is not required for the development proposed in this amendment. However, if additional development is proposed in the future on the subject property, a new PPS and final plat may be required.

No conditions of approval related to PPS 4-79155 are relevant to the review of this application. However, the plat for this PPS notes that access to MD 202 should be denied, and a relevant note is provided on the DSP.

PPS 4-86107 was approved subject to two conditions. The conditions relevant to the review of the subject application, and staff analysis of the project's conformance to the conditions are presented as follows:

1. Approval of a conceptual stormwater management plan by the Washington Suburban Sanitary Commission prior to Final Plat of Subdivision.

The final plat of subdivision for the subject lot was approved on November 18, 1982. The applicant has indicated that a conceptual SWM plan was approved prior to final plat approval on July 20, 1989. The applicant submitted a new approved SWM concept plan (44959-2021-00) with this application, and it is consistent with the layout shown on the DSP.

2. Denial of access on Arena Drive.

The subject property does not have frontage on Arena Drive. Denial of access is labeled on the property's frontage with MD 202, in accordance with the approved PPS.

11. Detailed Site Plan DSP-95073 and its amendments: The DSP has been approved and amended five times for various reasons.

DSP-95073 was approved by the Planning Board on December 21, 1995 (PGCPB Resolution No. 95-417), for the development of a 127,000-square-foot medical facility, subject to one condition to be addressed prior to certification.

DSP-95073-01 was approved by the Planning Board on June 24, 2010 (PGCPB Resolution No. 10-74) for a 106,700-square foot-expansion to the medical center, and a 245,000-square-foot parking garage, subject to one condition to be addressed prior to certification.

None of the conditions related to the prior approvals are applicable to the subject application.

12. **2010 Prince George’s County Landscape Manual:** Page 132 of the Largo Town Center Sector Plan and SMA states that the provisions of the 2010 *Prince George’s County Landscape Manual* (Landscape Manual) regarding alternative compliance and buffering of incompatible uses do not apply within the development district. All other standards and regulations of the Landscape Manual apply. The DSP has been reviewed for conformance with these requirements and provides all the necessary schedules and plantings based on the scope of these improvements.
13. **Prince George’s County Woodland and Wildlife Habitat Conservation Ordinance:** The project is exempt from the requirements of the Prince George’s County Woodland and Wildlife Habitat Conservation Ordinance because it does not have a minimum of 10,000 square feet of woodland, nor any prior approved tree conservation plans, and was issued a Standard Letter of Exemption (S-118-2021) on June 17, 2021. However, the site plan submitted for this exemption does not match the DSP. A condition has been included herein, to revise the exemption, so it matches.
14. **Prince George’s County Tree Canopy Coverage Ordinance:** The site is subject to the Prince George’s County Tree Canopy Coverage Ordinance because it proposes more than 5,000 square feet of disturbance. Based on the prior M-U-I zoning of the site, 10 percent of the site is to be covered in tree canopy. The overall site measures 14.713 acres, and the subject application includes a schedule demonstrating that the required amount of tree canopy coverage is provided in this DSP amendment.
15. **Referral Comments:** The subject application was referred to the concerned agencies and divisions. The referral comments are incorporated herein by reference and summarized as follows:
 - a. **Historic Preservation**—In a memorandum dated March 23, 2022 (Stabler to Bishop), the Historic Preservation Section noted that a search of current and historic photographs, topographic and historic maps, and locations of currently known archeological sites indicates the probability of archeological sites within the subject property is low. The subject property does not contain and is not adjacent to any Prince George’s County historic sites or resources. The proposal will not impact any historic sites, historic resources, or known archeological sites. A Phase I archeology survey is not recommended.
 - b. **Community Planning**—In a memorandum dated March 25, 2022 (Umeozulu to Bishop), the Community Planning Division provided an analysis of the subject DSP’s conformance with the recommendations of the Largo Town Center Sector Plan and SMA and provided an analysis of the proposed alternative development district standards requirements, which are included in Findings 7 and 8 above.
 - c. **Transportation Planning**—In a memorandum dated March 29, 2022 (Ryan to Bishop), it was noted that there are no prior conditions of approval on the subject property related to transportation improvements. The number and locations of points of access are sufficient. It was also noted that the resolution for DSP-95073-01 contains a section regarding the trip cap on the subject site. The resolution states, “Neither 4-79155 nor 4-86107 includes trip cap conditions or other restrictions on the quantity of development that can occur within the site. Neither plan file includes traffic study information. Both lots that comprise this site were platted pursuant to preliminary plans that included a finding of transportation adequacy. The expansion is occurring on lots that were recorded without a trip cap or other similar restriction.

The off-site transportation impact of the expansion is not an issue in the detailed site plan process.” The assessment and methodology for analyzing the trip cap used in the previously approved DSPs is acceptable and suitable for the current development proposal. Recommendations related to vehicular circulation were included to address potential stacking along Mercantile Lane and provide additional stop controls at the main entrance and drop off location. Conditions related to these improvements have been included herein. From the standpoint of transportation, this plan is acceptable, subject to conditions related to site circulation.

- d. **Subdivision**—In a memorandum dated April 1, 2022 (Heath to Bishop), the Subdivision Section provided an analysis of the subject DSP’s conformance with the prior approvals, as included in Finding 10 above, and noted that the DSP is in substantial conformance with the approved PPSs. There are no subdivision issues at this time.
- e. **Permits**—In a memorandum dated March 28, 2022 (Bartlett to Bishop), one comment was provided, which has been included as a condition herein.
- f. **Environmental Planning**—In a memorandum received March 28, 2022 (Rea to Bishop), the Environmental Planning Section offered the following summarized comments:

The site has a Natural Resources Inventory Equivalency Letter, NRI-154-2018-02, which was issued on June 17, 2021. The site is currently developed with a medical building. A small woodland area is located along the southeast corner of the property. There are no regulated environmental features located on this site.

The site has an approved SWM Concept Plan, 44959-2021, that is in conformance with the current code and is valid until February 8, 2025. The approved plan proposes a combination of micro-bioretenion facilities and a green roof over the proposed Advanced Urgent Care addition to treat on-site stormwater. The approved concept plan is consistent with the DSP.

The Environmental Planning Section recommends approval of DSP-95073-06, with one condition, which has been included herein.

- g. **Prince George’s County Fire/EMS Department**—In an email dated February 22, 2022 (Reilly to Bishop), the Fire and EMS Department indicated that they have no comments on this DSP.
- h. **Prince George’s County Department of Permitting, Inspections and Enforcement (DPIE)**—In a memorandum dated March 8, 2022 (Giles to Bishop), DPIE noted that the DSP is consistent with the approved SWM concept plan, and they have no objections to the application.
- i. **Prince George’s County Police Department**—At the time this staff report was written, the Police Department did not provide comments on the subject application.

- j. **Prince George’s County Health Department**—In a memorandum dated February 28, 2022 (Adepoju to Bishop), the Health Department provided a desktop health impact assessment of the application and provided comments related to dust and noise requirements during construction. Conditions related to these requirements have been included herein.
 - k. **Maryland State Highway Administration (SHA)**—In an email dated February 28, 2022 (Woodroffe to Bishop), SHA indicated that they had no comments on the subject application.
 - l. **Washington Suburban Sanitary Commission (WSSC)**—At the time this staff report was written, WSSC did not provide comments on the subject application.
 - m. **Largo Development Review Committee**—At the time this staff report was written, the Largo Development Review Committee did not provide comments on the subject application.
16. Based on the foregoing, and as required by Section 27-285(b)(1) of the Zoning Ordinance, the DSP, as revised, in accordance with the conditions of this approval, represents a reasonable alternative for satisfying the site design guidelines of Subtitle 27, Part 3, Division 9, of the County Code without requiring unreasonable cost and without detracting substantially from the utility of the proposed development for its intended use.
17. As required by Section 27-285(b)(4) of the Zoning Ordinance, the Planning Board must also find that the regulated environmental features on a site have been preserved and/or restored in a natural state to the fullest extent possible, in accordance with the requirements of Subtitle 24-130(b)(5) of the prior Prince George’s County Subdivision Regulations. No regulated environmental features are located within the limits of the current application.

RECOMMENDATION

Based upon the foregoing evaluation and analysis, the Urban Design Section recommends that the Planning Board adopt the findings of this report and APPROVE this application, as follows:

- A. APPROVAL of the following alternative Development District Overlay Zone standards:
 - 1. **Urban Design Criteria Block Length** (page 133)—To allow a maximum block length of 675 feet.
 - 2. **Urban Design Criteria Build to Line** (page 135)—To allow the proposed garage addition to be constructed within 45 feet of the Pedestrian Zone.
 - 3. **Urban Design Criteria Frontage** (page 138)—To allow a minimum building frontage of 45 percent.
 - 4. **Urban Design Criteria Building Heights** (page 141)—To allow the proposed garage expansion to maintain the existing clear height.
 - 5. **Architectural Design Criteria Fenestration** (page 159)—To allow the fenestration of the proposed garage expansion to be consistent with the fenestration of the existing parking garage.

6. **Architectural Design Criteria Fenestration** (page 170)—To allow the signage on Mercantile Lane to exceed the allowed sign square footage by 274 square feet for the installation of a graphic panel design on the western façade of the parking garage facing Mercantile Lane.
- B. APPROVAL of Detailed Site Plan DSP-95073-06, Kaiser Permanente Largo Medical Center, subject to the following condition:
1. Prior to certificate approval of the detailed site plan (DSP), the applicant shall submit the following documentation or revise the plans, as follows:
 - a. Modify the northbound approach lane use configuration along Mercantile Lane to include a single left-through and single right-through lane configuration. The exact details shall be accepted by the Transportation Planning Section.
 - b. Add additional stop controls at the eastbound approach and outbound lane of the pickup/drop-off area internal to the site at Mercantile Lane. The exact details shall be accepted by the Transportation Planning Section.
 - c. Add signage indicating temporary parking along the pickup/drop-off area. The exact location and sign profiles shall be accepted by the Transportation Planning Section.
 - d. Revise the Woodland Conservation Exemption so that the plan submitted for exemption matches the DSP.
 - e. Add site plan notes to the DSP as follows:

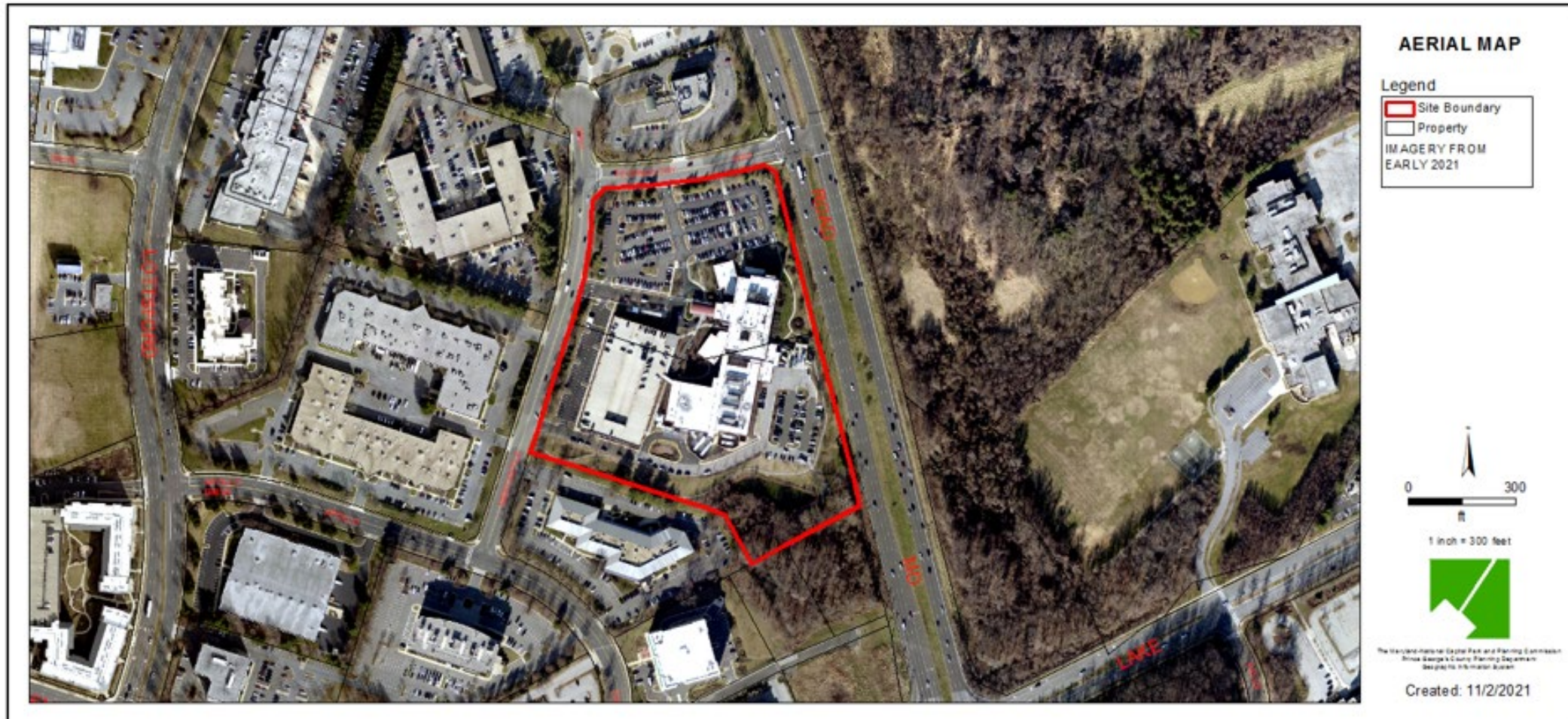
“During the construction phases of this project, noise should not be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements, as specified in Subtitle 19 of the Prince George’s County Code.”

“During the construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements, as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.”
 - f. Provide labels and dimensions for the loading spaces on the site plan.
 - g. Clearly label the building on the site plan as proposed or existing and include the square footage, dimensions, height, and setbacks.

KAISER PERMANENTE LARGO

Detailed Site Plan Amendment

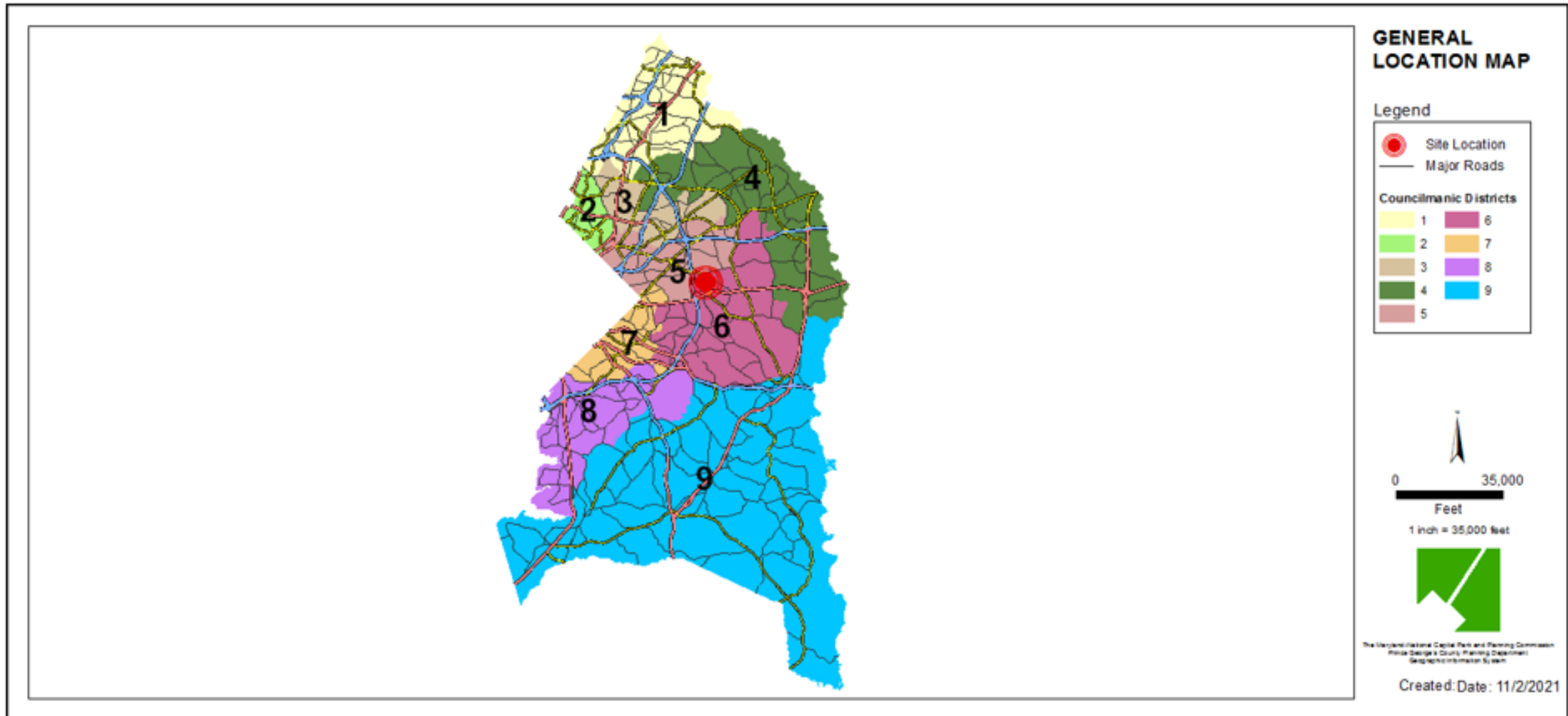
Staff Recommendation: APPROVAL with conditions



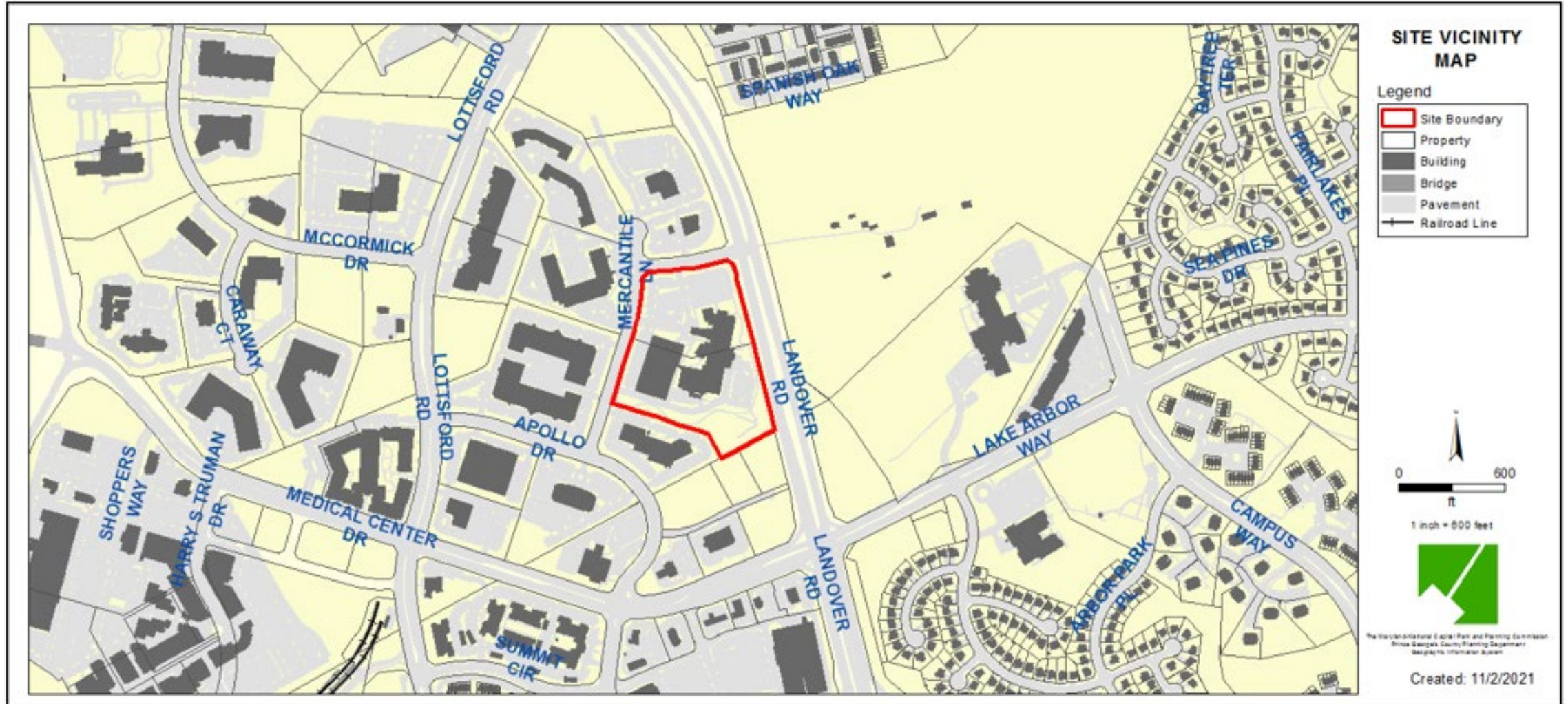
GENERAL LOCATION MAP

Council District: 06

Planning Area: 73

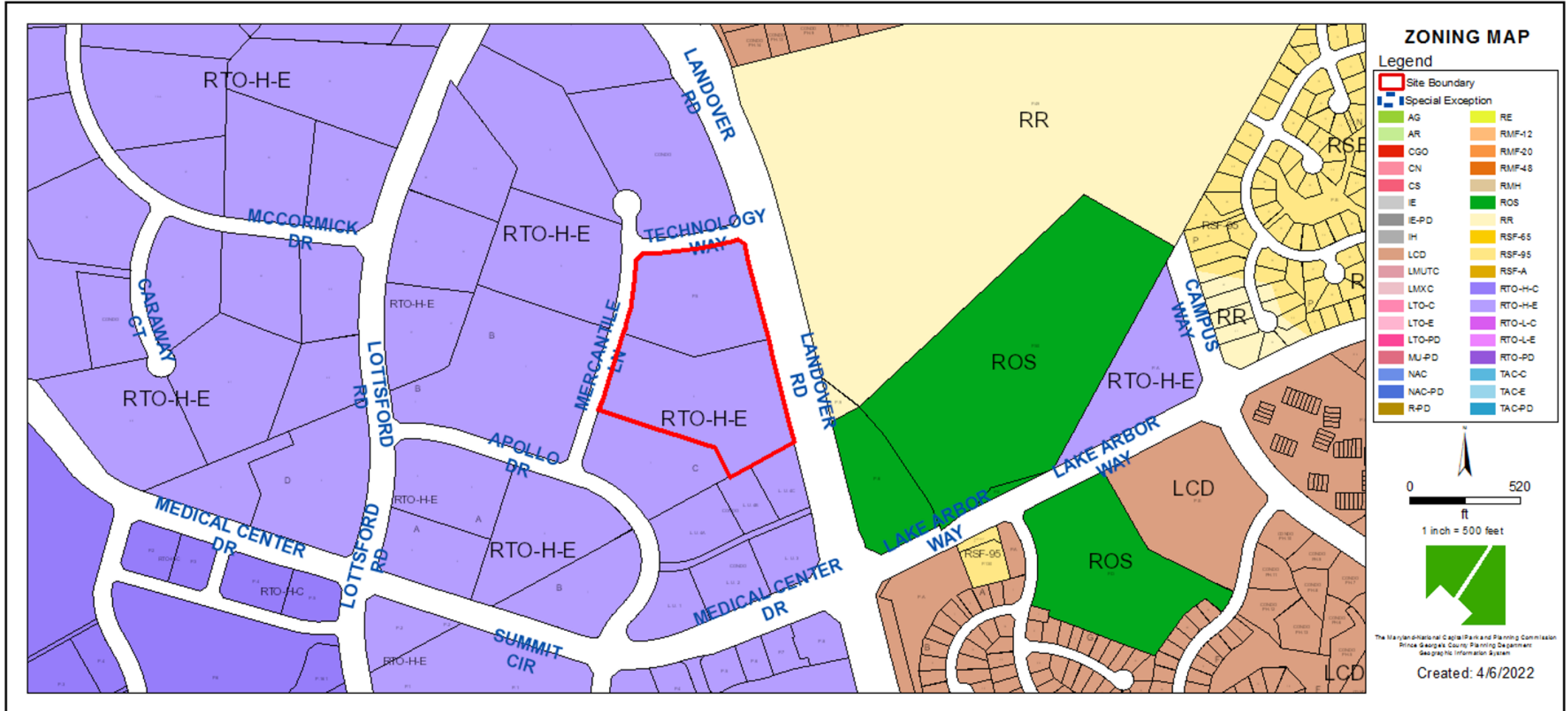


SITE VICINITY MAP



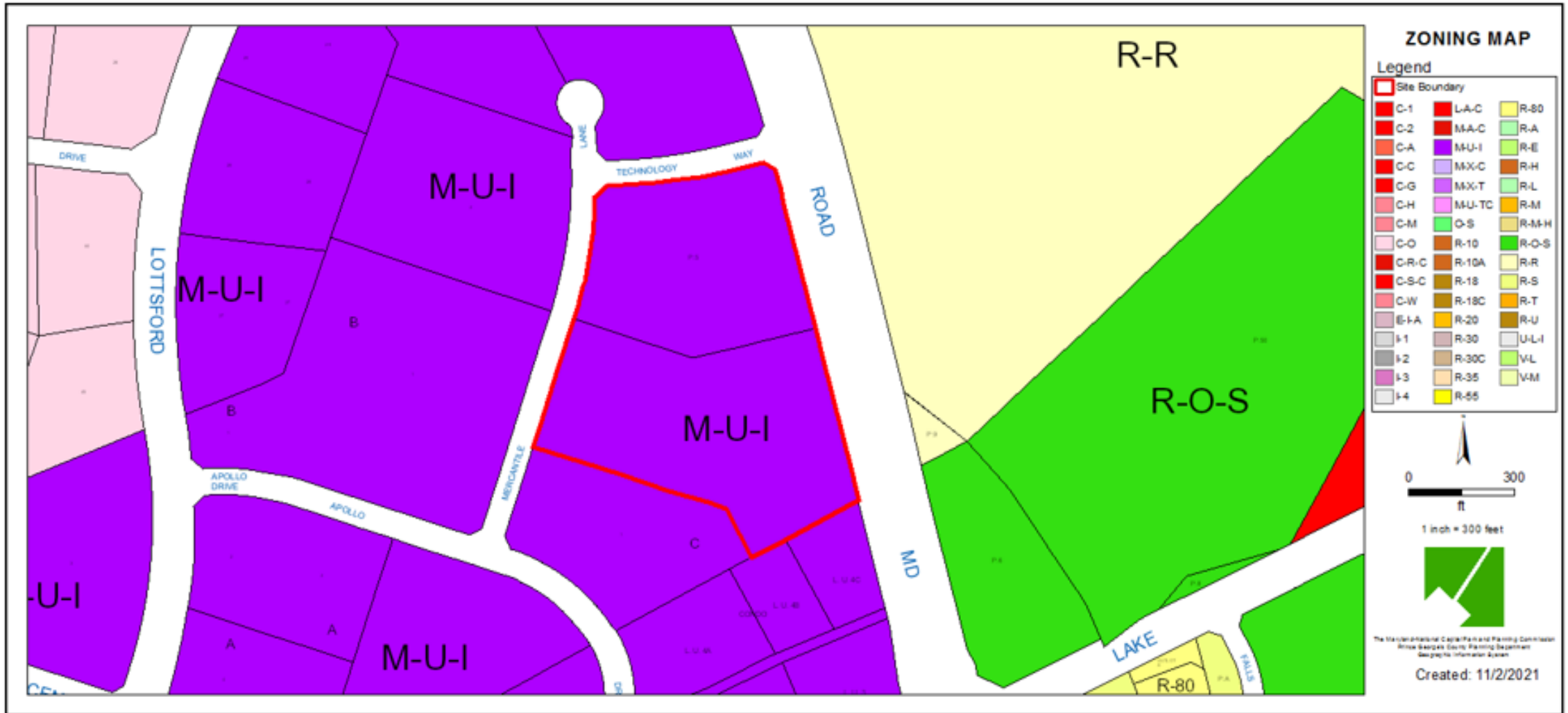
NEW ZONING MAP

Property Zone: RTO-H-E

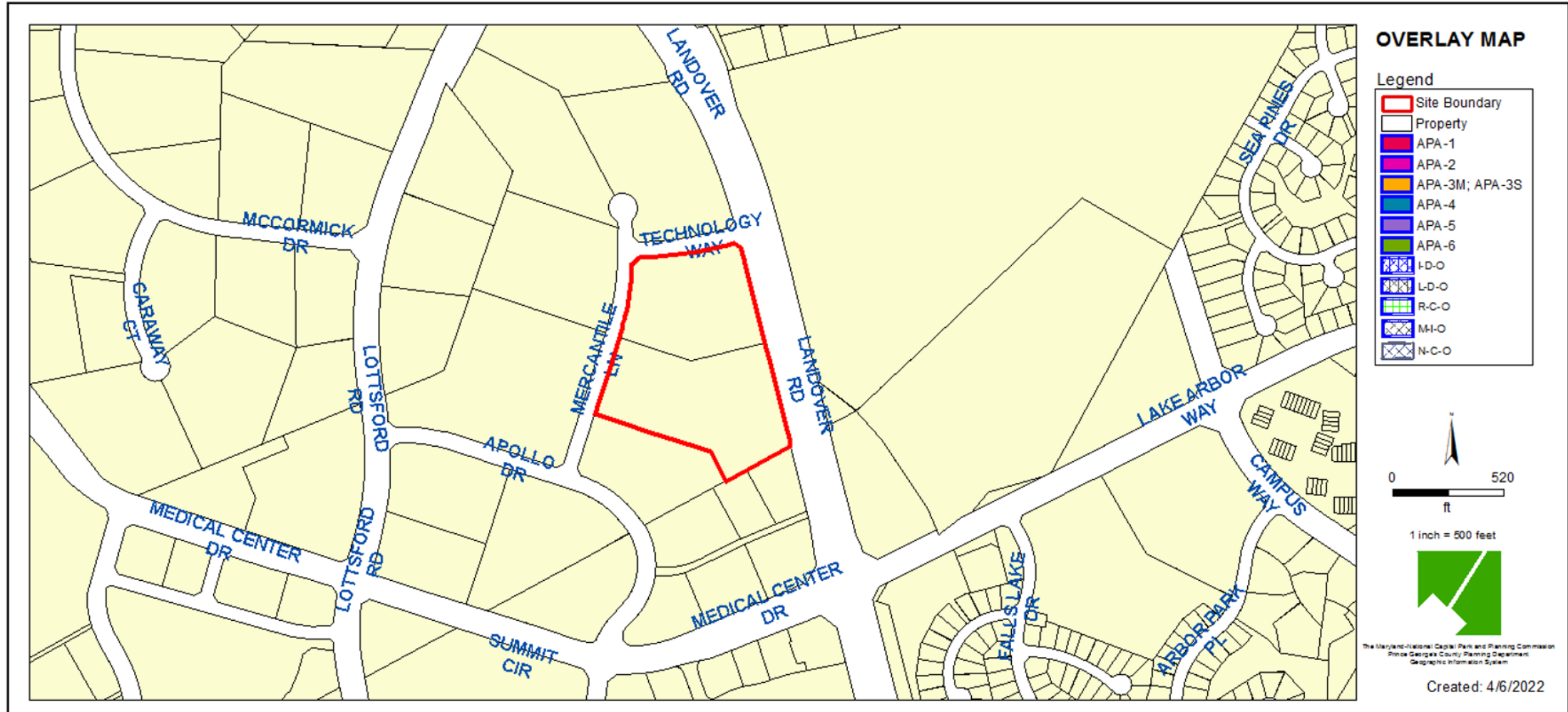


PRIOR ZONING MAP

Property Zone: M-U-I/D-D-O



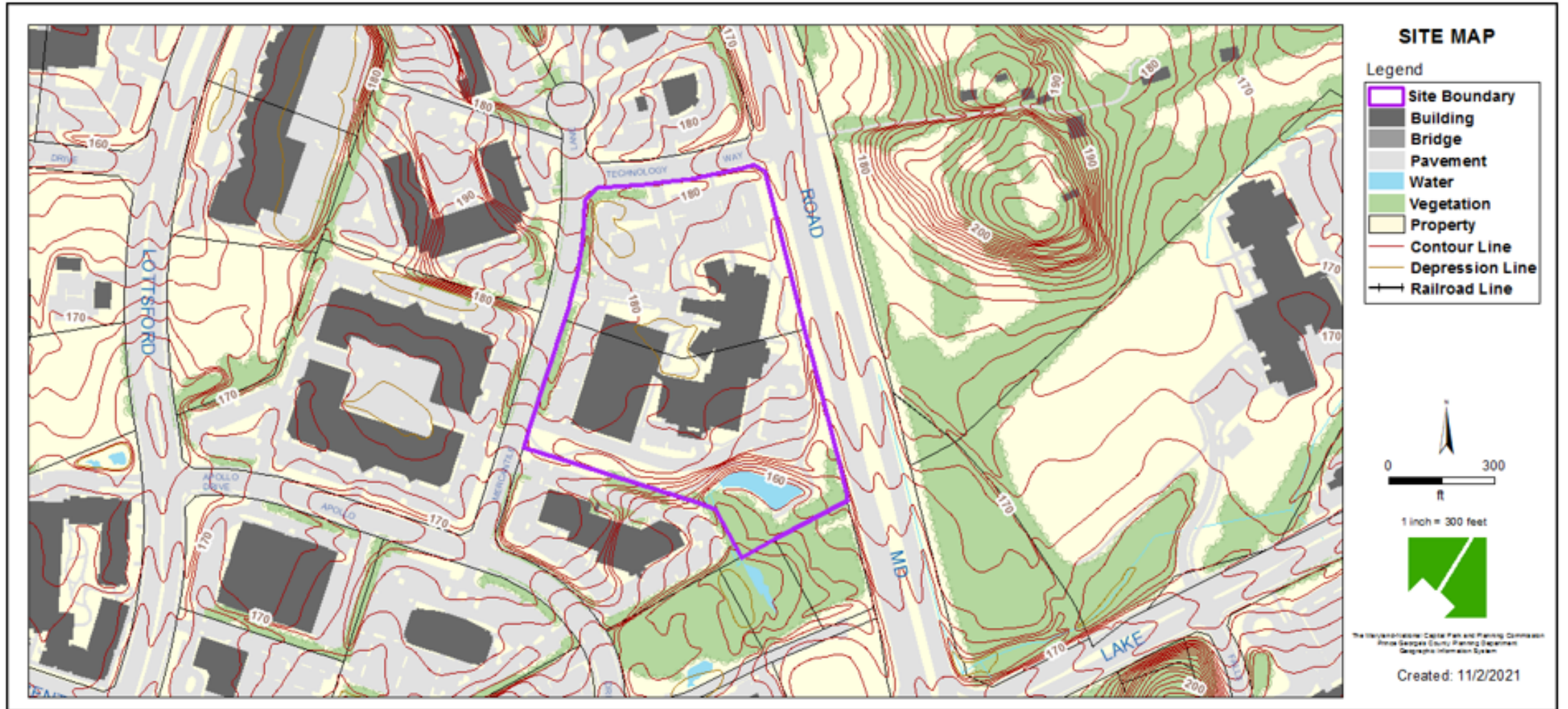
OVERLAY MAP



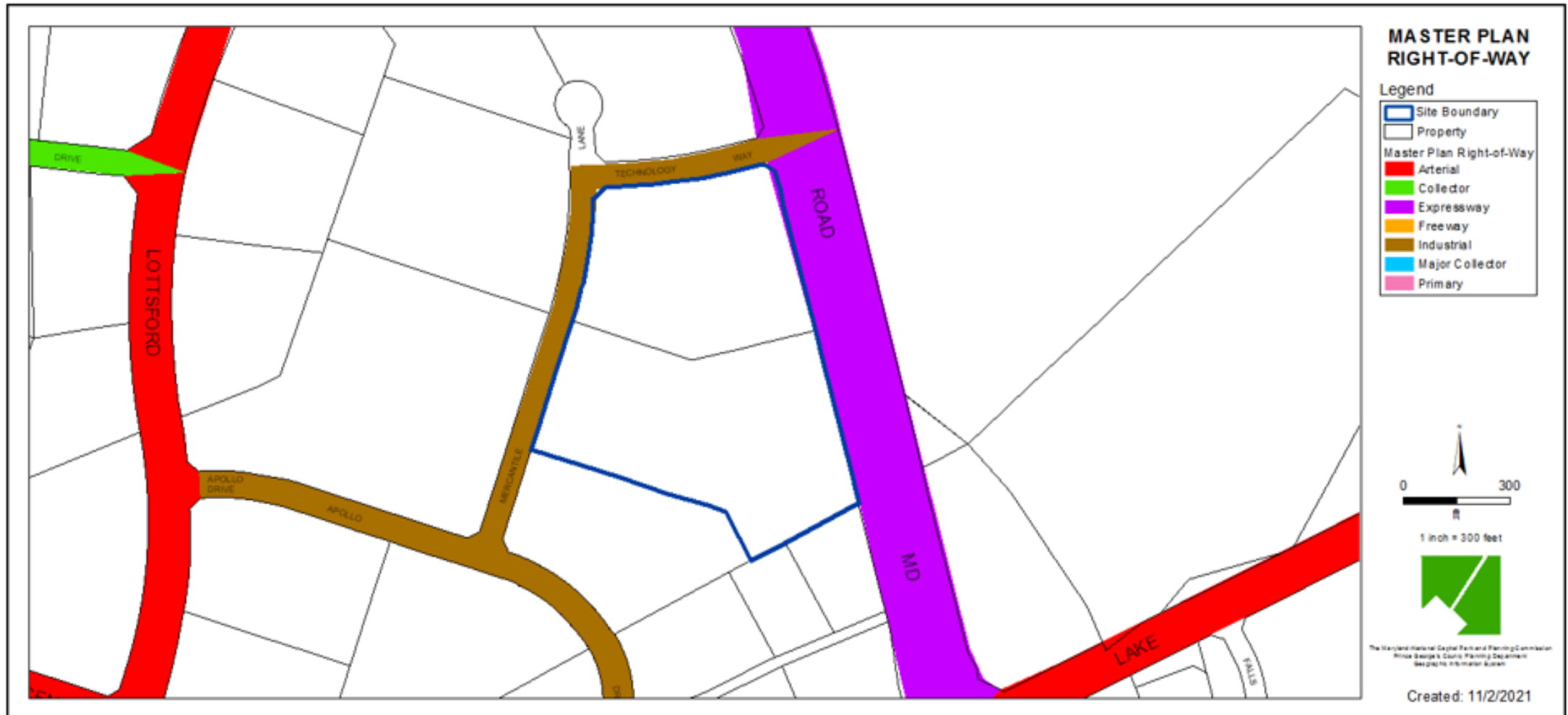
AERIAL MAP



SITE MAP



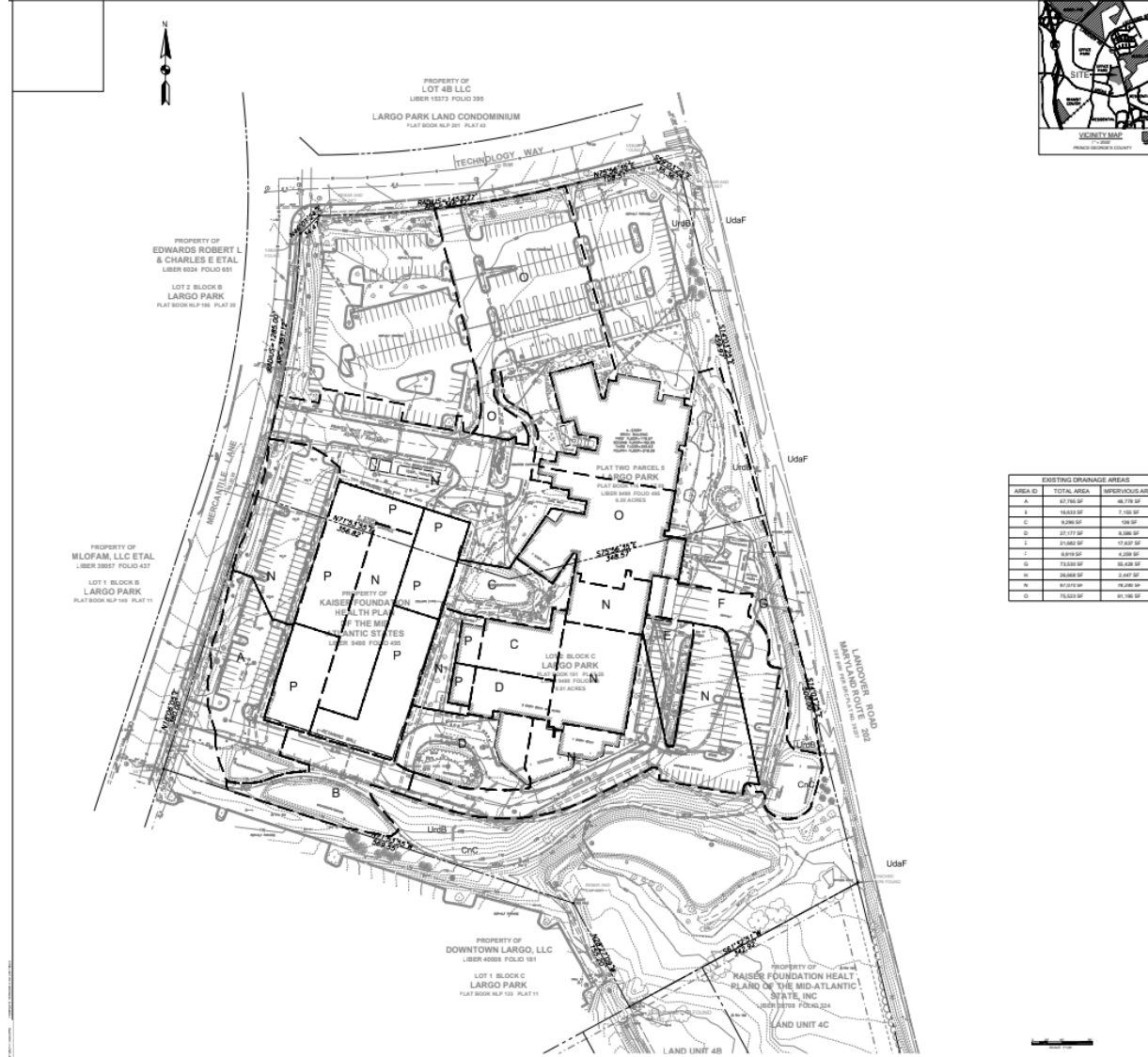
MASTER PLAN RIGHT-OF-WAY MAP



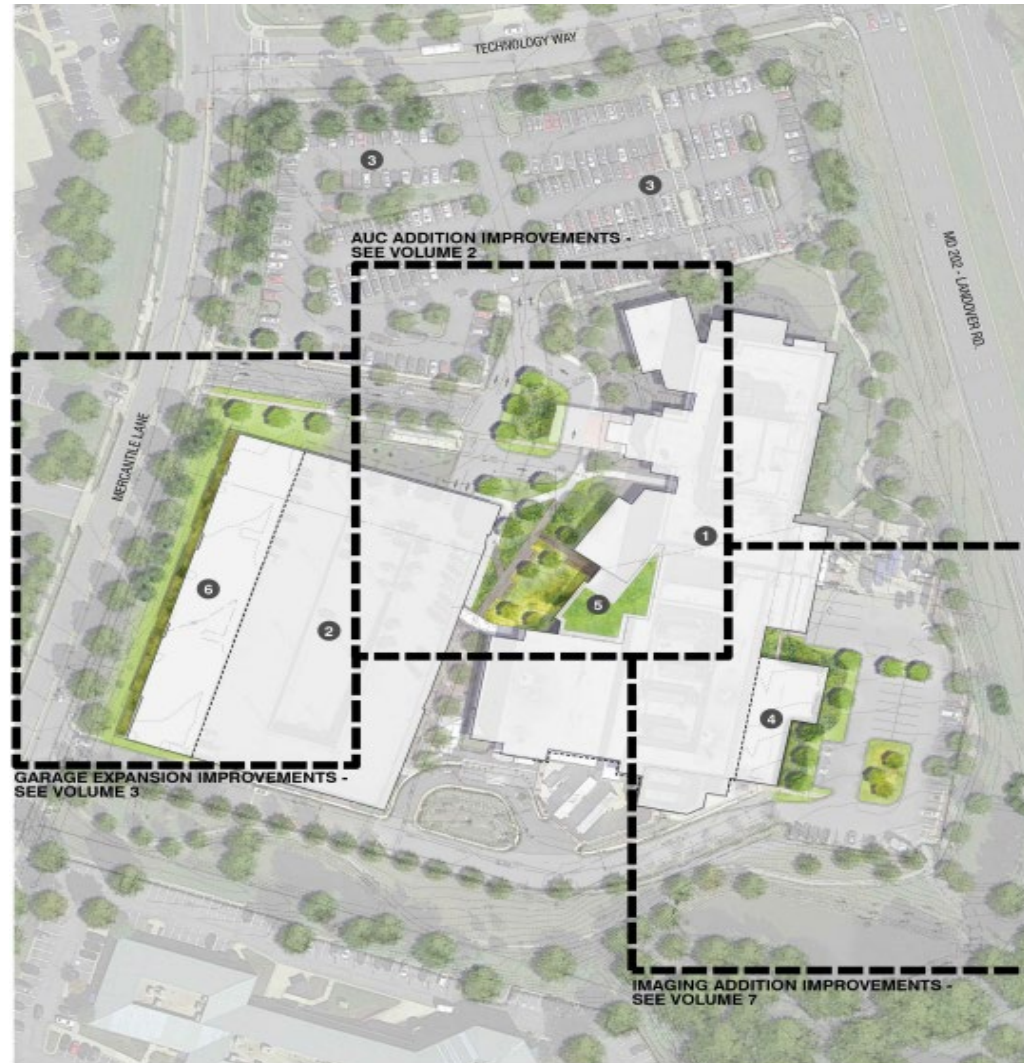
BIRD'S-EYE VIEW



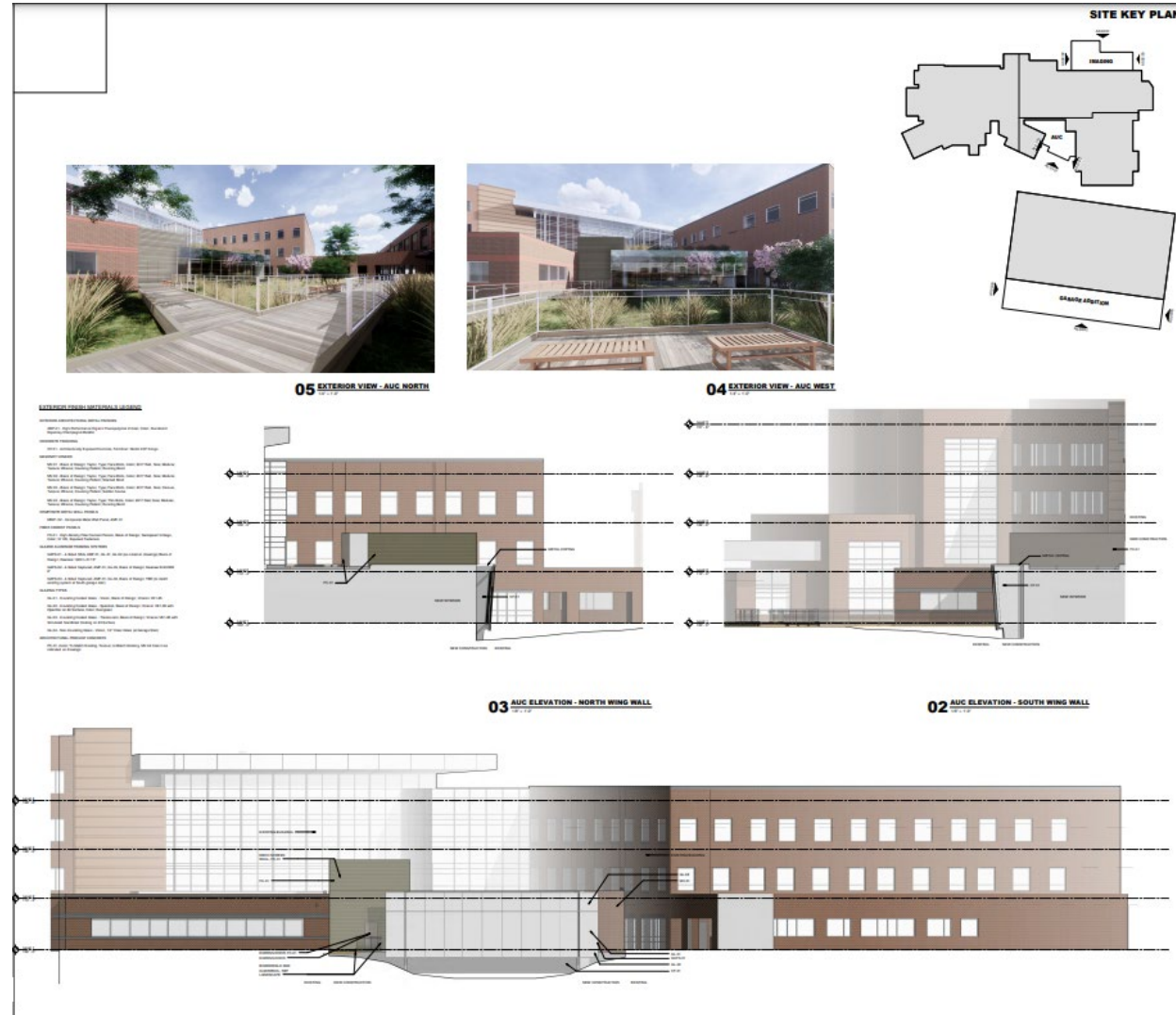
SITE PLAN



OVERALL SITE PLAN



EXTERIOR ELEVATIONS



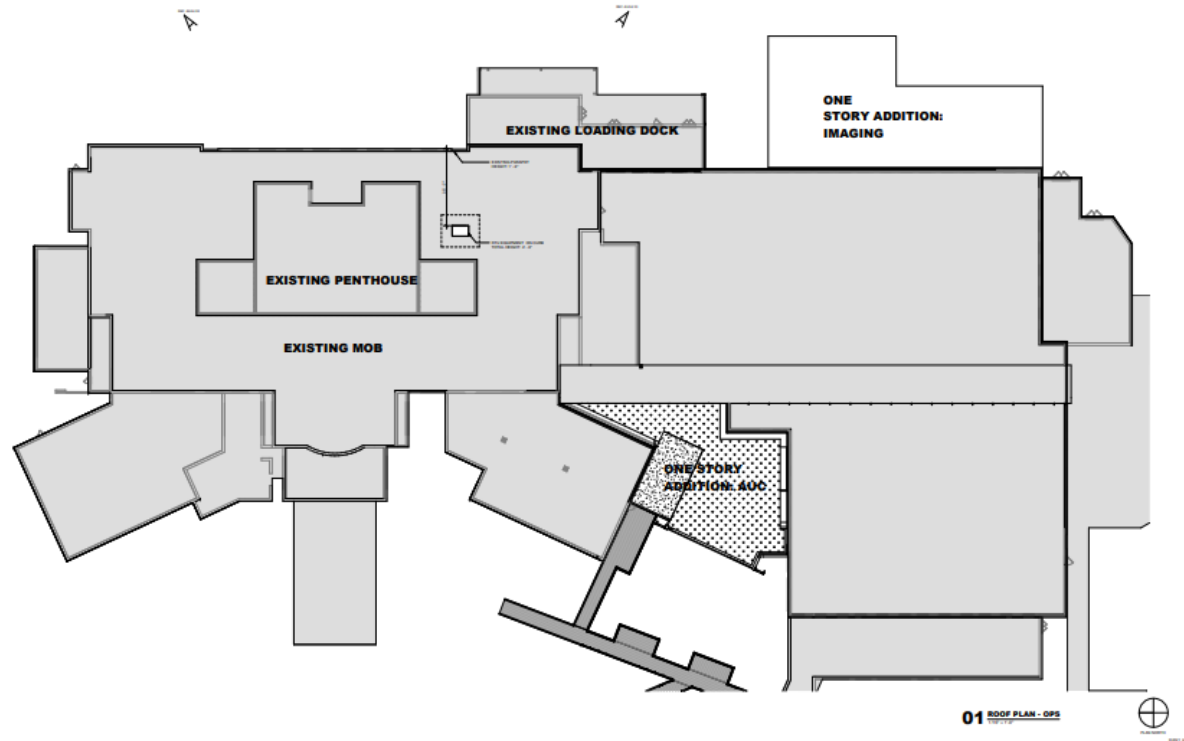
EXTERIOR ELEVATIONS



03 EXTERIOR VIEW - EXISTING EAST FACADE 03



02 EXTERIOR VIEW - EXISTING EAST FACADE 01



01 ROOF PLAN - GPS

SIGNAGE DETAILS

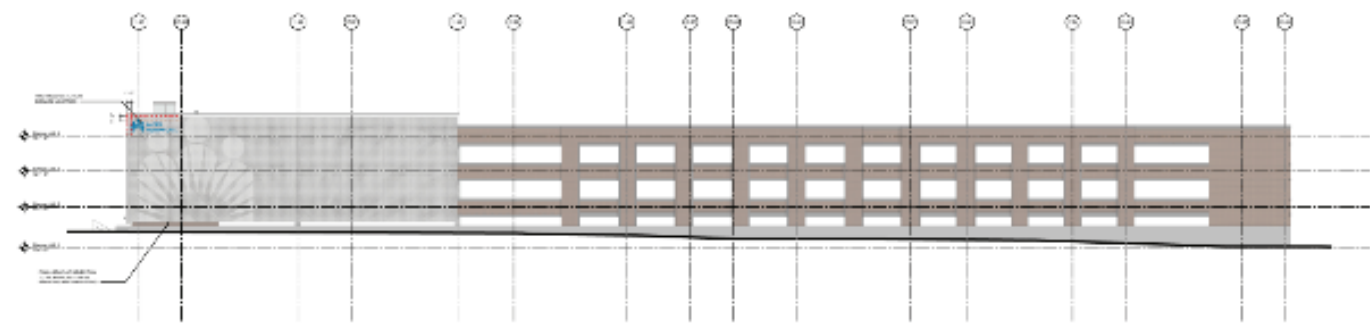


Elevation Detail
 Scale: 2"=1'-0"

NEW SIGNAGE NOTES:
 PERCENTAGE OF COMMERCIAL FRONTAGE: 4.12%
 POWER NEEDED: 120V/277V

COLORS:

Color Name	Color	Application	Material	Finish	Notes
Blue	Blue	Logo and Text	Aluminum	Paint	See Spec. 2
White	White	Background	Aluminum	Paint	See Spec. 2
Grey	Grey	Structural	Aluminum	Paint	See Spec. 2



West Elevation
 Scale: 3/32"=1'-0"

STAFF RECOMMENDATION

APPROVAL with conditions

- ONE Condition with Sub conditions

Minor Issues:

- Technical corrections to DSP plans

Applicant Community Engagement:

- Informational Mailing: November 4, 2021
- Acceptance Mailing: February 10, 2022

Back-up

For

**Detailed Site Plan DSP-95073-06
Kaiser Permanente Largo Medical
Center**

CERTIFICATE OF APPROVAL

LARGO PARK REVISED CONCEPT PLAN
SP-87168/01

This revision to a Detailed Site Plan was approved on August 3, 1989, by the Prince George's County Planning Board in accordance with Subtitle 27, Part 3, Division 9, of the Prince George's County Code. The official decision of this case is embodied in Prince George's County Planning Board Resolution No. 89-420, which contains the conditions listed below:

1. A minimum 30-foot landscaped yard shall be provided along the property line adjacent to MD 202 (as required by I-3 Zone regulations of the Zoning Ordinance, Subtitle 27).
2. A minimum 30-foot landscaped yard shall be provided along the line adjacent to Arena Drive (as required by I-3 Zone standards).
3. A minimum 10-foot landscaped yard shall be provided along Apollo Drive (reduced from 30 feet per I-3 Zone standards).
4. A reduction of the requirement for a yard adjoining non-residential property from 20 feet to 10 feet, shall be granted, as set forth in Section 27-474(b). Except as to Conditions 3 and 4, all other standards for building and parking setbacks, yards, building coverage and green area, and amount of parking in main entrance yard that are set forth in Subtitle 27 must be complied with at Detailed Site Plan.
5. Access to Lots 1 and 2 shall be limited to Apollo Drive.
6. Individual trees on site that are worthy of saving will be flagged by staff from the Natural Resources Division and the Development Review Division.
7. Every effort will be made to incorporate these trees into the overall building and parking lot layout. This could mean the reconfiguration of the building and parking lot to accomplish this goal. The possibility that drastic overall site grading will be necessary for drainage of surface waters will be carefully reviewed at Detailed Site Plan and would eliminate the tree-save requirement.

8. The exact location of the 8-foot biker trail shall be determined at Detailed Site Plan. It may be either inside or outside of the public right-of-way for Arena Drive. The 8-foot wide hard-surfaced trail shall be constructed by applicant along the property's entire frontage on Arena Drive.

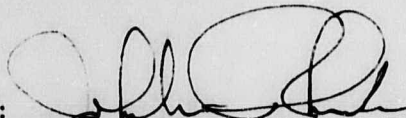
This approval includes:

1 Conceptual Site Plan

Any departure from this plan shall be resubmitted to the Planning Board for approval.

This Conceptual Site Plan is valid indefinitely.

Signed:



John W. Rhoads
Chairman
Prince George's County Planning Board



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772

PGCPB No. 86-297

File No. 4-86107

R E S O L U T I O N

WHEREAS, Robert L. Edwards, et al., are the owners of a 47.1425-acre parcel of land known as Largo Park (Lots 1-3, Block A; Lots 1-3, Block B; Lots 1 & 2, Block C; Lots 1-3, Block D), said property being in the 13th Election District of Prince George's County, Maryland, and being zoned I-3; and

WHEREAS, on June 9, 1986, The Knollwood Development Corporation filed an application for approval of a Preliminary Subdivision Plat (Staff Exhibit #1) for 11 lots; and

WHEREAS, the application for approval of the aforesaid Preliminary Subdivision Plat, also known as Preliminary Plat 4-86107 was presented to the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission by the staff of the Commission on July 24, 1986, for its review and action in accordance with Article 28, Section 7-116, Annotated Code of Maryland and the Regulations for the Subdivision of Land, Subtitle 24, Prince George's County Code; and

WHEREAS, the staff of The Maryland-National Capital Park and Planning Commission recommended APPROVAL of the application with modifications; and

WHEREAS, on July 24, 1986, the Prince George's County Planning Board heard testimony and received evidence submitted for the record on the aforesaid application.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to the provisions of Subtitle 24, Prince George's County Code, the Prince George's County Planning Board approved Preliminary Plat of Subdivision 4-86107 with the following modifications:

1. Approval of a conceptual stormwater management plan by the Washington Suburban Sanitary Commission prior to Final Plat of Subdivision.
2. Denial of access on Arena Drive.

BE IT FURTHER RESOLVED, that the findings and reasons for the decision of the Prince George's County Planning Board are as follows:

1. The subdivision, as modified, meets the legal requirements of Subtitle 24 of the Prince George's County Code and of Article 28, Annotated Code of Maryland.
2. Stormwater management is required for this site.
3. For the purpose of public health, safety and welfare, ingress/egress to lots along Arena Drive should be denied.

* * * * *

This is to certify that the foregoing is a true and correct copy of a resolution adopted by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the motion of Commissioner Keller, seconded by Commissioner Yewell, with Commissioners Keller, Yewell and Dabney voting in favor of the motion, with Commissioner Rhoads temporarily absent and with Commissioner Botts absent at its regular meeting held on Thursday, July 24, 1986, in Upper Marlboro, Maryland.

Thomas H. Countee, Jr.
Executive Director

APPROVED AS TO LEGAL SUFFICIENCY


M-NCPPC Legal Department

Date 2/23/89


By Robert D. Reed
Public Affairs Officer

THC:RDR:ASH:lg



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TDD: (301) 952-3796

PGCPB No. 95-417

File No. SP-95073

RESOLUTION

WHEREAS, the Prince George's County Planning Board is charged with the approval of Detailed Site Plans pursuant to Part 3, Division 9 of the Zoning Ordinance of the Prince George's County Code; and

WHEREAS, in consideration of evidence presented at a public hearing on December 2, 1995, regarding Detailed Site Plan SP-95073 for Kaiser Permanente, the Planning Board finds:

1. The Detailed Site Plan is in conformance with the conditions of the revised Conceptual Site Plan, SP-87168/01, particularly with regards to on-site tree preservation along the southern boundary. Conditions 6 and 7 require that every effort shall be made to incorporate individual trees into the overall layout of the plan. The original submittal proposed tree removal along the southern boundary to accommodate a stormwater management pond. The tree removal was greater than 5,000 SF and would have required conformance with the Woodland Conservation Ordinance. However, the pond was reconfigured so that no tree removal was necessary, exempting the Detailed Site Plan from the requirements of the Woodland Ordinance and satisfying the requirements of the conditions of the Conceptual Site Plan.
2. The Detailed Site Plan is in conformance with Preliminary Plat of Subdivision 4-86107 and applicable conditions of approval.
3. The Detailed Site Plan generally meets the requirements of the Zoning Ordinance for development in the I-3 Zone. Section 27-471.(f)(2) of the Zoning Ordinance permits not more than 25 percent of any parking lot to be located in the yard to which the building's main entrance is located, except that the Planning Board may approve up to an additional 15 percent in its discretion if increased parking better serves the efficiency of the particular use; improves views from major arteries or interstate highways; and makes better use of existing topography or compliments the architectural design of the building. The building in this case is four stories and consists of 127,000 SF. The main entrance to the building faces away from MD Route 202 and towards Mercantile Lane, an interior street to the development. By placing the building closer to MD Route 202, parking has been minimized along the highway and views to the site have been improved. Also, the architecture of the building is distinctive and will be a focal point from the highway as it is sited. The additional parking in the front of the building is justified because of the need for users of the building to find and park within view of the building's main entrance. The total number of required parking spaces is 635. The parking is provided in four major compounds. Two of the compounds located in front of the building's main entrance consist of 228 parking spaces or

36 percent of the total parking. Also, a 0.5-acre area of green space in the front yard could hold up to 26 additional parking spaces in the future, bringing the total parking in the front yard to 254 spaces or 40 percent. The Urban Design Staff supports the additional 15% parking in the front of the building's main entrance.

4. Site Data

Zone	I-3
Total Site Area	14.7 acres
Maximum Building Coverage	45%
Building Coverage Provided	7%
Green Area Required (25%)	3.68 acres
Green Area Provided (51.4%)	7.56 acres
Parking Required: (1 SP. Per each 200 SF.)	
Medical Offices = 116,600 SF	583 spaces
Outpatient Facilities = 10,400 SF	52 spaces
Handicapped Spaces Required:	
Medical Offices = 2% x 583 SP	12 spaces
Outpatient Facilities = 10% x 52 SP	6 spaces
Total	18 spaces
Parking Provided:	
Standard Spaces	476 spaces
Compact Spaces	141 spaces
Handicapped Spaces (Regular)	14 spaces
Van Spaces	4 spaces
Total Spaces Provided	635 spaces

5. The Detailed Site Plan represents a reasonable and workable resolution of the Site Design Guidelines of Subtitle 27, Part 3, Division 9, of the Prince George's County Code. The proposed architecture for the site will consist of a very attractive, well landscaped four-story signature building that is prominently sited and will be a focal point along Landover Road. The building will be constructed predominantly of brick. The dominant brick color used on the main building mass will be "red ironspot" with a buff colored brick used for projected, horizontal banding on the flanking wings and stairwells. Concrete banding is also used for coping and to separate the larger building masses. The architecture presents a very well balanced, unified appearance with the use of a variety of building materials and styles. In addition, the Detailed Site Plan has included proposed signage and lighting for the project. The proposed signage consists of various ground and building mounted signage. At the intersection of Technology Way and Landover Road, a new, "V"-shaped, lighted ground sign will be placed in front of an existing Largo Park pylon sign that is approximately 15 feet high and at a 45 degree angle to Landover Road. The new sign consists of two 7'-6" high by 14'-3" long internally illuminated signs joined together at 45 degrees to the road. The actual sign face is 5'-0" high by 12'-6" wide. The sign will be construct-

ed of a champagne colored, anodized aluminum face with dark bronze reveals. The logo and letters will be cut out of the aluminum and backed up with white plexiglass with blue film applied on the surface, leaving a white outline around the logo and letters. Two smaller ground signs, each with a 3'-6" x 9'-0" face, are proposed at the entrances on Mercantile Lane and Technology Way. They are both internally lit and constructed of the same materials. Non-lit, building mounted signs are proposed on the stair walls of the side elevations and on the canopy at the main entrance. The number of signs and their locations are appropriate for the Medical Center.

A variety of lighting is proposed for the project. Lighting for the main drive-aisles and front drop-off areas consist of a very attractive, stylized fixture with a 12-foot high pole and 175 watt, high pressure sodium bulb. Lighting for the parking areas consist of 30-foot high, 400 watt, high pressure sodium fixtures. Various ground and building mounted flood and spot lights are also proposed.

6. The Detailed Site Plan is in general conformance with the requirements of the *Landscape Manual*. Although a low berm has been provided along Landover Road, staff believes that the loading area will be visible from the northbound lanes. In order to provide an effective screen, additional evergreen plantings should be provided closer to the loading area. Plant materials have been provided on the berm along Landover Road, however a landscape schedule for Section 4.3, Commercial Landscape Strip, should be provided on the Landscape Plan.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to Subtitle 27 of the Prince George's County Code, the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission adopted the findings contained herein and APPROVED the Detailed Site Plan for the above-described land, subject to the following conditions:

1. Prior to certificate approval, the following modifications or revisions shall be made to the Detailed Site Plan:
 - a. Additional evergreen trees shall be provided in order to screen the loading spaces from Landover Road, subject to the review and approval of the Urban Design Section.
 - b. A landscape schedule for the commercial landscape strip along Landover Road shall be provided on the Landscape Plan.

* * * * *

This is to certify that the foregoing is a true and correct copy of the action taken by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the motion of Commissioner McNeill, seconded by Commissioner Brown, with

PGCPB No. 95-417
File No. SP-95073
Page 4

Commissioners McNeil, Brown and Hewlett voting in favor of the motion, and with Commissioners Boone and Dabney absent at its regular meeting held on Thursday, December 21, 1995, in Upper Marlboro, Maryland.

Adopted by the Prince George's County Planning Board this 21st day of December 1995.

Trudye Morgan Johnson
Executive Director

By *Frances J. Guertin*
Frances J. Guertin
Planning Board Administrator

TMJ:FJG:GW:aj

APPROVED AS TO LEGAL SUFFICIENCY.

JH Mark
M-NCPPC Legal Department
Date 12/22/95

RESOLUTION

WHEREAS, the Prince George's County Planning Board is charged with the approval of Detailed Site Plans pursuant to Part 3, Division 9 of the Zoning Ordinance of the Prince George's County Code; and

WHEREAS, in consideration of evidence presented at a public hearing on June 24, 2010, regarding Detailed Site Plan DSP-95073/01 for Kaiser Permanente, Largo, the Planning Board finds:

1. **Request:** The proposed detailed site plan (DSP) amendment is for a 106,700-square-foot, three-story addition to the existing 129,500-square-foot, four-story medical practitioners' office use on the property and construction of a new 713-space, four-level parking garage, with associated revisions to the existing surface parking and landscaping. This expansion will allow for improved services for Kaiser Permanente members and will include the following: expansion of certain departments including cardiology, orthopedics, podiatry, and pulmonary; improvements to such ancillary services as urgent care, medical imaging, radiology, outpatient surgery, and recovery rooms; and such indirect support services as staff facilities and housekeeping areas.

2. **Development Data Summary:**

	Existing	Approved
Zone	I-3	I-3
Total Site Area	14.7 acres	14.7 acres
Total Building Gross Floor Area	129,500 sq. ft.	236,200 sq. ft. 245,200 sq. ft. (Garage)
Building Coverage (45% maximum)	7%	22%
Green Area (25% required)	51.4% (7.56 acres)	43% (6.35 acres)
Impervious Area	7.32 acres	8.36 acres

Parking Required: (1 space per each 200 sq. ft.)

	Square Footage	Parking Spaces
Existing Structure	129,500	648
Additional Facilities	106,700	533
Total	236,200	1,181

Handicapped Spaces Required:

Site with over 1,000 required spaces	20 ADA
(plus 1 space for each 100 over 1,000)	22 spaces
Van-Accessible Spaces (1 of 4 ADA spaces)	6 spaces

Parking Provided:

Standard Spaces	798
Compact Spaces	363
Handicapped Spaces (Regular)	16
Van Spaces (1 of 4 ADA spaces)	6
Total Spaces Provided	1,183

Loading Spaces Required:	3
Loading Spaces Provided:	3

3. **Location:** The property is located at 1221 Mercantile Lane in Largo, situated on the west side of Landover Road (MD 202) at the southwest corner of its intersection with Technology Way.
4. **Surrounding Uses:** The subject property is located in Largo Park, which is bounded on the east by Landover Road (MD 202), on the west by Lottsford Road, and on the south by Arena Drive. The entirety of Largo Park is also zoned I-3 and is similarly developed with a range of general office and medical practitioners' office uses in an employment park-like setting with low-rise office buildings typically situated near the center of properties and surrounded by surface parking.
5. **Previous Approvals:** The case history of this site plan precedes much of the more recent development in the Largo area. The site plan indicates that the property is Parcel 5 (Plat Book NLP 116 @ 89) and Lot 2 (Plat Book NLP 151 @ 28), located on Tax Map 67 in Grid F-1, and is 14.71 acres in the I-3 Zone. The northern portion of the property, Parcel 5, was previously subdivided through Preliminary Plan of Subdivision 4-79155. Lot 2 is the subject of Preliminary Plan 4-86107. The resolution of approval for Preliminary Plan 4-86107 was adopted by the Planning Board on July 24, 1986 (PGCPB Resolution No. 86-297).

A Detailed Site Plan (DSP-95073) was approved by the Planning Board on December 2, 1995 for a 127,000-square-foot medical center and 635 parking spaces in the I-3 Zone. The applicant is now proposing to construct a 106,700-square-foot building addition to include an expansion of medical services. There is also a 245,200-square-foot parking garage in the subject proposal. Per conversations with the applicant, the Kaiser Permanente expansion will provide an additional 40 to 60 new employment opportunities in Prince George's County.

6. **Design Features:** The existing building on the property is situated on the eastern side of the property closest to Landover Road (MD 202). There is no existing or proposed ingress or egress from Landover Road. The existing building is constructed primarily of two tones of brick, and the proposed addition, which will extend the existing building southward, utilizes the two existing brick colors as well as a significant glass-like façade feature (aluminum curtain wall).

The site plan proposes a parking garage which will similarly utilize one of the existing brick colors as well as a glass-like façade (aluminum curtain wall) along the stairwells, with significant cutout openings to allow for architectural interest, light, and air circulation. The cutouts will also feature planters that will play a role in the stormwater management system and will be planted with appropriate vegetation.

The proposed building addition and the parking garage will remove a significant portion of the existing surface parking on the property. A large surface parking lot will be retained on the northern end of the property and the DSP revision also proposes the addition of a surface parking lot to the east of the proposed building addition near Landover Road. A loading dock for the existing building and building extension is on the Landover Road side of the property and will be accessed from this parking lot as well. There are currently three entrances to the property, all of which are proposed to remain: one from Technology Way on the northern end of the property and two from Mercantile Lane on the western side of the property.

Additional Features

The applicant also plans to include an occasional farmer's market use, to be located under the canopy overhang of the proposed parking garage. Pursuant to Section 27-473(b)(1)(E), a farmer's market is a permitted use in the I-3 Zone provided it is in compliance with Sections 27-260 and 27-261 of the Zoning Ordinance. The Department of Environmental Resources (DER) may issue temporary use and occupancy permits for specific temporary uses and structures provided the requirements of Section 27-261 of the Zoning Ordinance are satisfied. The applicant is a not-for-profit organization that plans to operate the occasional farmer's market use on the property in full compliance with the applicable regulations and in order to complement its mission of encouraging health and wellness for its employees and patients.

COMPLIANCE WITH EVALUATION CRITERIA

7. **Zoning Ordinance:** The proposed amendment to the detailed site plan meets the purposes of the I-3 Zone. The main purposes of the I-3 Zone, as referenced in Section 27-471(a)(1) of the Zoning Ordinance, are to provide increased and enhanced employment opportunities for the residents of the county; provide a mix of industrial, research, and office uses; minimize detrimental effects on uses of adjacent land; assure the compatibility of proposed land uses with surrounding land uses; maximize open space so as to create a park-like setting; and improve the overall quality of industrial/employment areas in Prince George's County.

The DSP revision has been reviewed for compliance with the requirements of the I-3 (Planned Industrial/Employment Park) Zone, as follows:

Section 27-471(f). Regulations.

- (1) **Additional regulations concerning the location, size, and other provisions for all buildings and structures in the I-3 Zone are as provided for in Divisions 1 and 5 of this Part, the Regulations Tables (Division 4 of this Part), General (Part 2), Off-Street Parking and Loading (Part 11), Signs (Part 12), and the Landscape Manual.**

Additional regulations referenced above have been reviewed as applicable and are discussed in this resolution.

- (2) **Not more than twenty-five (25%) of any parking lot and no loading space shall be located in the yard to which the building's main entrance is oriented, except that the Planning Board may approve up to an additional fifteen percent (15%) in its discretion if increased parking better serves the efficiency of the particular use; improves views from major arteries or interstate highways; and makes better use of existing topography or complements the architectural design of the building.**

The applicant has submitted a variance request to this section. The site plan shows 42 percent of the surface parking located in the yard to which the building's main entrance is located. This percentage does not include the parking located in the parking garage. The Planning Board may approve up to 40 percent of parking in this yard without a variance request. More details about the variance request can be found in Finding 8, the variance section of the resolution.

- (3) **No loading docks shall be permitted on any side of a building facing a street except where the lot is bounded by three (3) or more streets.**

The site is bounded by three streets. A loading dock is proposed on the east side of the building which faces Landover Road (MD 202). The loading area will be screened from Landover Road through the use of a six-foot-high, sight-tight, brick veneer screening wall and proposed landscaping.

Section 27-471(h). Required access.

- (1) **Each Planned Industrial/Employment Park (including each property in separate ownership) shall have frontage on, and direct vehicular access to, a street having a right-of-way width of at least seventy (70) feet.**

Landover Road (MD 202) has a right-of-way width of 200 feet. Technology Way has a right-of-way width of 80 feet and Mercantile Lane has a right-of-way width of 70 feet. Direct vehicular access exists through Mercantile Lane and Technology Way.

Section 27-471(i). Minimum area for the development.

- (1) **The minimum area for the development of any Planned Industrial/Employment Park shall be twenty-five (25) gross acres.**

The gross acreage for the entire Largo Park planned employment park meets this requirement.

Section 27-471(d). Site plans.

- (2) **In addition to the requirements of Part 3, Division 9, the Detailed Site Plan shall show the design and size of lettering, lighting, and all other features of signs proposed (except those for directional or informational purposes containing not more than four (4) square feet). These signs shall be reviewed and approved or disapproved at the same time the Detailed Site Plan is acted upon.**

A detailed signage package was submitted with the detailed site plan submittal. The signage package has been reviewed in accordance with Part 12 (Signs) of the Zoning Ordinance.

Section 27-613. Signs Attached to a building or canopy.

- (a) **Location.**

- (2) **In the I-3 Zone, the signs may be located anywhere on a building that the Planning Board deems appropriate, subject to the height limitations below.**

A signage location plan has been included in the detailed site plan. No new signage is proposed along Landover Road (MD 202).

- (b) **Height.**

- (2) **In the I-3 Zone the sign shall not extend above the lowest point of the roof of the building to which it is attached.**

No signage extends above the lowest point of any roof.

- (c) **Area.**

- (4) **I-3 Zone.**

- (A) **In the I-3 Zone, the area of all of the signs on a building wall facing a street shall be not more than one (1) square foot for each one (1) lineal foot of building width facing that street.**

All of the signage proposed falls well within the limits of one square foot for each one lineal foot of building width facing that street. There is no new building-mounted signage proposed along Landover Road (MD 202). Along Technology Way there are approximately 400 lineal feet of building width, and thus 400 square feet of signage are permitted, but only 84 square feet of building-mounted signage is proposed. Along Mercantile Lane there are approximately 600 lineal feet of building width, and thus 600 square feet of signage are permitted, but only 171 square feet of signage are proposed. Finally, along the side of the property facing the southern property line there are 414 lineal feet of building width, and thus 418 square feet of signage are permitted with only 169 square feet of signage proposed.

Most of the building-mounted signage is lettering mounted directly on the architectural face of a building with no other decorative wall work. The actual faces of these letters and figures constitute the sign, and the spaces between them are not to be considered part of the sign; therefore, the square footage of those signs may be reduced by an additional 50 percent. This further illustrates that the sign proposal for this development is particularly modest, as those deductions are not accounted for in the summary of square footage above.

(e) Design.

(1) In the I-3 Zone, signs shall be either:

- (A) Designed as a part of the architectural design of the building; or**
- (B) Approved as an element of the proposed development on the lot, taking into account its relationship to the other proposed improvements.**

The proposed signs are incorporated into the architectural design of the building and aid functionality on-site by providing direction to users and visitors. The proposed building-mounted signage includes one sign affixed to the walls of the proposed building addition stating the name of the applicant (Kaiser Permanente), one sign affixed to the wall of the proposed parking garage stating the name of the applicant, and one identification sign affixed to the wall of the proposed parking garage instructing appropriate lane usage for ingress and egress.

Section 27-614. Freestanding signs.

(a) Location.

- (3) In the I-3 Zone, signs may be located anywhere that the Planning Board deems appropriate, subject to the height limitations below.**

(b) Height.

In the I-3 Zone, the maximum height shall not be greater than the lowest point of the roof of any building in the employment park

The maximum height of all freestanding signs in the signage package is ten feet, and below the roof line of a single-story building. Signage heights comply with the Zoning Ordinance regulations.

(c) Area.

- (4) In the I-3 Zone, the area of the sign shall be not more than one (1) square foot for each five (5) lineal feet of street frontage along the street on which the sign faces.**

The intersection of Landover Road (MD 202) and Technology Way share a refurbished V-shaped sign. The sign has an area of 202.5 square feet which is justified by utilizing linear feet of street frontage from both Landover Road and Technology Way. There are 1,004 lineal feet of street frontage along Landover Road and 552 lineal feet along Technology Way. There is an additional 59 square feet of freestanding signage visible from Technology Way.

Along Mercantile Lane there are 768 lineal feet of street frontage, and thus 154 square feet of signage is permitted with 64.5 square feet of freestanding signage proposed. Along the side of the property facing the southern property line there is approximately 1,068 lineal feet of street frontage, and thus 213 square feet of signage is permitted with only nine square feet proposed.

(d) Quantity.

- (3) In the I-3 Zone, the number of freestanding on-site signs permitted shall be determined by the Planning Board at the time of Detailed Site Plan review.**

There are eight freestanding signs included in the signage package.

In addition to the requirements of the Zoning Ordinance, as it relates to signs, the applicant worked extensively with neighboring community associations including Largo, Lake Arbor, and Kettering to determine the appropriateness of the sign package submitted. The number of signs proposed was voluntarily reduced by the applicant in light of feedback received from the neighboring communities. During the detailed site plan review, Development Review Division staff received phone calls from the community associations confirming support of the final site plan and signage package.

8. **Variance Request VD-95073-01:** The applicant has submitted a variance request to permit the construction of 42 percent of the surface parking lot in the yard to which the building's main entrance is oriented. The applicant provides the following justification for the variance request:

“The Applicant proposes to construct 187 surface parking spaces in the yard of the Property closest to the main building entrance off of Mercantile Lane, as well as to construct the parking garage closest to the main building entrance off of Mercantile Lane.

Pursuant to Section 27-471(f)(2) of the Zoning Ordinance, ‘not more than twenty-five (25%) of any parking lot and no loading space shall be located in the yard to which the building’s main entrance is oriented, except that the Planning Board may approve up to an additional fifteen percent (15%) in its discretion if increased parking better serves the efficiency of the particular use; improves views from major arteries or interstate highways; and makes better use of existing topography or complements the architectural design of the building.’ Thus, the Planning Board may authorize up to 40% of any parking lot to be located in the yard to which the building’s main entrance is oriented, which in this case would amount to 177 of the total 442 proposed surface parking spaces on the Property. Because the Applicant proposes to construct 187 surface parking spaces in this yard, which amounts to approximately 42.3% of the total (surface) parking lot on the Property, this Variance Application is necessary in order to permit the 10 additional spaces in the yard adjacent to Mercantile Lane closest to the main building entrance. While the restriction contained in Section 27-471(f)(2) applies only to ‘parking lots’ and not to a parking garage¹ and thus the Applicant does not believe that a variance is required in order to construct the parking garage in the proposed location, because the Applicant is required to submit this Variance Application in order to permit 10 additional surface parking spaces on the Mercantile Lane side of the Property, the Applicant has included the parking garage within the scope of the Variance Application as well.”

“ Footnote 1: Section 27-107.01. Definitions.

“(173) Parking Garage: A ‘Building’ (other than a motor vehicle sales room) used for parking private motor vehicles. A ‘Parking Garage’ shall not be used for the storage of dismantled or wrecked vehicles, motor vehicle parts, or junk. A ‘Parking Garage’ shall not be considered an ‘Accessory Building’ unless it is used for parking vehicles in accordance with Part 11.

“(174) Parking Garage, Private: A ‘Parking Garage’ used for housing private ‘Passenger Vehicles’ and such ‘Commercial Vehicles’ as are allowed to be parked in the zone in which such garage is located. Not over fifty percent (50%) of the garage shall be used for vehicles not owned by the occupants of the premises. The garage shall be either an ‘Accessory Building’ or part of a ‘Main Building.’

“(176) Parking Lot: An area of land (other than a ‘Vehicle Sales Lot’) used for parking private ‘Passenger Vehicles.’ A ‘Parking Lot’ shall not be used for the storage of dismantled or wrecked vehicles, motor vehicle parts, or junk. (‘Parking Lots’ used in accordance with Part 11 prior to March 1, 1985, were called ‘Automobile Parking Compounds.’)”

Section 27-230(a) of the Zoning Ordinance outlines the findings that must be made in order to approve a variance application. The variance application satisfies the requisite findings outlined in Section 27-230(a) as follows:

- 1. A specific parcel of land has exceptional narrowness, shallowness, or shape, exceptional topographic conditions, or other extraordinary situations or conditions;**

Applicant's Justification:

“As described above, the Property is uniquely situated directly adjacent to three roadways, a situation which affects just this Property and one other lot in the entirety of Largo Park. Such a hindrance places a severe constraint on location of parking lots and garages in the I-3 Zone, which encourages planned employment parks in which parking is typically located between the adjacent roadway and the main building entrance facing such roadway. In fact, the entirety of the Largo Park employment park is essentially developed in the same manner, with low-rise office buildings typically situated near the center of properties and surrounded by surface parking. Thus, the direct adjacency of the three roadways to the Property is an extraordinary condition that limits this Property in terms of placement of its parking facilities. Further, along the perimeter of the Property there are trees, microbioretention areas, and an existing water quality pond both currently protected and additionally proposed that would be detrimentally affected if the parking lot and the parking garage are not located as proposed.”

The site is adjacent to three roadways at the perimeter of the site and one micro-bioretention area to the southeast. The location of the additional proposed parking is most sensitive to the existing site conditions. It builds upon existing impervious surfaces approved in the original DSP without infringing on the area needed for the micro-bioretention areas and water quality pond.

- 2. The strict application of this Subtitle will result in peculiar and unusual practical difficulties to, or exceptional or undue hardship upon, the owner of the property; and**

Applicant's Justification:

“Applying the strict standards of the Zoning Ordinance would be illogical because the Applicant could not place any significant parking area on the side of the Property to which a main building entrance would typically be oriented in an employment park – facing the adjacent roadway and the main access point to the site on the Mercantile Lane side of the Property. Under the strict application of the Zoning Ordinance, the Applicant could locate the parking lot and parking garage in one of the other three yards of the Property, all of which, however, would have detrimental effects on the surrounding area. If the parking lot and garage were located in the southern yard of the Property, it would be directly adjacent to the only abutting property owner to the Property. In addition, on the southern side of the site is a large existing water quality pond and proposed microbioretention areas, all of which play an important role in the landscaping, buffering, and green area features of the Property. There is insufficient space in which to locate the parking garage on the eastern side of the site adjacent to Landover Road. In addition, the Landover Road frontage is the most visible aspect of the Property, and planning principles dictate that parking typically not be located on the most visible frontage of the Project adjacent to a high-use road. Finally, the Applicant has avoided locating the parking garage on the northern end of the Property in order to minimize the area

disturbed as part of the construction process. Currently, the northern end of the Property is not proposed to be disturbed, as the proposed building addition and also the parking garage are located towards the southern end of the Property. In addition, there is simply no room in the parking lot for additional surface parking in order to remove spaces from the Mercantile Lane side of the Property. As a result, strict application of the Zoning Ordinance would have detrimental effects by requiring the parking garage to be located in illogical and inefficient locations. Rather, granting of the Variance Application in order to permit construction of the parking garage in the location proposed will allow the Applicant to most efficiently utilize the site and maximize the open space provided by consolidating the surface parking into a more efficient, environmentally-friendly parking structure.

“Finally, because the use on the Property is that of a medical practitioners’ office, undue hardship would not only be placed on the Applicant but would also be experienced by the patients visiting the medical offices were parking required to be placed in a location far removed from the main building entrance. By the very nature of the use, many of the patients visiting the Property are experiencing physical difficulties and patients, as well as visitors, are often under duress. Thus, it is imperative that parking be located in a clear location easily accessible from the main vehicular entrance to the Property and within a minimal walk of the main building entrance. As a result, strict compliance with the requirements of the Zoning Ordinance would place serious practical difficulties on the use of the Property for medical practitioners’ offices and its accompanying patients.”

The strict application of this Subtitle would result in peculiar design outcomes and unusual practical difficulties. The applicant prefers to minimize the area disturbed by the construction process; therefore, locating the parking garage north of the existing building is not a viable option. Understanding this, further redesign of the parking lots would result in the relocation of more parking to the sides and rear of the building that are highly visible from Landover Road (MD 202). Currently, the eastern portion of the site has adequate buffers for screening the additional proposed lot along Landover Road. More parking in this location would result in reduced landscape buffer width and increased visibility of parking lots along Landover Road. The relocation of parking to the southeast would also impact an existing water quality pond.

The parking, as located, is least visible from the highest-use roads and most accessible to users of the Kaiser Permanente medical offices. Relocation of parking away from the building entrance would also be an inconvenience to patients with physical difficulties and those in need of medical care.

3. The variance will not substantially impair the intent, purpose, or integrity of the General Plan or Master Plan.

Applicant’s Justification:

“As explained above, the Master Plan recommends employment uses on the Property and continued I-3 zoning. The proposed Variance Application would allow for more efficient utilization of the Property in order to accommodate the expansion of the current employment use, thereby fulfilling the intent of the Master Plan and its recommendations for the Property.

“For the foregoing reasons, the Variance Application satisfies the findings that must be made for approval of a variance. The unique conditions and limitations of the Property result in unusual practical difficulties in placement of the 10 surface parking spaces and the parking garage if strict application of the Zoning Ordinance requirements for location of parking lots were followed. The Variance Application furthers the goals of the Master Plan and results in a more efficient utilization of the Property, allowing the Applicant to expand the existing employment use on the Property in furtherance of the purposes of the I-3 Zone.”

The Planning Board finds the analysis and justification presented by the applicant to be acceptable and recommends approval of the variance request. The variance will not impair the intent or integrity of the *Prince George’s County Approved General Plan*. The original detailed site plan was approved locating 36 percent of the site’s surface parking in the yard closest to the building’s main entrance. This proposal builds upon the existing approved layout and provides enhancements by reducing the need for additional surface parking by adding a parking garage. Additionally, locating parking in the yard closest to the building entrance minimizes views of parking from Landover Road (MD 202).

9. **Detailed Site Plan SP-95073:** The subject detailed site plan revision is consistent with approved Detailed Site Plan SP-95073. The original detailed site plan was approved with few conditions of approval. There are no conditions of approval which are relevant to the review of this detailed site plan revision.
10. **Conceptual Site Plan SP-87168/01:** The property is subject to Conceptual Site Plan SP-87168/01, approved for Largo Park by the Planning Board on August 3, 1989. The subject revision to the detailed site plan satisfies the relevant CSP conditions as explained below:
 1. **A minimum 30-foot landscaped yard shall be provided along the property line adjacent to MD 202 (as required by I-3 Zone regulations of the Zoning Ordinance, Subtitle 27).**

The 30-foot landscaped yard is provided.

4. **A reduction of the requirement for a yard adjoining non-residential property from 20 feet to 10 feet, shall be granted, as set forth in Section 27-474(b). Except as to Conditions 3 and 4, all other standards for building and parking setbacks, yards, building coverage and green area, and amount of parking in main entrance yard that are set forth in Subtitle 27 must be complied with at Detailed Site Plan.**

Applicant's Justification:

“The Project satisfies this requirement by providing a minimum 103’-5” setback from the adjoining non-residential property located to the south of the Property which exceeds the 10-foot minimum, as shown on the Detailed Site Plan submitted herewith. Further, parking setbacks, yards, building coverage, and green area are all satisfied as explained in detail above.”

The location of the parking lots relative to the main entrance of the building has also been satisfied by filing a variance application. See Finding 8 for more discussion.

6. Individual trees on site that are worthy of saving will be flagged by staff from the Natural Resources Division and the Development Review Division.

No individual trees have been identified by staff.

7. Every effort will be made to incorporate these trees into the overall building and parking lot layout. This could mean the reconfiguration of the building and parking lot to accomplish this goal. The possibility that drastic overall site grading will be necessary for drainage of surface waters will be carefully reviewed at Detailed Site Plan and would eliminate the tree-save requirement.

No individual trees have been identified by staff.

11. **Preliminary Plans of Subdivision 4-79155 and 4-86107:** The site plan indicates that the property is Parcel 5 (Plat Book NLP 116 @ 89) and Lot 2 (Plat Book NLP 151 @ 28), located on Tax Map 67 in Grid F-1, and is 14.71 acres in the I-3 Zone. The applicant, Kaiser Permanente, is proposing to add a 106,700-square-foot medical building and a 245,200-square-foot garage to the existing medical practitioners' offices currently on-site.

Parcel 5 is the subject of Preliminary Plan 4-79155. Lot 2 is the subject of Preliminary Plan 4-86107. The resolution of approval for 4-86107 was adopted by the Planning Board on July 24, 1986 (PGCPB Resolution No. 86-297) and contains two conditions:

1. Approval of a conceptual stormwater management plan by the Washington Suburban Sanitary Commission prior to Final Plat of Subdivision.

Conceptual Site Plan SP-79029 was approved by the Prince George's County Planning Board on October 11, 1979.

2. Denial of access to Arena Drive.

The subject plan accurately reflects this denial.

12. ***Prince George's County Landscape Manual:*** Landscaping, screening, and buffering of development in the I-3 Zone shall be provided as set forth in the *Prince George's County Landscape Manual*. The site plan is subject to Section 4.2, Commercial and Industrial Landscaped Strip Requirements; Section 4.3, Parking Lot Requirements; Section 4.4, Screening Requirements; and Section 4.7, Buffering Incompatible Uses of the Landscape Manual.
- a. The subject site is bounded by public rights-of-way on three sides of the property. The landscape plan provides a more than 30-foot-wide landscape strip along Landover Road (MD 202) in accordance with Section 4.2 of the Landscape Manual and Section 27-474 of the Zoning Ordinance. The other two rights-of-way require a parking lot landscape strip in accordance with Section 4.3.a of the Landscape Manual.
 - b. Section 4.3(a), Parking Lot Landscape Strip, requires a landscape strip in any zone when a parking lot is adjacent to a public right-of-way. The landscape strip requirements are met through the subject detailed site plan revision.
 - c. Section 4.3(c), Interior Planting, requires a certain percentage of the parking lot, according to the size of the lot, to be interior planting area and to be planted with one shade tree for each 300 square feet of interior landscaped area provided. The DSP has approximately 88,974 square feet of surface parking lot. A minimum eight percent of the parking lot, approximately 7,118 square feet, should be interior planting area. The applicant provides 8,257 square feet of interior landscaped area which complies with Section 4.3(c).
 - d. Section 4.4, Screening Requirements, requires that all dumpsters and loading spaces be screened from all adjacent public roads. A loading dock is proposed on the east side of the building which faces Landover Road (MD 202). The loading area will be screened from Landover Road through the use of a six-foot-high, sight-tight, brick veneer screening wall and proposed landscaping. The screening of the loading spaces, generators, and transformers conform with Section 4.4.
 - e. The site shares a boundary with the Largo Commons Condominiums property on the southeast property line. A Section 4.7 bufferyard for buffering incompatible uses is provided by existing woodland which is more than 50 feet in depth.
13. **Woodland Conservation and Tree Preservation Ordinance:** The subject property is subject to the provisions of the Prince George's County Woodland Conservation Ordinance since the gross tract area is greater than 40,000 square feet and there are more than 10,000 square feet of existing woodlands on-site. The activity proposed requires no additional clearing of woodlands. A tree conservation plan is not required with this application. The application is in conformance with the Woodland Conservation and Tree Preservation Ordinance.

14. **Referral Agencies and Departments:** The subject application was referred to relevant agencies and divisions. The referral comments are summarized as follows.
- a. **Historic Preservation Section**—The development will have no effect on identified historic sites, resources, or districts.
 - b. **Archeological Review**— The Planning Board accepts the staff archeologist findings that note a Phase I archeological survey is not recommended on the above-referenced 14.71-acre Kaiser Permanente property located at 1221 Mercantile Lane in Largo, Maryland.
 - c. **Community Planning North Division**—The application, which is located in the Northampton Community, is consistent with the General Plan Development Pattern policies for employment areas within the Developing Tier and conforms to the land use recommendations of the *1990 Approved Master Plan and Adopted Sectional Map Amendment for Largo-Lottsford, Planning Area 73* for employment-related development at this site.
 - d. **Transportation Planning Section**—The Transportation Planning Section provided a review of the detailed site plan application. The case history background of this site plan goes back to a time prior to considerable development in the Largo area. Preliminary Plans of Subdivision 4-79155 and 4-86107 are the relevant cases, as well as Conceptual Site Plan SP-86168 (the /01 revision applied to a different part of the overall conceptual plan). The prior detailed site plan is not relevant to the transportation review.

There are no transportation-related conditions on the conceptual site plan. Between the two preliminary plans, there are three transportation-related conditions:

- A condition on 4-79155 involves preservation of a right-of-way for a Metrorail extension on Lots 1 and 2 of Block D. This site does not involve either of these lots.
- Another condition on 4-79155 required that driveway access to individual lots along MD 202 be directed to internal streets. The site plan complies with this requirement.
- Condition 3 of the resolution approving 4-86107 required the denial of driveway access to Arena Drive. This site has no frontage on Arena Drive.

Access to and circulation within the site is acceptable as shown.

Neither 4-79155 nor 4-86107 includes trip cap conditions or other restrictions on the quantity of development that can occur within the site. Neither plan file includes traffic study information. Both of the lots that comprise this site were platted pursuant to preliminary plans that included a finding of transportation adequacy. The expansion is

occurring on lots that were recorded without a trip cap or other similar restriction. The off-site transportation impact of the expansion is not an issue in the detailed site plan process.

Landover Road (MD 202) is shown on the master plan as an expressway facility, and Technology Way and Mercantile Lane are shown as industrial roadways. Adequate right-of-way has previously been dedicated or deeded consistent with master plan recommendations; therefore, this plan poses no right-of-way issues. Therefore, the Transportation Planning Section deems the submitted site plan to be acceptable from the standpoint of transportation.

- e. **Subdivision Section**—The Subdivision Section provided review of the revision to the detailed site plan. There are no issues related to subdivision.
- f. **Trails**—In terms of pedestrian and bicycle features, the detailed site plan is in conformance with Section 27-281 of the Zoning Ordinance. Sidewalks and crosswalks are recommended in the area master plan for this area (pages 39 & 132-133). The sidewalk network is supportable as shown on the detailed site plan and the sidewalks appear to be adequate for the proposed use. The plan includes concrete sidewalks ranging in width from five to eight feet in width along internal roads and leading to the buildings. A five-foot-wide covered walkway is proposed for the three-story building addition and around the parking garage. The proposed sidewalks will connect to the existing sidewalk network. All of the sidewalks are set back from the curb edge as is recommended in the area master plan. A zebra-striped crosswalk is provided across the main entrance road and appears to be adequate.

The five-foot-wide asphalt walking trail that is proposed is a nice addition to the site, and it will serve employees in the area. The walking trail has a connection to Mercantile Lane and to the parking area that is north of the buildings. It could be extended in the future to MD 202 when/if the road is improved in the future by the State Highway Administration (SHA) with sidewalks. The width of the walking trail is adequate for the proposed use and is not part of a master planned trail system.

There are four conditions of approval related to trails on the site.

- g. **Permit Review Section**—The Permit Review Section made several comments requesting clarification of several plan elements. The Permit Review Section's comments have been addressed through revisions to the plans.
- h. **Environmental Planning Section**— The Planning Board accepts the Environmental Planning Section's findings for Detailed Site Plan, DSP-95073-01. The Environmental Planning Section has recommended one condition of approval that has been included in this technical staff report.

- i. **Department of Public Works and Transportation (DPW&T)**—In a memorandum dated May 13, 2010, DPW&T, Office of Engineering, offered the following:
 - Frontage improvements in accordance with DPW&T urban commercial/industrial road standards are required for Mercantile Lane and Technology Way.
 - The detailed site plan is consistent with the Approved Stormwater Concept Plan 3064-2010 dated March 17, 2010.
 - DPW&T has no objection to the variance request for the subject application.
 - j. **Washington Suburban Sanitary Commission (WSSC)**—In a memorandum dated May 14, 2010, WSSC offered the following:
 - The site has an existing and active water and sewer connection for the subject site.
 - Show and label, with Liber and Folio, the right-of-way limits on the plan for the existing eight-inch gravity sewer main on Mercantile Lane.
 - All existing mains shown on the plan should be labeled with correct pipe size and WSSC contract numbers.
 - WSSC facilities/structures cannot be located within a public utility easement (PUE). Revise the existing right-of-way to separate the WSSC easement from the PUE, leaving a minimum 20-foot easement for the existing sewer.

The plans have been revised to provide a 20-foot easement for the existing sewer.
 - k. **Verizon**—Verizon provided comment at the May 14, 2010 Subdivision and Development Review Committee meeting. The PUEs dedicated on the record plat were not shown on the plan. The applicant has revised this issue on the most recent site plan.
15. As required by Section 27-285(b) of the Zoning Ordinance, the detailed site plan represents a reasonable alternative for satisfying the site design guidelines of Subtitle 27, Part 3, Division 9, of the Prince George's County Code without requiring unreasonable cost and without detracting substantially from the utility of the proposed development for its intended use.

NOW, THEREFORE, BE IT RESOLVED, that pursuant to Subtitle 27 of the Prince George's County Code, the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission adopted the findings contained herein and APPROVED the Detailed Site Plan DSP-95073/01 and further approved Variance Application No. VD-95073/01 subject to the following conditions:

1. Prior to signature approval of the detailed site plan, the following information shall be provided or revisions made:
 - a. The following note shall be added to the detailed site plan:

“If the trimming, cutting, removal, or injury of a roadside tree is proposed or required for implementation of the approved detailed site plan, a roadside tree permit must be obtained prior to the issuance of a site development permit.”
 - b. Provide a chart on the landscape plan demonstrating that the existing parking lot meets the requirements of Section 4.3.c, Interior Planting, of the *Prince George’s County Landscape Manual*.
 - d. Show details of the asphalt walking trail in conformance with the Department of Public Works and Transportation (DPW&T) specifications and standards.
 - e. Show details of the concrete sidewalks, crosswalk striping, and ramps in conformance with DPW&T specifications and standards. Asphalt shall not be used a paving material for sidewalks.
 - f. Provide two U-shaped bicycle racks near the front entrance of the building, and two u-shaped bicycle racks in the parking garage. Details of the bicycle racks shall be provided. The racks shall be anchored into a concrete base. Bicycle parking area signs (MUTCD D4-3) shall be erected at the two parking locations (see MUTCD Part 9, Traffic Control for Bicycle Facilities, Section 9B.23.)
 - g. Provide pedestrian-scaled lighting along the entire length of the proposed walking trail.

BE IT FURTHER RESOLVED, that an appeal of the Planning Board’s action must be filed with the District Council of Prince George’s County within thirty (30) days following the final notice of the Planning Board’s decision.

* * * * *

This is to certify that the foregoing is a true and correct copy of the action taken by the Prince George's County Planning Board of The Maryland-National Capital Park and Planning Commission on the motion of Commissioner Squire, seconded by Commissioner Vaughns, with Commissioners Squire, Vaughns, Clark, Cavitt and Parker voting in favor of the motion at its regular meeting held on Thursday, June 24, 2010, in Upper Marlboro, Maryland.

Adopted by the Prince George's County Planning Board this 8th day of July 2010.

Patricia Colihan Barney
Executive Director

By Frances J. Guertin
Planning Board Administrator

PCB:FJG:MF:arj

KAISER PERMANENTE LARGO

DETAILED SITE PLAN

DSP-95073-06

STATEMENT OF JUSTIFICATION

December 16, 2021

I. INTRODUCTION

Kaiser Permanente (the “**Applicant**”) by and through its attorneys, Lerch, Early & Brewer, Chtd. and CLHatcher LLC, submits this Detailed Site Plan (“**DSP**”) Justification Statement to demonstrate that the proposed development is in compliance with the applicable provisions of Subtitle 27 of the Prince George’s County Code (the “**Zoning Ordinance**”), the *2013 Approved Largo Town Center Sector Plan & Sectional Map Amendment* (the “**Master Plan**”) and other applicable review requirements and criteria. The subject property consists of approximately ±14.71 acres located at the southeast quadrant of Mercantile Lane and Technology Way (the “**Property**”). The Property is currently zoned M-U-I (Mixed Use – Infill) / D-D-O (Development District Overlay) and is subject to the Development District Standards and recommendations of the Master Plan. The Property is located within the Largo Town Center Metro Regional Transit District and Downtown, as designated by *Plan Prince George’s 2035 Approved General Plan* (the “**General Plan**”).

As described in detail herein and shown on the subject Detailed Site Plan, the Applicant proposes an expansion of the existing medical office building and structured parking garage to accommodate additional medical services and meet demand brought on by the COVID-19 Pandemic. The proposed expansion will include improvements to modernize the existing Advanced Urgent Care (AUC) area and add 15 new treatment rooms, streamline on-site vehicular circulation with efficient traffic patterns and a restructured drop-off area, incorporate new environmental site design features such as green roofs, native plantings, and a stormwater management facility, and incorporate additional structured parking (the “**Proposed Development**”). The improvements proposed in this DSP will complement the existing medical office building to meet demands for high-quality, accessible healthcare services in Largo Town Center and Prince George’s County. Additionally, the implementation of environmental site design techniques and upgrades to landscaping will mitigate the facility’s environmental impact and enhance on-site stormwater management. The Applicant respectfully requests Planning Board approval of this Detailed Site Plan.

II. PROPERTY DATA

<i>Location:</i>	Southeast quadrant of Mercantile Lane and Technology Way.
<i>Parcel / Lot:</i>	Parcel 5. Lot 2, Block C.
<i>Tax Map #:</i>	67-F1.
<i>Frontage:</i>	Mercantile Lane. Technology Way.
<i>Election District:</i>	13.
<i>Legislative District:</i>	24.
<i>Councilmanic District:</i>	6.
<i>Acreage:</i>	±14.71 acres.
<i>Zoning:</i>	M-U-I/D-D-O. ¹
<i>Planning Area:</i>	73.
<i>Subdivision:</i>	Largo Park. 4-79155. 4-86107.
<i>Existing Water Category:</i>	W-3.
<i>Existing Sewer Category:</i>	S-3.
<i>Historic:</i>	N/A.
<i>Aviation Policy Area:</i>	N/A.
<i>Master Plan & SMA:</i>	<i>2013 Approved Largo Town Center Sector Plan & Sectional Map Amendment.</i>
<i>General Plan:</i>	<i>Plan Prince George's 2035.</i>

¹ The Property is currently zoned M-U-I / D-D-O. The Property will be rezoned RTO-H-e (Regional Transit-Oriented – High Intensity – edge), pursuant to the adopted but not yet implemented New Zoning Ordinance. The subject DSP application is processed and reviewed pursuant to the current M-U-I / D-D-O Zoning.

III. PROPERTY DESCRIPTION / EXISTING NEIGHBORHOOD

The Property consists of Parcel 5 and Lot 2, Block C, and is located in the southeast quadrant of Mercantile Lane and Technology Way.² The Property is adjacent to three roadways along the perimeter of the site, and one micro-bioretenion area to the southeast. The existing medical office building is situated on the eastern side of the Property, closest to Landover Road (MD 202). Existing structured parking is located along the western portion of the Property, closest to Mercantile Lane. The location of additional proposed structured parking in this DSP is most sensitive to the existing site conditions, as it builds upon existing impervious surfaces on-site without infringing on areas needed for stormwater management, micro-bioretenion, and environmental site design.

There are currently two (2) ingress/egress access points to the Property, both of which are proposed to remain: one along Technology Way at the northern end of the Property and another along Mercantile Lane at the southern end of the Property. A separate egress point is located along Mercantile Lane, approximately 250 feet south of the intersection of Mercantile Lane and Technology Way.

IV. PROPOSED DEVELOPMENT

The subject DSP proposes an expansion of the existing 236,200 square foot medical office building and structured parking garage on-site to accommodate additional medical services at the Property. Specific expansion elements of the medical office building include an expansion of the Advanced Urgent Care (AUC) area to add 15 new treatment rooms and a supporting laboratory space, as well as an expansion of the existing diagnostic imaging clinical function to accommodate two (2) Magnetic Resonance Imaging (MRI) diagnostic units and two (2) computer-aided tomography (CT) diagnostic units. This DSP also proposes additional structured parking, which will be incorporated into the existing parking garage on-site and

² The northern portion of the Property is identified as Parcel 5 in the Largo Park, Plat Two Subdivision, as recorded in the Land Records of Prince George's County (the "**Land Records**") at Plat Book NLP 116, page 89. The southern portion of the Property is identified as Lot 2, Block C in the Largo Park Subdivision, as recorded in the Land Records at Plat Book NLP 151, page 28. Parcel 5 is approximately 6.195 acres and Lot 2, Block C is approximately 8.51 acres in land area.

replace existing surface parking adjacent to Mercantile Lane. In addition to new infill development, this application incorporates environmental site design strategies, such as new landscaping, green roofs, native plantings, and a modern stormwater management facility. Circulation to the main entrance of the medical office building will be improved with a new two-way traffic flow at the primary entrance off of Mercantile Lane to provide more efficient traffic patterns and an upgraded drop-off area to accommodate increased traffic and pedestrian activity at the facility. These improvements are delineated as Phase I and Phase II on the subject DSP.³

The majority of the Applicant's proposed improvements emanate from the increased demand for healthcare services as a result of increased membership and projected growth within Prince George's County. Additional procedure and treatment rooms featuring state of the art technology will allow the existing medical office facility to scale up to meet additional demand. While the primary intent of the subject DSP is to increase the existing medical facility's capacity for treatment, proposed improvements to landscaping, stormwater management, and additional structured parking are indicative of the Applicant's willingness to mitigate any potential environmental impacts of the existing facility and the proposed infill development. In sum, the proposed improvements are designed to accommodate additional healthcare service demands, while offsetting impacts through efficient land use, utilization of existing impervious surfaces, and environmental site design.

V. LAND USE BACKGROUND

A. General Plan

The Property is located within the Largo Town Center Metro Regional Transit District and Downtown, as designated by the General Plan. The General Plan envisions Regional Transit Districts as locations with "extensive transit and transportation infrastructure and the long-term capacity to become mixed-use, economic generators for the County."⁴ The Property is also located within one of the

³ See DSP Sheet 0.CS1.01, "Site Plan." The proposed improvements are categorized as Proposed Site Work in Phase I or Phase II.

⁴ See General Plan, page 19, "Regional Transit Districts." The Largo Town Center Metro Regional Transit District is one of eight designated Regional Transit Districts in Prince George's County.

County’s three designated Downtowns. Along with Prince George’s Plaza Metro and New Carrollton Metro, the Largo Town Center Metro Downtown is identified as a location ripe for near-term development as a vibrant regional-serving center with “robust economic and employment space, a distinct sense of place and identity, a varied housing stock, a multimodal transportation network, and diverse, mixed-income communities.”⁵ The Property is also designated within the Established Communities Growth Policy Area. The General Plan stipulates that Established Communities are “most appropriate for context-sensitive infill and low- to medium-density development.”

B. 2013 Approved Largo Town Center Sector Plan & Sectional Map Amendment

The Property is located within the boundaries of the Master Plan. More specifically, the Property is located within the Master Plan’s Northeast Quadrant Subarea – which is intended for mixed-use / institutional land uses. The Sectional Map Amendment rezoned Parcel 5 and Lot 2, Block C from the I-3 (Planned Industrial / Employment Park) Zone to the M-U-I / D-D-O Zone. Pursuant to the Master Plan, the primary purpose of the M-U-I Zone is to encourage residential, commercial, and mixed-use residential or commercial development in or adjacent to established communities within the DDOZ.

The subject DSP conforms to the Master Plan’s future land use recommendation for the Property and advances several Master Plan strategies and policies for future development in Largo Town Center. The proposed improvements to the existing medical office facility complement the Master Plan’s preferred “Mixed-Use (Office and Residential)” land use for the Northeast Quadrant Subarea and the Property.⁶ Additionally, the subject DSP advances the Master Plan’s objectives to create additional economic and employment opportunities by improving and

⁵ *Id* at page 23, “Strategic Investment Map.” Investment in each of the County’s Downtowns is strategically targeted to expand the County’s commercial tax base by (1) attracting and retaining new employers and workers, (2) leveraging private investment, and (3) capitalizing on transit-oriented development opportunities.

⁶ *Id* at page 103, “Map 25: Preferred Land Use Plan.”

modernizing an existing institutional anchor within Largo Town Center. Master Plan environmental goals and objectives related to improving stormwater runoff quality and encouraging infill development on existing impervious surfaces are met through the implementation of green roofs, a new stormwater management facility on-site, and strategic infill development that efficiently utilizes existing infrastructure. This DSP also conforms to the majority of applicable Development District Standards provided by the Master Plan, and proposes compatible alternative Development Standards to benefit the development and greater Regional Transit District where necessary.⁷

C. SP-87168-01

The Property is subject to Conceptual Site Plan SP-87168-01, which was approved for Largo Park in the I-3 Zone by the Planning Board on August 3, 1989.

D. Preliminary Plans of Subdivision: 4-79155 & 4-86107

The Property was subdivided through two separate Preliminary Plans of Subdivision. The northern portion of the Property, Parcel 5, was previously subdivided through Preliminary Plan of Subdivision 4-79155. Lot 2, Block C, the southern portion of the Property, is the subject of Preliminary Plan of Subdivision 4-86107. The Resolution of approval for PPS 4-86107 was adopted by the Planning Board on July 24, 1986.

E. DSP-95073

On December 2nd, 1995, the Planning Board approved DSP-95073 for the development of a 127,000 square foot medical center and 635 surface parking spaces in the I-3 Zone.

F. DSP-95073-01

On June 24th, 2010, the Planning Board approved DSP-95073-01 for the development of a 106,700 square foot expansion of the existing 129,500 square foot medical office building and a 245,200 square foot parking garage to accommodate additional medical services on-site in the I-3 Zone. PGCPB No. 10-74, the Resolution

⁷ See attached Exhibit A, "Development District Standards Analysis."

of Approval for DSP-95073-01, lists the existing total building gross floor area as 129,500 square feet. PGCPB No. 95-417, the Resolution of Approval for the original 1995 DSP-95073, approved a 127,000 square foot medical office building. We have been unable to identify why these different numbers were utilized. Per Lerch, Early & Brewer's conversations with Development Review Staff, this discrepancy has been noted herein.

G. Subsequent Amendments to DSP-95073

The Planning Director approved the following amendments to DSP-95073, after Planning Board approval of DSP-95073-01 in 2011:

1. DSP-95073-02

March 28th, 2012 revision to reflect changes to parking space sizes and parking counts on-site in the I-3 Zone.

2. DSP-95073-03

April 27th, 2018 Planning Director approval and December 12th, 2018 Planning Staff verification of an amendment to add trailers within on-site surface parking and renovate the existing medical office to include two modular computerized tomography (CT) / magnetic resonance imaging (MRI) units in the M-U-I / D-D-O Zone.

3. DSP-95073-04

September 6th, 2018 Planning Director approval and December 12th, 2018 Planning Staff verification of an amendment to relocate access doors for modular CT scanner and MRI facility on-site in the M-U-I / D-D-O Zone.

4. DSP-95073-05

June 1st, 2021 Planning Director approval of amendment to install five natural gas energy serves on-site in the M-U-I / D-D-O Zone.

VI. ANALYSIS

A. Compliance with Zoning Ordinance

1. §27-548.25 Site Plan Approval

(a) Prior to issuance of any grading permit for undeveloped property or any building permit in a Development District, a Detailed Site Plan for individual development shall be approved by the Planning Board

in accordance with Part 3, Division 9. Site plan submittal requirements shall be stated in the Development District Standards. The applicability section of the Development District Standards may exempt from site plan review or limit the review of specific types of development or areas of the Development District.

Comment: The subject DSP is not exempt from site plan review, pursuant to the applicability section of the Master Plan's Development District Standards.⁸ This DSP is submitted in accordance with Part 3, Division 9 and the site plan submittal requirements stated in the Development District Standards.

(b) In approving the Detailed Site Plan, the Planning Board shall find that the site plan meets applicable Development District Standards.

Comment: The subject DSP meets applicable Development District Standards and proposes several alternate Development District Standards. Analysis of the Proposed Development's conformance with the Master Plan's applicable Development District Standards is contained in "Exhibit A."

(c) If the Applicant so requests, the Planning Board may apply development standards which differ from the Development District Standards, most recently approved or amended by the District Council, unless the Sectional Map Amendment text specifically provides otherwise. The Planning Board shall find that alternate Development District Standards will benefit the development and the Development District and will not substantially impair implementation of the Master Plan, Master Plan Amendment or Sector Plan.

⁸ See Master Plan, pages 129-130. The proposed expansion of the on-site AUC and Imaging Center, circulation and drop-off area improvements, and new landscaping and stormwater management facility add less than 10,000 square feet of gross floor area to qualify for the Master Plan's Nonresidential Development Detailed Site Plan Review and Development District Standards Exemption. However, the total gross floor area associated with the proposed additional structured parking included in this application exceeds this exemption threshold and triggers Detailed Site Plan review, as well as applicability of Development District Standards.

Comment: The Applicant is requesting several alternate Development District Standards. The proposed alternate Development District Standards benefit the development and the Development District, and will not substantially impair implementation of the Master Plan. Extensive analysis of conformance with the Master Plan's Development District Standards is provided in the attached "Exhibit A." None of the requested Development District Standard amendments require District Council approval, pursuant to Sec. 27-548.26.⁹

(d) Special Exception procedures shall apply to uses within a Development District as provided herein. Uses which would normally require a Special Exception in the underlying zone shall be permitted uses only if the Development District Standards so provide within a table of uses, and such uses shall instead be subject to site plan review by the Planning Board. Development District Standards may restrict or prohibit any such uses. The Planning Board shall find in its approval of the site plan that the use complies with all applicable Development District Standards, meets the general Special Exception Standards in Section 27-317(a)(1),(4),(5), and (6), and conforms to the recommendations in the Master Plan, Master Plan Amendment, or Sector Plan.

Comment: The Proposed Development does not include uses that would normally require a Special Exception in the underlying M-U-I Zone.

(e) If a use would normally require a variance or departure, separate application shall not be required, but the Planning Board shall find in its approval of the site plan that the variance or departure conforms to all applicable Development District Standards.

Comment: The Proposed Development does not include uses that would normally require a variance or departure.

⁹ Pursuant to Sec. 27-548.26, there are three types of Development District Standards that require District Council approval: (1) changes to the boundary of the DDOZ, (2) changes from one zoning category to another, and (3) changes to the list of permitted uses. The subject DSP does not propose any such amendments.

2. §27-285 Planning Board Procedures

(b) Required Findings.

(1) The Planning Board may approve a Detailed Site Plan if it finds that the plan represents a reasonable alternative for satisfying the site design guidelines, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use.

Comment: The subject DSP represents the most reasonable alternative for satisfying the site design guidelines without requiring unreasonable costs, and it does not detract substantially from the utility of the Proposed Development for its intended use. The proposed improvements satisfy numerous site design guidelines related to efficient land use, infill development, and environmental site design in the Master Plan.

(2) The Planning Board shall also find that the Detailed Site Plan is in general conformance with the approved Conceptual Site Plan (if one was required).

Comment: The Property is subject to Conceptual Site Plan SP-87168-01, which was approved for Largo Park in the I-3 Zone by the Planning Board on August 3, 1989. The Property was rezoned from the I-3 Zone to the M-U-I / D-D-O Zone in 2013, through the Largo Town Center Sectional Map Amendment. Accordingly, SP-87168-01 is generally no longer applicable to development on the Property, pursuant to the current M-U-I / D-D-O Zoning on-site. Analysis of the subject DSP's conformance with SP-87168-01 is provided in Section VIII(A) of this Statement of Justification.

(3) The Planning Board may approve a Detailed Site Plan for Infrastructure if it finds that the plan satisfies the site design guidelines as contained in Section 27-274, prevents offsite property damage, and prevents environmental degradation to safeguard the public's health, safety, welfare, and economic well-being for grading, reforestation, woodland conservation, drainage, erosion, and pollution discharge.

Comment: This finding is not applicable to the subject DSP, as the application is not a Detailed Site Plan for Infrastructure.

(4) The Planning Board may approve a Detailed Site Plan if it finds that the regulated environmental features have been preserved and/or restored in a natural state to the fullest extent possible in accordance with the requirement of Subtitle 24-130(b)(5).

Comment: The Proposed Development does not impact any Regulated Environmental Features as defined by Section 24-101(b)(27) of the Prince George's County Code.

VII. MASTER PLAN & D-D-O STANDARDS

Please see "Exhibit A" for analysis of conformance with the Master Plan's Development District Standards and requests for alternative Development District Standards.

VIII. CONFORMANCE WITH PREVIOUS APPROVALS & OTHER APPLICABLE REVIEW CRITERIA

A. SP-87168-01

The Property is subject to Conceptual Site Plan SP-87168-01, which was approved for Largo Park in the I-3 Zone by the Planning Board on August 3, 1989. The Property was rezoned from I-3 Zone to the M-U-I / D-D-O Zone in 2013, through the Largo Town Center Sectional Map Amendment. Accordingly, several Conditions of Approval contained in SP-87168-01 are not applicable to the subject DSP.¹⁰ An analysis of relevant Conditions of Approval is provided below:

1. A minimum 30-foot landscaped yard shall be provided along the property line adjacent to MD 202 (as required by I-3 Zone regulations of the Zoning Ordinance, Subtitle 27)

Comment: A 30-foot landscaped yard currently exists along MD 202. The subject application is not proposing any modifications to this existing condition.

¹⁰ See PGCPB Resolution No. 89-420.

6. Individual trees on site that are worthy of saving will be flagged by staff from the Natural Resources Division and the Development Review Division.

Comment: Staff has identified trees worthy of saving in previous DSP approvals for the Property.

8. The exact location of the 8-foot biker trail shall be determined at Detailed Site Plan. It may be either inside or outside of the public right-of-way for Arena Drive. The 8-foot wide hard-surfaced trail shall be constructed by applicant along the property's entire frontage on Arena Drive.

Comment: DSP-95073-01 approved a pedestrian trail on the Property. The trail described in Condition 8 does not exist, as the Property does not have frontage along Arena Drive.

B. PPS 4-79155 & 4-86107

The subject DSP is in conformance with both governing Preliminary Plans of Subdivision. As previously discussed, the Property was subdivided through PPS 4-79155 and 4-86107. Both of the lots that comprise the site – Parcel 5 and Lot 2, Block C – were platted pursuant to Preliminary Plans that included a finding of transportation adequacy. Pursuant to Planning Board Resolution PGCPB No. 10-74 for the approval of DSP-95073-01, neither Preliminary Plan of Subdivision for the Property includes trip cap conditions, traffic study information, or other restrictions on the quantity of development that can occur on-site. Accordingly, the proposed expansion is occurring on lots that were recorded without a trip cap and off-site transportation impacts that are not at issue in the subject DSP.

PPS 4-79155 for Parcel 5 does not include any Conditions of Approval that are applicable to the subject DSP. PPS 4-86107 for Lot 2, Block C contains two Conditions of Approval:

1. Approval of a conceptual stormwater management plan by the Washington Suburban Sanitary Commission prior to Final Plat of Subdivision; and

2. Denial of access to Arena Drive.¹¹

Both Conditions of Approval have been satisfied. A conceptual stormwater management plan was approved prior to Final Plat approval on July 20, 1989 and the subject DSP does not propose access to Arena Drive.

C. DSP-95073 & Subsequent Revisions

The subject DSP is consistent with approved DSP-95073, DSP-95073-01, and each subsequent Planning Director approved amendment. The original Detailed Site Plan and DSP-95073-01 were approved by the Planning Board with few Conditions of Approval – none of which are applicable to the subject application. Amendments DSP-95073-02/05 were approved without relevant Conditions of Approval. Accordingly, there are no Conditions of Approval from any prior Detailed Site Plan approval or revision that are applicable and/or relevant to the subject DSP.

D. Prince George’s County Landscape Manual

This DSP is subject to the Development District Standards provided by the Master Plan. The Development District Standards replace the comparable Standards in the 2010 *Prince George’s County Landscape Manual* (the “**Landscape Manual**”). For applicable Landscape Manual Standards not addressed by the Development District Standards, the Landscape Manual serves as the governing requirement, unless explicitly stated otherwise in the Master Plan.

The provisions of the Landscape Manual regarding alternative compliance and buffering of incompatible uses do not apply within the Largo Town Center Development District. All other standards and regulations of the Landscape Manual apply to this DSP application. Analysis of the subject DSP’s compliance with any applicable Development District landscape requirements is detailed in “Exhibit A.”

E. Tree Canopy Coverage Ordinance

The subject DSP satisfies the Prince George’s County Tree Canopy Coverage Ordinance requirements outlined in Sec. 25-125(b). As shown on Sheet No. LS1.01,

¹¹ See PGCPB Resolution No. 86-297.


the Proposed Development – in combination with existing tree canopy on-site – provides more than the 10% tree canopy coverage required in the M-U-I Zone.

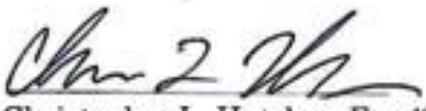
IX. CONCLUSION

The Applicant respectfully requests the Planning Board grant approval of the subject DSP for the Proposed Development. Additionally, the Applicant respectfully requests that the Planning Board also approve the minor modifications requested to Development District Standards, as detailed in “Exhibit A.” As discussed throughout this Statement, the improvements proposed in this DSP meet the Planning Board’s required findings. Moreover, the Proposed Development will complement the existing medical office building to meet increased demand for high-quality, accessible healthcare services in Largo Town Center and Prince George’s County, while mitigating the facility’s overall environmental impact. In sum, the subject DSP presents an important opportunity for Largo Town Center and the County to leverage existing infrastructure to increase access to modern healthcare services along the Blue Line Corridor and advance both the Master Plan and General Plan vision for this Regional Transit District. The above analysis, “Exhibit A” supplement, and submitted plans establish that the subject DSP satisfies the required findings that the Planning Board must make to approve a DSP application in accordance with the Zoning Ordinance.

Respectfully Submitted,

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¹² Please note that Christopher L. Hatcher is no longer a Partner at Lerch, Early & Brewer, Chtd. He is currently Principal of CLHatcher LLC.

SUPPLEMENTAL STATEMENT OF JUSTIFICATION
DDO SIGNAGE STANDARD MODIFICATION
KAISER PERMANENTE LARGO MEDICAL OFFICE BUILDING
DSP-95073-06
4/28/2022

This supplemental Statement of Justification is submitted to address an issue which arose since acceptance of the Detailed Site Plan related to signage. Specifically, two additional issues have been identified which may require the Planning Board's approval of modifications to the Master Plan's Development District Standards.

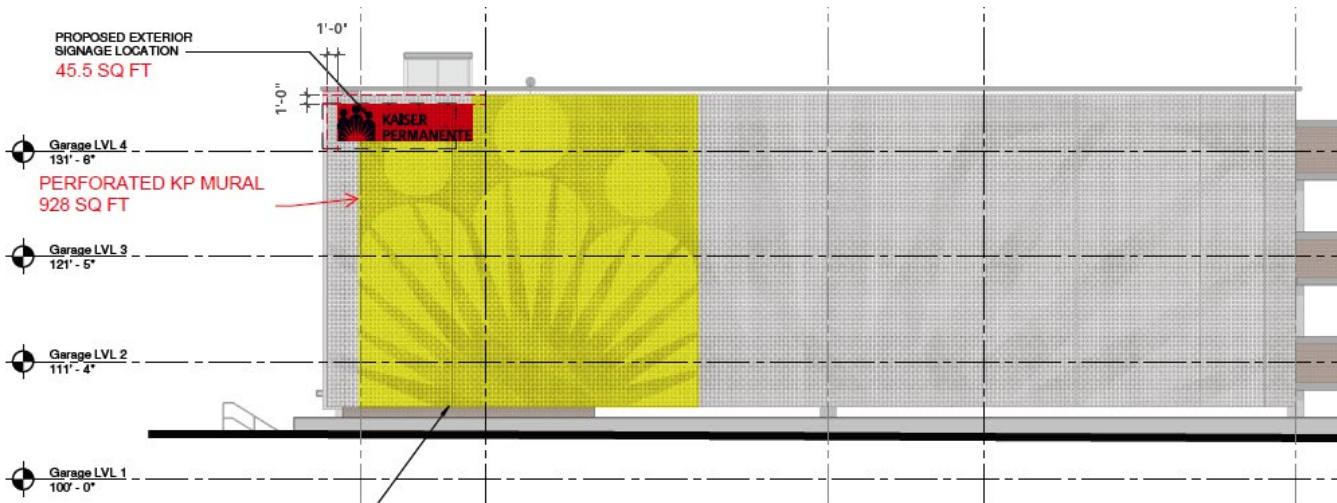
I. Background

The subject Detailed Site Plan and site renderings depict a sign along the eastern façade of the proposed parking garage expansion featuring the Kaiser Permanente logo. This sign is composed of internally illuminated channel letters and is approximately 45.5 square feet in total area. The sign is located at the top floor of the proposed parking garage and is intended to identify the facility along the Mercantile Lane streetscape.

The Detailed Site Plan also proposes a three dimensional perforated graphic panel art wall which is incorporated into the western façade of the parking garage architecture along Mercantile Lane. This art wall is intended to, again, introduce an artistic element into the architecture, activate the streetscape along Mercantile Lane, and add visual interest to the proposed parking garage expansion. The proposed image, which will be incorporated into the art wall, is the Kaiser Permanente Logo.



Art wall along the western façade of the parking garage.



Art wall signage highlighted in yellow.

II. Development District Standard Modifications

To the extent that the proposed art wall may be considered signage because it utilizes the company logo, modifications to applicable Signage Design Criteria Standards are proposed below:

MODIFICATION REQUESTS

Signage Design Criteria

General Provisions

Required: *The total sign area allowed per building shall be computed on the basis of two square feet of sign area for each one linear foot of building frontage. Where a building has multiple frontages, the allowed sign area should be distributed proportionally along each building frontage. Buildings with less than 60 linear feet of building frontage may be allowed up to 120 square feet of sign area.*

Provided: A modification to the General Provision Standard for signage area is necessary to accommodate the perforated graphic panel art wall along the western façade of the parking garage. This subject art wall is 2,960 square feet, of which 928 square feet is the distinguishable perforated Kaiser Permanente logo. This perforated logo exceeds the Standard's 654 square foot allotment along Mercantile

Lane by 274 square feet. As noted, this art wall is intended to be artistic and whimsical in nature and create a welcoming entrance to visitors of the facility. Importantly, the sign advances the Master Plan's streetscape activation goals by screening the structured parking facility and adding visual interest along Mercantile Lane. Accordingly, a modification of this Standard to permit the 928 square foot art wall will enhance the proposed development and promote the purposes of the Development District. Please note that a modification to this Standard is not necessary to accommodate the illuminated identity signage, as this sign is within the allotted 654 square feet.

Required: *Repetitious signage information on the same building frontage should be avoided regardless of the sign area square footage allowed.*

Proposed: If the perforated graphic art wall is indeed characterized as signage, a modification to the General Provision Standard for repetitious signage is likely necessary to accommodate the illuminated identity signage. The art wall is intended to, again, introduce an artistic element into the architecture along the Mercantile Lane streetscape. The proposed image, which will be incorporated into the art wall, is the Kaiser Permanente logo. Since this logo is utilized in both signs along the western façade of the garage, a modification to the subject Standard is requested. Although the logo is utilized twice at this location, each sign is unique and functions separately along the streetscape. This modification allows for the introduction of unique, creative architectural treatments which will provide for a more inviting and attractive streetscape, as well as valuable identification for pedestrian and vehicular traffic. Accordingly, a modification to this Standard benefits the proposed development and advances the goals of the overall Development District.

III. Conclusion

The Applicant respectfully requests that the Planning Board approve the proposed modifications to the Development District Standards for signage. As discussed above, the proposed alternative Development Standards for signage will benefit this development and the greater Largo Town Center Development District. Moreover, as discussed in the Statement of Justification, the proposed development advances numerous goals and objectives of the Master Plan and will not substantially impair its implementation. The above analysis and submitted plans establish that the subject DSP satisfies the required findings that the Planning Board must make to apply alternative Development Standards.

I. DEVELOPMENT DISTRICT STANDARDS / PROPOSED MODIFICATIONS

As to the language in the Development District Standards chapter of the Master Plan (Chapter 8), the following analysis discusses the compliance of the subject DSP with the Development District Standards for Building Envelopes and Blocks, Streetscapes, Architecture, and Parking.¹ The Property is located within the Master Plan’s designated Northeast Quadrant Focus Area.²

Section 27-548.25(c) provides that as part of a detailed site plan in a Development District Overlay Zone, the applicant may ask the Planning Board to apply Development Standards which differ from mandatory requirements in the Development District Standards, unless the Sectional Map Amendment specifically provides otherwise. The Planning Board shall find that the alternate Development District Standards will benefit the development and the Development District and will not substantially impair implementation of the Master Plan, Master Plan Amendment, or Sector Plan. As identified below, the Applicant has noted several Development District Standards for which the Applicant is requesting that the Planning Board apply alternate development standards.

Below are the applicable Development District Standards and necessary modifications, due in part to the proposed improvements being an expansion of an existing medical office building constructed under the prior I-3 zoning standards. Development Standards are *italicized*, with the Applicant’s proposed modifications listed in blue text.

¹ See Master Plan, pages 127-174, “Development District Standards.”

² *Id* at page 128, “Map 29.”

<u>Urban Design Criteria / Block Length</u>		
<p><i>Smaller blocks help create a walkable, engaging environment for pedestrians and provide better access for cyclists, drivers, and emergency vehicles through a connected street grid. Blocks should also be sized to allow for successful, functional development and building configurations that accommodate parking mid-block. For the Largo Town Center, block length is measured from BTL to BTL or to the edge of a defined public open space. (See Map 30: Illustrative TOD Core Concept Plan on page 134 and Urban Design Criteria-BTL on page 133.)</i></p>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p><i>1. Alleys or service drives are included within blocks and do not divide one block from another.</i></p>	<p>There are no alleys on the site. The service drive is on-site and does not divide one block from another.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>2. Common access easements (CAE) and pedestrian passages equal to or greater than 50 feet in width that divide the block and connect from one street or public open space to another are excluded from block measurements. (See Street Types on page 144 and Open Space Types on page 153 for additional criteria regarding CAE and Pedestrian Passages.)</i></p>	<p>Block measurement criteria is noted.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. No block length shall be greater than 450 feet without a CAE or pedestrian passage providing through access to another street, CAE, or public open space. In the east area, the block perimeter (the sum of a block's length) should not exceed 1,350 feet.</i></p>	<p>The Applicant is requesting a modification to this Development District Standard to amend the block length maximum to <u>675 feet</u>.</p>	<p>Modification 1: The existing block along Mercantile Lane is 671 feet long, and the proposed project will not impact the block length, street frontage, or site use.</p>

<i>Urban Design Criteria / Build-to Line</i>		
STANDARD	CONFORMANCE	JUSTIFICATION / MODIFICATION
<p>1. <i>The front Build-to Line (BTL) governs the placement of buildings along streets or open spaces and shall be measured from the existing street curb or open space edge. (See Figure 8. Plan View of Build-to Line Placements on page 136.)</i></p>	<p>Pursuant to Urban Design Criteria Standard 6, a building should be located 15 feet back from the Pedestrian Zone. The placement of the new garage bay is approximately 45 feet from the Pedestrian Zone. The Applicant is requesting a modification to this Development District Standard to amend the BTL placement to <u>45 feet</u>.</p>	<p>Modification 2(a): The subject property has existing buildings located in the middle of the site that were approved in accordance with previous I-3 Zoning. This DSP proposes a new garage bay to be constructed within 45 feet of the Pedestrian Zone, along Mercantile Lane. The proposed garage bay brings the building closer to the street, and promotes a more continuous street façade, as recommended by the Largo Town Center Master Plan and Development District Standards. In addition to bringing the building closer to the pedestrian zone, the Applicant also proposes to activate the street through the addition of 3 seating areas, which further enhance and connect to the existing pedestrian loop. Within these seating areas, the Applicant proposes benches, bollard lighting, and landscaping. These improvements advance the brand of New Urbanism envisioned by the Largo Town Center Master Plan.</p>
<p>2. <i>In the event this BTL falls within an existing or proposed right-of-way (ROW) and/or public use easement (PUE), a new BTL for the subject property shall be established within one foot of the outermost boundary of whichever public</i></p>	<p>The proposed, approximately 45’ BTL does not fall within an existing or proposed right-of-way or public use easement.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p><i>use zone (ROW or PUE) is furthest from the centerline and drive aisles are not considered to be part of the ROW.</i></p>		
<p><i>3. Expansions to existing buildings should be designed to emphasize the street either by redefining the street edge along the BTL (e.g., placing the expansion along the street front of the existing building to the extent feasible) or providing landscaped open spaces such as plazas, café seating areas, and other appropriate amenities that will reinforce the street as a major element of the public realm. If any such spaces or amenities are proposed, the applicant may shift the BTL back to a distance not to exceed 24 feet to accommodate the design and placement, and such landscaped spaces shall conform with the requirements of the Landscape Manual.</i></p>	<p>The new garage building front is approximately 45 feet from the Pedestrian Zone. This DSP proposes landscaping improvements and pedestrian street furniture along the Property’s frontage on Mercantile Lane that are appropriate for this area of the Largo Town Center DDOZ. The Applicant is requesting a modification to this Development District Standard to amend the BTL maximum distance to <u>45 feet</u>.</p>	<p>The subject property has existing buildings located in the middle of the site that were approved in accordance with previous I-3 Zoning. This DSP proposes a new garage bay to be constructed within 45 feet of the Pedestrian Zone, along Mercantile Lane. The proposed garage bay brings the building closer to the street, and promotes a more continuous street façade, as recommended by the Largo Town Center Master Plan and Development District Standards. In addition to bringing the building closer to the pedestrian zone, the Applicant also proposes to activate the street through the addition of 3 seating areas, which further enhance and connect to the existing pedestrian loop. Within these seating areas, the Applicant proposes benches, bollard lighting, and landscaping. These improvements advance the brand of New Urbanism envisioned by the Largo Town Center Master Plan.</p>

<p>4. <i>The area between the curb and building contains a Tree Zone (an area for street trees and plantings), a Pedestrian Zone (a clear route for unobstructed pedestrian circulation), a Semi-Private Zone.</i></p>	<p>The area between the curb and the existing and proposed garage contains a Tree Zone, a Pedestrian Zone, and a Semi-Private Zone as defined in the design criteria.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>5. <i>The Semi-Private Zone is an area between the pedestrian circulation route and the building façade where landscaped plantings, café tables, storefront displays, and similar may be placed, making the transition between the public and private realms.</i></p>	<p>The semi-private zone on this site is located between the sidewalk and the proposed garage, and is a grass berm with plantings. It serves as a buffer between the existing sidewalk and new garage bay.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>6. <i>BTLs shall be located within 15 feet back from the Pedestrian Zone, and the full width from face of the curb to building front should not exceed 25 feet. (See the Street Design Criteria on page 152 for additional information.)</i></p>	<p>The Applicant is requesting a modification to this Development District Standard to amend the BTL maximum distance to <u>45 feet</u> from the Pedestrian Zone.</p>	<p>Modification 2(c): The subject property has existing buildings located in the middle of the site that were approved in accordance with previous I-3 Zoning. This DSP proposes a new garage bay to be constructed within 45 feet of the Pedestrian Zone, along Mercantile Lane. The proposed garage bay brings the building closer to the street, and promotes a more continuous street façade, as recommended by the Largo Town Center Master Plan and Development District Standards. In addition to bringing the building closer to the pedestrian zone, the Applicant also proposes to activate the street through the addition of 3 seating areas, which further enhance and connect to the existing pedestrian loop. Within these seating areas, the Applicant proposes benches, bollard lighting, and landscaping. These improvements advance the brand of New Urbanism envisioned by the Largo Town Center Master Plan.</p>

Urban Design Criteria / Frontage

Maintaining street walls is important for the creation of vibrant streetscapes with good urban form. Frontage defines the way a building engages the public realm. Frontage refers to any building elevation or element, yard, area, fence, and similar that faces a street or open space. A corner building located at the intersection of two streets has two front elevations. The frontage criteria address both the percent of the block is occupied by building at the BTL) and the frontage type (what use occupies the ground-level floor along the BTL).

The TOD Concept Plan depicts the frontage types. (See Map 7: Illustrative TOC Core Concept Plan on page 24.) Storefront frontage is shown along a portion of North Harry S. Truman Drive Extended and near the Metro station. Storefront indicates where retail shops and restaurants are required on the ground floor, minimally to a depth of 30 feet back from the frontage elevation. Development within the TOD core with street frontage along Lottsford Road, Arena Drive, or Harry S. Truman Drive must face the street and include nonresidential development on the ground floor of the development on the site. Development on the property directly abutting the Capital Beltway right-of-way must face Harry S. Truman Drive (extended) and include non-residential development on the site. Office, institutional, hotel, and residential lobbies may be placed within the store frontage on the ground floor but should not dominate the façade. Upper stories may contain any use permitted in the applicable zoning category.

Development within the TOD Core with frontage abutting open space contiguous to the Largo Metro Station must face the metro station site and include non-residential development on the ground floor of the development on the site. Development within the TOD core with frontage abutting open space contiguous with the Regional Medical Center must be oriented with building frontage to face the hospital facility site and include nonresidential development on the ground floor of the development on site.

The remaining portions of the plan area are designated as general frontage. General frontage does not require a specific use on the ground floor or upper floors; any use permitted in the applicable zoning category may be included.

Civic buildings are exempt from all setback criteria. However, regardless of use, no new building shall be closer than 30 feet to any existing single-family detached or attached residential unit.

The percent of building frontage required along the BTL varies by quadrant/plan area following the criteria below.

<i>SOUTHEAST, NORTHEAST, AND NORTHWEST QUADRANTS</i>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p>1. <i>The percent of building frontage shall be 70-100 percent of block length (or individual lot).</i></p>	<p>The Applicant is requesting a modification to this Development District Standard to amend the minimum building frontage to <u>45%</u>. This is the percentage of the block frontage that is occupied by the existing and proposed garage.</p>	<p>Modification 3: The Applicant requests a modification to 45% minimum building frontage to accommodate existing development on-site. Both the existing block length and existing garage length are unchanged. Accordingly, the existing frontage percentage is unchanged as a result of this project. This project is adding one (1) garage bay to the existing garage over an existing surface lot to bring the garage closer to the street. In both the existing and proposed development, the block length is 671 feet and the garage length is 329 feet for a 49% ratio. This ratio is appropriate to accommodate the existing uses on-site, which were approved pursuant to previous I-3 Zoning at the Property.</p>
<p>2. <i>Building recesses, up to 14 feet back from the BTL or required front setbacks are permitted for no more than 25 percent of the required building frontage</i></p>	<p>There are no building recesses associated with the proposed parking garage.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Corner towers or bays may project up to 10 feet forward of the BTL and other building elements may project up to 4 feet forward of the BTL within the Semi-Private Zone for no more than 25 percent of the required building frontage.</i></p>	<p>There are no corner towers or bays projecting forward of the BTL.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p><u>Other Setbacks</u> <i>While BTLs determine building front setbacks, other parking, side, and rear setbacks apply as follows, varying by subarea from core to edge.</i></p>		
<p><i>Southeast, Northeast, and Northwest Quadrants</i></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Similar to the TOD core, in the southeast quadrant commercial and multifamily residential buildings shall be set back a minimum of 10 feet from the rear property line.</i></p>	<p>The project is not in the southeast quadrant, and the buildings are set back more than 10 feet from the rear property line.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>No new building shall be closer than 30 feet to any existing single-family residential area.</i></p>	<p>The proposed buildings are greater than 30 feet from any existing single-family residential area.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Building Heights</u> <i>For new development, building heights are governed by the Building Heights Plan Diagram (See Map 31: Building Heights on page 142.) Building height is measured in stories, with an ultimate limit in feet measured relative to the main entrance grade elevation. Attic storied do not count against the maximum story limit, <u>however mezzanines (greater than one-third of the associated story's floor area), penthouses, and lofts do count against the maximum story limit.</u> (See Map 31: Building Heights on page 142.) Story height is measured between one floor level and the next floor level, or if there is no floor above the ceiling or roof above. <u>(emphasis added)</u>.</i></p>		
<p><i>Southeast, Northeast, and Northwest Quadrants</i> <i>Buildings shall be at least 4 stories in height, but no greater than 8 stories or 130 feet in height except for pavilions (open-air or enclosed) up to 6,000 square feet or civic buildings that shall be a minimum of 2 stories or 30 feet in height. For single-family residential lots, garages may be one story in height. For commercial uses, the ground level should have an interior clear</i></p>		

<p><i>height (floor to ceiling) of at least 14 feet contiguous to the BTL frontage, for a minimum depth of 20 feet. The maximum ground-level story height for commercial uses is 22 feet.</i></p>		
<p><i>Story and Clear Height Requirements</i></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p><i>1. For commercial uses, the ground level should have an interior clear height (floor to ceiling) of at least 14 feet contiguous to the BTL frontage, for a minimum depth of 20 feet. The maximum ground-level story height for commercial uses is 22 feet.</i></p>	<p>The expansion within view to the public realm is a four (4) story structured parking garage, with levels that align with the existing garage.</p>	<p>Modification 4: The proposed garage expansion will maintain existing clear heights, which were approved pursuant to previous I-3 Zoning on-site, to align with the established floor (parking deck) levels and allow for contiguous and connected vehicular circulation throughout the parking structure.</p>
<p><i>2. For residential uses, the ground level should have an interior clear height (floor to ceiling) of at least 9 feet. The maximum ground-level story height for residential uses is 22 feet.</i></p>	<p>This project is not residential use.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. For all upper stories, the maximum story height should be 18 feet.</i></p>	<p>The expansion within view to the public realm is a four (4) story structured parking garage, with levels that align with the existing garage. The stair enclosure at the top level has an overall height of 9’-6” above the top level driving surface.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

Street Design Criteria

Complete Streets

Streets not only provide circulation but also are the community’s primary and most frequently used open space, which should be designed accordingly to serve this purpose with comfortable, shaded sidewalks and plaza areas. Connecting and extending existing streets appropriately is an important component of creating a functioning street grid as discussed in the Recommendations Section. (See Chapter 4. Recommendations for Future Development on page 63.) As existing streets are reconfigured and new streets are built, it is critical that they be designed and constructed as complete streets in order to balance the needs of all users, including pedestrians and cyclists and motorists.

Complete streets, or shared-use streets, are designed to provide for transit, pedestrian, cyclist, and private motor vehicle use and may also incorporate innovative stormwater management methods, such as rainwater planters, to address run-off from paved surfaces. (See Figure 11. Complete Streets Diagram.)

Parallel parking is required on all streets with retail frontage. (See Urban Design Criteria- Frontage on page 137 and Map 7: Illustrative TOD Core Concept Plan Diagram on page 24.) Further, parallel parking is encouraged on all area streets, except as noted in Neighborhood Street C (see Figure 16: Neighborhood Street Con page 148) or where unfeasible due to particular site constraints. Parallel parking along the block face shall not count toward the parking maximums for the adjacent development but may be counted towards the parking minimums (if counted, spaces shall be applied to one adjacent development project only). (See Parking Design Criteria on page 165.)

<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p><i>1. Parallel parking paving shall be either consistent in material with the travel lane paving or differentiated through a change in material (preferred). Additionally, an edge band denoting the border between the travel lane and parallel parking spaces is encouraged and may be differentiated by color and/or material. Porous pavement or permeable paver systems are encouraged within parallel parking spaces.</i></p>	<p>There is no parallel parking along the street frontage of the property. The project is not impacting the public right-of-way.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p>2. <i>The Tree Zone shall include a 2-foot-wide paved step-off zone adjacent to parallel parking. The Tree Zone may increase to 10 feet wide to accommodate particular site constraints or larger rainwater planters. (See Street Design Criteria- Tree Zone on page 150 for more information on rainwater planters).</i></p>	<p>Not applicable. There is no parallel parking along the street frontage of the property.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>The concept plan locates the street types, Neighborhood Streets A, B, and C within the TOD core. (See Map 7: illustrative TOD Core Concept Plan on page 24.) Other street types may be created to address unique conditions in the remaining portions of the plan area, and the street types in the TOD core may be adjusted but shall be based on the criteria in the standards. The street types and locations shall be shown in detailed site plan documents.</i></p>	<p>Not applicable. The project is not located within the TOD core, and the existing street frontage is not impacted as part of the proposed project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>All street components shall comply with the Prince George's County Specifications and Standards for Roadways and Bridges and/or the latest addition of the American Association of State Highway and Transportation Officials Guide for the Development of Bicycle Facilities.</i></p>	<p>To our knowledge, all street components are compliant. Existing street components were approved under a previous DSP, and are not impacted as part of the proposed project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<u>Crosswalks</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. All new street intersections shall have crosswalks to existing sidewalks or new sidewalks except in situations where there is no traffic control device.</i>	There are no new street intersections proposed for the project.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Crosswalks throughout the TOD core may be of a different material, texture, or color from the travel lanes, but the material chosen should be consistent throughout the TOD core.</i>	Not applicable. The project is not located in the TOD core.	The Applicant is not proposing a modification or alternative Standard.
<u>Curbs</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Bulb-outs shorten the pedestrian crossing distance curb to curb and are preferred at all intersections except where there are extenuating design considerations (such as accommodating the turning radius for transit, emergency, or large delivery vehicles).</i>	No intersections are impacted by the proposed project. Bulb-outs are not currently provided at existing intersections so turning radii can be provided.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Neighborhood streets (see Figures 14-16) should have 15- to 20-foot curb radii.</i>	No curb radii for public streets is impacted by this project.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Curb radii for arterial intersections shall be determined by the Department of Public Works & Transportation. The curb radii for expressway intersections shall be determined by the State Highway Administration (SHA).</i>	No curb radii for public streets is impacted by this project. Existing street geometry was previously approved by DPW&T.	The Applicant is not proposing a modification or alternative Standard.

<u><i>Tree Zone</i></u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Trees are required along all new and reconfigured streets in the Largo Town Center DDOZ.</i>	Not applicable. There are no new or reconfigured streets as part of this project.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Generally, street trees should be planted 25 to 35 feet on center and should be no more than 40 feet on center. Variation in tree spacing may be appropriate depending on the location and adjacent uses, underground utilities, and above-ground structures. The placement of street trees shall be coordinated with the placement of street lights and signs.</i>	Street tree placement was approved under a previous DSP, and no existing street trees are impacted by this project.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Street tree planting pits or strips shall be a minimum of 4 feet in width and a minimum of 28 square feet in overall size. Planting pits are appropriate in mixed-use areas and high-volume pedestrian areas. Planting strips are appropriate only in residential areas and areas with low-volume pedestrian activity.</i>	Not applicable. There are no tree planting pits or strips along the project frontage, and this project does not impact the existing street frontage.	The Applicant is not proposing a modification or alternative Standard.
<i>4. Planting pits may have grates, permeable pavers, or may be planted. (See Figure 17: Tree Pit Diagram on page 149.) Rainwater planters are encouraged throughout the plan area. (See Figure 19: Rainwater Planter Diagram on page 151.) Generally, street planter and pit materials and details throughout TOD core should be uniform to promote a consistent</i>	Not applicable. There are no tree planting pits or strips along the project frontage, and this project does not impact the existing street frontage.	The Applicant is not proposing a modification or alternative Standard.

<p><i>character and identity within the transit center neighborhood.</i></p>		
<p>5. <i>The Tree Zone (see Figure 11 on page 143) accommodates permanent features such as street trees, rainwater planters, light poles, signage, benches and bike racks. This zone may also incorporate non-permanent elements, including restaurant menu signs, waste and recycling receptacles, potted plants, and additional seating.</i></p>	<p>The existing tree zone along the property frontage accommodates street trees, light poles, signage, fire hydrants, and mailboxes. The existing tree zone is not impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>6. <i>Refer to the Landscape Manual for approved tree species. Trees and other plantings within state controlled and maintained rights-of-way shall meet SHA standards.</i></p>	<p>Street trees are existing and not impacted as part of this project. Tree species were approved under a previous DSP.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Pedestrian Zone</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>The Pedestrian Zone (see Figure 11 on page 143) is reserved for pedestrian circulation and shall remain clear of all street furniture, signs, and similar obstructions.</i></p>	<p>The existing pedestrian zone consists of a sidewalk along the property frontage, and is clear of obstructions. This zone will not be impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Street sidewalks shall be constructed of concrete or brick pavers, stone, exposed aggregate concrete, or brushed concrete. Porous pavement and permeable paver systems are encouraged where appropriate; however, porous asphalt is not allowed for sidewalks.</i></p>	<p>The existing street sidewalks along the property frontage are concrete and will not be impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<i>Street Furnishings</i>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Street furnishings, including transit shelters, bike racks, benches, bollards, tree grates, trash/recycling receptacles, and similar shall be consistent in material, style, and color throughout the TOD core and are encouraged to be consistent throughout the four quadrants of the DDOZ west of Landover Road.</i>	The project is not located in the TOD core. Existing street furnishings will not be impacted by the proposed project.	The Applicant is not proposing a modification or alternative Standard.
<i>2. East of Landover Road, the street furnishing should coordinate with those existing or planned in the surrounding area.</i>	Not applicable. The project is not located east of Landover Road.	The Applicant is not proposing a modification or alternative Standard.
<i>3. All street furnishings shall be metal (aluminum, steel, or cast iron).</i>	The proposed benches along Mercantile Lane will be located on-site and are not considered street furnishings.	The Applicant is not proposing a modification or alternative Standard.
<i>4. Benches, tables, chairs, planters, and similar belonging to commercial tenants or placed within open spaces should vary in appearance from the standard street furnishings; however, if these furnishings are placed within or abutting a street or open space, they should be metal (aluminum, steel, or cast iron), a combination of wood and metal, stone, or other durable material.</i>	On-site tables, benches, and planters in open spaces are distinct from any furnishings that may be within the streetscape.	The Applicant is not proposing a modification or alternative Standard.
<i>5. All street components shall comply Waste and recycling receptacles shall be coupled</i>	There are no existing waste and recycling receptacles along the	The Applicant is not proposing a modification or alternative Standard.

<p><i>together conveniently located along streets. Waste and recycling receptacles shall be metal or a combination of wood and metal, and for sanitation purposes, receptacles shall have a rain guard over the main opening in order to conceal the main recycling or trash container.</i></p>	<p>streetscape. No waste and recycling receptacles are proposed along the streetscape.</p>	
<p><u>Streetscape Lighting</u> <i>A combination of pedestrian-scaled street light fixtures and intersection street lights fixtures will be required to ensure a well-lit street area and to establish a unifying element along the street.</i></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p><i>1. Pedestrian-scaled fixtures no higher than 14 feet should be used on all streets. Intersection and/or travel lane focused lighting may be higher.</i></p>	<p>Nine (9) existing street lights exist along the property frontage (or directly across the street). Existing streetlights were approved under a previous DSP, and will not be impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>2. Street light locations shall be coordinated with underlying utility locations and street plantings and should be placed to ensure even distribution of lighting levels.</i></p>	<p>Existing streetlights were coordinated with utilities and landscaping, approved under a previous DSP, and will not be impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. Light poles may include armatures that allow for the hanging of banners and other amenities (i.e., hanging flower baskets).</i></p>	<p>Noted. There are no armatures on the existing streetlights along the property frontage, and the lights will not be impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>4. LED fixtures are recommended with a targeted wattage between 70 to 100 watts on pedestrian-scaled poles. Higher wattage fixtures may be used on taller pole locations</i></p>	<p>Noted. Existing streetlights will not be impacted by this project. Wattage is unknown.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<i>where required.</i>		
5. <i>All street lights shall use full cut-off optics to direct their light downward to minimize or eliminate glare and light pollution.</i>	Existing streetlights will not be impacted by this project. It is unknown if the existing lights use full cut-off optics to direct their light downward.	The Applicant is not proposing a modification or alternative Standard.
<p><u>Alleys/Loading</u> <i>Alleys (service lanes) provide vehicular access; however, unlike streets, alleys occur within blocks, are more utilitarian in character, and are not considered part of the public realm.</i></p>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>Views into alleys should be limited or screened from streets, public plazas, squares, greens, or pedestrian passages.</i>	Not applicable. There are no alleys on the property.	The Applicant is not proposing a modification or alternative Standard.
2. <i>Alleys serving single-family detached or attached residential areas shall be no wider than 30 feet measured building to building at the alley-street intersection.</i>	Not applicable. There are no alleys on the property.	The Applicant is not proposing a modification or alternative Standard.
3. <i>At alley curb entryways, the street sidewalk material(s) shall be carried across the access lane where possible.</i>	Not applicable. There are no alleys on the property.	The Applicant is not proposing a modification or alternative Standard.
4. <i>Service for small businesses and retail establishments (under 6,000 square feet) is permitted at the front door provided such service is not during primary business hours and does not adversely disrupt traffic movement.</i>	Not applicable. The site use is not a small business or retail establishment.	The Applicant is not proposing a modification or alternative Standard.
5. <i>Off-street loading areas that make it necessary for vehicles to back out directly into a street are</i>	The existing loading area in the back of the building does not force vehicles to	The Applicant is not proposing a modification or alternative Standard.

<i>discouraged.</i>	back out directly into a street.	
<p><u>OPEN SPACE DESIGN CRITERIA</u> <i>Open spaces are critical in helping define a neighborhood’s sense of place. Open spaces can be as small as an outdoor living room or large enough to hold a communitywide event. For a rich urban environment, a variety of open spaces are needed.</i></p>		
<p><u>General</u></p>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>ALTERNATIVE STANDARD</u>
<p>1. <i>Public open spaces, including plazas, squares, and greens, should constitute no less than eight percent of the net lot area of each subarea (as shown in Map 29: Sector Plan Subareas on page 128) No open space may be counted that has a width or breadth dimension less than 25 feet.</i></p>	<p>The property consists of about 15% open space that is not impacted by the proposed project. There is a walking trail along the south and east sides of the property that traverses open green space.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Open spaces should be evenly distributed throughout the Largo Town Center area.</i></p>	<p>Acknowledged.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>New open spaces should be designed with sustainable features (i.e., rainwater planters, bioswales, and porous pavement).</i></p>	<p>Existing open spaces within the property are not impacted by this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Public/private partnerships should be formed in order to ensure appropriate funding and maintenance is provided for the public spaces envisioned within the Largo Town Center sector plan area including the Greenway trail, the Largo Town Center Lake Park, the one-acre “main square” civic facility abutting the Largo Metro Station, and the iconic focal points that herald arrival and a sense of place at the Largo Town Center TOD core: (1) Harry S Truman Drive/ Lottsford Drive, and (2)</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<i>'the Green' at the north entrance of the TOD core at Arena Drive.</i>		
<u>Open Space Types</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Plazas are public open spaces typically located at important street intersections and defined by building façades. Plazas should contain primarily pavement and may contain site furnishings, public art, fountains, and trees, all formally arranged.</i>	Noted. There are no plazas on the property.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Squares, like plazas, are typically located at important street intersections and are defined by building façades. Squares consist of more planted landscape areas than paved hardscape areas. Squares may contain site furnishings, public art, fountains, and trees in a formal arrangement.</i>	Noted. There are no squares on the property.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Greens are typically smaller and quieter with less intense activities than squares and contain more planted landscape areas than paved hardscape areas (less than 50 percent hardscape). Greens may be defined with a combination of landscaping and buildings.</i>	Noted. There are existing greens along the south and east sides of the property. These spaces are not impacted by this project.	The Applicant is not proposing a modification or alternative Standard.

<p>4. <i>Passages are linear open spaces that typically make a pedestrian connection along a tree-lined walk from one street to another or to an open space. Passages may be either formal or informal in their design, reflecting the character of the surrounding.</i></p>	<p>Noted. There are two areas on the property that could be considered “passages”. There is an existing walking trail along the south and east sides of the property that is not impacted by this project, and there is a boardwalk connecting the parking garage to the existing building that will be upgraded as part of this project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
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Hardscape

<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p>1. <i>Hardscape areas within open spaces are encouraged to differ from and contrast with the typical street sidewalk paving.</i></p>	<p>Open spaces on the property are not impacted by this project. The existing pavers at the building entrance contrast with the adjacent street and sidewalk paving.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Hardscape in open spaces should be concrete or brick pavers, stone, exposed aggregate concrete, brushed concrete, or other innovative paver type. Asphalt is not allowed in squares and plazas.</i></p>	<p>Open spaces on the property are not impacted by this project. There is no asphalt in squares or plazas.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

Landscape

<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p>1. <i>Landscaping, screening, and buffering in Largo Town Center shall conform to the Landscape Manual requirements for landscape plan submission process, recommended tree and plant selections, quantities, and similar.</i></p>	<p>The landscape design is in accordance with the Landscape Manual requirements.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p>2. <i>Rainwater gardens and other forms of bioretention should be encouraged in open spaces, where appropriate, in a manner that is consistent with an urban environment.</i></p>	<p>Bioretention is used where possible on the site to meet local SWM requirements. New surface bioretention facilities are proposed to treat the rear parking lot and the new building addition, and there are structured bioretention planters along the frontage of the proposed parking garage bay.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
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ARCHITECTURAL DESIGN

The architecture design criteria applies only to exterior, front elevations and building elements clearly visible from the public realm. The purpose of the criteria is to create a consistent, high-quality built environment that respects the surrounding context and supports the vision for a vibrant and unique urban center. Buildings should be designed in a healthy manner, use natural resources effectively, and be adaptable over time to serve future uses. The leadership in energy and environmental design (LEED) green building rating system is a nationally accepted benchmark for the design, construction, and operation of high-quality performance green buildings and should be a rating system encouraged at the Largo Town Center.

The criteria guides the design and character of all building types, except civic structures, which are excluded. The criteria include requirements that specify acceptable building materials, configurations of materials, and techniques for construction.

Building Form

Multiple architectural features, such as massing, horizontal elements, recesses and projections, corner elements, and fenestration, articulate and define buildings and streetscapes. Massing is the combined height and width of a building, including changes in plane such as projections, recesses, tower, and corner elements. Horizontal elements are horizontal band lines on a building elevation used to demarcate the boundaries between the base, middle, and top; a change in plane; or a change in materials (i.e., cornice lines, belt or water table courses, and band or skirt boards). Recesses and projections are any horizontal change in plane affecting a building’s elevation. Corner elements are pronounced building elements either recessed or projected and taller or shorter located at significant intersections, gateways, or open spaces. Fenestration defines the relationship between continuous wall surface (solid) and windows and doors (void).

<u>Massing</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>Buildings should predominantly define the perimeter of the block and be generally continuous along the BTL or front setback. (See the Urban Design Criteria on page 133 for location-specific frontage requirements.)</i>	The expansion within view to the public way is a (4) story structured parking garage, with levels and dimensions that align with the existing garage.	The Applicant is not proposing a modification or alternative Standard.
2. <i>Buildings should transition from more continuous in the TOD core near the transit station to more porous moving out to the edges, particularly the east area.</i>	This project is not in the TOD core, and is in alignment with the guideline being more porous.	The Applicant is not proposing a modification or alternative Standard.
3. <i>Buildings should take into account their relationship to existing or planned adjacent buildings. Upper story setbacks, changes in materials or color, or changes in the roof plane may be used to transition appropriately.</i>	This structured parking garage expansion is designed to match the existing structured parking garage in detailing, materials, and form. The massing is intended to respect the established hierarchy used to inform wayfinding.	The Applicant is not proposing a modification or alternative Standard.
4. <i>Elevations over 120 feet in length at the BTL shall be visually broken into smaller sections through material and plane changes, variations in window groupings, and/or the addition of bays.</i>	This structured parking garage addition elevation is broken into a series of bays, with recurring openings that enlarge in size at either end (matching the architectural language of the existing parking garage). A perforated metal panel screen is proposed at the north east corner to enhance the elevation and visually inform entry to the KP campus.	The Applicant is not proposing a modification or alternative Standard.

<u>Horizontal Elements</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Buildings shall be composed of a base, middle, and top. Cornices and belt/water table courses shall be used to delineate different building elevation zones.</i>	The expansion within view to the public way is designed with a base, middle and top that matches the existing architectural expression. In addition, a metal cornice is proposed above the perforated metal panel to meet this criteria.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Horizontal band lines shall be used on an elevation where there is a change in primary materials or colors.</i>	Horizontal band lines are designed to match the established detailing of the existing structured parking garage, which meet this criteria.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Transitions between primary elevation materials shall occur along horizontal lines.</i>	Transitions from brick to precast to open occur along horizontal lines.	The Applicant is not proposing a modification or alternative Standard.
<u>Recesses and Projections</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Recesses and projections may be used to reinforce a building’s verticality. However, for large gestures, they should be used sparingly for emphasis.</i>	This design uses a single projection via the perforated metal panel screen.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Recesses and projections should be reflected in variations in the roofline.</i>	The perceived roof line of the structured parking garage is higher at the perforated metal screen projection, and is distinguished with a metal cornice.	The Applicant is not proposing a modification or alternative Standard.

<p>3. <i>Recesses and projections may be used to provide shading and cooling opportunities as well as location for balconies.</i></p>	<p>The perforated metal panel screen provides shading / screening to the parking spaces located behind it.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Changes in materials should occur only at changes in plane. A horizontal band line shall be used where there is a change in primary building materials or colors on an elevation.</i></p>	<p>The perforated metal panel screen occurs at a change in plane.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Corner Elements</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Tower or other corner elements shall be used to terminate an important view or as a focal element.</i></p>	<p>The perforated metal panel screen is used at the NW corner, providing a visual cue to the entry to the campus.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Corner elements may be either recessed or projected and either taller or shorter than the surrounding building elevations.</i></p>	<p>The perforated metal panel screen corner element is projected and taller than the surrounding elevations.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Recesses and projections may be used to provide shading and cooling opportunities as well as location for balconies.</i></p>	<p>The perforated metal panel screen provides shading / screening to the parking spaces located behind it.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Changes in materials should occur only at changes in plane. A horizontal band line shall be used where there is a change in primary building materials or colors on an elevation.</i></p>	<p>The perforated metal panel screen occurs at a change in plane.</p>	

<u>Fenestration</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p>1. <i>The relationship between solid building wall and openings (fenestration) is critical; the ratios should vary according to use and shall be calculated per elevation and floor-to-floor:</i> Percentage of Openings (Windows & Doors): <i>Ground Floor Retail: 60-95</i> <i>Ground Floor Other</i> <i>Commercial/Institutional: 40-90</i> <i>Ground Floor Residential: 15-40</i> <i>Upper Commercial/Institutional: 40-90</i> <i>Upper Floor Residential: 15-60</i></p>	<p>The Percentage of Openings for the existing structured garage will be retained and are as follows: North Elevation: Ground Floor: 34.9% Upper Floors: 31.4% West Elevation: Ground Floor: 15.2% Upper Floors: 25.6% South Elevation: Ground Floor: 26.8% Upper Floors: 20%</p>	<p>Modification 5: The existing parking garage was built pursuant to the I-3 Zone standards. The fenestration size proposed in this DSP is coordinated and consistent with the existing structured parking garage. Furthermore, maintaining this relationship is necessary to retain the structural integrity.</p>
<p>2. <i>The placement and groupings of windows and doors should be used to provide hierarchy and order to building elevations.</i></p>	<p>A large area of glass is provided at the stair (matching other existing conditions in the building) to provide intuitive wayfinding and enhance security for pedestrians inside the garage. The fenestration along the parking elevations are consistent with the existing conditions, provide</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

	recurring openings that expand at the ends to match the variation in bay sizes and enhance the overall composition.	
3. <i>Openings should occur in rhythm with the architectural bays.</i>	The openings create a rhythm in alignment with the architectural bays.	The Applicant is not proposing a modification or alternative Standard.
4. <i>The shape and proportion of the openings should be in keeping with the architectural style of the building.</i>	The openings match the shape and proportions of the existing building.	The Applicant is not proposing a modification or alternative Standard.
5. <i>The shape and proportion of the openings shall be in keeping with the architectural style of the building.</i>	The openings match the shape and proportions of the existing building.	The Applicant is not proposing a modification or alternative Standard.

Architectural Elements / Building Materials and Elements

All new construction within Largo Town Center shall comply with the following materials and elements criteria except storefronts. For storefront criteria, refer to the above section.

Exterior Walls

All new construction within Largo Town Center should comply with the following materials and elements criteria except storefronts. For storefront criteria, refer to the above section.

<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>Exterior walls visible from the public realm should be brick (brick veneer), stone, cast stone, pre-cast, glass, and/ or metal components. Additionally, for buildings of four to six stories, cementitious siding or panels in a smooth_ or stucco finish or metal panels may be used at the fourth floor level and above where residential is the primary use. For buildings of three to five stories, masonry or stone shall be the predominant building material. For buildings of</i>	The structured parking garage expansion matched the existing palette of materials which are brick, precast, glass and metal.	The Applicant is not proposing a modification or alternative Standard.

<p><i>one to two stories, cementitious siding or panels in a smooth or stucco finish may be the predominant building material where residential is the primary use; for other uses, metal panels may be the predominant building material. Where cementitious siding or panels meet foundation walls, a minimum 10 inch nominal trim board is required on all elevations.</i></p>		
<p><i>2. Exterior walls, as they turn the corner from a street or public open space frontage condition to an exterior service area or courtyard condition, should be consistent in material and detail with the frontage façade to a minimum depth equal to the width of the service accessway or courtyard opening (measured building to building).</i></p>	<p>The palette of materials of the existing building and addition are consistent on all elevations.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. Vinyl and aluminum siding products are not permitted as a primary exterior wall material.</i></p>	<p>There is not any vinyl or metal siding on this building.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>4. The use of EIFS on an exterior wall above 22 feet (measured vertically from grade) may be allowed subject to the approval of the Planning Board. However, the use of EIFS on an exterior wall within 22 feet of grade is not permitted. When used, the color(s) of the EIFS should be complimentary, but not identical, to adjacent materials.</i></p>	<p>There is not any EIFS on this building.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>5. Building walls between the foundation and the eave should be no more than three primary materials (e.g., pre-cast on the ground floor,</i></p>	<p>The design of this structured parking garage addition is in alignment with this requirement.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<i>brick on the second through fourth floors, and cementitious panels on the fifth floor).</i>		
6. <i>Lighter appearing material (lighter in color, texture, and/or weight) should be used on top of the heavier appearing materials.</i>	The material palette of this structured parking garage addition is in alignment with this requirement.	The Applicant is not proposing a modification or alternative Standard.
7. <i>Arcades, piers, columns, and pilasters should be stone, cast stone, pre-cast, brick, or composite material.</i>	The pilasters on this addition are brick and precast to match the existing conditions.	The Applicant is not proposing a modification or alternative Standard.
8. <i>Trim should be metal, cementitious fiber board, fiberglass composite, polymer composite, or solid cellular PVC. Wood for exterior trim is not encouraged. The use of aluminum trim on an exterior wall within 12 feet of grade is not permitted.</i>	The design of this structured parking garage addition matches the existing structure. There is not any exterior trim.	The Applicant is not proposing a modification or alternative Standard.
<u>Doors and Entries</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>Primary building entries should be distinct and enhance the building façade. Residential lobby entries may be secondary but shall be identifiable from the street.</i>	The subject DSP is a structured parking garage addition and does not have a primary entrance (the primary entrance is located in the existing structure). The only entrance is provided for egress purposes and matches the established language in the existing portions of the garage.	The Applicant is not proposing a modification or alternative Standard.
2. <i>On single-family residential units, exterior steps to the front door should not exceed eight risers. However, additional risers up to a total of 12, may be included if separated by a</i>	This is not a residential project.	The Applicant is not proposing a modification or alternative Standard.

<i>landing.</i>		
<i>3. All exterior, individual residential unit entry doors shall have glass, recessed panels, or both</i>	This is not a residential project.	The Applicant is not proposing a modification or alternative Standard.
<i>4. Roll down doors should be painted and/or designed to blend with the building façade.</i>	This project does not have any roll down doors.	The Applicant is not proposing a modification or alternative Standard.
<u>Windows</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Windows should be wood, aluminum-clad wood, or aluminum.</i>	This windows are part of an aluminum storefront system that matches the existing conditions of the existing structure.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Windows should be single-, double-, or triple-hung, fixed, or casement.</i>	The windows are fixed, and part of a storefront system.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Windows on residential units shall be operable.</i>	This is not a residential project.	The Applicant is not proposing a modification or alternative Standard.
<u>Shutters</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. In the quadrants west of Landover Road, shutters may be wood or solid cellular PVC composite. In the east area, shutters may also be vinyl.</i>	This project does not have any shutters.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Shutters should be, or appear to be, operable and should be of the required size both</i>	This project does not have any shutters.	The Applicant is not proposing a modification or alternative Standard.

<p><i>horizontally and vertically to cover the opening if closed.</i></p>		
<p><u>Roofs</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p><i>1. Roofs should preferably be flat (except on single-family residential units) or symmetrically pitched between a 6:12 and 14:12 slope and only in the configuration of gables and hips.</i></p>	<p>The only roof on this project is at the egress stair and is a flat roof.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>2. Flat roofs should be a white or light membrane material, should have light-colored pavers or aggregate, and/or may be vegetated.</i></p>	<p>The flat roof at the egress stair will be a light color.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. The rooftop installation of solar thermal or photovoltaic energy systems is encouraged wherever practical.</i></p>	<p>This project does not have any integrated solar systems.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>4. Roofs (with the exception of mid-block end lot townhouses) should overhang a gable end a minimum of 12 inches measured from the face of fascia board to the face of the building wall.</i></p>	<p>This project does not have a gable end.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Porches, Stoops, and Bays</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>

<p>1. <i>Porches, stoops, and bays are encouraged. These elements may occur forward of the BTL but may not extend into the right-of-way. Porches shall be a minimum depth of eight feet. Stoops shall be a minimum depth of four feet. Bays shall be a minimum depth of three feet.</i></p>	<p>This project does not have any porches, stoops, or bays.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Porches, stoops, and/or bays should match the architectural style and detailing of the primary building.</i></p>	<p>This project does not have any porches, stoops, or bays.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Bays on elevations fronting the public realm shall extend to the ground, extend to the retail cornice, or be structurally supported by brackets.</i></p>	<p>This project does not have any porches, stoops, or bays.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Service and Loading Areas</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Service and loading areas shall be located in the interior of blocks or enclosed within the building and shall be screened from public realm view by walls, fencing, and/ or landscaping; or minimized along a street edge and screened by an overhead door.</i></p>	<p>The existing service and loading area is located in the interior of the site behind the existing building, and is screened from MD-202 by walls and landscaping. This area will not be impacted by the proposed project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Trash enclosures and other ancillary service structures shall be located away from streets and public open spaces and screened from view using walls and/or landscaping. Enclosure walls shall be brick, architectural concrete block, or steel.</i></p>	<p>The existing trash enclosures are located in the interior of the site behind the existing building, and is screened from MD-202 by brick walls and landscaping. This area will not be impacted by the proposed project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<u>Utility and Mechanical Equipment</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. All new permanent utility lines shall be installed underground where feasible.</i>	Utilities will be located underground.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Above-ground utility equipment shall be located away from the public realm to the greatest extent possible. Additionally, transformers shall be located away from major pedestrian routes and outdoor seating areas. If equipment is located within 15 feet of the front façade of a building, screening measures shall be used to ensure that the equipment is visually minimized.</i>	Any utility equipment will be out of the public realm. No additional transformers are anticipated for this project.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Commercial antenna and communication towers may be permitted subject to applicable zoning and other regulations.</i>	This project does not have any commercial antenna or communication towers.	The Applicant is not proposing a modification or alternative Standard.
<i>4. Electrical and mechanical equipment, other equipment, enclosed stairs, storage spaces, blank walls, and other elements that are not pedestrian-oriented should be located in alleys or service and loading areas; mechanical equipment may also be located on rooftops.</i>	This project does not have any of these elements.	The Applicant is not proposing a modification or alternative Standard.
<i>5. Rooftop mechanical equipment shall be screened from the public realm view (from street and public open space view) using sloped roofs, parapets, and/or screens.</i>	This project meets this requirement.	The Applicant is not proposing a modification or alternative Standard.
<u>Site Walls</u>		

<p><i>Site walls (including screening, retaining, and accent walls) should use materials, patterns, and colors consistent with the adjacent building(s) and if visible from the public realm (from streets or public open spaces) shall be brick, pre-cast, cast stone, or vegetated screen wall.</i></p>		
<p><u>Railings, Fences, and Gates</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Railings, fences, and gates shall be metal. Metal materials shall be prefinished in a powder-coated color coordinated with adjacent materials, or painted a low-luster, dark neutral color. Any field welding shall be ground smooth and cleaned before painting. On single-family residential lots, in side and rear yards only, fences may also be vinyl up to six feet in height.</i></p>	<p>New stair rails will be metal and will be finished in accordance with the design criteria. There are no proposed fences or gates as part of the project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Terminal posts (corners, openings, and ends) shall be wider and taller than other posts or panels.</i></p>	<p>Not applicable. There are no new fences proposed on the project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Railing picket spacing shall be no more than four inches on center and must comply with life- safety code requirements.</i></p>	<p>Not applicable. There is no proposed picket fences as part of the project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Chain link fencing (except where required by law or for temporary security), barbed wire, and paneled materials are not permitted.</i></p>	<p>There is no new chain link fencing, barbed wire, or paneled materials proposed as part of the project.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Surface Parking Lots and Structured Parking Garages.</u></p>		

The purpose of the parking design criteria is to promote a “park once” environment that enables people to conveniently park and access a variety of uses in a pedestrian-friendly environment; reduce uncoordinated, inefficient single-purposed parking; and maximize on- street parking. Streetscapes should be vibrant and active, not dominated by parking lots or garages.

<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p><i>1. All surface parking lots or structured parking garages shall be accommodated mid-block or below grade and screened from the public realm. Structured parking should be located internal to blocks or below grade.</i></p>	<p>Surface parking lots are existing and screened from the public realm by grading and trees. One (1) new bay is proposed on the existing parking garage. It is located internal to the block, and the trees that screen the existing garage and new parking bay will not be impacted.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>2. Surface parking lots are not permitted in the TOD core with the exception of dedicated surface parking for a hospital or medical office building. Surface parking between the front of a building and the street or open space right-of-way is prohibited within the Largo Town Center DDOZ.</i></p>	<p>The project is not located within the TOD core. Surface parking lots on this project will be reduced, and are not between the front of the building and the street.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. A parking garage setback line of 40 feet is established from the BTL to accommodate the required public utility/access easement and “liner” ground-floor retail or office uses.</i></p>	<p>The new parking garage bay is set back within 45 feet from the Pedestrian Zone. There are no ground-floor retail or office uses proposed for the garage.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>4. In instances where surface parking lots front a street or public plaza, square, or green, the parking shall be set back a minimum of 40</i></p>	<p>Existing surface parking lots have appropriate setbacks, landscaping, screening, and buffering. There are no</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p><i>feet from the BTL. Landscaping, screening, and buffering of surface parking lots shall conform to the Landscape Manual requirements.</i></p>	<p>new surface parking lots proposed as part of the project.</p>	
<p>5. <i>Building façades along streets should have structured parking wrapped with retail, office, or residential uses.</i></p>	<p>No new retail, office, or residential use is associated with the proposed parking garage bay. The new garage addition will not change the use of the existing garage.</p>	<p>Modification 6: Neither the existing parking garage nor the proposed parking garage bay has retail, offices, nor residential uses associated with the structure. The structure is appropriate for its use as parking for employees and users of the existing Medical Office Building, and the proposed use will not change as a result of this project. The proposed parking garage will, however, contribute to a more defined streetscape along Mercantile Lane by reducing the existing setback, eliminating a strip of surface parking along the Property’s frontage, and adding illuminated landscaped seating areas.</p>
<p>6. <i>Buildings in which structured parking is the sole use are strongly discouraged throughout the plan area and are not permitted in the southwest quadrant (TOD core). If necessary, parking structures may be exposed on upper floors but shall have ground-floor usable space fronting a street or public plaza, square, or green.</i></p>	<p>The proposed addition to the parking garage does not change the characteristics of the existing structure.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>7. <i>Structured parking garage entrances shall</i></p>	<p>There are no proposed parking garage</p>	<p>The Applicant is not proposing a</p>

<p><i>not dominate the building street façade and should be a minimal opening in the building at the sidewalk level.</i></p>	<p>entrances along the building street façade. The two existing vehicular entrances on the north and south faces of the garage will remain. They are both minimal openings at sidewalk level.</p>	<p>modification or alternative Standard.</p>
<p><i>8. Reserved parking for hybrid, electric, and/or carpool vehicles; charging stations; solar panel shading structures; and similar environmentally friendly parking design features are encouraged within the Largo Town Center DDOZ.</i></p>	<p>Reserved spaces and charging stations for hybrid/electric vehicles are at the northeast corner of the proposed garage near the building entrance. These spaces will not be impacted by the new garage bay.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>9. Bicycle parking should be provided in structured parking garages and surface parking lots based on a site-by-site needs basis. Appropriate location, number of racks, and level of access for each facility depends on the anticipated use of the site or building. Conformance to LEED or similar federal, state, and county bicycle parking criteria is strongly encouraged.</i></p>	<p>Eight (8) bicycle racks are located at the front entrance of the existing building, and will not be impacted as part of this project. Four (4) existing inverted U-style bicycle racks are provided in the structured parking garage on-site.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>10. Pedestrian access to and from mid-block parking shall be continuously lit (to eliminate dark areas) and provide direct connection(s) to the primary street or open space where possible.</i></p>	<p>Appropriate lighting is proposed for the new garage bay, and existing site lighting either remains or is slightly shifted. A photometric analysis will be included with the DSP to ensure site lighting is adequate to and from on-site parking.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<u>Parking Design Criteria</u>		
<u>Parking Dimensional Requirements</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p>1. <i>Drive aisles shall be designed and located so that adequate visibility is ensured for pedestrians, bicyclists, and motorists when entering individual parking spaces, circulating within a parking facility, and entering or leaving a parking facility.</i></p>	<p>Drive aisles in the new parking structure are designed in accordance with this requirement.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>The minimum size for standard (non-compact), non-parallel off-street parking spaces shall be 9 feet by 18 feet. The minimum size for standard (non-compact), parallel on-street parking spaces shall be 8 feet by 22 feet.</i></p>	<p>Parking spaces within the new parking structure are designed in accordance with this requirement.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Up to one-third of the required number of parking spaces may be compact car spaces. All compact car spaces shall be marked as such. The minimum size for compact, non-parallel off-street parking spaces shall be 8 feet by 16.5 feet. The minimum size for compact, parallel on-street parking spaces shall be 8 feet by 19</i></p>	<p>Compact spaces exist onsite in surface parking lots and in the garage. The total number of compact spaces is less than one-third of the total parking spaces on-site per the requirement. The proposed parking bay does not contain any compact spaces.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<i>feet.</i>		
<u>Surface Parking Lot Landscape Requirements</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>Surface parking lot landscaping shall comply with the standards found in the Landscape Manual.</i>	Proposed surface parking lot landscaping complies with the standards found in the Landscape Manual.	The Applicant is not proposing a modification or alternative Standard.
2. <i>Durable, pervious surfaces should be used for surface parking lots when feasible. Gravel and similar loose materials prone to dust and erosion are prohibited.</i>	Proposed surface parking lots are asphalt.	The Applicant is not proposing a modification or alternative Standard.
<u>Parking Space Requirements by Use</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>In order to encourage alternative transportation choices and leverage the existing transit infrastructure, maximum rather than minimum parking space requirements apply to the Largo Town Center sector plan area. The maximum parking space requirements vary based on proximity to transit. For the TOD core area, which includes the Metro station, parking is</i>	Noted. The plan complies with the maximum parking space requirement of 5.00 spaces per 1,000 GSF.	The Applicant is not proposing a modification or alternative Standard.

<i>reduced.</i>		
<p>2. <i>The following maximum parking space requirements shall apply for each use unless an alternative strategy is approved by the Planning Board. For uses not specifically listed, the requirement of the most similar use shall apply.</i></p> <p><i>Institutional/ Educational:</i> <i>NORTHEAST, NORTHWEST, SOUTHEAST, EAST AREA:</i> <i>Medical Office:</i></p> <ul style="list-style-type: none"> • <i>Minimum: 4 Spaces / 1000 Square Feet</i> • <i>Maximum: 5 Spaces / 1000 Square Feet</i> 	<p>The plan complies with the minimum and maximum parking space requirements for the proposed use. The final site will have 1,221 parking spaces and 247,250 GSF total building area, for a 4.94 spaces per 1,000 SF ratio.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>SIGNAGE DESIGN CRITERIA</u> The signage design criteria are intended to ensure that signs are an integral part of an overall vision aimed at achieving an aesthetically pleasing and high-quality urban environment. Signage should establish a coordinated and harmonic streetscape and be expressive of the intended dynamic character of the sector plan area. Creative signage design using artistic imagery, lighting, color, texture, graphics, and materials is strongly encouraged.</p>		
<p><u>General Provisions</u></p>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<p>1. <i>Any sign required by county, state, or federal regulations shall be governed by those regulations as well as by the signage design criteria. Where conflicts exist, the county, state, or federal regulations shall prevail.</i></p>	<p>Signs will meet county / state / federal regulations.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p>2. <i>All new signs shall be attached to the façade. Signs may be flat against the façade or mounted projecting or hanging from the façade.</i></p>	<p>The sign will be attached to the facade.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Signage submitted for multitenant buildings should be coordinated and present a unified approach.</i></p>	<p>This is not a multitenant building.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Building signs shall be constructed of durable, high quality materials such as metal, wood, and/or glass. Signs should be simply designed to contain only essential information and serve to identify the name, business type, company logo, and street address of the establishment. Tag lines, bylines, merchandise, telephone numbers, web addresses, or other information which is not part of the business name is prohibited.</i></p>	<p>The new building sign will be a high quality sign that includes the owners name (Kaiser Permanente) and their logo.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>5. <i>The total sign area allowed per building shall be computed on the basis of two square feet of sign area for each one linear foot of building frontage. Where a building has multiple frontages, the allowed sign area should be distributed proportionally along each building frontage. Buildings with less than 60 linear feet of building frontage may be allowed up to 120 square feet of sign area.</i></p>	<p>The total allowed square feet for signs along Mercantile Lane is 654 square feet. The perforated Kaiser Permanente logo is a total of 928 square feet, which exceeds the Standard’s 654 square foot allotment along Mercantile Lane by 274 square feet.</p>	<p>Modification 7: A modification to the General Provision Standard for signage area is necessary to accommodate the perforated graphic panel art wall along the western façade of the parking garage. This subject art wall is 2,960 square feet, of which 928 square feet is the distinguishable perforated Kaiser Permanente logo. This perforated logo exceeds the Standard’s 654 square foot allotment along Mercantile Lane by 274 square feet. As noted, this art wall is intended to be artistic and whimsical in nature and create a welcoming entrance to visitors of the facility. Importantly, the sign</p>

		<p>advances the Master Plan’s streetscape activation goals by screening the structured parking facility and adding visual interest along Mercantile Lane. Accordingly, a modification of this Standard to permit the 928 square foot art wall will enhance the proposed development and promote the purposes of the Development District. Please note that a modification to this Standard is not necessary to accommodate the illuminated identity signage, as this sign is within the allotted 654 square feet.</p>
<p>6. <i>Repetitious signage information on the same building frontage should be avoided regardless of the sign area square footage allowed.</i></p>	<p>An illuminated identity sign and an art wall featuring the Kaiser Permanente logo are both proposed along Mercantile Lane.</p>	<p>Modification 8: If the perforated graphic art wall is indeed characterized as signage, a modification to the General Provision Standard for repetitious signage is likely necessary to accommodate the illuminated identity signage. The art wall is intended to, again, introduce an artistic element into the architecture along the Mercantile Lane streetscape. The proposed image, which will be incorporated into the art wall, is the Kaiser Permanente logo. Since this logo is utilized in both signs along the western façade of the garage, a modification to the subject Standard is requested. Although the logo is utilized twice at this location, each sign is unique and functions separately along the streetscape. This modification allows for the introduction of unique, creative architectural treatments which will provide for a more inviting and attractive</p>

		streetscape, as well as valuable identification for pedestrian and vehicular traffic. Accordingly, a modification to this Standard benefits the proposed development and advances the goals of the overall Development District.
7. <i>Signs shall be properly repaired and maintained such that they are always in clean, working condition.</i>	This will be a new sign.	The Applicant is not proposing a modification or alternative Standard.
<u>Pole-Mounted Banners</u> <i>Pole-mounted banners enliven streetscapes, add color, and can help promote a sense of community.</i>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
1. <i>Pole-mounted banners shall not exceed 16 square feet.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.
2. <i>Pole-mounted banners shall be mounted with no less than 10 feet of clearance above the sidewalk and no less than 18 feet of clearance above any road, driveway, or alley.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.
3. <i>Pole-mounted banners may be placed on private land or in the public right-of-way subject to county approval.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.

<p><u>Building-Mounted Signs</u> <i>Building-mounted signs comprise flat wall, projecting (including blade and under-canopy signs), awning, marquee, and storefront window signage. Building-mounted signs can count toward the total allowed sign area of the building.</i></p>		
<p><u>Flat Wall Signs</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Flat wall signs should be centered on horizontal surfaces, such as a storefront opening, and placed within a clear signage area.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>On tall signature buildings, building-mounted signs may be placed between the top of the highest floor’s windows and the top of the roof parapet.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Sign locations should respect the design of a building, including the arrangement of bays and openings, and shall not obscure windows, grillwork, piers, pilasters, or ornamental features</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Projecting Signs</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Blade signs shall be hung perpendicular to the face of a building above or at the entrance to a storefront.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Under-canopy signs are generally smaller than blade signs and are oriented to pedestrians passing under them. These</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p><i>signs should be used primarily at ground floor locations but may be considered for upper floor businesses with covered entry porches and balconies.</i></p>		
<p>3. <i>Each face of a blade or under-canopy sign shall be counted towards the total allowed sign area of the building, and, each/ace shall not exceed fifty (50) square feet.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Blade signs shall not project more than forty-eight (48) inches from the wall of a building.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>5. <i>Blade and under-canopy signs shall be mounted with no less than eight (8) feet of clearance above the sidewalk.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Awnings and Awning Signs</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Awnings, with or without signage, must be sturdy and permanently attached to buildings. They should be designed and placed to compliment the building’s architecture.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Awnings should be made of canvas, metal, or glass. Shiny or reflective materials are discouraged.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>The text and graphics area of an awning sign should not exceed 35 percent of the awning background surface area. Lettering on awnings should be no taller than 30 inches.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p>4. <i>The text and graphics area of an awning sign should not exceed 35 percent of the awning background surface area. Lettering on awnings should be no taller than 30 inches.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>5. <i>Awnings shall be mounted with no less than 10 feet of clearance above the sidewalk. The horizontal clearance between an awning and the street curb line shall not be less than 15 feet.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Marquees Signs</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Marquee signs are mounted vertically along a building face. Marquee signs shall accentuate primary building entrances, major tenant entrances, or other significant building entry. Theaters, cinemas, and performing arts facilities are encouraged to utilize this sign type.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Trash enclosures and other ancillary service structures shall be located away from streets and public open spaces and screened from view using walls and/or landscaping. Enclosure walls shall be brick, architectural concrete block, or steel.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<u>Storefront Window Signs</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. Storefront window signs should be professionally painted, posted, displayed, or etched on interior translucent or transparent surfaces, including windows or doors.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.
<i>2. Window signs generally contain text, but also may contain graphic logos or images combined with color.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.
<i>3. Permanent window signs should cover up to 20 percent of the glass area and be designed so that visibility into and out of the window is not obscured.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.
<i>4. Window signs shall be created from high-quality materials that may include paint, gold leaf, and transparent, opaque, and frosted vinyl materials. Window signs should be applied directly to the interior face of the glazing or hung inside the window concealing all mounting hardware and equipment.</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.
<u>Free-Standing and Monumental Signs</u>		
<u>STANDARD</u>	<u>CONFORMANCE</u>	<u>JUSTIFICATION / MODIFICATION</u>
<i>1. A maximum of one freestanding or monument sign shall be permitted for each</i>	This Standard is not applicable to the proposed development.	The Applicant is not proposing a modification or alternative Standard.

<p><i>commercial shopping center, office park, or mixed-use development exceeding 100,000 square feet in size.</i></p>		
<p><i>2. A maximum of one freestanding or monument sign shall be permitted for each residential development exceeding 200 dwelling units.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>3. Freestanding and monument signs shall not exceed six feet in height, and the maximum area of any single freestanding or monument sign shall not exceed 50 square feet. Freestanding and monument signs shall be constructed of durable, high-quality materials such as, but not limited to, decorative masonry, wrought iron, or weatherized decorative metals.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>4. Smaller commercial and mixed-use developments containing a minimum of 15,000 square feet but not exceeding 100,000 square feet shall be permitted a maximum of one ground-mounted monumental sign not exceeding four feet in height or a maximum area of 24 square feet.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><i>5. Smaller residential developments containing a minimum of 30 dwelling units but not exceeding 200 dwelling units shall be permitted a maximum of one ground-mounted monumental sign not exceeding four feet in height or a maximum area of 24 square feet.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p>6. <i>Signs shall be externally lit from the front with a full-spectrum light source. Internal and back lighting are permitted as an exception only for individual letters or numbers, such as for "channel letter" signage (panelized back lighting and box lighting fixtures are prohibited).</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>7. <i>Building numbers are required, and commercial buildings require building numbers on both the front and rear.</i></p>	<p>This Standard is not applicable to the proposed development.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Illumination</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Internal and back lighting are permitted as an exception only for individual letters or numbers such as for "channel letter" signage (panelized back lighting and box lighting fixtures are prohibited). Signage within a shop front may be neon lit.</i></p>	<p>The “Kaiser Permanente” channel letters and Logo will be internally lit. It will replace in kind the existing sign.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>2. <i>Projecting light fixtures used to externally illuminate signs should be simple and unobtrusive in appearance. They should not</i></p>	<p>The design does not have any projected light fixtures to illuminate signs.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p><i>obscure the graphics of the sign and should be designed as part of the architecture of the sign.</i></p>		
<p>3. <i>Sign lighting shall be designed to limit environmental impacts such as glare and light pollution.</i></p>	<p>The sign as designed will limit environmental impacts.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>All electrical connections including junction boxes, transformers, conduit, raceways, and tubing required for any sign items shall be concealed from public view. Where the attachment of a sign may severely damage or impact the façade of a building or canopy, an architectural signage raceway may be allowed. If allowed, the raceway shall be fabricated to conceal all electrical wiring components and painted to match adjacent sign and/or building façade elements.</i></p>	<p>The sign will conceal or use an architectural signage raceway, to conceal the components and painted to match adjacent finishes.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>5. <i>Sign illumination shall promote energy conservation by utilizing energy efficient illumination techniques. This may include, but is not limited to, LED lighting components and solar-based illumination techniques where applicable.</i></p>	<p>The existing sign will be replaced in kind and upgraded to LED to meet this requirement.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p><u>Prohibited Sign Types & Material</u></p>		
<p><u>STANDARD</u></p>	<p><u>CONFORMANCE</u></p>	<p><u>JUSTIFICATION / MODIFICATION</u></p>
<p>1. <i>Animated, rotating, flashing, or scrolling signs.</i></p>	<p>The proposed sign will not have any movement.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

<p>2. <i>Internally illuminated box signs.</i></p>	<p>The “Kaiser Permanente” channel letters and Logo will be internally lit. It will replace in kind the existing sign.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>3. <i>Internally illuminated awnings.</i></p>	<p>This project does not propose any canopies.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>4. <i>Signs mounted at the rooftop above the building roof line.</i></p>	<p>There are no proposed signs mounted above the roof top.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>5. <i>Signs related to specific businesses or private events that are placed in the public right-of way or on street furniture and fixtures including benches, fences, trash cans, bus shelters</i></p>	<p>No signs are proposed in these conditions.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>6. <i>Signs with exposed raceways, conduit junction boxes, transformers, lamps, tubing, or neon crossovers of any type.</i></p>	<p>The proposed sign will conceal or use an architectural signage raceway, to conceal the components and painted to match adjacent finishes.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>7. <i>Any sign designed to be mobile and moved from place to place (except sandwich boards neon crossovers of any type.</i></p>	<p>The proposed sign will be permanent.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>8. <i>Pennants, pinwheels, and similar circus- or carnival-type signs.</i></p>	<p>No signs are proposed in these conditions.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>9. <i>Balloons or inflatable signs.</i></p>	<p>No signs are proposed in these conditions.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>
<p>10. <i>Temporary signs attached to building facades.</i></p>	<p>No signs are proposed in these conditions.</p>	<p>The Applicant is not proposing a modification or alternative Standard.</p>

II. AMENDMENTS REQUIRED TO BE APPROVED BY THE DISTRICT COUNCIL

As noted in the Statement of Justification, none of the Applicant's requested Development District Standard amendments require District Council approval, pursuant to Sec. 27-548.26.³

³ Pursuant to Sec. 27-548.26, there are three types of Development District Standards that require District Council approval: (1) changes to the boundary of the DDOZ, (2) changes from one zoning category to another, and (3) changes to the list of permitted uses. The subject DSP does not propose any such amendments.



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Countywide Planning Division
Historic Preservation Section

301-952-3680

March 23, 2022

MEMORANDUM

TO: Andrew Bishop, Urban Design Section, Development Review Division

VIA: Howard Berger, Historic Preservation Section, Countywide Planning Division **HSB**

FROM: Jennifer Stabler, Historic Preservation Section, Countywide Planning Division **JAS**
Tyler Smith, Historic Preservation Section, Countywide Planning Division **TAS**

SUBJECT: DSP-95073-06 Kaiser Permanente Largo Medical Center


The subject property comprises 14.71-acres and is located in the southwest quadrant of the intersection of Mercantile Lane and Technology Lane. The subject application proposes the expansion of approximately 90,000 square-feet to an existing medical office building and structured parking garage to accommodate additional medical services. The subject property is Zoned D-D-O and M-U-I.


A search of current and historic photographs, topographic and historic maps, and locations of currently known archeological sites indicates the probability of archeological sites within the subject property is low. Most of the property has already been disturbed. The subject property does not contain and is not adjacent to any Prince George's County Historic Sites or Resources. This proposal will not impact any historic sites, historic resources or known archeological sites. A Phase I archeology survey is not recommended. Historic Preservation Section staff recommend approval of DSP-95073-06 Kaiser Permanente Largo Medical Center without conditions.

March 25, 2022

MEMORANDUM

TO: Andrew Bishop, Planner II, Urban Design Section, Development Review Division

VIA: David A. Green, MBA, Planner IV, Community Planning Division 

FROM: Chidy Umeozulu, Planner III, Neighborhood Revitalization Section, Community Planning Division 

SUBJECT: **DSP-95073-06 Kaiser Permanente Largo Medical Center**

FINDINGS

Community Planning Division staff finds that, pursuant to Section 27-548.25(c) of the Zoning Ordinance the alternate Development District Standards will benefit the development and the Development District and will not substantially impair implementation of the 2013 *Largo Town Center Approved Sector Plan and Sectional Map Amendment*.

BACKGROUND

Application Type: Revision to a Detailed Site Plan in a Development District Overlay Zone

Location: Southwest quadrant of the intersection of Mercantile Lane and Technology Way

Size: 14.71 acres.

Existing Uses: Kaiser Permanente medical facility

Proposal: Expansion of approximately 90,000 square feet to an existing medical office building and structured parking garage to accommodate additional medical services

GENERAL PLAN, MASTER PLAN, AND ZONING

General Plan: Plan Prince George's 2035 designates the subject property in the Regional Transit District Growth Policy area. The vision for Regional Transit District is a destination for regional workers and residents that contain a mix of office, retail, entertainment, public and quasi-public, flex, and medical uses. It is walkable, bikeable, and well-connected to a regional transportation network via a range of transit options.

Master Plan: The 2013 *Largo Town Center Approved Sector Plan and Sectional Map Amendment* placed the development site within the Largo Town Center Northeast Quadrant. The sector plan recommends Mixed-Use – Predominantly Office and Institutional.

Planning Area: 73

Community: Northampton

Aviation/MIOZ: This application is not located within an Aviation Policy Area or the Military Installation Overlay Zone.

SMA/Zoning: The 2013 Largo Town Center SMA classified the property to the Mixed-Use Infill (M-U-I) Zone with a Development District Overlay that guides development of the property.

DEVELOPMENT DISTRICT MANDATORY STANDARDS

This Kaiser Permanente medical facility was approved and developed under the previous I-3 (Planned Industrial/Employment Park) Zone standards prior to the adoption of the 2013 *Largo Town Center Approved Sector Pan and Sectional Map Amendment*. This application is being processed and reviewed as an amendment to a previously approved DSP.

Page 129 of the 2013 *Largo Town Center Approved Sector Pan and Sectional Map Amendment* states, “An addition to a nonresidential structure that was lawful or could be certified as legal nonconforming use on the date of SMA approval is exempt from the development district standards and detailed site plan review, if the addition (and the accumulated sum of all additions since the approval of the SMA) does not increase the Gross Floor Area (GFA) by more than 15 percent or 10,000 square feet, whichever is less.” Given that the 90,000 square feet addition is much more than the threshold required for exempt from development standards, this application is required to comply with the design standards or request modification to the Development District Standard.

Since this is not a new construction, it may be challenging to fully comply with the standards relating to block length, build-to line (BTL)/setback, building frontage, floor height, fenestration, and garage placement requirements. The applicant proposes modifications to those standards in accordance with Section 27-548.25(c) of the Zoning Ordinance. In recognition of the constraints and challenges, the applicant proposes to activate the street through the addition of three seating areas, which enhance and connect to the existing pedestrian loop. Within these seating areas, the application proposes benches, bollard lighting, and landscaping. These improvements advance the brand of New Urbanism envisioned by the Largo Town Center Sector Plan.

The application of the alternative Development District Standards will benefit the development and the development district and will not substantially impair the implementation of the 2013 *Largo Town Center Approved Sector Pan and Sectional Map Amendment*.

c: Long-range Agenda Notebook

Frederick Stachura, Planning Supervisor, Neighborhood Revitalization Section, Community Planning Division

Countywide Planning Division
Transportation Planning Section


301-952-3680

March 29, 2022

MEMORANDUM

TO: Andrew Bishop, Urban Design Review Section, Development Review Division

FROM: Benjamin Ryan, Transportation Planning Section, Countywide Planning Division

VIA:  William Capers III, PTP, Transportation Planning Section, Countywide Planning Division

SUBJECT: DSP-95073-06: Kaiser Permanente Largo

Proposal:

The subject application seeks to expand the existing medical office building and the existing parking garage in two phases. Phase 1 proposes to construct a 4,850 square-foot (SF) addition to the medical office building and a 78,000 SF addition to the existing parking garage. Phase 2 includes a 6,200 SF addition to the medical office building. Additionally, the applicant proposes to improve the site's primary access along Mercantile Lane into a new bi-directional drive aisle.

Prior Conditions of Approval:

The property is subject to Conceptual Site Plan SP-87168-01, Preliminary Plan of Subdivision 4-79155 and 4-86107, Detailed Site Plan DSP-9507,3 and its subsequent revisions DSP-95073-01 through DSP-95073-05.

While there are no prior conditions of approval on the subject property related to transportation improvements, the transportation planning section memo for DSP-95073-01 contains a section regarding trip cap conditions on the subject site and states the following, "Neither 4-79155 nor 4-86107 includes trip cap conditions or other restrictions on the quantity of development that can occur within the site. Neither plan file includes traffic study information. Both lots that comprise this site were platted pursuant to preliminary plans that included a finding of transportation adequacy. The expansion is occurring on lots that were recorded without a trip cap or other similar restriction. The off-site transportation impact of the expansion is not an issue in the detailed site plan process."

Comment: Staff analysis of the prior conditions of approval did not display any additional findings or restrictions related to trip cap conditions. Staff finds the assessment and methodology for analyzing the trip cap used in the previously approved DSPs acceptable and are suitable for the current development proposal.

Master Plan Compliance

This application is subject to 2009 *Approved Countywide Master Plan of Transportation* (MPOT).

Master Plan Roads

The subject property fronts MD 202 (E-6) along its eastern border. The 2009 *Countywide Master Plan of Transportation* (MPOT) recommends this portion of MD 202 as a 4-8 lane expressway constructed within 150-200 feet of right-of-way. This portion of MD 202 also falls within the bounds of the 2013 *Approved Largo Town Center Sector Plan and Sectional Map Amendment* which does not provide any additional right-of-way recommendations. The applicant's submission accurately displays this portion of MD 202 fronting the subject site as being within 200-feet of right-of-way. No additional dedication is required on the subject property's side of the road.

The subject property also fronts Mercantile Lane along its western border and Technology Way along its northern border. These roads both carry the MPOT designation of I-312. The MPOT recommends these portions of Mercantile Lane and Technology Way as 4 lane industrial roads within 70-feet of right-of-way. Both roads also fall within the bounds of the 2013 *Approved Largo Town Center Sector Plan and Sectional Map Amendment* which does not provide any additional right-of-way recommendations. The applicant's submission accurately displays these roads as being within 70-feet of right-of-way. No additional dedication is required on the subject property's side of the road.

Master Plan Pedestrian and Bike Facilities

The 2009 *Approved Countywide Master Plan of Transportation* (MPOT) recommends the following facilities:

Planned Side Path: Landover Road

The MPOT provides policy guidance regarding multimodal transportation and the Complete Streets element of the MPOT recommends how to accommodate infrastructure for people walking and bicycling.

Policy 4: Develop bicycle-friendly roadways in conformance with the latest standards and guidelines, including the 1999 AASHTO *Guide for the Development of Bicycle Facilities*.

Comment: Landover Road fronts the subject site. No additional right-of-way is being sought with the proposed application. The Maryland State Highway Administration (SHA) can require the construction of the master plan recommended side path along Landover Road as appropriate, or it may be installed by SHA as part of a future roadway repaving or capital improvement project.

The subject property falls within the developing tier. Mercantile Lane and Technology Way already contain constructed sidewalk facilities fronting the subject property. The applicant's submission displays an existing sidewalk and crosswalk network, providing pedestrian access to the building from the outdoor parking areas and from the parking garage. The applicant has updated plans per staff recommendations to display bicycle parking areas on the east side of the building as well as additional bicycle parking within the parking garage. Staff finds the applicant's submission to be acceptable in evaluating bicycle and pedestrian access to the site.

Transportation Planning Review
Zoning Ordinance Compliance

Section 27-283 of the Prince George's County Zoning Ordinance (Ordinance) provides guidance for detailed site plans. The section references the following design guidelines described in Section 27-274(a):

(2) Parking, loading, and circulation

I Vehicular and pedestrian circulation on a site should be safe, efficient, and convenient for both pedestrians and drivers. To fulfill this goal, the following guidelines should be observed:

(ix) Pedestrian and vehicular routes should generally be separate and clearly marked.

(x) Crosswalks for pedestrians that span vehicular lanes should be identified by the use of signs, stripes on the pavement, change of paving material, or similar techniques

(xi) Barrier-free pathways to accommodate the handicapped should be provided

(6) Site and streetscape amenities

(A) Site and streetscape amenities should contribute to an attractive, coordinated development and should enhance the use and enjoyment of the site. To fulfill this goal, the following guidelines should be observed:

(i) The design of light fixtures, benches, trash receptacles, bicycle racks, and other street furniture should be coordinated in order to enhance the visual unity of site.

In addition to the zoning ordinance, the development proposal is also subject to the 2013 *Approved Largo Sector Plan and Sectional Map Amendment*. There are no specific transportation design recommendations that are related to the subject application.

Comment: In regard to circulation, the site has three access points of, two of which are located along Mercantile Lane and the third along Technology Way. Presently, the northern vehicular access point on Mercantile Lane only allows outbound movements from the site. The subject application seeks to reconfigure this access drive aisle into a two-way access point, allowing vehicles to enter and exit. This will allow for vehicles to enter or exit from all three points of vehicle access. Staff finds the reconfiguration of the site access drive aisle acceptable but recommends that the applicant modify the lane use configuration for vehicles entering the site to a two-lane single left-through and single right lane approach to eliminate conflicts and potential stacking along Mercantile Lane. In addition, staff recommends providing additional stop controls at the drop off exit point and at the westbound approach at the main entrance and drop off location for adequate circulation and traffic operations on site.

The proposed addition to the medical office building totals 11,050 square-feet between the two proposed phases. This will raise the overall square footage of the building from 236,200 square-feet to 247,250 square-feet. The property falls within the M-U-I (Mixed-Use Infill) Zone which allows for a parking requirement of 4 to 5 spaces per 1,000 gross square footage of medical office use. This results in a parking requirement of 989 to 1,235 total spaces upon completion of both phases of development. The applicant's submission displays a total number of 1,221 total spaces upon completion of the development proposal, which includes 58 ADA compliant spaces for standard

vehicles and 6 ADA compliant spaces for vans. Staff finds the applicant's parking proposal to be sufficient for the proposed use and expansion of the medical office building.

Conclusion:

From the standpoint of transportation, the Transportation Planning Section finds that Detailed Site Plan, DSP-95073-06, will be served by adequate transportation facilities and is acceptable, if the following conditions are met:

1. Prior to certification of the detailed site plan, the applicant and the applicant's heirs, successors and/or assignees shall revise the plan to provide:
 - a. Modify the northbound approach lane use configuration along Mercantile Lane to include a single left-through and single right lane configuration. The exact details shall be accepted by the Transportation Planning Section prior to certification of the DSP
 - b. Add additional stop controls at the eastbound approach and outbound lane of the pickup/drop area internal to the site at the Mercantile Lane. The exact details shall be accepted by the Transportation Planning Section prior to certification of the DSP
 - c. Additional signage indicating temporary parking along the pickup/drop-off area onsite. The exact location and sign profiles shall be accepted by the Transportation Planning Section prior to the certification of the DSP.

April 1, 2022

MEMORANDUM

TO: Andrew Bishop, Planner II, Urban Design Section
VIA: Mridula Gupta, Planner III, Subdivision Section *MG*
FROM: Antoine Heath, Planner II, Subdivision Section
SUBJECT: DSP-95073-06; Kaiser Permanente Largo Medical Center

The subject property considered in this amendment to Detailed Site Plan (DSP-95073-06) consists of one parcel and one lot known as Parcel 5 recorded among the Land Records of Prince George's County in Plat Book NLP 116 page 89 dated November 1982, and Lot 2 recorded in Plat Book NLP 151 page 28 dated July 1987. Both record plats are titled Largo Park. The property is located in the Mixed Use Infill (M-U-I) and Development District Overlay (D-D-O) Zones, and is subject to the 2013 *Approved Largo Town Center Sector Plan and Sectional Map Amendment*.

This DSP amendment proposes 11,050-square-foot of addition to an existing 236,200-square-foot medical building. The applicant is also proposing an 80,000-square-foot addition to an existing 245,000-square-foot parking garage.

DSP-95073 was approved by the Prince George's County Planning Board on December 21, 1995 (PGCPB Resolution No. 95073), for the development of a 127,000-square-foot medical facility on the subject property. DSP-95073-01 was approved by the Planning Board on June 24, 2010 (PGCPB Resolution No. 10-74) for a 106,700-square foot-expansion to the medical center, and a 245,000-square-foot parking garage. The subsequent amendments to the DSP were approved by the Planning Director. DSP-95073-02, approved, revised parking space sizes and counts on site. DSP-95073-03 approved the use of trailers within on-site surface parking area and renovations to the existing medical office. DSP-95073-04 approved revisions to access points to the existing medical facility. DSP-95073-05 approved installation of natural gas service on the site.

This property is subject to Preliminary Plans of Subdivision (PPS) 4-79155 and 4-86107. Parcel 5 is subject to PPS 4-79155, which approved 17 lots by the Prince George's County Planning Board on December 3, 1979, for uses permitted under the I-3 Zone. Lot 2 is subject to PPS 4-86107, which approved 11 lots by the Prince George's County Planning Board on July 24, 1986 (PGCPB Resolution No. 86-297), for uses permitted under the I-3 Zone. Since the available records for the two PPS's do not include a trip cap or establish specific development quantities, the determination of

development entitlement for Parcel 5 and Lot 2 was deferred to Transportation Planning Section (TPS) to analyze the traffic capacity that was considered in the approval of the two PPS's. TPS determined that while PPS 4-79155 and 4-86107 included a finding of transportation adequacy, Parcel 5 and Lot 2 were recorded without a trip cap or other similar restriction on the quantity of development. Based upon their analysis of prior conditions of approval for the subject property, TPS found that the assessment and methodology for analyzing the trip cap used in the previously approved DSP's is acceptable and suitable for the current development proposal. A new preliminary plan of subdivision or final plat is not required for the development proposed in this amendment. If additional development is proposed in the future on the subject property, a new PPS and final plat may be required.

Parcel 5 and Lot 2 are depicted on this DSP in conformance with the final plats. No conditions of approval from PPS 4-79155 are relevant to the review of this application. However, the plat for this PPS notes that access to MD 202 (Landover Road) should be internal. The applicant has provided a note on the DSP denying access to MD 202.

PPS 4-86107 was approved subject to 2 conditions. The conditions relevant to the review of the subject application, and staff analysis of the project's conformance to the conditions are presented below:

1. Approval of a conceptual stormwater management plan by the Washington Suburban Sanitary Commission prior to Final Plat of Subdivision.

The final plat of subdivision for the subject lot was approved on November 18, 1982. The applicant has indicated that a conceptual stormwater management (SWM) plan was approved prior to final plat approval on July 20, 1989. The applicant submitted a new approved SWM Concept Plan #44959-2021-00 with this application. Conformance to this condition should be further reviewed by the Environmental Planning Section.

2. Denial of access on Arena Drive.

The subject property does not have frontage on Arena Drive. Denial of access is labeled on the property's frontage with MD 202 in accordance with approved PPS.

Additional Plan Comments:

None.

Recommended Revisions:

None.

This referral is provided for the purposes of determining conformance with any underlying subdivision approvals for the subject property and Subtitle 24. The DSP has been found to be in substantial conformance with the preliminary plan of subdivision. All bearings and distances must be clearly shown on the DSP and must be consistent with the record plats, or permits will be placed on hold until the plans are corrected. There are no subdivision issues at this time.

March 28, 2022

MEMORANDUM

TO: Andrew Bishop, Urban Design

FROM: Jason Bartlett, Permit Review Section, Development Review Division

SUBJECT: Referral Comments for DSP-95073-06, Kaiser Permanente Largo Medical Center

1. The structures shown on the DSP should be clearly labeled as PROPOSED or EXISTING with their square footage, dimension, height, and setbacks shown on plan. For buildings with additions added, the existing bldg. SF should be show, as well as the proposed SF of the addition and the resultant total SF.



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

Countywide Planning Division
Environmental Planning Section

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-4366
www.mncppc.org/pgco

301-952-3650

March 28, 2021

MEMORANDUM

TO: Andrew Bishop, Planner II, Urban Design Section, DRD

VIA: Thomas Burke, Supervisor, Environmental Planning Section, CWPD TB

FROM: Mary Rea, Planner II, Environmental Planning Section, CWPD MAR

SUBJECT: **Kaiser Permanente Largo Medical Center; DSP-95073-06**

The Environmental Planning Section (EPS) has reviewed Detailed Site Plan (DSP-95073-06), received by the Countywide Planning Division on February 22, 2022. The Environmental Planning Section finds this application to be in conformance with the environmental requirements of Subtitle 24 (Subdivision), Subtitle 25 (Woodland and Wildlife Habitat Conservation Ordinance), and Subtitle 27 (Zoning Ordinance), and recommend approval of the application with one condition.

Environmental Review

The site has a Natural Resource Inventory Equivalency Letter (NRI-154-2018-02) which was issued on June 17, 2021. The site is currently developed with a medical building. A small woodland area is located along the southeast corner of the property. There are no Regulated Environmental Features (REF) located on this site. According to the U.S. Department of Agriculture (USDA) Natural Resource Conservation Service (NRCS) Web Soil Survey (WSS), the site contains Collington-Wist Complex (2-10% slopes), Udorthents, Highway (0-65% slopes), and Chillum silt loam (0-6% slopes). Neither Marlboro Clay nor Christiana complexes are known to occur onsite.

This site is not located within a Sensitive Species Protection Review Area (SSPRA) based on a review of the SSPRA GIS layer, prepared by the Maryland Department of Natural Resources Natural Heritage Program (DNR NHP). The site is located within the Environmental Strategy Area 1 (formerly the Developed Tier) of the Regulated Environmental Protection Areas Map, as designated by *Plan Prince George's 2035 Approved General Plan (2014)*. The site is also located in the *Approved 2010 Approved Central Annapolis Road Approved Sector Plan*.

The site has a Standard Letter of Exemption (S-118-2021) from the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance (WCO) that was issued on June 17, 2021, since the site contains less than 10,000 square-feet of woodland. However, the site plan that was submitted for this exemption does not match the detailed site plan for this application. The woodland exemption will need to be revised.

The site has an approved Stormwater Management Concept Plan #44959-2021 that is in conformance with the current code, which is valid until February 8, 2025.

The approved plan proposes micro-bioretenion facilities and a green roof. The approved concept plan is in general conformance with the detailed site plan.

Based on the proposed layout, the project demonstrates conformance with the applicable policies and strategies of the 2017 *Countywide Green Infrastructure Plan* of the *Approved Prince George's Resource Conservation Plan* because the site contains no Regulated or Evaluation Areas within the existing network. The project was found to be in conformance with applicable environmental policies within the *General Plan*, *Master Plan*, and *Countywide Green Infrastructure Plan*.

Recommended Conditions:

The Environmental Planning Section recommend approval of DSP-95073-06 with the following condition:

1. Prior to certification of the detailed site plan, the Woodland Conservation Exemption shall be revised so that the plan submitted for exemption matches the detailed site plan.

If you have any questions concerning this review, please contact me by e-mail at Mary.Rea@ppd.mncppc.org or call 301-952-3661.

From: [Reilly, James V](#)
To: [Bishop, Andrew](#)
Cc: [PGCReferrals](#); [Reilly, James V](#)
Subject: FW: ACCEPTANCE REFERRAL: DSP-95073-06 KAISER PERMANENTE LARGO MEDICAL CENTER
Date: Tuesday, February 22, 2022 9:47:52 PM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
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[image006.png](#)
[image007.png](#)
[image008.png](#)
[DSP-95073-06 SUMMARY.pdf](#)
Importance: High

[EXTERNAL EMAIL] Exercise caution when opening attachments, clicking links, or responding.

Good Evening Andrew,

The Fire/EMS Department has reviewed the referral for DSP-95073-06 Kaiser Permanente Largo Medical Center and we have no comments at this time. Regards. Jim

James V. Reilly

Contract Project Coordinator III



Office of the Fire Marshal
Division of Fire Prevention and Life Safety
Prince George's County Fire and EMS Department
6820 Webster Street, Landover Hills, MD 20784

Office: 301-583-1830

Direct: 301-583-1838

Cell: 240-508-4931

Fax: 301-583-1945

Email: jvreilly@co.pg.md.us

From: ePlan <ePlan@ppd.mncppc.org>

Sent: Tuesday, February 22, 2022 10:24 AM

To: Smith, Tyler <Tyler.Smith@ppd.mncppc.org>; Stabler, Jennifer <Jennifer.Stabler@ppd.mncppc.org>; Hall, Ashley <Ashley.Hall@ppd.mncppc.org>; Henderson, Tamika <Tamika.Henderson@ppd.mncppc.org>; Franklin, Judith <Judith.Franklin@ppd.mncppc.org>; Green, David A <davida.green@ppd.mncppc.org>; Capers, William <William.Capers@ppd.mncppc.org>; Masog, Tom <Tom.Masog@ppd.mncppc.org>; Gupta, Mridula <Mridula.Gupta@ppd.mncppc.org>; Conner, Sherri <sherri.conner@ppd.mncppc.org>; Dixon, June <june.dixon@ppd.mncppc.org>; Brooke E. Larman <brooke.larman@ppd.mncppc.org>; PPD-EnvDRDreferrals <ppd-envdrdreferrals@ppd.mncppc.org>; Reilly, James V <JVReilly@co.pg.md.us>; tgaskins@co.pg.md.us; De Guzman, Reynaldo S. <rsdeguzman@co.pg.md.us>; Edelen, William K. <WKEdelen@co.pg.md.us>; Giles, Mary C. <mcgiles@co.pg.md.us>; Tayyem, Mahmoud <mtayyem@co.pg.md.us>; Abdullah, Mariwan <MABdullah@co.pg.md.us>; Formukong, Nanji W.



Angela D. Alsobrooks
County Executive

THE PRINCE GEORGE'S COUNTY GOVERNMENT
Department of Permitting, Inspections and Enforcement
Site/Road Plan Review Division



MEMORANDUM

March 8, 2022

TO: Andrew Bishop, Urban Design Section
Development Review Division, M-NCPPC

FROM: Mary C. Giles, P.E., Associate Director *Mary Giles*
Site/Road Plan Review Division, DPIE

Re: DSP-95073-06,
Kaiser Permanente Largo Medical Center

CR: Technology Way (County)
CR: Mercantile Lane (County)

This is in response to the Detailed Site Plan No. DSP-95073-06. The Department of Permitting, Inspections and Enforcement (DPIE) offers the following:

- The property is located 1201 and 1221 Mercantile Lane, Upper Marlboro, in the southwest quadrant of the intersection of Mercantile Lane and Technology Lane
- The applicant proposes the expansion of approximately 90,000 square feet to an existing medical office building and structured parking garage to accommodate additional medical services.
- The Detailed Site Plan No. DSP-95073-06 is consistent with the approved Stormwater Management Concept Plan No. 44959-2021, approved February 8, 2022
- DPIE has no objection to DSP No. DSP-95073-06.

If you have any questions or need additional information, please contact Mr. Steve Snyder, P.E, the District Engineer for the area, via (301) 883-5710.


cc: Steve Snyder, P.E., District Engineer, S/RPRD, DPIE,
Kaiser Foundation Health Plan Mid-Atlantic States Inc., 2101 east Jefferson St., Rockville,
MD 20852
A. Morton Thomas & Associates, Inc., 800 King Farm Blvd, 4th Floor, Rockville MD,
20850



Division of Environmental Health/Disease Control

Date: February 28, 2022

To: Andrew Bishop, Urban Design, M-NCPPC

From:  Adebola Adepoju, Environmental Health Specialist, Environmental Engineering/ Policy Program

Re: DSP-95073-06 KAISER PERMANENTE LARGO MEDICAL CENTER

The Environmental Engineering / Policy Program of the Prince George's County Health Department has completed a desktop health impact assessment review of the detailed site plan submission for the Kaiser Permanente Largo Medical Center and has the following comments / recommendations:

1. During the construction phases of this project, noise should not be allowed to adversely impact activities on the adjacent properties. Indicate intent to conform to construction activity noise control requirements as specified in Subtitle 19 of the Prince George's County Code.
2. During the construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Indicate intent to conform to construction activity dust control requirements as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control.

If you have any questions or need additional information, please contact me at 301-883-7677 or aoadepoju@co.pg.md.us.



Environmental Engineering/Policy Program
Largo Government Center
9201 Basil Court, Suite 318, Largo, MD 20774
Office 301-883-7681, Fax 301-883-7266, TTY/STS Dial 711
www.princegeorgescountymd.gov/health

From: [Kwesi Woodroffe](#)
To: [Bishop, Andrew](#)
Cc: [PGCReferrals](#)
Subject: RE: ACCEPTANCE REFERRAL: DSP-95073-06 KAISER PERMANENTE LARGO MEDICAL CENTER: SHA; KW
Date: Monday, February 28, 2022 11:40:24 AM
Attachments: [image010.png](#)
[image011.png](#)
[image012.png](#)
[image013.png](#)
[image014.png](#)
[image015.png](#)
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[image020.png](#)
[image021.png](#)
[image022.png](#)
[image023.png](#)

[EXTERNAL EMAIL] Exercise caution when opening attachments, clicking links, or responding.

Good morning Andrew.

I have no comments on the subject referral.

Thanks, Kwesi

Kwesi Woodroffe
Regional Engineer
District 3 Access Management
MDOT State Highway Administration
KWoodroffe@mdot.maryland.gov
301-513-7347 (Direct)
1-888-228-5003 – toll free
Office Hours
M-Thurs.: 6:30a-3:30p
Fr: 6:30a-10:30a
9300 Kenilworth Avenue,
Greenbelt, MD 20770
<http://www.roads.maryland.gov>



Additional Back-up

For

Detailed Site Plan DSP-95073-06 Kaiser Permanente Largo Medical Center

Largo Medical Center Planning Updates



Pedestrian Loop Improvements

LEGEND:

- 1 EXISTING MEDICAL OFFICE BLDG.
- 2 EXISTING PARKING GARAGE
- 3 EXISTING PARKING LOT
- 4 IMAGING ADDITION
- 5 AUC EXPANSION
- 6 PARKING GARAGE EXPANSION

--- PROPERTY LINE

MERCANTILE LANE SEATING NODES



Applicant's Proposed Revisions to Findings/Condition:

(Staff Report Pages 6-7)

Architecture

The proposed one-story building additions range in height and are approximately 14 to 18 feet in height. The Advanced Urgent Care addition is located on the west side of the building near the main entrance and the imaging addition is located at the rear of the building on the east side of the structure. The building additions are designed to blend seamlessly with the existing building with proposed materials, including a mix of brick, glass, concrete, fiber cement siding, and metal siding. Trim, coping, and other detail elements such as horizontal banding have been proposed to add architectural interest. The parking garage expansion is located on the west side of the existing parking garage. It is approximately 51 feet in height, 4 stories, and is constructed in brick to match the existing parking garage. The proposed western façade of the garage includes [a replacement building-mounted identity sign and](#) a decorative metal wall panel on the ~~western~~ ~~northern~~ side of the structure that includes a graphic design imposed on the metal surface and provides visual interest from the street.

* * * * *

(Staff Report Pages 9-10)

Signage

The application proposes one building-mounted [replacement identity](#) sign on the parking garage that is ~~not~~ illuminated and includes the corporate logo and the name of the facility, [and one perforated graphic panel which incorporates portions of the Kaiser Permanente logo.](#) ~~Both~~ ~~The~~ proposed signs ~~are~~ ~~is~~ located on the west elevation of the parking garage ~~facing~~ ~~and faces~~ Mercantile Lane and ~~- in combination -~~ exceeds the allowed square footage. An amendment has been filed by the applicant and staff recommends approval of this request, as discussed in Finding 7.

(Staff Report Page 12)

COMPLIANCE WITH EVALUATION CRITERIA

- f. **Signage Design Criteria (page 170):** The total sign area allowed per building shall be computed on the basis of two square feet of sign area for each one linear foot of building frontage. Where a building has multiple frontages, the allowed sign area should be distributed proportionally along each building frontage. Buildings with less than 60 linear feet of building frontage may be allowed up to 120 square feet of sign area.

The applicant indicates the total allowed square footage for signs along Mercantile Lane is 654 square feet. The perforated graphic panel design includes portions of the Kaiser Permanente logo and is considered signage. The graphic design is a total of 928 square feet, which ~~- in combination with the 45.5 square foot replacement identity signage -~~ exceeds the ~~total~~ allowed square footage [along Mercantile Lane](#) by ~~319.5~~ ~~274~~ square feet. The image is intended to be a mural, to screen the structured parking facility and add visual interest to the streetscape of Mercantile Lane. Its approval will enhance the proposed development and promote the goals and objectives of the development district by screening parking from the public realm, defining the vertical definition of the street, and activating the streetscape. For these reasons, staff recommends approval of this modification



Applicant's Proposed Revisions to Findings/Condition:

(Staff Report Pages 20-21)

RECOMMENDATION

Based upon the foregoing evaluation and analysis, the Urban Design Section recommends that the Planning Board adopt the findings of this report and APPROVE this application, as follows:

- A. APPROVAL of the following alternative Development District Overlay Zone standards:
 6. **Architectural Design Criteria Fenestration** (page 170)—To allow the signage on Mercantile Lane to exceed the allowed sign square footage by ~~319.5~~ **274** square feet for the installation of a **replacement 45.5 square foot identity sign and a new 928 square foot** graphic panel design on the western façade of the parking garage facing Mercantile Lane.
- B. APPROVAL of Detailed Site Plan DSP-95073-06, Kaiser Permanente Largo Medical Center, subject to the following condition:
 1. Prior to certificate approval of the detailed site plan (DSP), the applicant shall submit the following documentation or revise the plans, as follows:
 - a. Modify the **striping of** northbound approach lane use configuration along Mercantile Lane to include a single left-through and single right-through lane configuration. The exact details shall be accepted by the Transportation Planning Section, **and are subject to the approval of DPIE.**

PGCPB Agenda: 4/28/22
PGCPB Item #: 7
Application: Kaiser Permanente Largo Medical Center, DSP-95073-06
Reviewer Name: Andrew Bishop

PROPOSED REVISIONS TO STAFF REPORT

The Applicant proposes the following revisions to the DSP-95073-06 Staff Report to provide clarifying language to Condition 1(a) and to correct an administrative error to reflect the total proposed signage square footage along Mercantile Lane. The Applicant proposes new language shown as **bold and underlined in blue** and all deleted language *italicized-stricken-through-in-red*.

(Staff Report Pages 6-7)

Architecture

The proposed one-story building additions range in height and are approximately 14 to 18 feet in height. The Advanced Urgent Care addition is located on the west side of the building near the main entrance and the imaging addition is located at the rear of the building on the east side of the structure. The building additions are designed to blend seamlessly with the existing building with proposed materials, including a mix of brick, glass, concrete, fiber cement siding, and metal siding. Trim, coping, and other detail elements such as horizontal banding have been proposed to add architectural interest. The parking garage expansion is located on the west side of the existing parking garage. It is approximately 51 feet in height, 4 stories, and is constructed in brick to match the existing parking garage. The proposed western façade of the garage includes **a replacement building-mounted identity sign and** a decorative metal wall panel on the **western** *northern* side of the structure that includes a graphic design imposed on the metal surface and provides visual interest from the street.

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(Staff Report Pages 9-10)

Signage

The application proposes one building-mounted **replacement identity** sign on the parking garage that is ~~not~~ illuminated and includes the corporate logo and the name of the facility, **and one perforated graphic panel which incorporates portions of the Kaiser Permanente logo.** ~~Both~~ *The* proposed signs **are** ~~is~~ located on the west elevation of the parking garage **facing** ~~and faces~~ Mercantile Lane and **- in combination -** ~~exceeds~~ the allowed square footage. An amendment has been filed by the applicant and staff recommends approval of this request, as discussed in Finding 7.

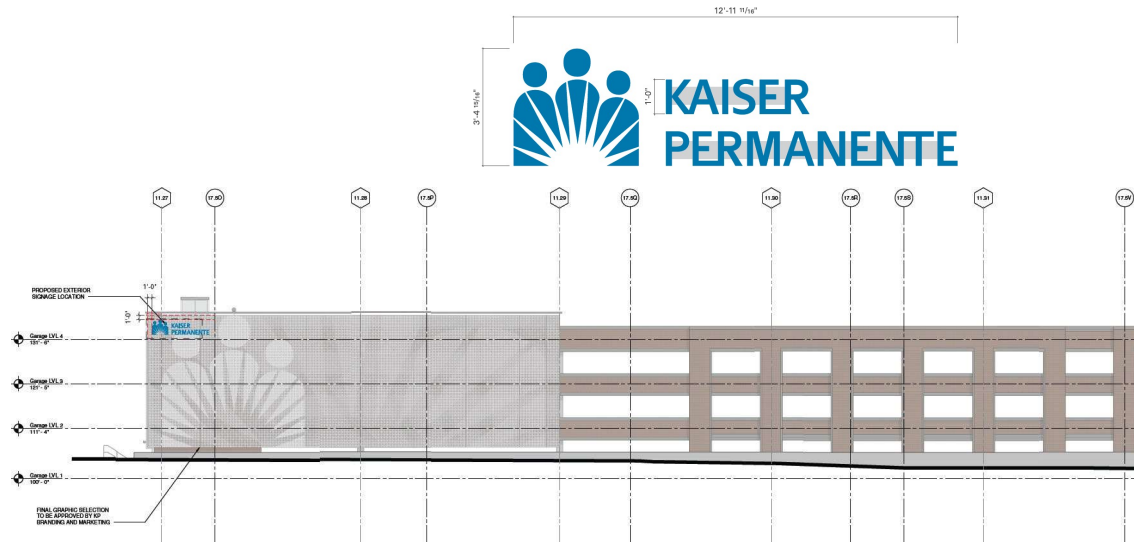


Figure 5: Proposed Signage

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(Staff Report Page 12)

COMPLIANCE WITH EVALUATION CRITERIA

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* * * * *

(Staff Report Pages 20-21)

RECOMMENDATION

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